

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
HIGHWAY PLANS**

FAP 383 (OHIO ST.)  
BRIDGE REPAIR  
SECTION 2020-143-BR&BJR  
PROJECT HIBR-4849(127)  
COOK COUNTY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	I
		ILLINOIS	CONTRACT NO. 62L31	

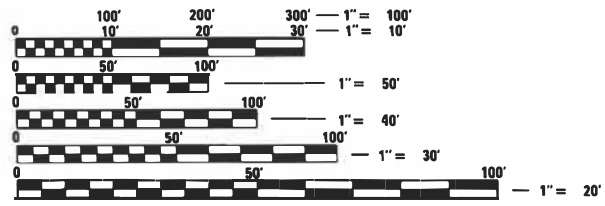
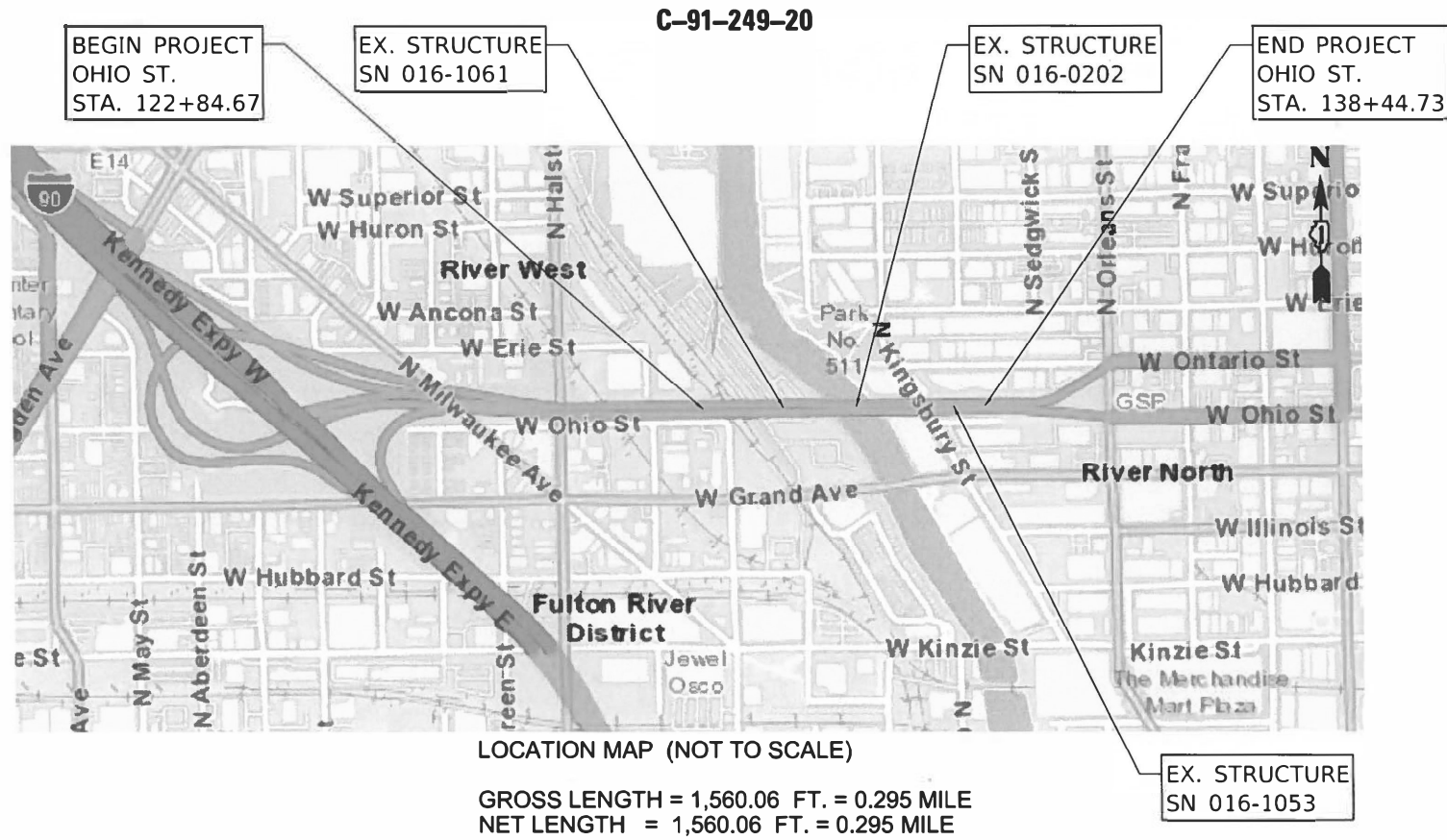
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN CITY OF CHICAGO AND COOK COUNTY

**TRAFFIC DATA**

OHIO ST.

POSTED SPEED LIMIT = 45 MPH  
DESIGN SPEED LIMIT = 50 MPH  
2022 ADT = 80,400  
2046 ADT = 92,112



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

MEADE ELECTRIC CO.  
DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR  
LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672

PROJECT ENGINEER: TIMOTHY SCHMIDT (847) 705-4238  
PROJECT MANAGER: JOSEPH CROSS (847) 705-4605  
CONTRACT NO. 62L31

EPSTEIN  
SIGNED *Greg Osborne*  
DATE 3/16/2023  
EXPIRES 11/30/2023  
SHEET NO. SHEETS 1-36; 165-176

EPSTEIN  
SIGNED *Timothy Gall*  
DATE 3/16/2023  
EXPIRES 11/30/2024  
SHEET NO. SHEETS 37-164

DEPARTMENT OF TRANSPORTATION

SUBMITTED March 21 2023  
*Jose Rios* REGIONAL ENGINEER  
May 12, 2023  
*Joe A. Elk* ENGINEER OF DESIGN AND ENVIRONMENT  
May 12, 2023  
*Stephen M. Smith* DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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**HIGHWAY STANDARDS**

**STANDARD NUMBER**

**DESCRIPTION**

701400-11	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
701422-10	LANE CLOSURE, MULTILANE FOR SPEEDS > 45 MPH TO 55 MPH
701423-10	LANE CLOSURE, MULTILANE WITH BARRIER, FOR SPEEDS > 45 MPH TO 55 MPH
701427-05	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
701428-01	TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701601-09	LANE CLOSURE, URBAN MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	LANE CLOSURE, URBAN MULTILANE INTERSECTION
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

**LIST OF DISTRICT 1 DETAILS**

**DETAIL NUMBER**

**DESCRIPTION**

TC-08	ENTRANCE AND EXIT RAMP CLOSURE DETAILS
TC-09	FREEWAY SINGLE AND MULTI LANE WEAVE
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
TC-12	MULTI-LANE FREEWAY PAVEMENT MARKING
TC-13	DISTRICT 1 TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-17	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
TC-18	FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
TC-25	FREEWAY CENTER LANE CLOSURE SHOULDER LANE

**GENERAL NOTES**

1. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
3. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
4. THE ENGINEER SHALL CONTACT KALPANA KANNAN-HOSADURGA, AREA TRAFFIC FIELD ENGINEER, AT (847) 705-4091 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
5. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
6. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION OF ALL EMERGENCY SERVICES, SCHOOL DISTRICTS, I.D.O.T.'S COMMUNICATIONS CENTER, SPRINGFIELD TRUCK PERMIT SECTION AND OTHER AGENCIES AFFECTED BY THE CLOSURE. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR POSTING SIGNS THAT WILL INDICATE THE DATES THE CLOSURE WILL BE IN PLACE.
8. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL JULIE AT 811, OR (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED. WHEN WITHIN THE CITY OF CHICAGO'S CORPORATE LIMITS ALSO CALL 811 OR (312) 744-7000.
9. IDOT FACILITIES ARE NOT LOCATED BY JULIE OR DIGGER. IDOT ELECTRICAL FACILITIES INCLUDING ROADWAY LIGHTING, FIBER OPTIC, ITS EQUIPMENT, TRAFFIC SIGNAL AND PUMP STATION FACILITIES ARE LOCATED BY THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR. AS OF THE LETTING DATE, CONTACT THE MEADE ELECTRIC COMPANY AT 733-287-7672.
10. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
11. THE DEPARTMENT HAS NOT COORDINATED ACCESS TO THE CHICAGO TRIBUNE PARKING LOT. THE CONTRACTOR SHALL COORDINATE ACCESS TO THE CHICAGO TRIBUNE PARKING LOT THROUGH THE FOLLOWING PHONE NUMBER: (312) 222-2159

**GENERAL NOTES: (CITY OF CHICAGO)**

1. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO AND OBTAIN ALL NECESSARY PERMITS.
2. THE CITY OF CHICAGO IS TO MAKE ADJUSTMENTS TO THEIR STREET LIGHTING AND/OR TRAFFIC SIGNAL FACILITIES. THE CONTRACTOR SHALL COORDINATE HIS WORK AND COOPERATE WITH THE CITY OF CHICAGO IN THESE ADJUSTMENTS. THIS COORDINATION AND COOPERATION BY THE CONTRACTOR WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COSTS OF THE CONTRACT.
3. BENCH MONUMENT LOCATIONS WITHIN THE LIMITS OF THE IMPROVEMENT CAN BE OBTAINED IN SUITE 410 - 333 SOUTH STATE STREET, CHICAGO, IL 60604.

**GENERAL NOTES: (UNION PACIFIC RAILROAD)**

1. THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE UPRR PRIOR TO COMMENCING WORK ON RAILROAD RIGHT OF WAY. REFERENCE FOLDER NO. 0515530 WHEN COMMUNICATING WITH THE UPRR.
2. THE CONTRACTOR MUST CONTACT A FLAGGING COMPANY DIRECTLY AND SCHEDULE FLAGGERS TO BE PRESENT DURING THE CONTRACTOR'S WORK OPERATIONS. VISIT [HTTPS://WWW.UP.COM/REAL\\_ESTATE/THIRD-PARTY-FLAGGING/INDEX.HTM](https://www.up.com/real_estate/third-party-flagging/index.htm) FOR INFORMATION ON THIRD-PARTY APPROVED UPRR FLAGGING COMPANIES:
3. THE CONTRACTOR AND SUB-CONTRACTOR(S) SHALL PARTICIPATE IN A SHORT PRE-CONSTRUCTION MEETING WITH REPRESENTATIVES OF THE ENGINEER AND THE UPRR. THE TOPICS TO BE DISCUSSED INCLUDE, BUT ARE NOT LIMITED TO, FLAGGING, TRAFFIC CONTROL, SAFETY, AND BADGING REQUIREMENTS.
4. ALL CONTRACTOR AND SUB-CONTRACTOR EMPLOYEES THAT INTEND TO ACCESS THE UPRR ROW MUST SUCCESSFULLY COMPLETE ERAILSAFE TRAINING. THE CONTRACTOR AND SUB-CONTRACTOR(S) SHALL PROVIDE COPIES OF EACH EMPLOYEE'S SUCCESSFULLY COMPLETED TRAINING CERTIFICATE TO THE APPROPRIATE UPRR REPRESENTATIVE. VISIT [HTTPS://WWW.UP.COM/ABOUTUP/COMMUNITY/SAFETY/ERAILSAFE/UP-PAT/INDEX.HTM](https://www.up.com/aboutup/community/safety/erailsafe/up-pat/index.htm) FOR INFORMATION ON ERAILSAFE TRAINING. THE AUDIT NUMBER REFERENCED IN THE TRAINING IS THE UPRR FOLDER NO. 0515530.

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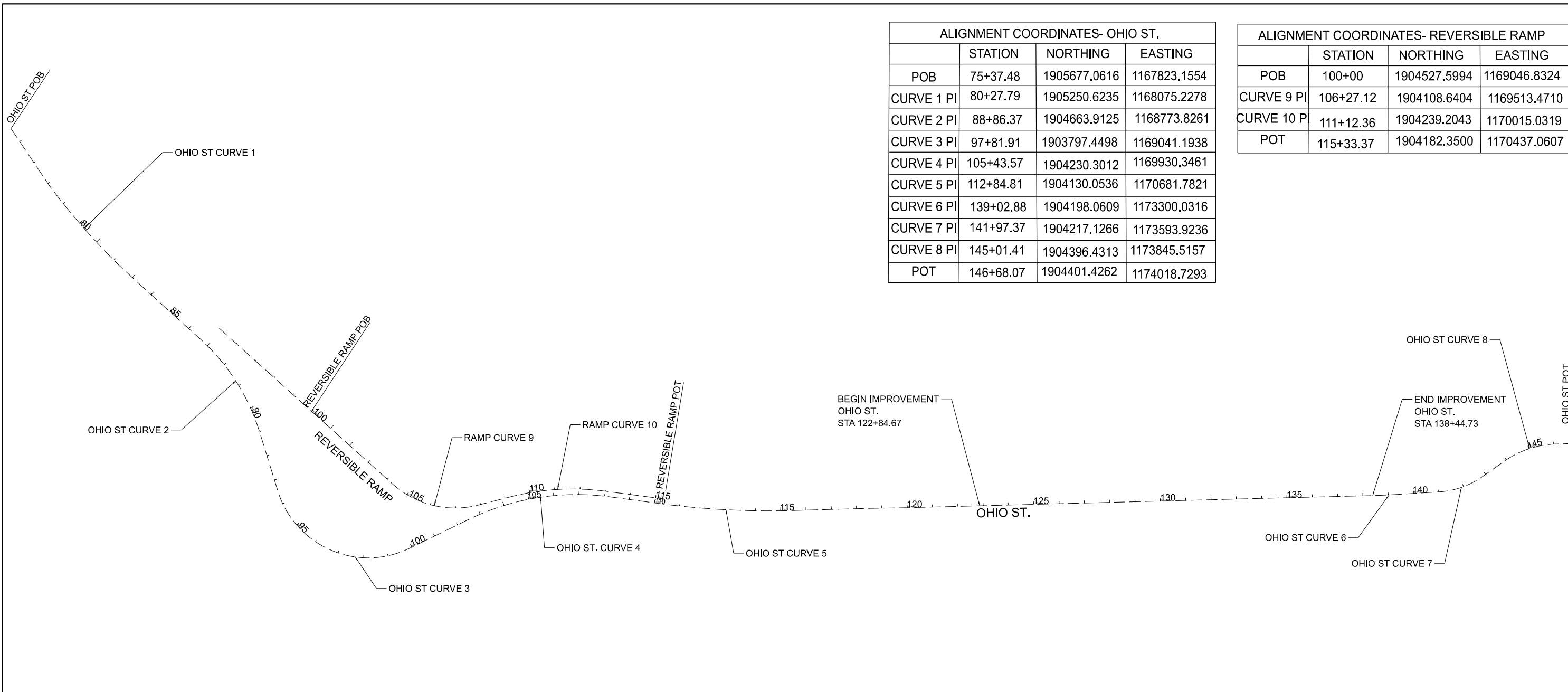
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DATE - 02/28/2023	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS,  
LIST OF HIGHWAY STANDARDS AND GENERAL NOTES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. NONE TO STA. NONE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	2
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



ALIGNMENT COORDINATES- OHIO ST.			
	STATION	NORTHING	EASTING
POB	75+37.48	1905677.0616	1167823.1554
CURVE 1 PI	80+27.79	1905250.6235	1168075.2278
CURVE 2 PI	88+86.37	1904663.9125	1168773.8261
CURVE 3 PI	97+81.91	1903797.4498	1169041.1938
CURVE 4 PI	105+43.57	1904230.3012	1169930.3461
CURVE 5 PI	112+84.81	1904130.0536	1170681.7821
CURVE 6 PI	139+02.88	1904198.0609	1173300.0316
CURVE 7 PI	141+97.37	1904217.1266	1173593.9236
CURVE 8 PI	145+01.41	1904396.4313	1173845.5157
POT	146+68.07	1904401.4262	1174018.7293

ALIGNMENT COORDINATES- REVERSIBLE RAMP			
	STATION	NORTHING	EASTING
POB	100+00	1904527.5994	1169046.8324
CURVE 9 PI	106+27.12	1904108.6404	1169513.4710
CURVE 10 PI	111+12.36	1904239.2043	1170015.0319
POT	115+33.37	1904182.3500	1170437.0607

**OHIO ST CURVE 1**  
 PI STA. = 80+27.79  
 $\Delta = 9^\circ 15' 22''$  (LT)  
 R = 2,643.86'  
 T = 214.02'  
 L = 427.11'  
 E = 8.65'  
 e = N/A  
 P.C. STA. = 78+13.77  
 P.T. STA. = 82+40.88

**OHIO ST CURVE 2**  
 PI = 88+86.37  
 $\Delta = 30^\circ 55' 25''$  (RT)  
 R = 833.00'  
 T = 230.41'  
 L = 449.59'  
 E = 31.28'  
 e = N/A  
 P.C. = 86+55.96  
 P.T. = 91+05.55

**OHIO ST CURVE 3**  
 PI STA. = 97+81.91  
 $\Delta = 98^\circ 48' 31''$  (LT)  
 R = 373.00'  
 T = 435.25'  
 L = 643.25'  
 E = 200.21'  
 e = N/A  
 P.C. STA. = 93+46.66  
 P.T. STA. = 99+89.91

**OHIO ST CURVE 4**  
 PI STA. = 105+43.57  
 $\Delta = 33^\circ 33' 23''$  (RT)  
 R = 972.00'  
 T = 293.06'  
 L = 569.27'  
 E = 43.22'  
 e = N/A  
 P.C. STA. = 102+50.51  
 P.T. STA. = 108+19.78

**OHIO ST CURVE 5**  
 PI STA. = 112+84.81  
 $\Delta = 9^\circ 10' 16''$  (LT)  
 R = 3,130.00'  
 T = 251.04'  
 L = 501.01'  
 E = 10.05'  
 e = N/A  
 P.C. STA. = 110+33.77  
 P.T. STA. = 105+34.78

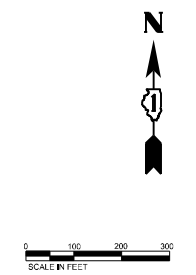
**OHIO ST CURVE 6**  
 PI STA. = 139+02.88  
 $\Delta = 2^\circ 09' 18''$  (LT)  
 R = 3,130.00'  
 T = 58.87'  
 L = 117.72'  
 E = 0.55'  
 e = N/A  
 P.C. STA. = 138+44.01  
 P.T. STA. = 139+61.73

**OHIO ST CURVE 7**  
 PI STA. = 141+97.37  
 $\Delta = 31^\circ 45' 54''$  (LT)  
 R = 335.00'  
 T = 95.32'  
 L = 185.73'  
 E = 13.30'  
 e = N/A  
 P.C. STA. = 141+02.06  
 P.T. STA. = 142+87.78

**OHIO ST CURVE 8**  
 PI STA. = 145+01.41  
 $\Delta = 33^\circ 49' 30''$  (RT)  
 R = 373.00'  
 T = 113.41'  
 L = 220.20'  
 E = 16.86'  
 e = N/A  
 P.C. STA. = 143+88.00  
 P.T. STA. = 146+08.20

**RAMP CURVE 9**  
 PI STA. = 106+27.12  
 $\Delta = 56^\circ 30' 34''$  (LT)  
 R = 373.00'  
 T = 200.46'  
 L = 367.88'  
 E = 50.45'  
 e = N/A  
 P.C. STA. = 104+26.66  
 P.T. STA. = 107+94.54

**RAMP CURVE 10**  
 PI STA. = 111+12.36  
 $\Delta = 22^\circ 15' 49''$  (RT)  
 R = 972.00'  
 T = 191.26'  
 L = 377.69'  
 E = 18.64'  
 e = N/A  
 P.C. STA. = 109+21.10  
 P.T. STA. = 112+98.79



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DATE - 02/28/2023	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT AND BENCHMARK**

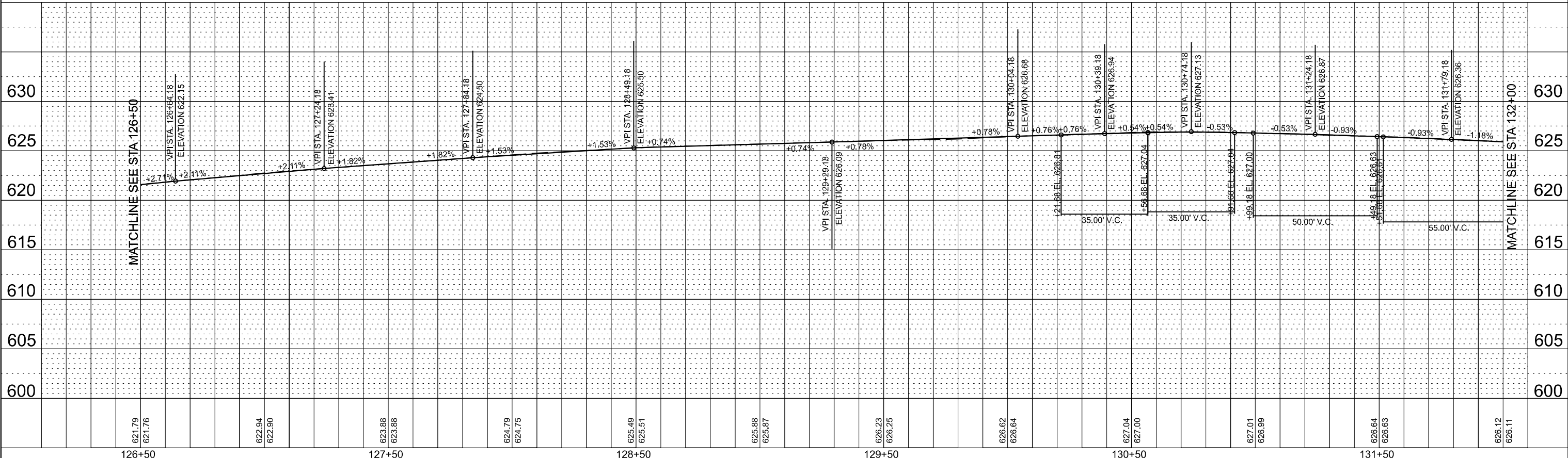
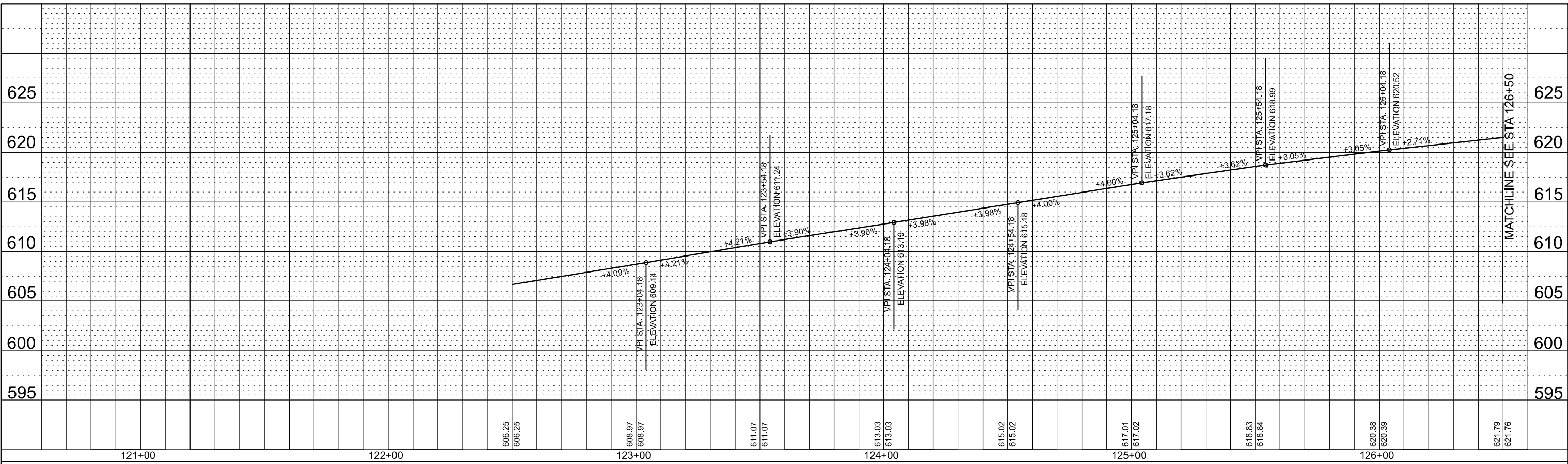
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F.A.P. RTE. 383	SECTION D-91-450-20	COUNTY COOK	TOTAL SHEETS 177	SHEET NO. 11
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

PLAN	STRIKED	DATE
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	ALIGNMENT CHECKED	
	GRADE CHECKED	
	STRUCTURE NOTATIONS CHKD	
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PROFILE	REVIEWED	DATE
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126+50	127+50	128+50	129+50	130+50	131+50
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

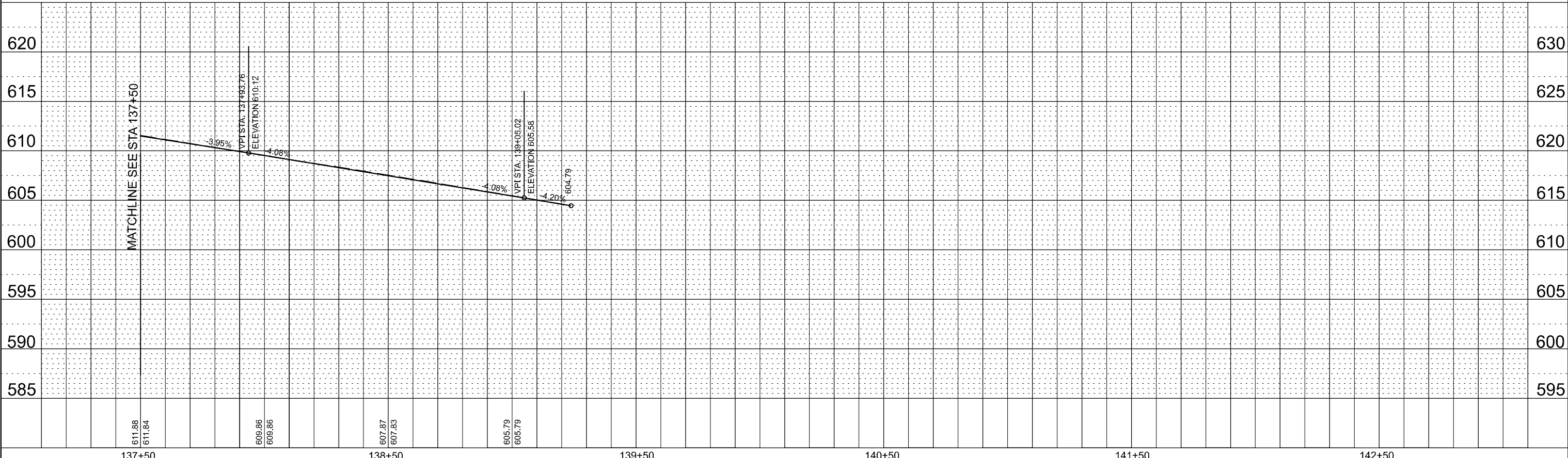
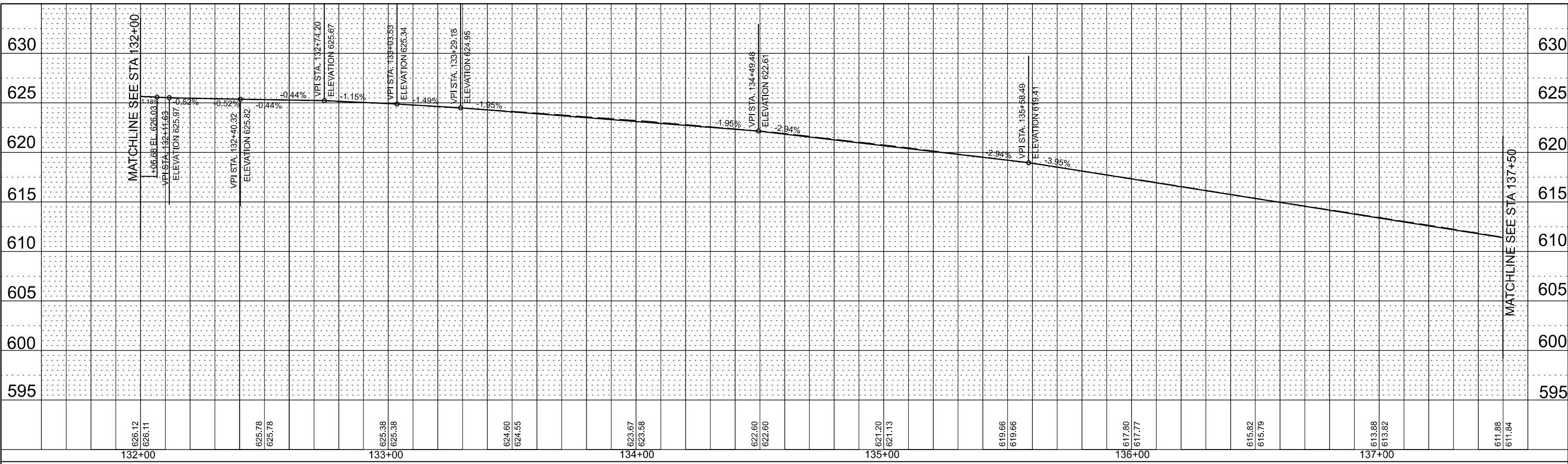
<b>FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER</b>			
<b>EASTBOUND PROFILE</b>			
SCALE: 1" = 20'	SHEET 1 OF 4 SHEETS	STA. 121+00	TO STA. 132+00

F.A. RTE. 3887	SECTION 8HB-2	COUNTY COOK	TOTAL SHEETS 177	SHEET NO. 12
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

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	STRUCTURE NOTATIONS CHRD	

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132+00	626.12	626.11	133+00	625.78	625.78	625.38	625.38	624.60	624.55	625.78	623.67	623.58	622.60	622.60	621.20	621.13	619.66	619.66	617.80	617.77	615.82	615.79	613.88	613.82	611.88	611.84
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137+50	611.88	611.84	138+50	609.86	609.86	607.87	607.83	605.79	605.79	604.79
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		DATE	- 02/28/2023	REVISED	-

139+50	140+50	141+50	142+50
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STATE OF ILLINOIS  
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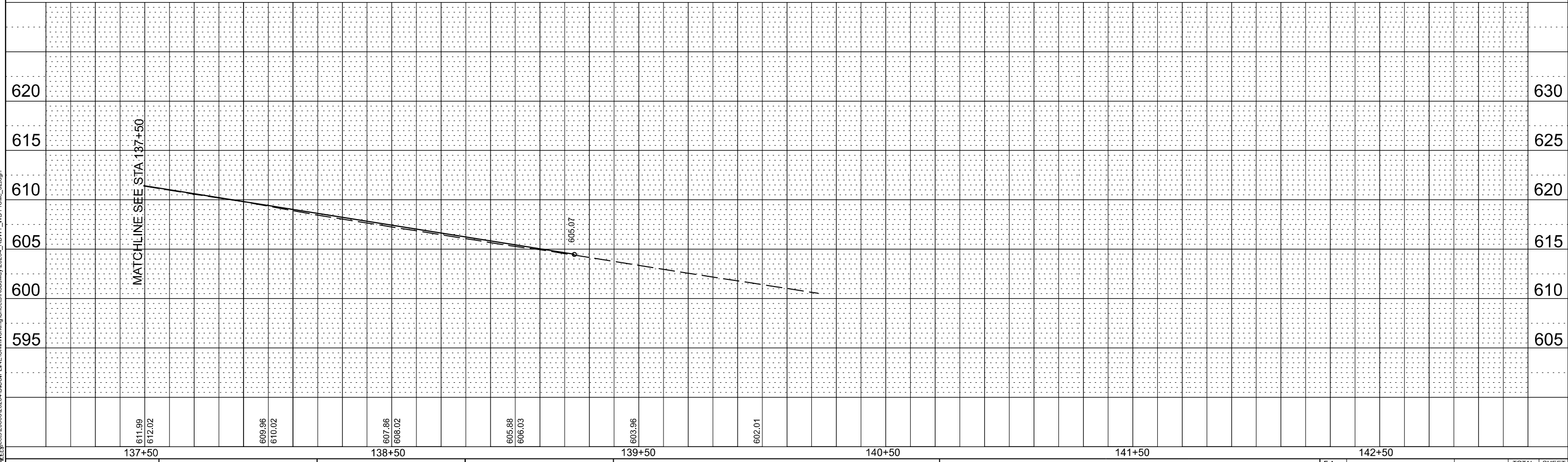
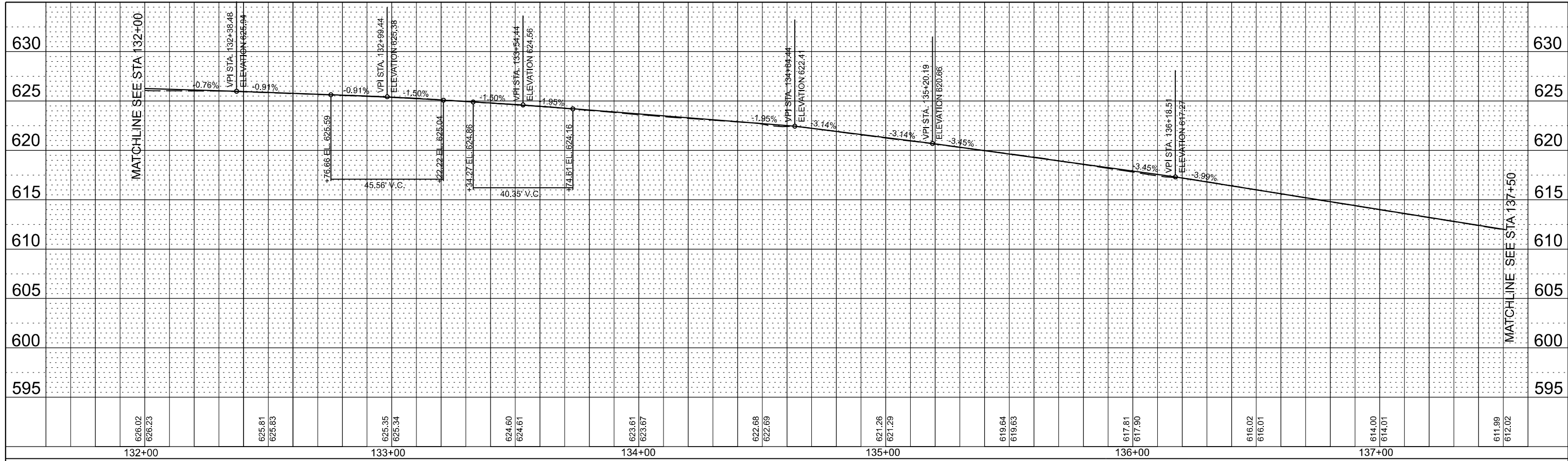
FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
 EASTBOUND PROFILE  
 SCALE: 1" = 20' SHEET 2 OF 4 SHEETS STA. 132+00 TO STA. 143+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	BHB-2	COOK	177	13
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
 WESTBOUND PROFILE  
 SCALE: 1" = 20' SHEET 4 OF 4 SHEETS STA. 132+00 TO STA. 143+00

F.A. RTE. 3887	SECTION 8HB-2	COUNTY COOK	TOTAL SHEETS 177	SHEET NO. 15
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

## SUGGESTED STAGE OF CONSTRUCTION & TRAFFIC CONTROL

THE FOLLOWING SEQUENCE OF TRAFFIC CONTROL INCLUDING THREE STAGES FOR MAINTENANCE OF TRAFFIC IS SUGGESTED. VARIATIONS MAY BE MADE, WITH THE APPROVAL OF THE ENGINEER, IF THE PREVAILING SITE CONDITIONS AT THE TIME OF CONSTRUCTION ALLOW. CONTRACTOR SHALL MAINTAIN TWO LANE TRAFFIC OPEN FOR BOTH EASTBOUND AND WESTBOUND ON OHIO STREET DURING ALL MOT STAGES.

FOR EACH STAGE OF CONSTRUCTION, PROVIDE TRAFFIC CONTROL AS SHOWN ON THE SUGGESTED STAGES OF CONSTRUCTION & TRAFFIC CONTROL PLANS. COORDINATE INSTALLATION OF OTHER TRAFFIC CONTROL DEVICES WITH THE EXISTING TRAFFIC PATTERNS AT THE LIMITS OF THE PROJECT.

### STAGE 1

1. REMOVE EXISTING PAVEMENT MARKINGS & RAISED PAVEMENT MARKERS IN CONFLICT WITH STAGE 1 STRIPING. PLACE TEMPORARY STRIPING & ADVANCED SIGNS TO INDICATE LANE CLOSURES ON EB AND WB OHIO STREET.
2. ESTABLISH EASTBOUND AND WESTBOUND LANE CLOSURES BY PLACING THE TEMPORARY CONCRETE BARRIERS, DRUMS AND BARRICADES AS SHOWN ON CLOSURE A PLANS. TWO LANES SHALL REMAIN OPEN ON EACH DIRECTION DURING THE MOT STAGE 1.
3. COMPLETE ANY BRIDGE MAINTENANCE, REHABILITATION, AND BRDGE DECK SLAB PATCHES/REPAIRS WITHIN THE STAGE 1 WORK ZONE.

### STAGE 2

1. REMOVE EXISTING PAVEMENT MARKINGS & RAISED PAVEMENT MARKERS IN CONFLICT WITH STAGE 2 STRIPING. PLACE TEMPORARY STRIPING & ADVANCED SIGNS TO INDICATE MIDDLE LANE CLOSURES ON EB AND WB OHIO STREET.
2. ESTABLISH EASTBOUND AND WESTBOUND MIDDLE LANE CLOSURES BY PLACING THE TEMPORARY CONCRETE BARRIERS, DRUMS AND BARRICADES AS SHOWN ON CLOSURE B PLANS. TWO LANES SHALL REMAIN OPEN IN EACH DIRECTION DURING THE MOT STAGE 2.
3. COMPLETE ANY BRIDGE MAINTENANCE, REHABILITATION, AND BRDGE DECK SLAB PATCHES/REPAIRS WITHIN THE STAGE 2 WORK ZONE.

### STAGE 3

1. REMOVE EXISTING AND TEMPORARY PAVEMENT MARKINGS & RAISED PAVEMENT MARKERS IN CONFLICT WITH STAGE 3 STRIPING. PLACE TEMPORARY STRIPING & ADVANCED SIGNS TO INDICATE LANE CLOSURES ON EB AND WB OHIO STREET.
2. ESTABLISH EASTBOUND AND WESTBOUND LANE CLOSURES BY PLACING THE TEMPORARY CONCRETE BARRIERS, DRUMS AND BARRICADES AS SHOWN ON CLOSURE A PLANS. TWO LANES SHALL REMAIN OPEN ON EACH DIRECTION DURING THE MOT STAGE 3.
3. COMPLETE ANY BRIDGE MAINTENANCE, REHABILITATION, AND BRDGE DECK SLAB PATCHES/REPAIRS WITHIN THE STAGE 3 WORK ZONE.

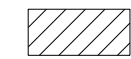









### DETOUR


THE DETOUR ROUTES ARE INCLUDED FOR THE CONTRACTORS INFORMATION. THE NEED AND IMPLEMENTATION OF THE DETOUR ROUTES SHALL BE COORDINATED WITH THE ENGINEER, IDOT, AND CDOT. CONTRACTOR SHALL PROVIDE TO IDOT A REQUEST FOR THE DETOUR, REASONS FOR THE DETOUR AND A SCHEDULE INCLUDING DURATION OF THE DETOUR. BOTH DETOURS CAN NOT OCCUR CONCURRENTLY.

## MAINTENANCE OF TRAFFIC GENERAL NOTES

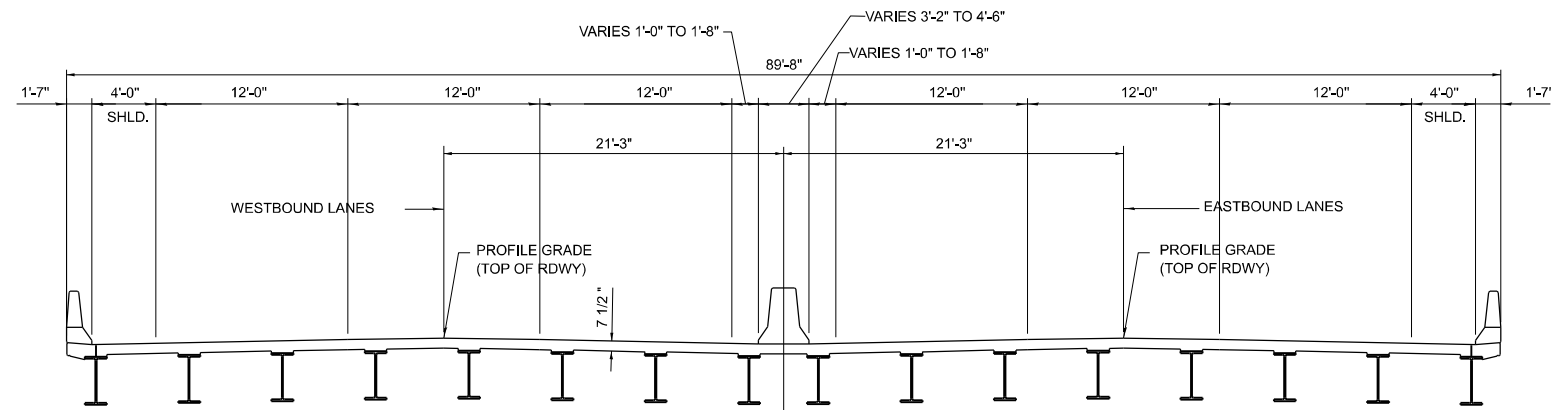
1. ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES. ALL ARROW BOARDS SHALL HAVE SOLAR POWER CAPABILITY.
2. PRIOR TO THE START OF CONSTRUCTION, REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
3. IDOT TRAFFIC CONTROL STANDARDS LISTED WITH THE INDEX OF SHEETS SHALL BE USED AS GUIDES IN IMPLEMENTING THE TRAFFIC CONTROL, INCLUDING SIGN LOCATIONS, FOR THIS PROJECT.
4. DEPICTED TRAFFIC CONTROL IS A MINIMUM REQUIREMENT, AND OTHER WORK OR SIGNING MAY BE REQUIRED AS PER THE INSTRUCTION OF THE ENGINEER IN CHARGE. THE COST INCURRED TO COMPLY WILL BE INCLUDED IN THE PAY ITEMS "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)".
5. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF THEIR REPRESENTATIVES ON THE CONSTRUCTION SITE PRIOR TO THE START OF WORK.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
7. THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH IDOT SPECIAL PROVISIONS, IDOT HIGHWAY STANDARDS, IDOT STANDARD SPECIFICATIONS, PLAN SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER.
8. STAGED CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL CONFLICTING SIGNS, PAVEMENT MARKINGS, AND TRAFFIC CONTROL DEVICES HAVE BEEN REMOVED OR COVERED AND TEMPORARY TRAFFIC CONTROL MEASURES HAVE BEEN PLACED TO THE SATISFACTION OF THE ENGINEER.
9. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH EACH TRAFFIC CONTROL STAGE, AND SHALL BE PAID AS WATER BLASTING.
10. THE FURNISHING, INSTALLATION AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD), ILLINOIS SUPPLEMENT TO THE MUTCD, IDOT SPECIAL PROVISIONS, IDOT HIGHWAY STANDARDS, IDOT STANDARD SPECIFICATIONS, CONTRACT SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL).
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING EXISTING AND/OR PROPOSED DRAINAGE AND ROADWAY LIGHTING ARE MAINTAINED DURING THE DURATION OF THE PROJECT.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING ALL BARRIERS, LABOR, SIGNS AND TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC UNLESS NOTED OTHERWISE IN THE SPECIAL PROVISIONS.
13. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED, REMOVED, DAMAGED, OR OTHERWISE AFFECTED BY CONSTRUCTION.
14. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC CONTROL STAGE CHANGES AND ROAD CLOSURES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING, AND/OR MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
15. A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGES CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION. ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR THE PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT INTO THIS 72 HOURS NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.

### LEGEND

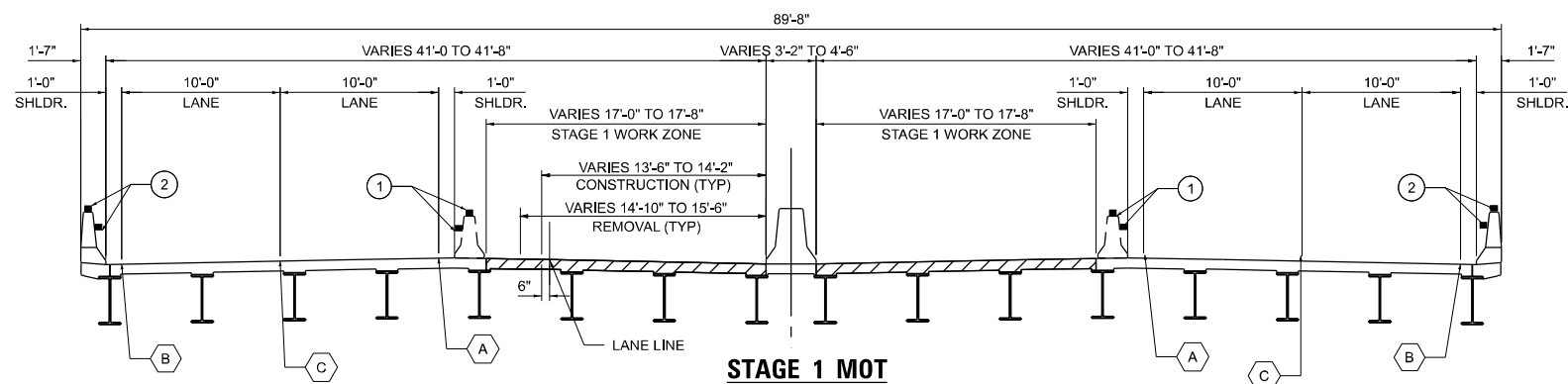
	CONSTRUCTION WORK ZONE
	DRUMS WITH STEADY BURN BEACONS
	TYPE III BARRICADE
	TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
	IMPACT ATTENUATORS, (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
	DIRECTION OF TRAFFIC FLOW
	MOT SIGNAGE
	EXISTING SIGNAGE
	ARROW BOARD
	FLASHER BEACON

	DESIGNED - _____	REVISED - _____	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER MAINTENANCE OF TRAFFIC NOTES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - _____	REVISED - _____		SCALE: NTS	SHEET 1	OF 1	SHEETS	STA. NTS	TO STA. NTS	ILLINOIS	FED. AID PROJECT	
PLOT SCALE = 10.0000' / in.	CHECKED - _____	REVISED - _____					383	0-91-450-20	COOK	177	16	
PLOT DATE = 3/21/2023	DATE - 02/28/2023	REVISED - _____								CONTRACT NO. 62L31		

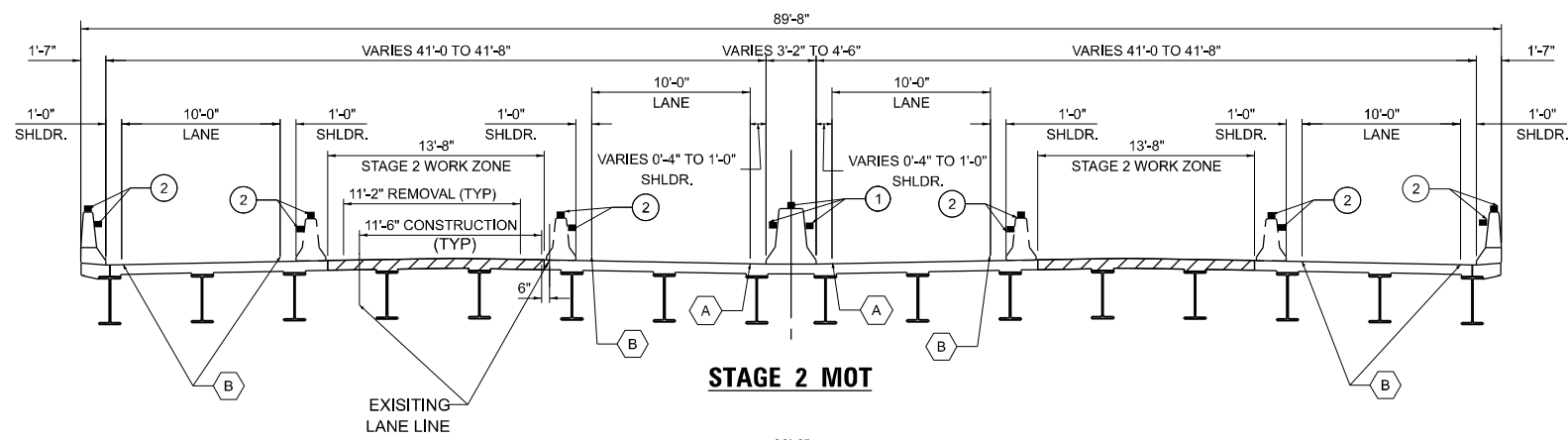




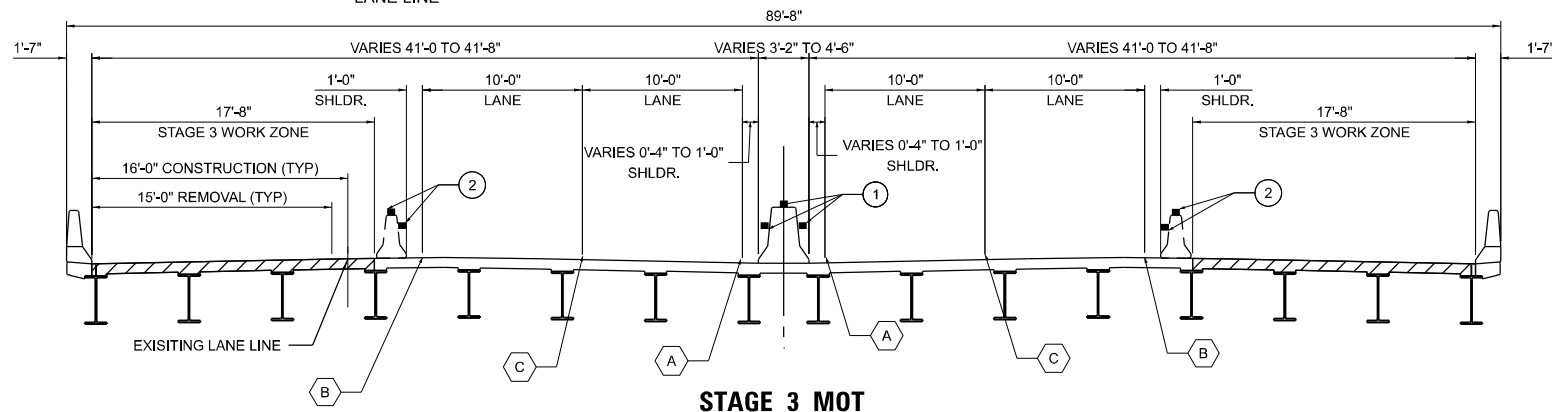
**EXISTING CROSS SECTION**



**STAGE 1 MOT**



**STAGE 2 MOT**



**STAGE 3 MOT**

**TAGGED NOTES**

- ① MONO-DIRECTIONAL AMBER TYPE C REFLECTORS PER STANDARDS 704001 AND 782006
- ② MONO-DIRECTIONAL CRYSTAL TYPE C REFLECTORS PER STANDARDS 704001 AND 78206

**PAVEMENT MARKING DESCRIPTIONS:**

- A PAVEMENT MARKING TAPE TYPE IV, 4" YELLOW EDGE LINE (TYP.)
- B PAVEMENT MARKING TAPE TYPE IV, 4" WHITE EDGE LINE (TYP.)
- C PAVEMENT MARKING TAPE TYPE IV, 5" WHITE LANE LINE (TYP.)

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CHECKED - _____	REVISED - _____
DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
TYPICAL SECTIONS STAGE 1, 2, AND 3**

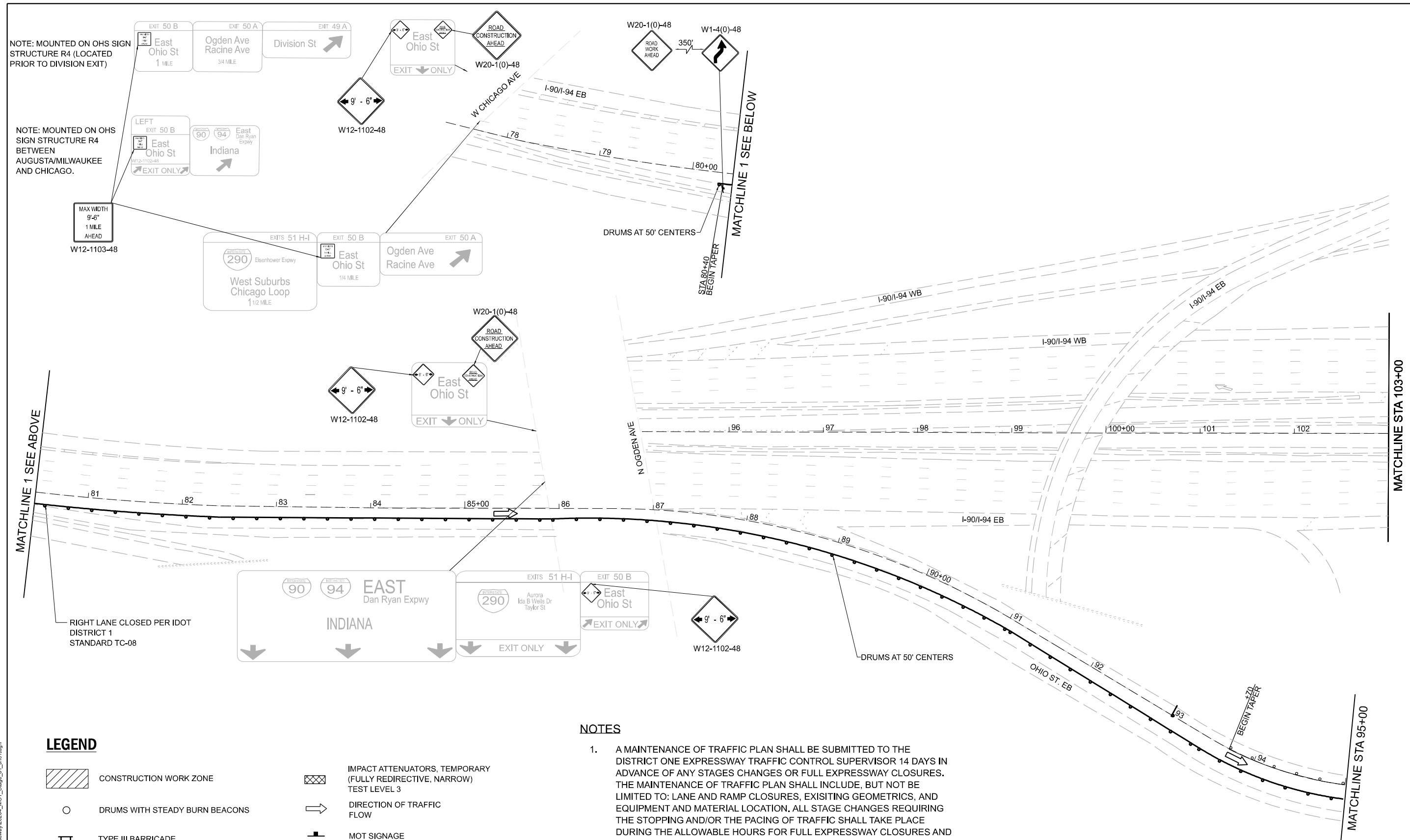
SCALE: NTS SHEET 1 OF 1 SHEETS STA. 122+84.67 TO STA. 138+44.73

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	17
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



NOTE: MOUNTED ON OHS SIGN STRUCTURE R4 (LOCATED PRIOR TO DIVISION EXIT)

NOTE: MOUNTED ON OHS SIGN STRUCTURE R4 BETWEEN AUGUSTA/MILWAUKEE AND CHICAGO.



MATCHLINE 1 SEE ABOVE

MATCHLINE STA 103+00

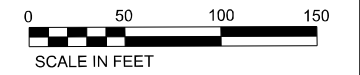
MATCHLINE STA 95+00

**LEGEND**

- CONSTRUCTION WORK ZONE
- DRUMS WITH STEADY BURN BEACONS
- TYPE III BARRICADE
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
- DIRECTION OF TRAFFIC FLOW
- MOT SIGNAGE
- EXISTING SIGNAGE
- ARROW BOARD
- FLASHER BEACON

**NOTES**

- A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGES CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION. ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR THE PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT INTO THIS 72 HOURS NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.
- EXISTING PAVEMENT MARKING THAT ARE IN CONFLICT WITH THE PROPOSED STAGED CONSTRUCTION SHALL BE REMOVED



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DATE - 02/28/2023	REVISED - _____











**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

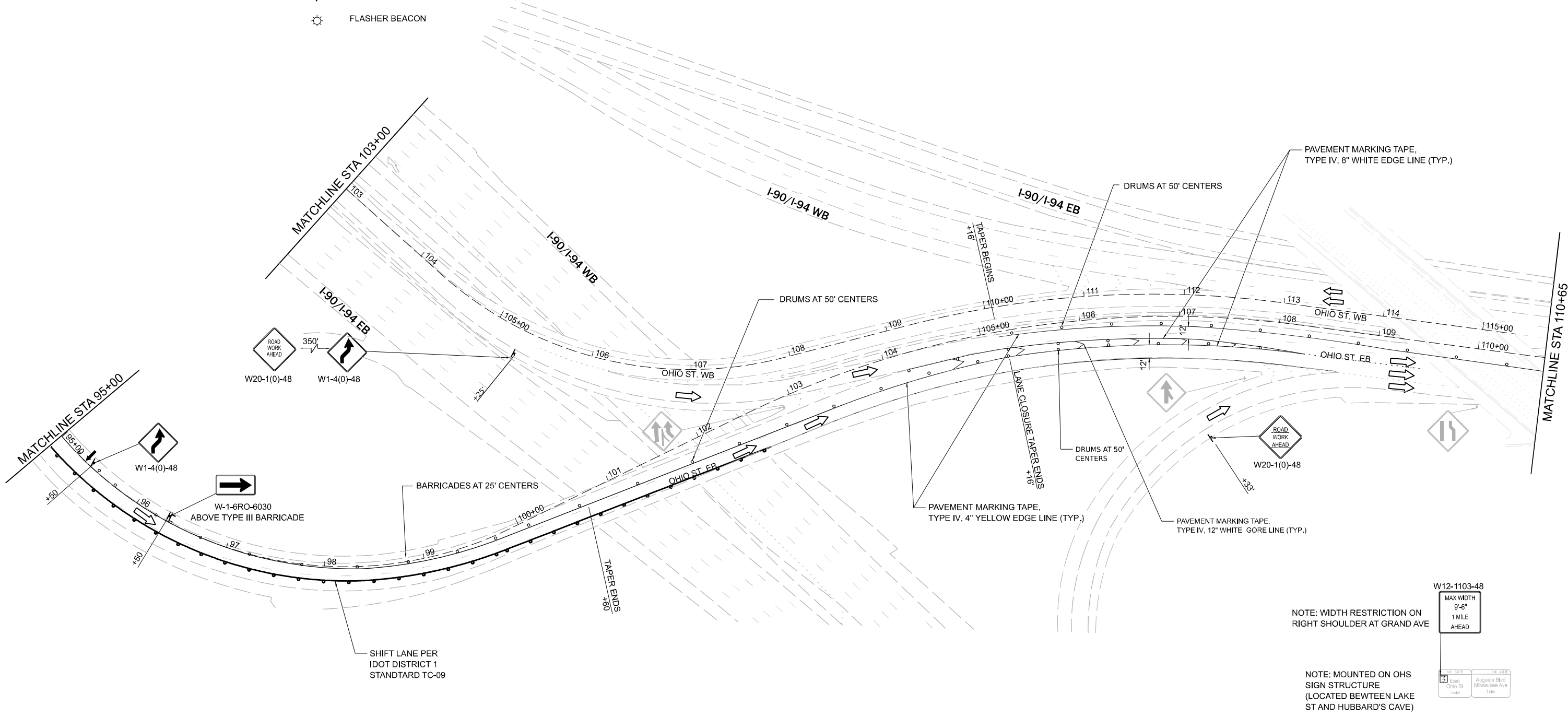
**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 1**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	18
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

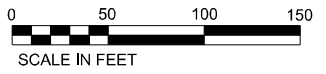
SCALE: 1" = 50'  
SHEET 1 OF 5 SHEETS STA. 80+30 TO STA. 95+00  
1" = 50'

**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



NOTES:  
 1. CONTRACTOR SHALL PROVIDE AN ADDITIONAL WIDTH RESTRICTION SIGN (SKIDMOUNTED WITH FLASHING BEACON) AT THE GRAND AVENUE OVERPASS/ EXIT TO EB OHIO STREET



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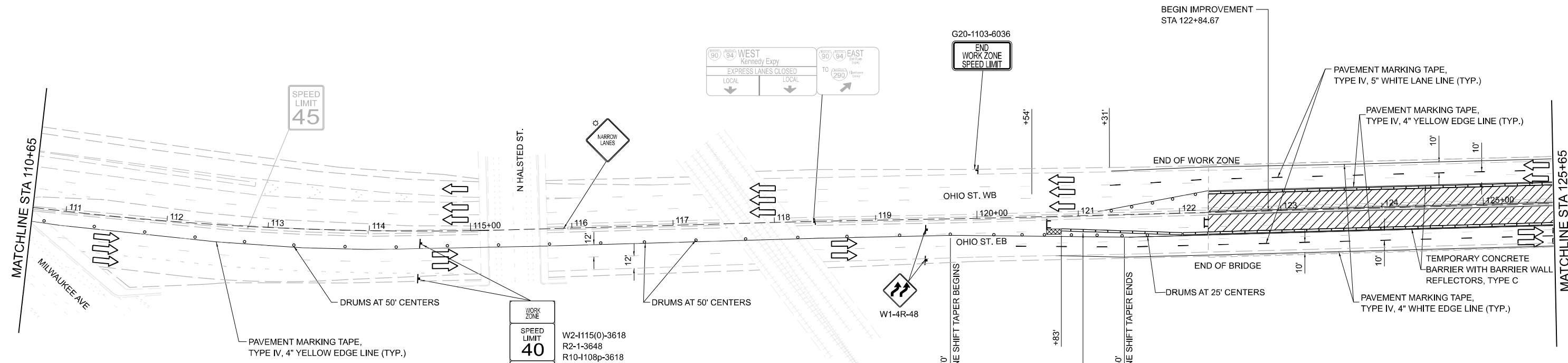
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CHECKED - _____	REVISED - _____
DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 1**

SCALE: 1" = 50'    SHEET 2 OF 5 SHEETS    STA. 95+00 TO STA. 110+65

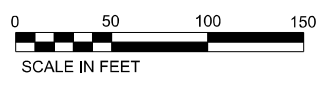
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	19
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



WORK ZONE
SPEED LIMIT 40
PHOTO ENFORCED
\$250 FINE MINIMUM

W2-1115(0)-3618  
R2-1-3648  
R10-1108p-3618  
R2-1106p-3618

- LEGEND**
- CONSTRUCTION WORK ZONE
  - DRUMS WITH STEADY BURN BEACONS
  - TYPE III BARRICADE
  - TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
  - DIRECTION OF TRAFFIC FLOW
  - MOT SIGNAGE
  - EXISTING SIGNAGE
  - ARROW BOARD
  - FLASHER BEACON



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DATE - 02/28/2023	REVISED - _____

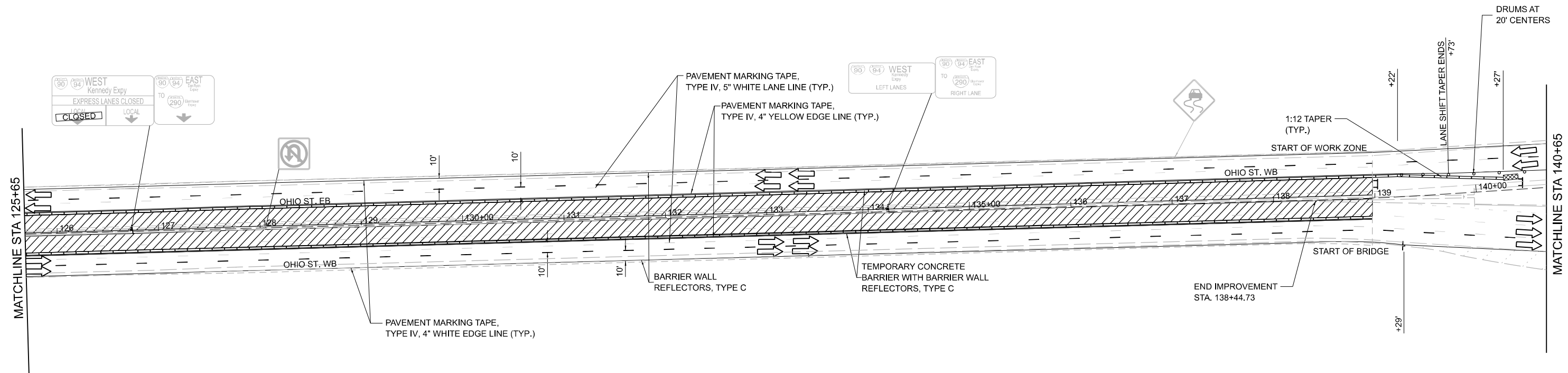
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 1**











SCALE: 1" = 50' SHEET 3 OF 5 SHEETS STA. 110+65 TO STA. 125+65

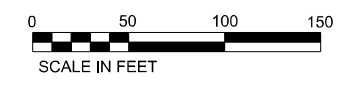
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	20
				CONTRACT NO. 62L31

ILLINOIS FED. AID PROJECT



**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



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DATE - 02/28/2023	REVISED - _____

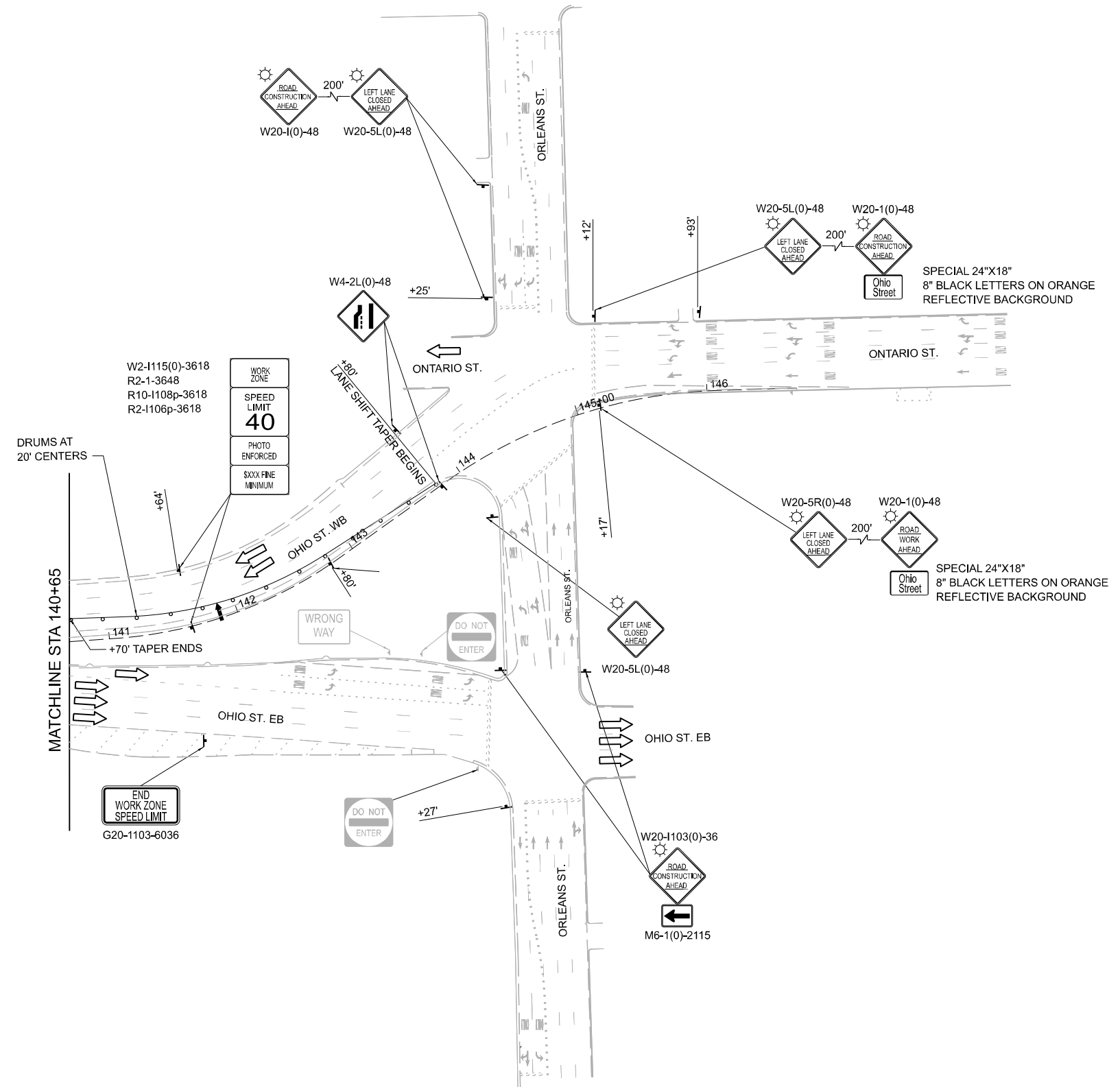
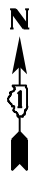
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 1**

SCALE: 1" = 50' SHEET 4 OF 5 SHEETS STA. 125+65 TO STA. 140+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	21
CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT



**LEGEND**

- CONSTRUCTION WORK ZONE
- DRUMS WITH STEADY BURN BEACONS
- TYPE III BARRICADE
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
- DIRECTION OF TRAFFIC FLOW
- MOT SIGNAGE
- EXISTING SIGNAGE
- ARROW BOARD
- FLASHER BEACON

0 50 100 150  
SCALE IN FEET

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DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 1**

SCALE: 1" = 50' SHEET 5 OF 5 SHEETS STA. 140+65 TO STA. 147+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	22
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



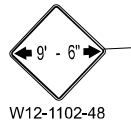
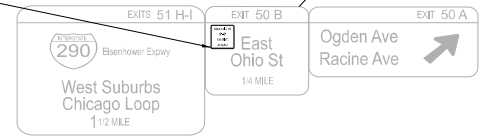
NOTE: MOUNTED ON OHS SIGN STRUCTURE R4 (LOCATED PRIOR TO DIVISION EXIT)

NOTE: MOUNTED ON OHS SIGN STRUCTURE R4 BETWEEN AUGUSTA/MILWAUKEE AND CHICAGO.

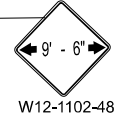
MAX WIDTH  
9'-6"  
1 MILE  
AHEAD  
W12-1103-48



W12-1102-48



W12-1102-48



W12-1102-48

MATCHLINE SEE ABOVE

MATCHLINE SEE BELOW

MATCHLINE STA 103+00

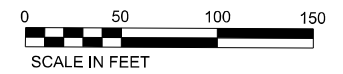
MATCHLINE STA 95+00

**LEGEND**

- CONSTRUCTION WORK ZONE
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
- DRUMS WITH STEADY BURN BEACONS
- DIRECTION OF TRAFFIC FLOW
- TYPE III BARRICADE
- MOT SIGNAGE
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
- EXISTING SIGNAGE
- ARROW BOARD
- FLASHER BEACON

**NOTES**

- A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGES CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION. ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR THE PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT INTO THIS 72 HOURS NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.
- EXISTING PAVEMENT MARKING THAT ARE IN CONFLICT WITH THE PROPOSED STAGED CONSTRUCTION SHALL BE REMOVED



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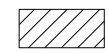


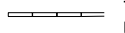






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DEPARTMENT OF TRANSPORTATION**

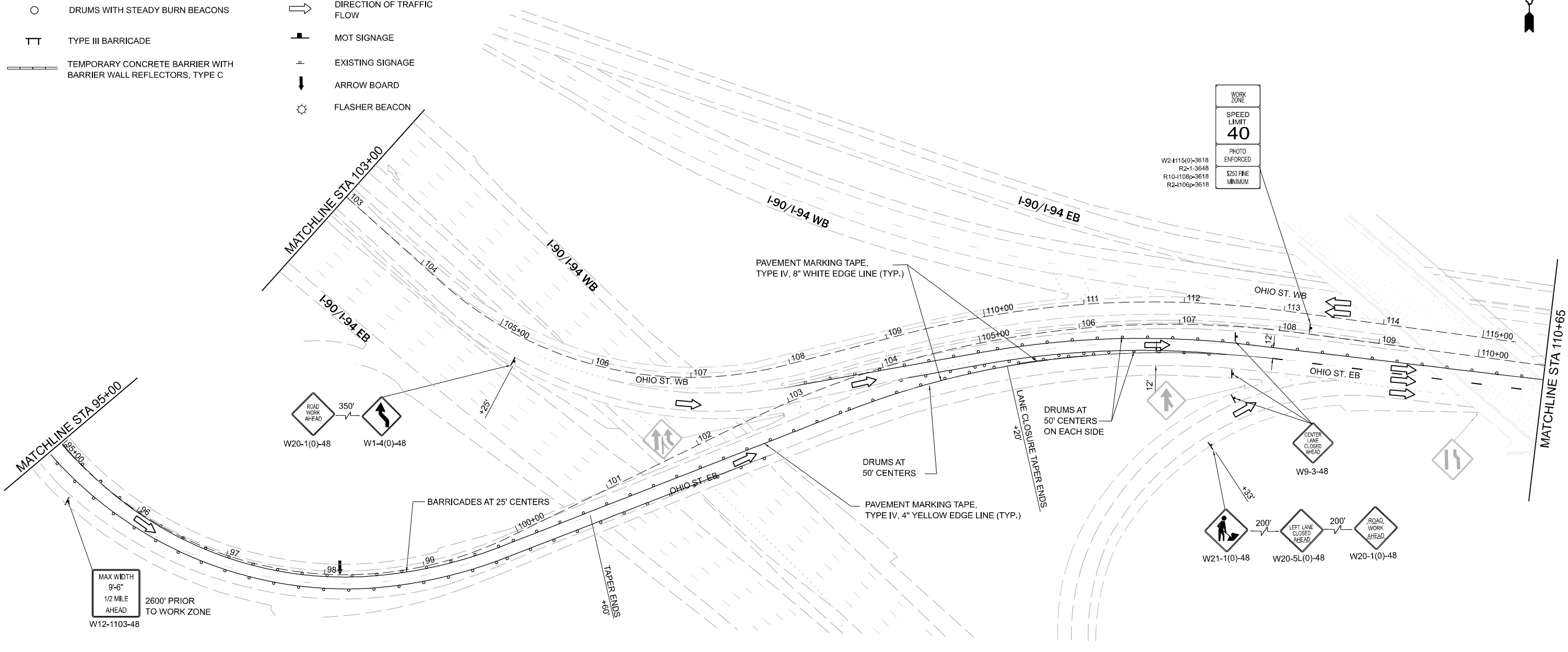
**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 1**

SCALE: 1" = 50' SHEET 1 OF 5 SHEETS STA. 80+30 TO STA. 95+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	23
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



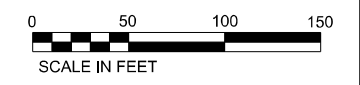
MAX WIDTH  
9'-6"  
1/2 MILE  
AHEAD  
2600' PRIOR  
TO WORK ZONE  
W12-1103-48

WORK ZONE  
SPEED LIMIT  
**40**  
PHOTO ENFORCED  
W2-4115(0)-3618  
R2-1-3648  
R10-1108p-3618  
R2-1106p-3618  
\$250 FINE  
MINIMUM

W12-1103-48  
MAX WIDTH  
9'-6"  
1 MILE  
AHEAD

NOTE: MOUNTED ON OHS SIGN STRUCTURE (LOCATED BETWEEN LAKE ST AND HUBBARD'S CAVE)

- NOTES:
- CONTRACTOR SHALL PROVIDE AN ADDITIONAL WIDTH RESTRICTION SIGN (SKIDMOUNTED WITH FLASHING BEACON) AT THE GRAND AVENUE OVERPASS/ EXIT TO EB OHIO STREET



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DEPARTMENT OF TRANSPORTATION**

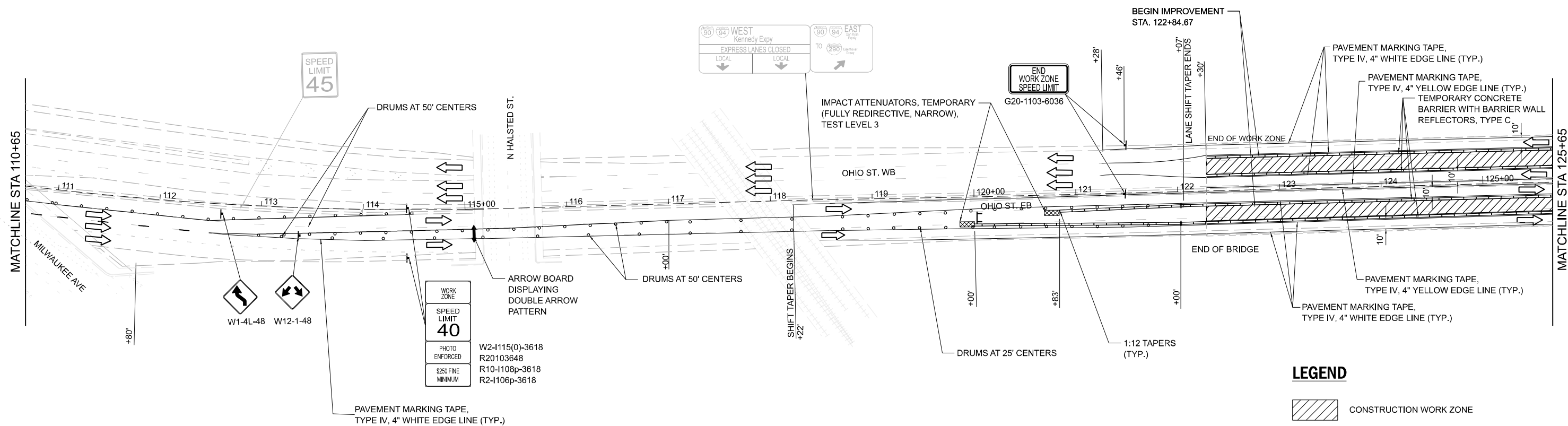
**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 2**

SCALE: 1" = 50' SHEET 2 OF 5 SHEETS STA. 85+60 TO STA. 110+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	24
CONTRACT NO. 62L31				











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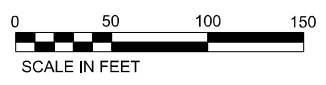




WORK ZONE	
SPEED LIMIT	40
PHOTO ENFORCED	W2-1115(0)-3618 R20103648
\$250 FINE MINIMUM	R10-1108p-3618 R2-1106p-3618

**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



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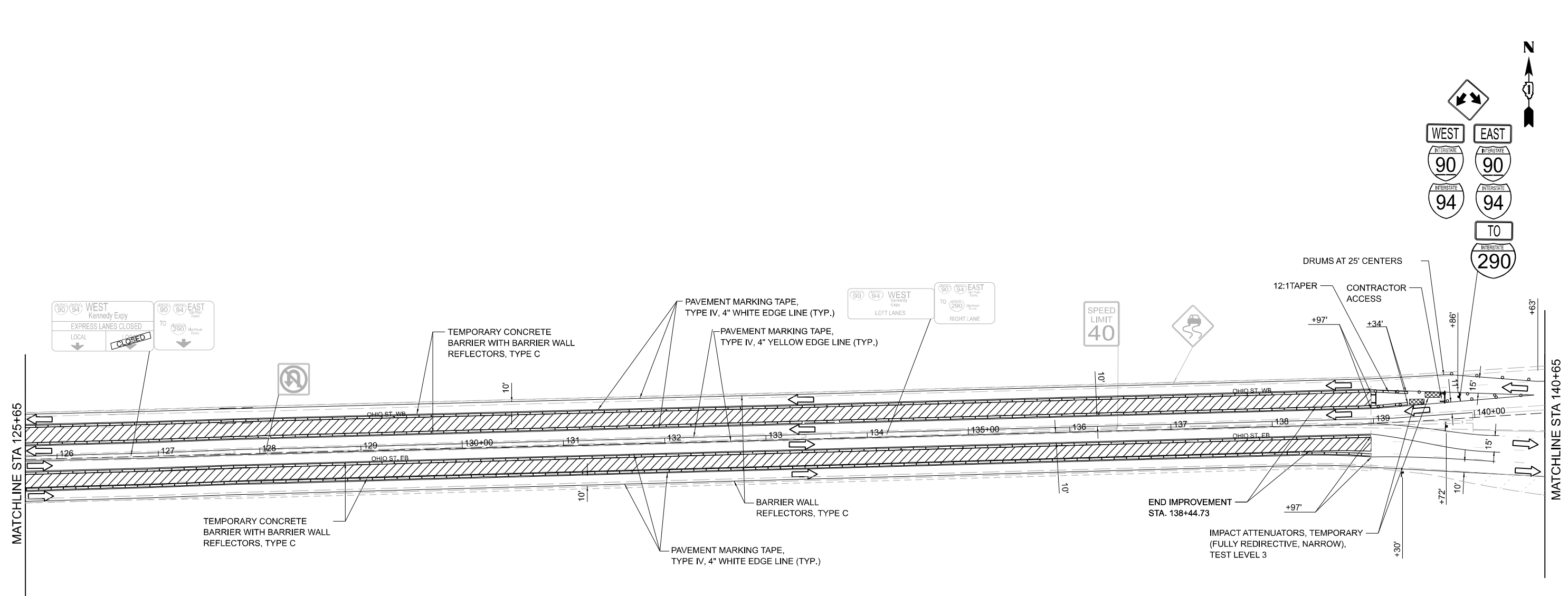
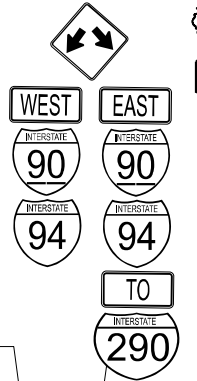
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 2**











SCALE: 1" = 50' SHEET 3 OF 5 SHEETS STA. 110+65 TO STA. 125+65

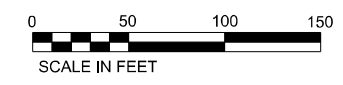
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	25
CONTRACT NO. 62L31				

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**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



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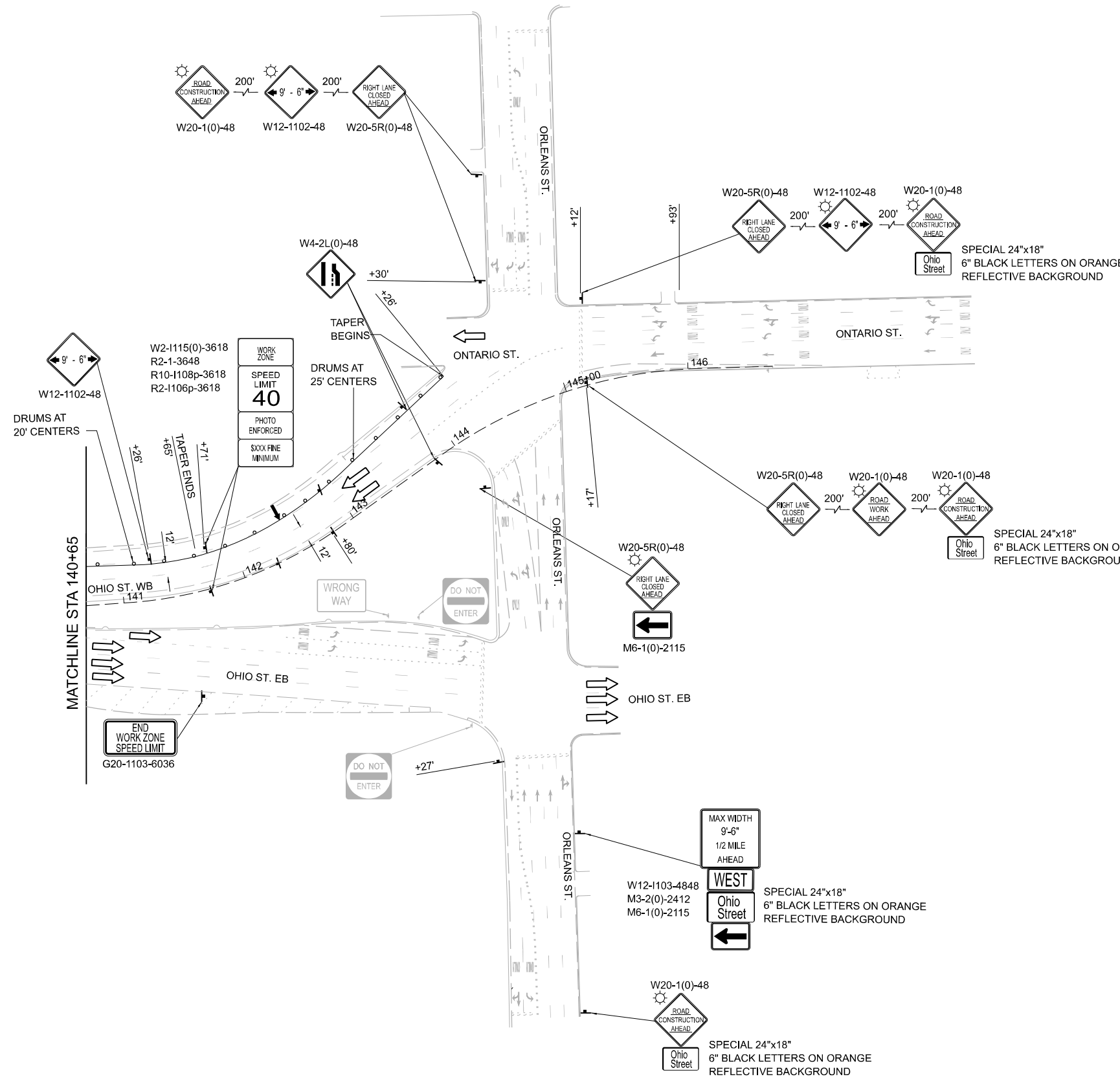
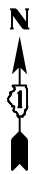
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DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 2**

SCALE: 1" = 50' SHEET 4 OF 5 SHEETS STA. 125+65 TO STA. 140+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	26
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



- LEGEND**
- CONSTRUCTION WORK ZONE
  - DRUMS WITH STEADY BURN BEACONS
  - TYPE III BARRICADE
  - TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
  - DIRECTION OF TRAFFIC FLOW
  - MOT SIGNAGE
  - EXISTING SIGNAGE
  - ARROW BOARD
  - FLASHER BEACON



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









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DEPARTMENT OF TRANSPORTATION**

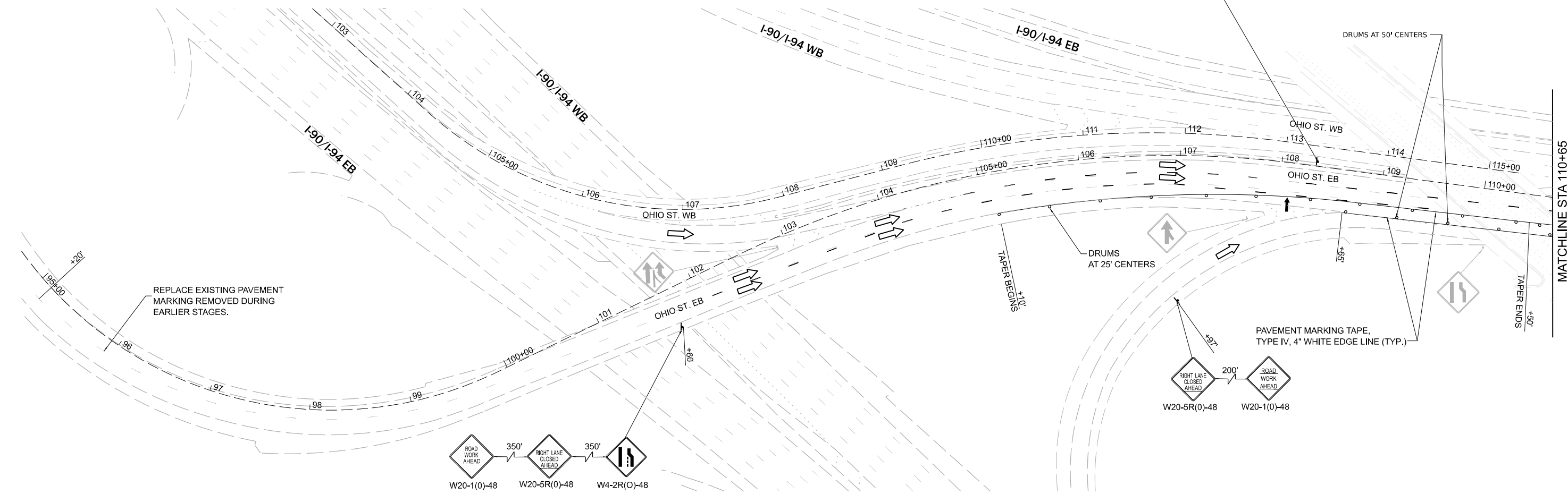
**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 2**

SCALE: 1" = 50' SHEET 5 OF 5 SHEETS STA. 140+65 TO STA. 147+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	27
CONTRACT NO. 62L31				
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**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON

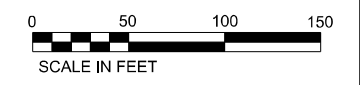
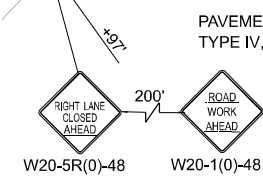
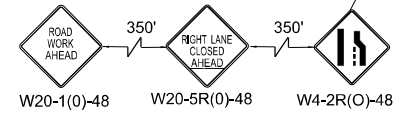


WORK ZONE  
SPEED LIMIT  
40  
PHOTO ENFORCED  
\$250 FINE MINIMUM  
W2-115(0)-3618  
R2-1-3648  
R10-1108p-3618  
R2-1106p-3618

**NOTES**

1. A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGES CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION. ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR THE PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT INTO THIS 72 HOURS NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.
2. EXISTING PAVEMENT MARKING THAT ARE IN CONFLICT WITH THE PROPOSED STAGED CONSTRUCTION SHALL BE REMOVED

REPLACE EXISTING PAVEMENT MARKING REMOVED DURING EARLIER STAGES.



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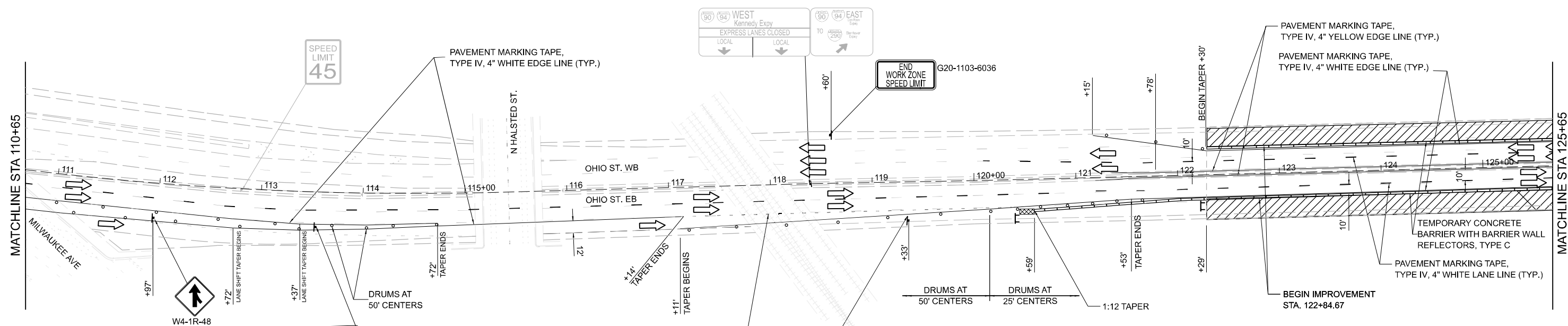
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DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 3**

SCALE: 1" = 50' SHEET 1 OF 4 SHEETS STA. 85+60 TO STA. 110+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	28
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- CONSTRUCTION WORK ZONE
- DRUMS WITH STEADY BURN BEACONS
- TYPE III BARRICADE
- TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
- DIRECTION OF TRAFFIC FLOW
- MOT SIGNAGE
- EXISTING SIGNAGE
- ARROW BOARD
- FLASHER BEACON

0 50 100 150  
SCALE IN FEET

WORK ZONE  
SPEED LIMIT  
**40**  
PHOTO ENFORCED  
\$220 FINE  
MINIMUM

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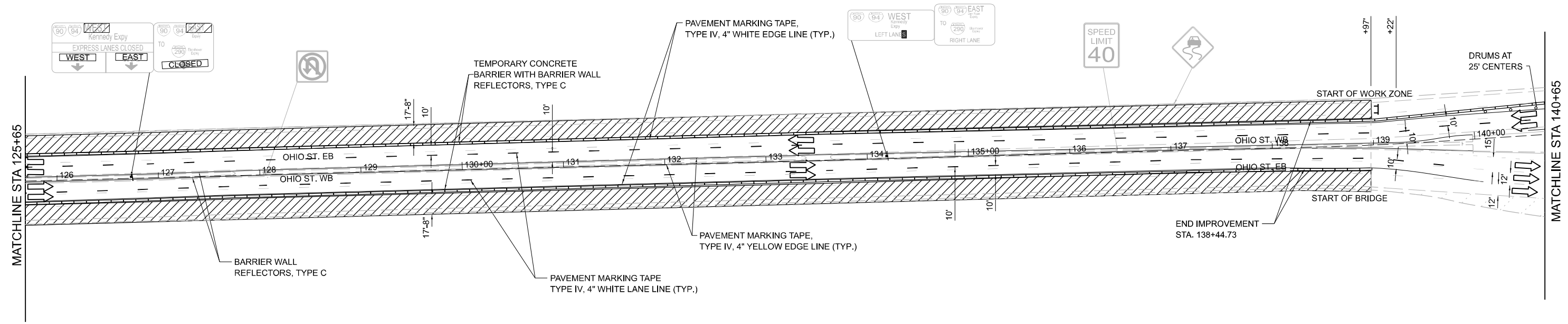
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 3**










SCALE: 1" = 50' SHEET 2 OF 4 SHEETS STA. 110+65 TO STA. 125+65

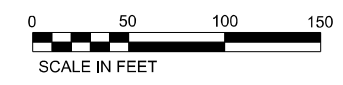
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	29
CONTRACT NO. 62L31				

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**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



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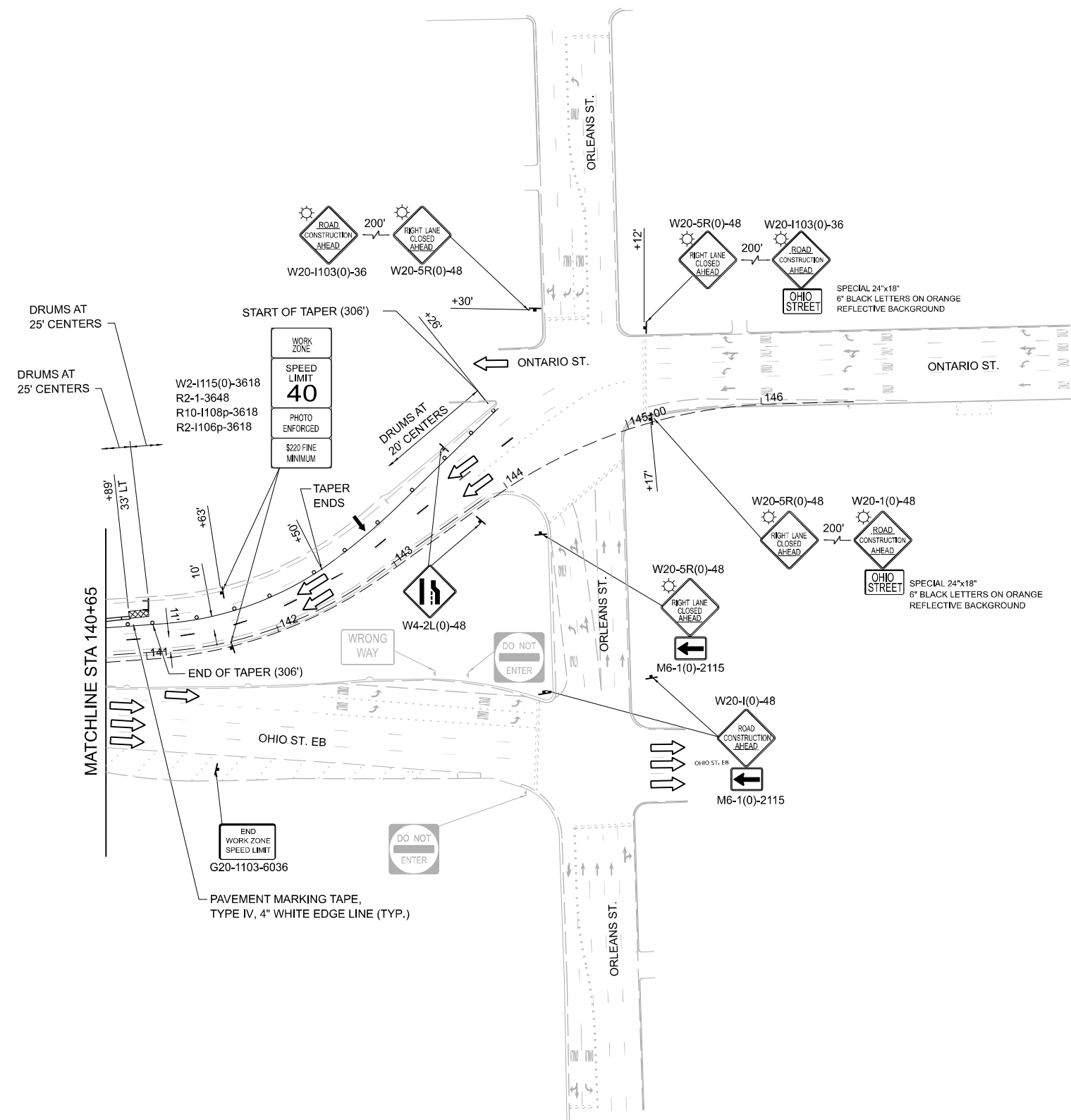
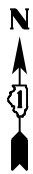
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**











**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 3**

SCALE: 1" = 50' SHEET 3 OF 4 SHEETS STA. 125+65 TO STA. 140+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	30
CONTRACT NO. 62L31				
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**LEGEND**

-  CONSTRUCTION WORK ZONE
-  DRUMS WITH STEADY BURN BEACONS
-  TYPE III BARRICADE
-  TEMPORARY CONCRETE BARRIER WITH BARRIER WALL REFLECTORS, TYPE C
-  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3
-  DIRECTION OF TRAFFIC FLOW
-  MOT SIGNAGE
-  EXISTING SIGNAGE
-  ARROW BOARD
-  FLASHER BEACON



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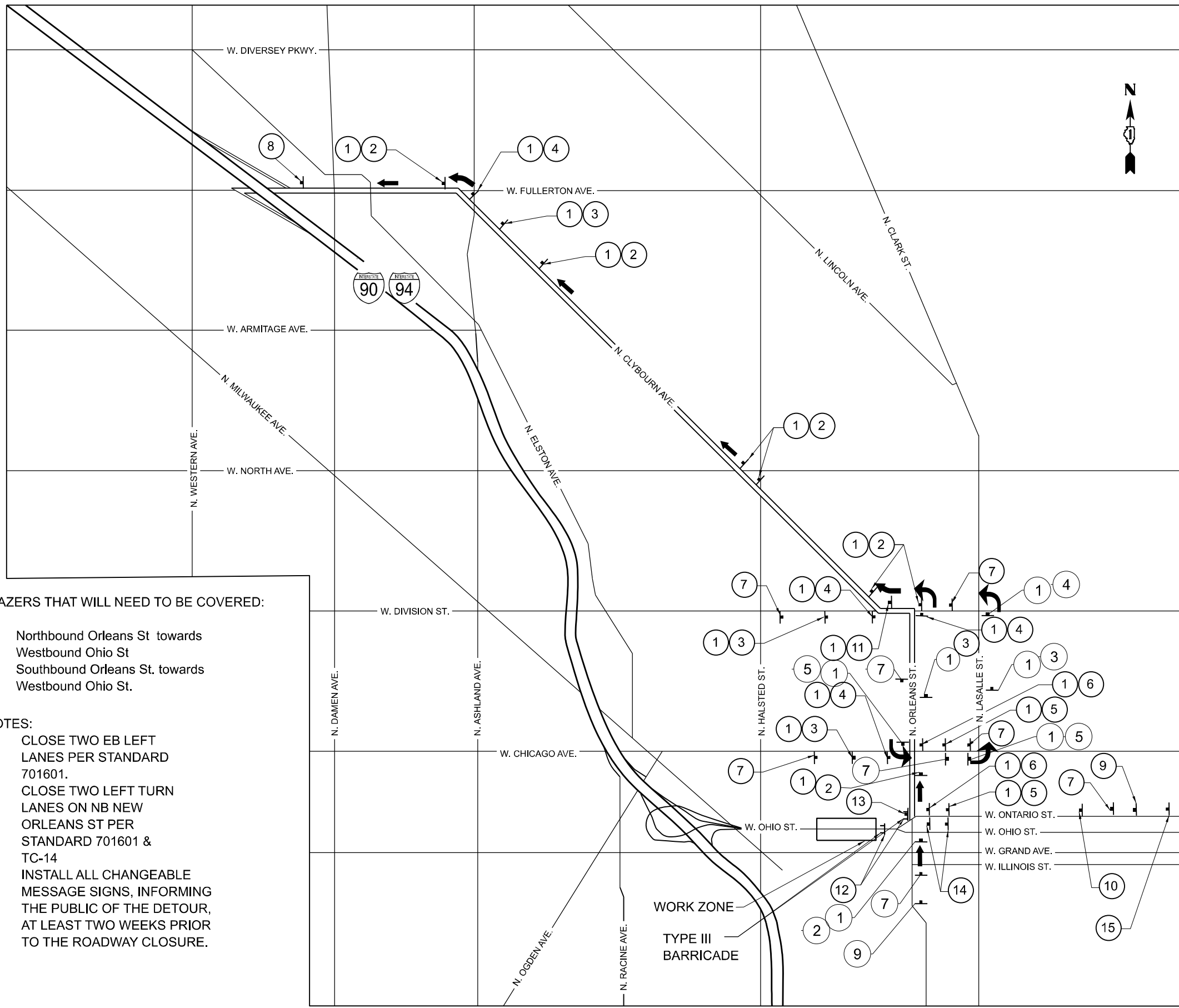
**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
STAGE 3**

SCALE: 1" = 50'    SHEET 4 OF 4 SHEETS    STA. 140+65 TO STA. 147+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	31
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				







**TRAIL BLAZERS THAT WILL NEED TO BE COVERED:**

- A. Northbound Orleans St towards Westbound Ohio St
- B. Southbound Orleans St. towards Westbound Ohio St.

**NOTES:**

1. CLOSE TWO EB LEFT LANES PER STANDARD 701601.
2. CLOSE TWO LEFT TURN LANES ON NB NEW ORLEANS ST PER STANDARD 701601 & TC-14
3. INSTALL ALL CHANGEABLE MESSAGE SIGNS, INFORMING THE PUBLIC OF THE DETOUR, AT LEAST TWO WEEKS PRIOR TO THE ROADWAY CLOSURE.

**LEGEND:**

→ INTERSTATE 90-94  
DETOUR ROUTE

**SIGN**

M4-5, 38x18 M4-8, 24x12 M1-1, 24x24		M-6-3, 21x15	
	1	2	
M-5-1, 21x15		M-6-1, 21x15	
	3	4	
M-5-1, 21x15		M-6-1, 21x15	
	5	6	
W20-2, 36x36	7	8	
W20-3, 36x36	9	10	
M-6-2, 21x15	11	12	
R11-2, 48x30 M4-10, 48x18	13	14	
		15	

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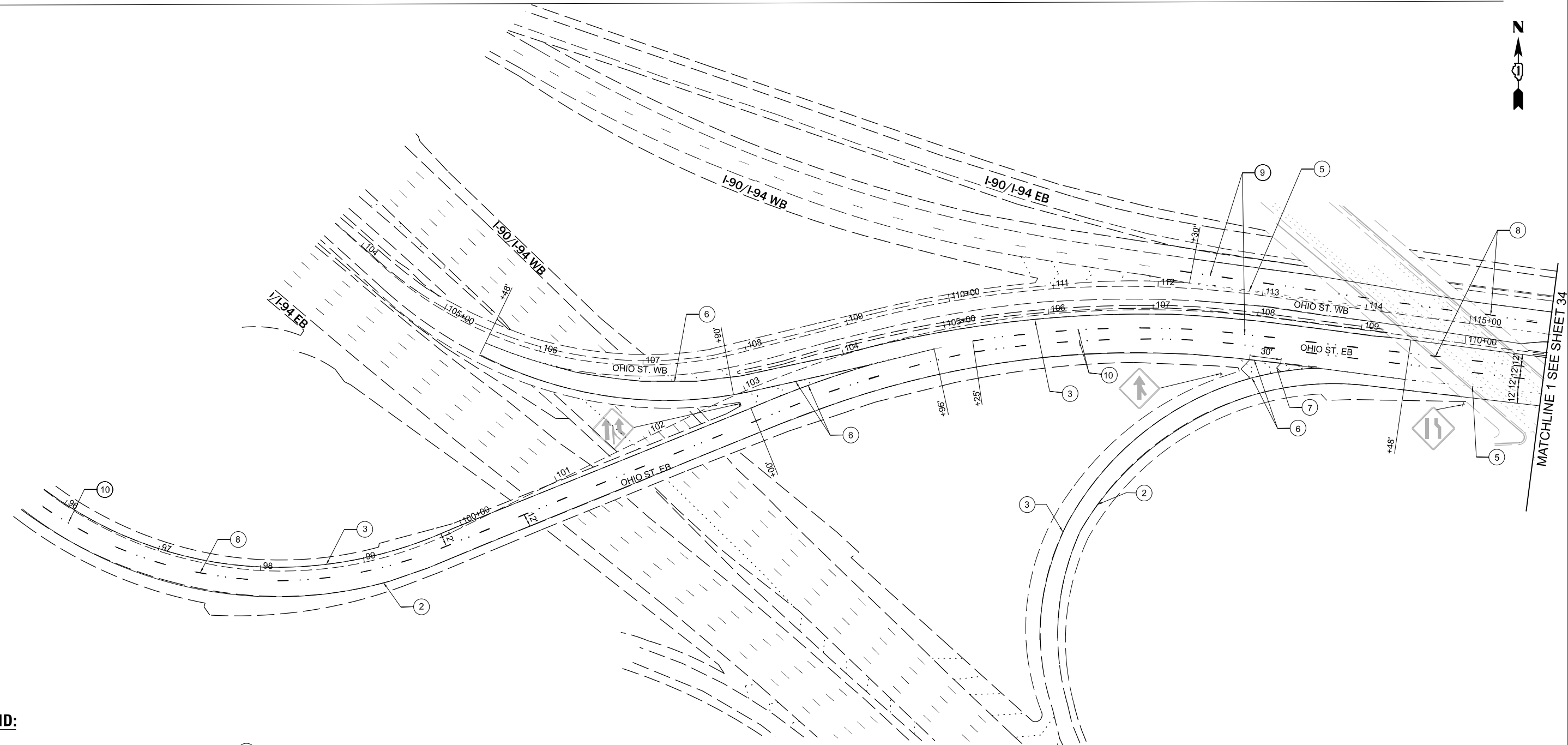
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
MAINTENANCE OF TRAFFIC  
WESTBOUND DETOUR**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	33
CONTRACT NO. 62L31				

SCALE: NTS SHEET 2 OF 2 SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

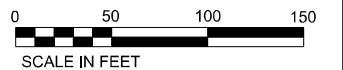


**LEGEND:**

- 1 MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS (78009000)
- 2 MODIFIED URETHANE PAVEMENT MARKING - LINE 4" WHITE EDGE LINE (78009004)
- 3 MODIFIED URETHANE PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE (78009004)
- 4 MODIFIED URETHANE PAVEMENT MARKING - LINE 5" WHITE LANE LINE 10' DASH 30' SKIP (78009005)
- 5 MODIFIED URETHANE PAVEMENT MARKING - LINE 6" WHITE 2' DASH 6' SKIP (78009006)
- 6 MODIFIED URETHANE PAVEMENT MARKING - LINE 8" WHITE LINE (78009008)
- 7 MODIFIED URETHANE PAVEMENT MARKING - LINE 12" WHITE LINE (78009012)
- 8 GROOVING FOR RECESSED PAVEMENT MARKING 9" (78011045)  
PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8" (1.5" BLACK, 5" WHITE, 1.5" BLACK)  
WHITE LANE LINE 10' DASH 30' SKIP (78004640)
- 9 RAISED REFLECTIVE PAVEMENT MARKERS INSTALLED PER IDOT STANDARDS (78100100)
- 10 RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT (Z0049100)
- 11 MODIFIED URETHANE PAVEMENT MARKING - LINE 24" WHITE STOP BAR (78009024)

**NOTES:**

1. REMOVE EXISTING BRIDGE RAISED REFLECTIVE PAVEMENT MARKERS AND DO NOT REPLACE ON BRIDGE.



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

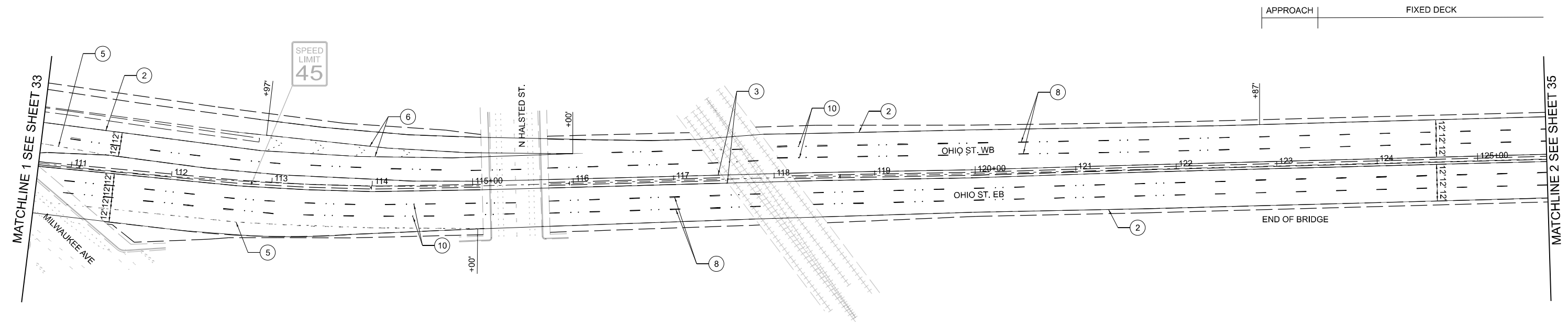
**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
PAVEMENT MARKING**

SCALE: 1" = 50' SHEET 1 OF 4 SHEETS STA. 85+60 TO STA. 110+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	34
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



PREFORMED PLASTIC LANE LINES  
AND MODIFIED URETHANE EDGE LINES

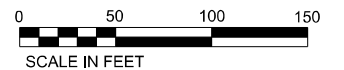


**LEGEND:**

- ① MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS (78009000)
- ② MODIFIED URETHANE PAVEMENT MARKING - LINE 4" WHITE EDGE LINE (78009004)
- ③ MODIFIED URETHANE PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE (78009004)
- ④ MODIFIED URETHANE PAVEMENT MARKING - LINE 5" WHITE LANE LINE 10' DASH 30' SKIP (78009005)
- ⑤ MODIFIED URETHANE PAVEMENT MARKING - LINE 6" WHITE 2' DASH 6' SKIP (78009006)
- ⑥ MODIFIED URETHANE PAVEMENT MARKING - LINE 8" WHITE LINE (78009008)
- ⑦ MODIFIED URETHANE PAVEMENT MARKING - LINE 12" WHITE LINE (78009012)
- ⑧ GROOVING FOR RECESSED PAVEMENT MARKING 9" (78011045)  
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- ⑩ RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT (Z0049100)
- ⑪ MODIFIED URETHANE PAVEMENT MARKING - LINE 24" WHITE STOP BAR (78009024)

**NOTES:**

1. REMOVE EXISTING BRIDGE RAISED REFLECTIVE PAVEMENT MARKERS AND DO NOT REPLACE ON BRIDGE.



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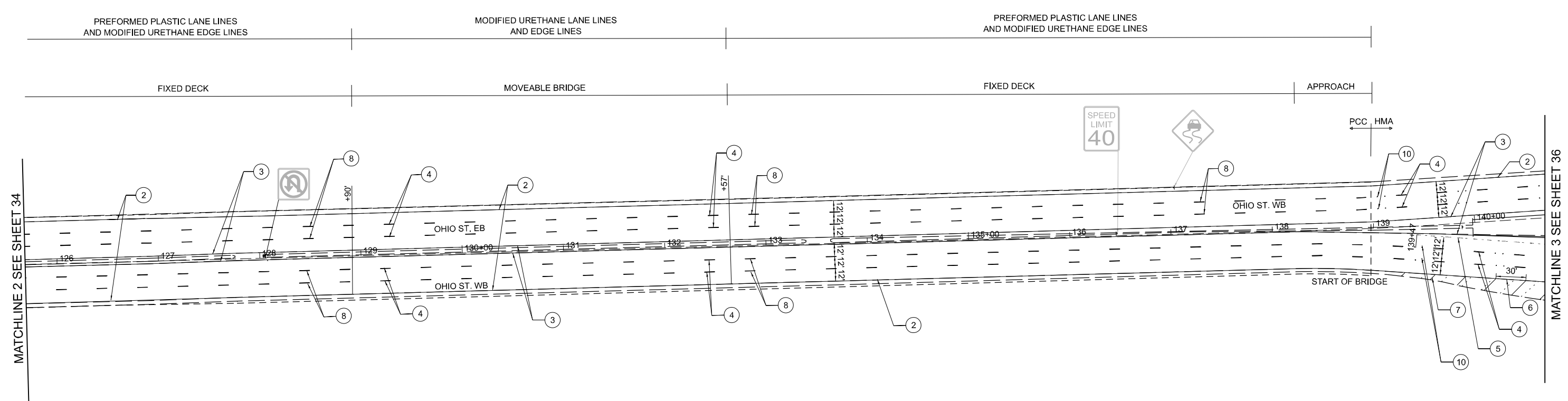
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DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
PAVEMENT MARKING**

SCALE: 1" = 50' SHEET 2 OF 4 SHEETS STA. 110+65 TO STA. 125+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	35
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

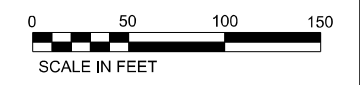


**LEGEND:**

- ① MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS (78009000)
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- ⑩ RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT (Z0049100)
- ⑪ MODIFIED URETHANE PAVEMENT MARKING - LINE 24" WHITE STOP BAR (78009024)

**NOTES:**

1. REMOVE EXISTING BRIDGE RAISED REFLECTIVE PAVEMENT MARKERS AND DO NOT REPLACE ON BRIDGE.



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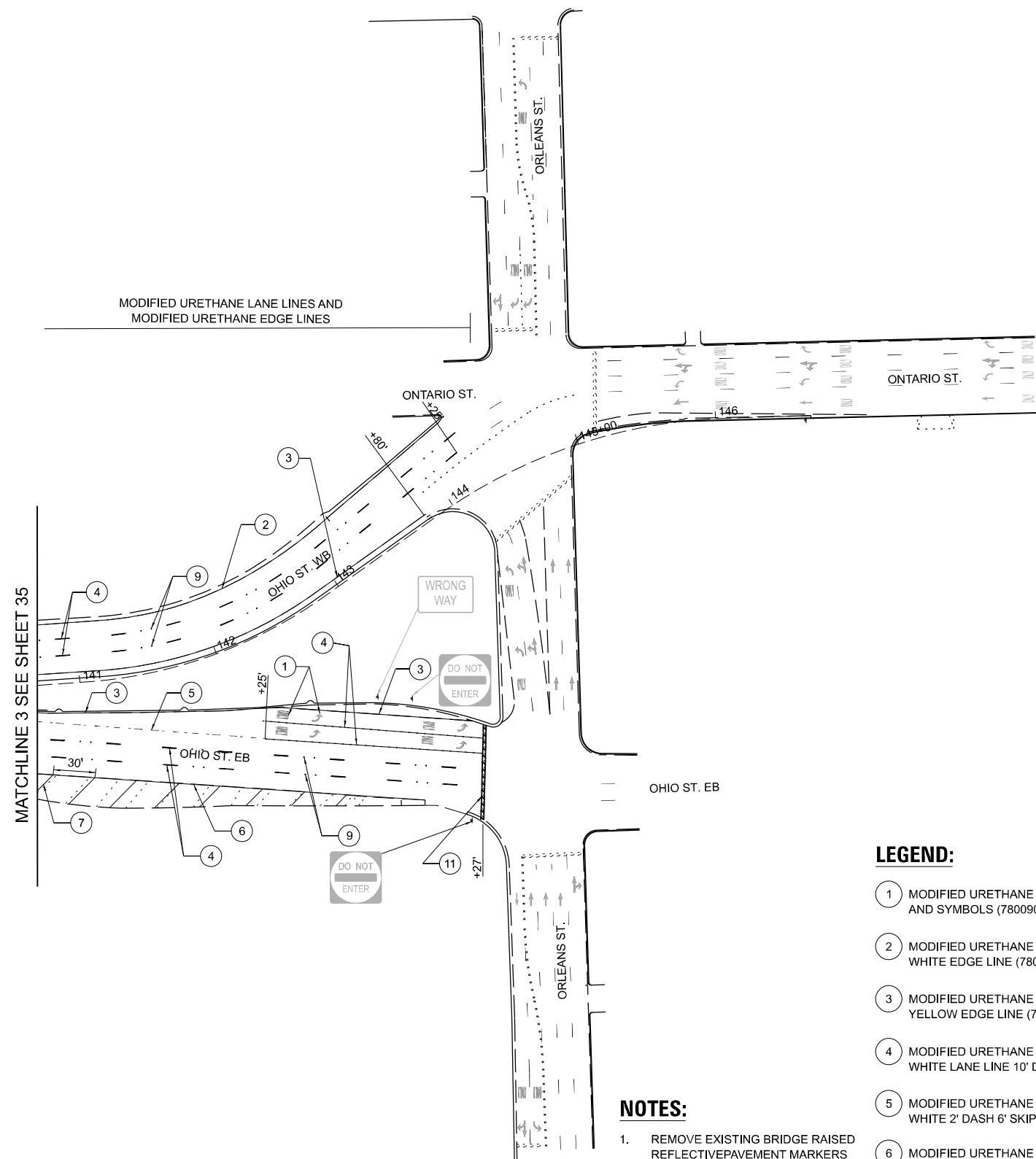
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
PAVEMENT MARKING**

SCALE: 1" = 50' SHEET 3 OF 4 SHEETS STA. 125+65 TO STA. 140+65

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	36
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

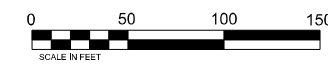


**LEGEND:**

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- 11 MODIFIED URETHANE PAVEMENT MARKING - LINE 24" WHITE STOP BAR (78009024)

**NOTES:**

1. REMOVE EXISTING BRIDGE RAISED REFLECTIVE PAVEMENT MARKERS AND DO NOT REPLACE ON BRIDGE.



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DATE - 02/28/2023	REVISED - _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FAP 383 (OHIO STREET) AT NORTH BRANCH CHICAGO RIVER  
PAVEMENT MARKING**

SCALE: 1" = 50' SHEET 4 OF 4 SHEETS STA. 140+65 TO STA. 147+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	D-91-450-20	COOK	177	37
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

Benchmark: Benchmark #1323 is a concrete cut square on top of northern most parapet. It is located adjacent to Southeast corner of the Northeast Bridge Tower. Elevation 629.27

Existing Structure: The Existing structure (No. 016-0202) was built in 1961. The structure is a double leaf Trunnion Bascule Bridge with a main span of 250'-7"  $\pm$  to  $\pm$  of trunnion and two fixed approaches. It carries 6 lanes of traffic and is 89'-8" out to out of deck. The structure underwent a rehabilitation in 1992 that included superstructure modifications, deck replacement and widening.

Traffic to be maintained utilizing Stage Construction.

No salvage.

**SCOPE OF WORK**

1. Place Bridge Deck Latex Concrete Overlay
2. Place Thin Polymer Overlay on Grid Deck
3. Perform Steel Repairs to Fixed and Movable Spans
4. Perform Substructure Concrete Repairs
5. Replace Inspection Walkway Grating and Support Members.
6. Drainage System Repairs
7. Replace Interior Machinery room corrugated Metal Envelope.
8. Perform Bridge Tender House Concrete Repairs
9. Perform Bridge Tender House Exterior Fixture Repairs

**LOADING HS-20-44**

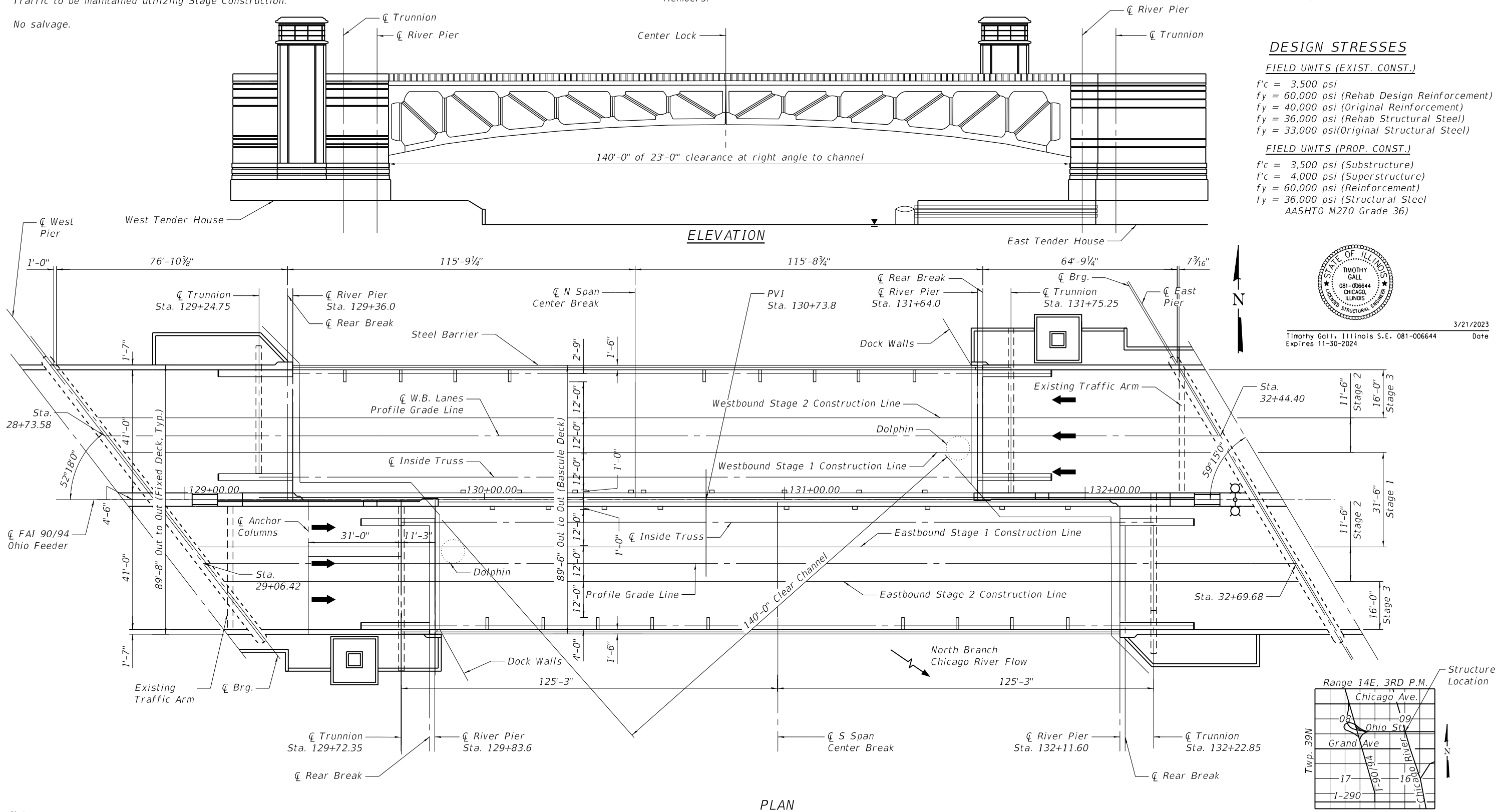
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HS20-S16-44 per AASHTO 1957  
PROP. CONST.:  
HS20-44 per AASHTO 2002

**DESIGN STRESSES**

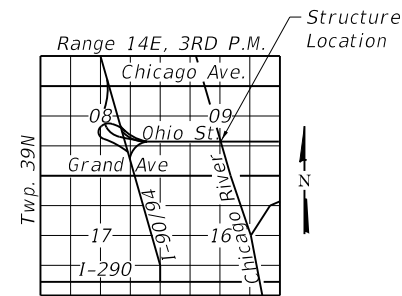
FIELD UNITS (EXIST. CONST.)  
f'c = 3,500 psi  
fy = 60,000 psi (Rehab Design Reinforcement)  
fy = 40,000 psi (Original Reinforcement)  
fy = 36,000 psi (Rehab Structural Steel)  
fy = 33,000 psi (Original Structural Steel)

FIELD UNITS (PROP. CONST.)

f'c = 3,500 psi (Substructure)  
f'c = 4,000 psi (Superstructure)  
fy = 60,000 psi (Reinforcement)  
fy = 36,000 psi (Structural Steel)  
AASHTO M270 Grade 36



Timothy Gall, Illinois S.E. 081-006644 Date 3/21/2023 Expires 11-30-2024



LOCATION SKETCH

Note:  
Up to 1/4 inch may be ground off the fixed span bridge deck.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION  
SN 016-0202

SHEET SB-01 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	80
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

**GENERAL NOTES**

1. Fasteners shall be ASTM F3125 Grade A325 Type 1, mechanically galvanized bolts in painted areas. Holes shall be subpunched or subdrilled 11/16" dia. and reamed in the field to 13/16" dia. for 3/4" dia. bolts, unless otherwise noted. Holes shall be subpunched or subdrilled 13/16" dia. and reamed in the field to 15/16" dia. for 7/8" dia. bolts, unless otherwise noted.

2. All structural steel shall be AASHTO M270 Grade 36.

3. No field welding is permitted except as specified in the contract documents.

4. Reinforcement bars designated (E) shall be epoxy coated.

5. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding 1#4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications

6. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

7. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

8. The Contractor shall complete all superstructure steel repairs prior to the placing of the concrete deck overlay.

9. The Inorganic Zinc Rich Primer/Acrylic/Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for steel surfaces shall be light gray, Munsell No. 10 Y 7/1.

10. Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning And Painting Contact Surface Areas of Existing Steel Structures".

11. Cleaning and Painting of existing steel barrier, existing steel plate along concrete parapet, existing U13-U15 top chord, U13'-U15' top chord, weep holes, stair and walkway attachments shall be in accordance with IDOT GBSP 25 "Cleaning and Painting Existing Steel Structures". The color of the final finish coat shall be Light Gray, Munsell No. 10 Y 7/1 at outside face of barrier, top chord, weep holes, stair and walkway attachments. The color of the final coat at inside face of barrier and steel plate shall be Reddish Brown, Munsell No. 2.5YR 3/4.

12. The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.

**INDEX OF SHEETS**

- SB-01 General Plan and Elevation
- SB-02 General Data
- SB-03 Existing Bridge Profiles
- SB-04 Deck Plan
- SB-05 Construction Staging
- SB-06 Fixed Deck Typical Sections
- SB-07 Bascule Deck Typical Sections
- SB-08 Deck Details 1 of 2
- SB-09 Deck Details 2 of 2
- SB-10 Temporary Concrete Barrier
- SB-11 Top of West Fixed Span Deck Elevations
- SB-12 Top of East Fixed Span Deck Elevations
- SB-13 Bascule Parapet Painting Details
- SB-14 Fixed Spans Framing Plan
- SB-15 North Bascule Framing Plan
- SB-16 South Bascule Framing Plan
- SB-17 Truss Repair Northwest Leaf North Truss U1-U3
- SB-18 Floor Beam 0-0 Repair
- SB-19 Floor Beam 12a-12a Repair
- SB-20 Sidewalk Bracket and Interior Lateral Bracing Repair
- SB-21 Curb Girder Repairs
- SB-22 Column and Stringer Repair Details
- SB-23 Truss Repair Node M14 Weep Hole
- SB-24 Lattice Strut L12-M14 Repair Details
- SB-25 Lattice Strut L12-M14 Working Platform Repair Details
- SB-26 Lateral Bracing Repairs
- SB-27 West Pier
- SB-28 West Pier Cap and Bearing Details
- SB-29 East Pier
- SB-30 East Pier Cap Details
- SB-31 East Pier Temporary Shoring Plan
- SB-32 East Pier Jacking and Shoring Sections
- SB-33 Counterweight Pit Plan
- SB-34 Southwest Counter Weight Pit Elevations
- SB-35 Northwest Counter Weight Pit Elevations
- SB-36 Northeast Counter Weight Pit Elevations
- SB-37 Southeast Counter Weight Pit Elevations
- SB-38 River Facing Counter Weight Pit Elevations
- SB-39 Drainage System Repairs
- SB-40 Operator House Elevations
- SB-41 Operator House Floor and Wall Concrete Repairs
- SB-42 Operator House Windows and Misc. Hardware
- SB-43 Pit Partition - Enclosure Walls - Plan
- SB-44 Pit Partition - Enclosure Walls - Details
- SB-45 Inspection Walkway Plan
- SB-46 Inspection Walkway Repair Details
- SB-47 Center Lock Platform Repair Details
- SB-48 Bar Splicer Assembly and Mechanical Splicer Details

13. The Contractor shall obtain Coast Guard approval for any work that may interfere with navigational operations of the navigable waters. A work plan shall be prepared by the Contractor, reviewed and approved by the Engineer and be submitted by the Engineer to the Coast Guard at the address listed below for approval.

Bridge Administrator  
 US Coast Guard  
 Ninth Coast Guard District  
 1240 E. Ninth St.  
 Cleveland, OH 44199-2060

14. The Contractor shall contact Mr. Luis Benitez of CDOT, Luis.Benitez@cityofchicago.org, 312-744-5807, for access to the SN 016-0202 bridge houses. The Contractor shall assume all maintenance and operational responsibilities for SN 016-0202 from CDOT for the duration of the contract. Upon completion of the contract, SN 016-0202 must be returned to its current operational state before maintenance and operational responsibilities are transferred back to CDOT. The work required to maintain and operate SN 016-0202 shall be considered included in the cost of the contract.

15. The Contractor shall take all necessary measures to assure that no debris falls into the River. The cost of this work shall be included in the various Pay Items.

16. The Contractor shall retain the services of an engineering firm, prequalified in the IDOT consultant selection category of Highway Bridges (Advanced Typical), for preparation of the Structural Assessment Report(s). Contractor's pre-approval shall not be applicable for this project. See Special Provision."

17. Contractor to field verify member measurements, connections, bolt/rivet size and spacing prior to ordering or fabricating any new steel. The Contractor's shop drawings must indicate which dimensions were obtained by actual field measurements.

18. It is intended that the structural repair work for SN 016-0202 is performed with all moveable leaves of the bridge in the closed (down) position. If the Contractor wishes to perform structural repair work on SN 016-0202 with moveable leaves in the open (up) position, the Contractor shall verify the stability of the bridge in the open position per the requirements of the "STRUCTURAL ASSESSMENT REPORTS FOR CONTRACTOR'S MEANS AND METHODS" special provision. Any bridge openings shall be coordinated with the Engineer and CDOT.

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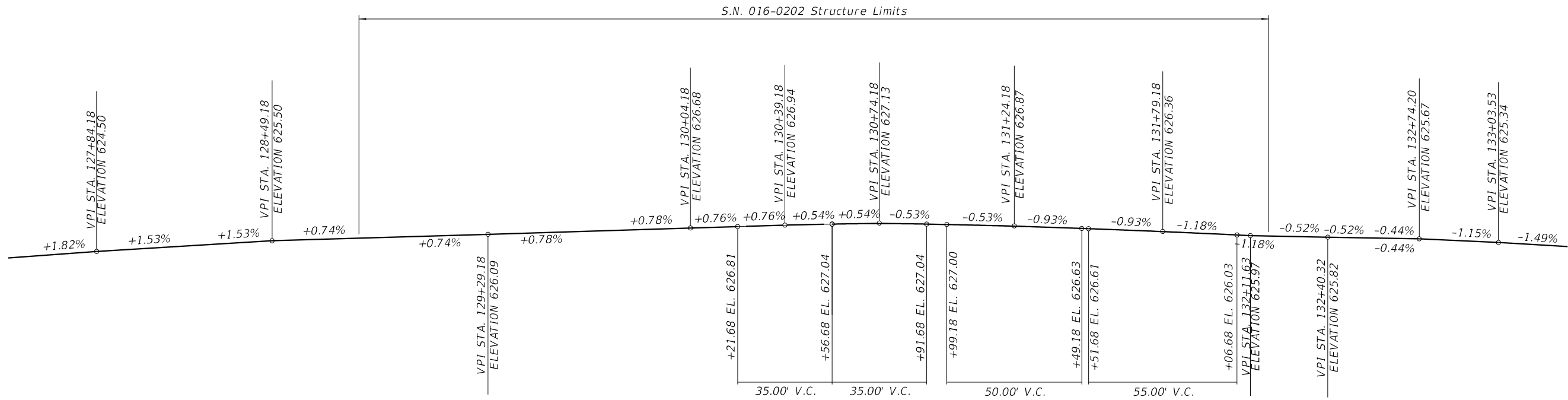
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

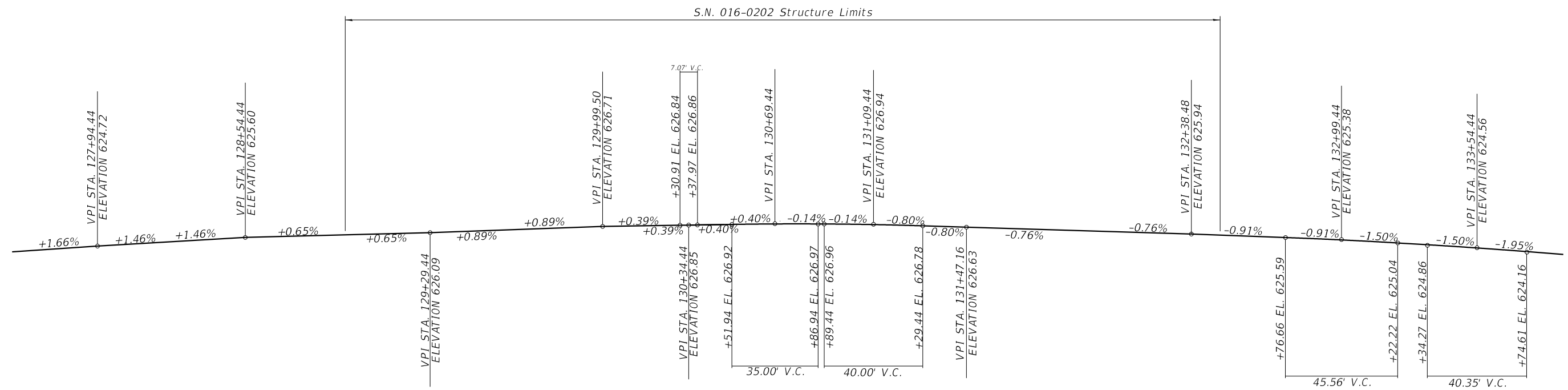
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 SN 016-0202**

SHEET SB-02 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	81
CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	



PROFILE GRADE - EASTBOUND P.G.L.



PROFILE GRADE - WESTBOUND P.G.L.

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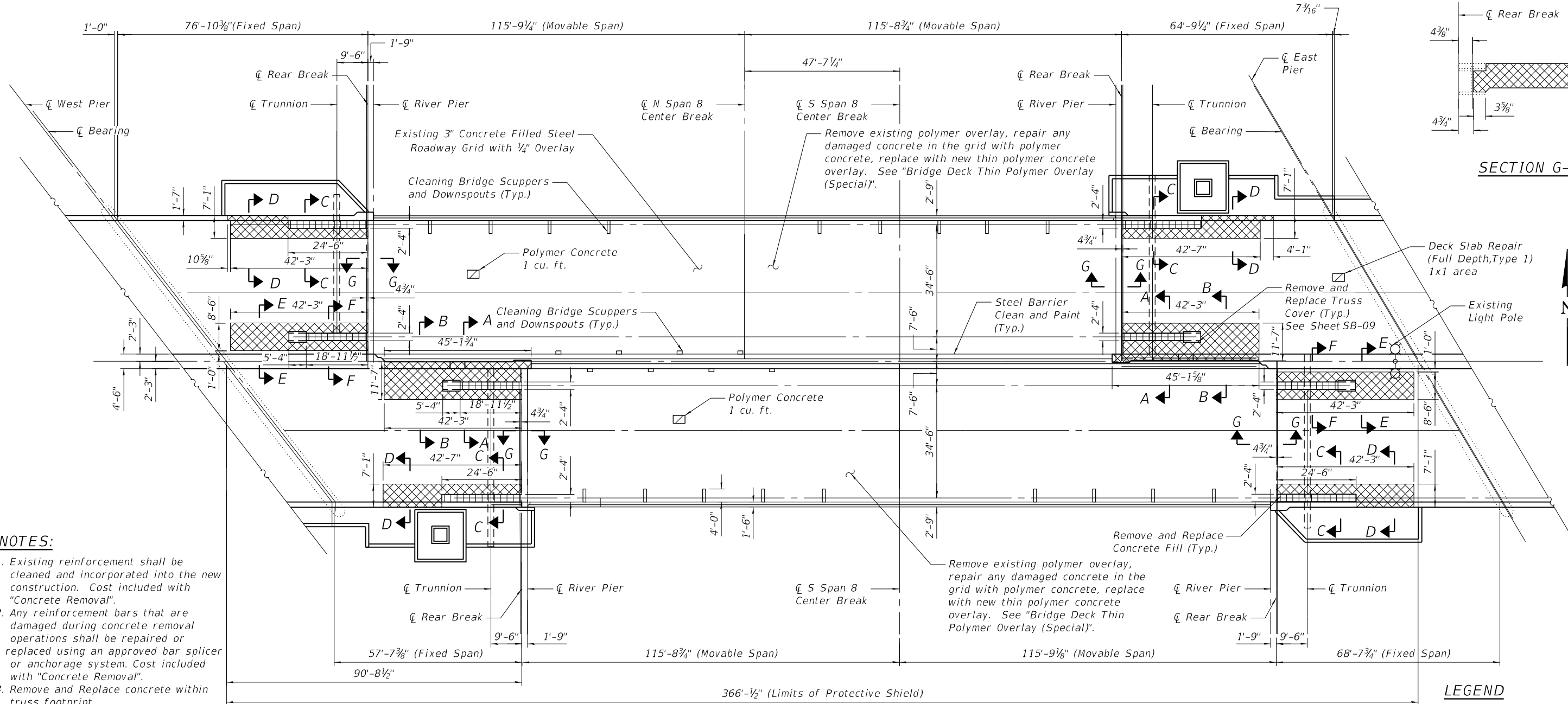
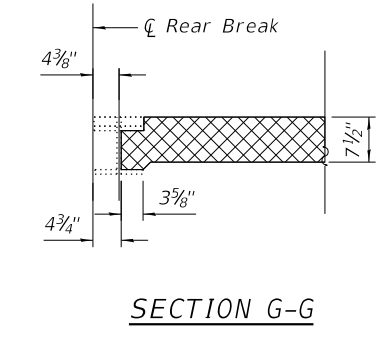
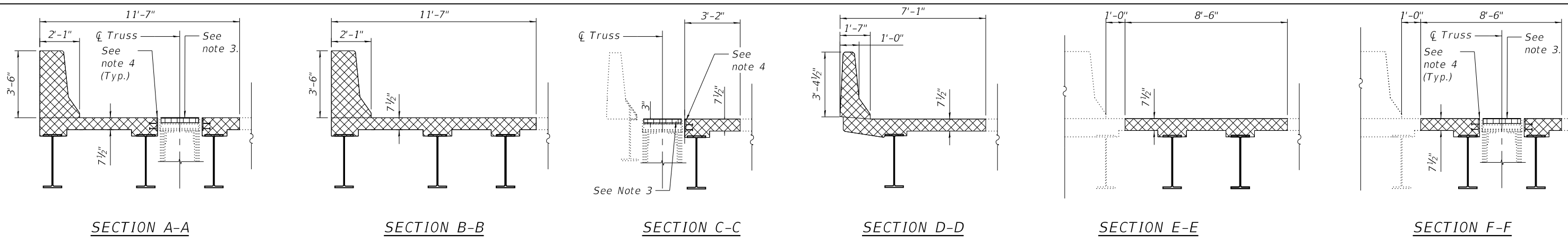
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING BRIDGE PROFILES  
 SN 016-0202**

SHEET SB-03 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	82
CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	





- NOTES:**
- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
  - Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
  - Remove and Replace concrete within truss footprint.
  - Remove and replace existing embedded plate, typ. See Sheet SB-21 for details.

**LEGEND**

	Concrete Removal
	Concrete Removal within Truss Footprint

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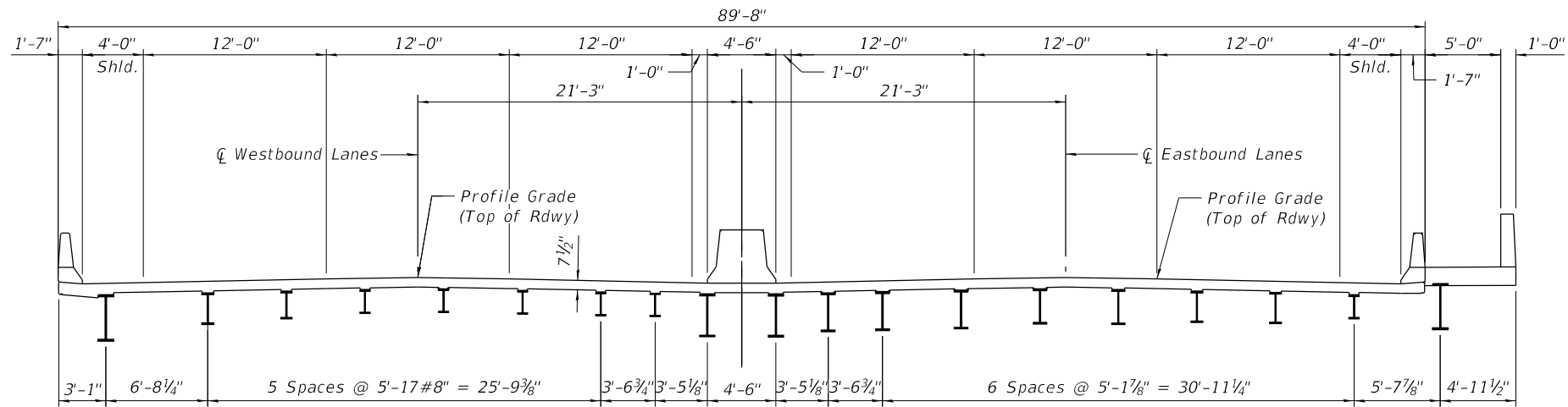
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

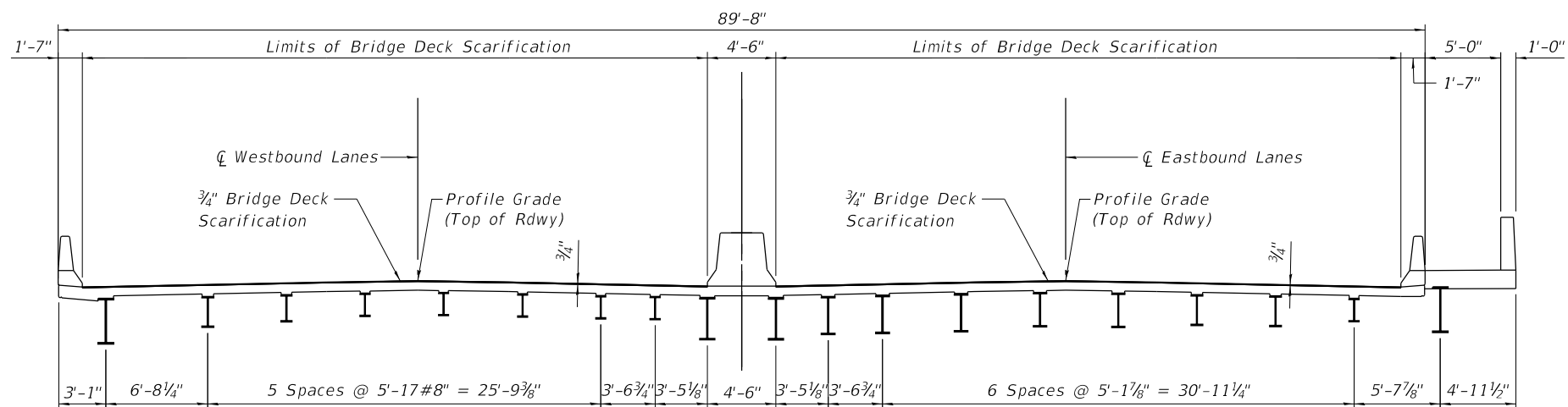
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<b>SN 016-0202</b>	
SHEET SB-04	OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	83
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ILLINOIS FED. AID PROJECT				

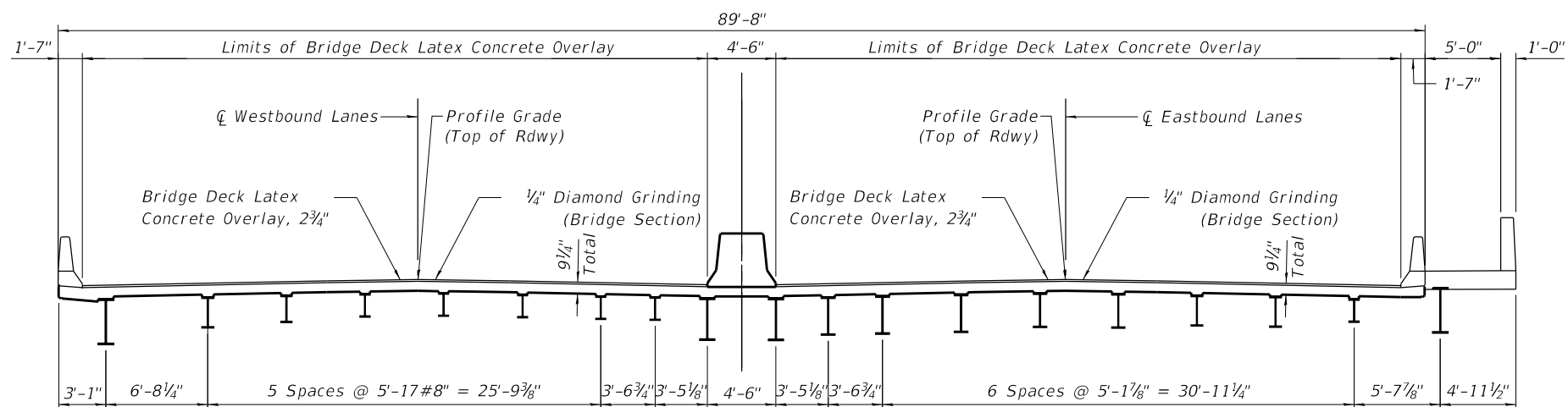




EXISTING CROSS SECTION



REMOVAL CROSS SECTION



FINAL CROSS SECTION

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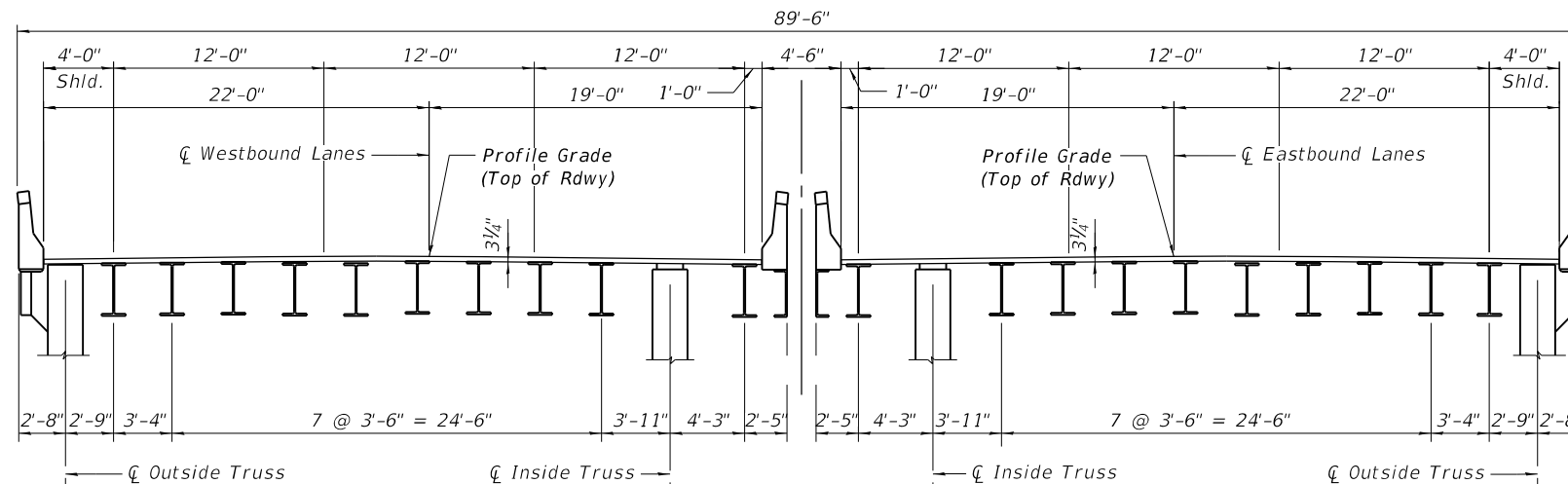
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

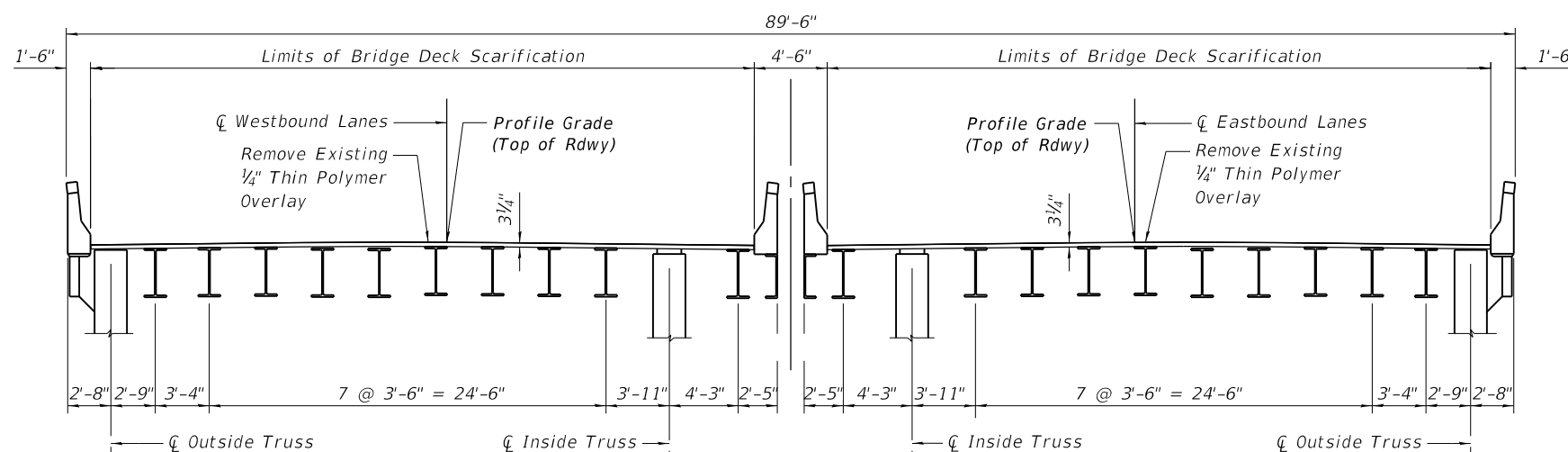
FIXED DECK TYPICAL SECTIONS  
 SN 016-0202

SHEET SB-06 OF SB-48 SHEETS

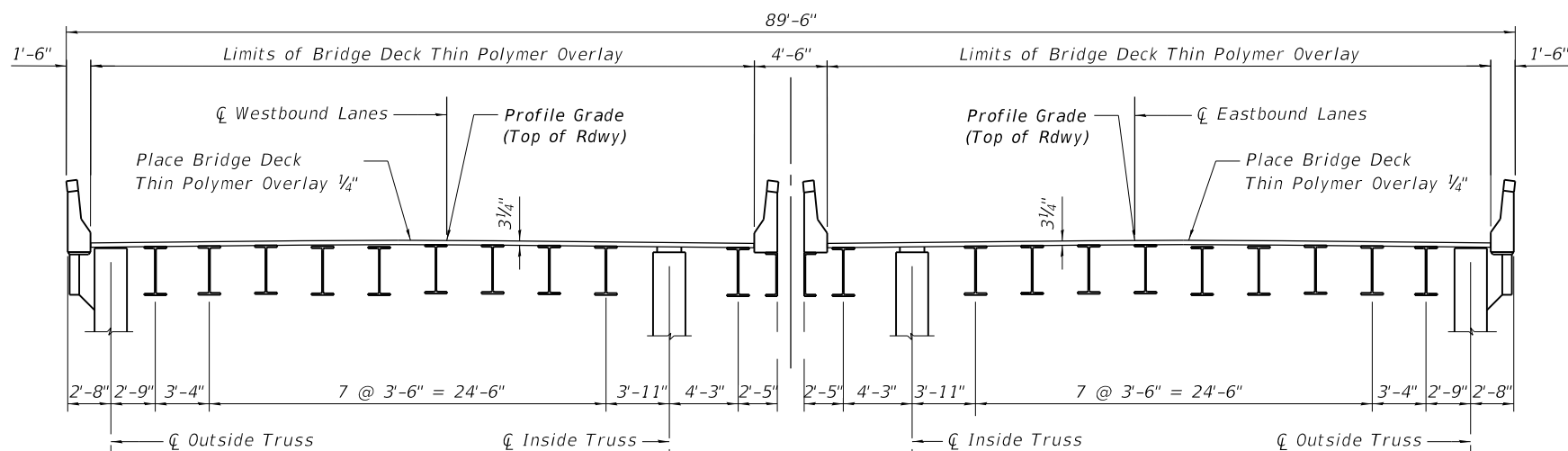
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	85
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



EXISTING CROSS SECTION



REMOVAL CROSS SECTION



FINAL CROSS SECTION

**NOTES:**

- Repairs to bascule grid deck in accordance with IDOT Guide Bridge Special Provision 45.
- Remove the existing polymer overlay using scarification techniques approved by the Engineer and in accordance with IDOT Guide Bridge Special Provision 45.

MODEL: Default  
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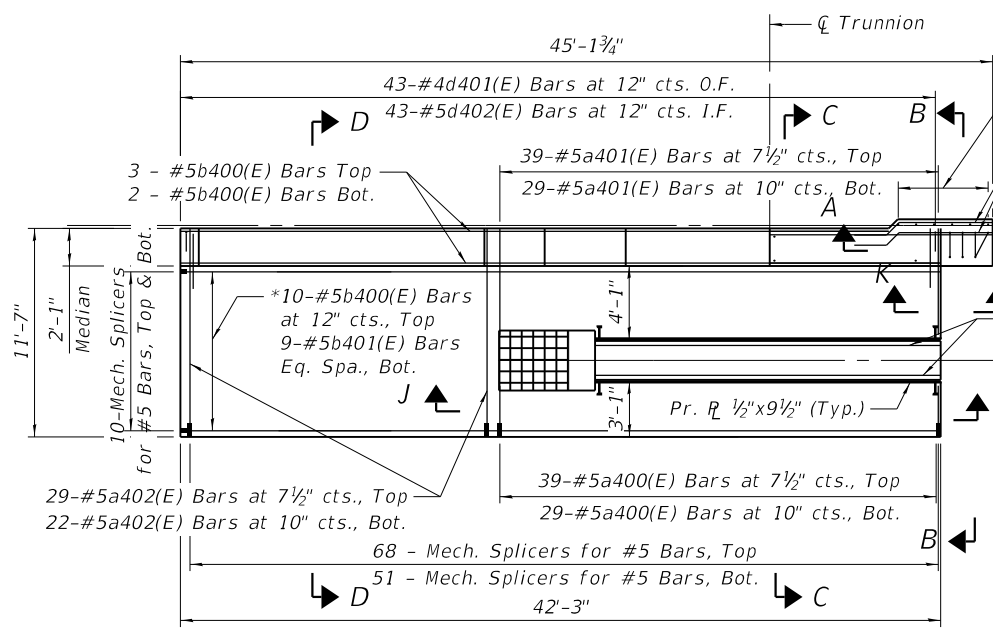
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BASCULE DECK TYPICAL SECTIONS  
SN 016-0202

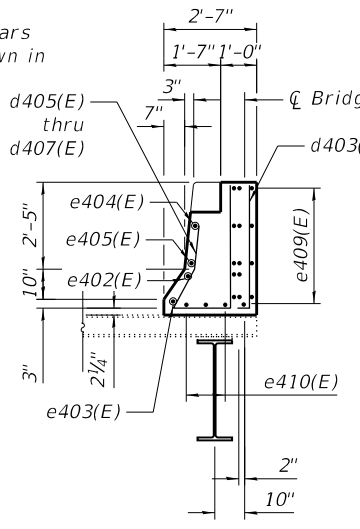
SHEET SB-07 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	86
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

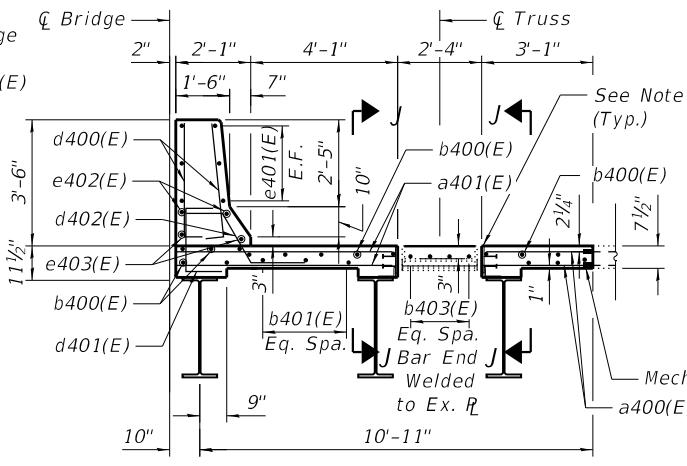


**PLAN**

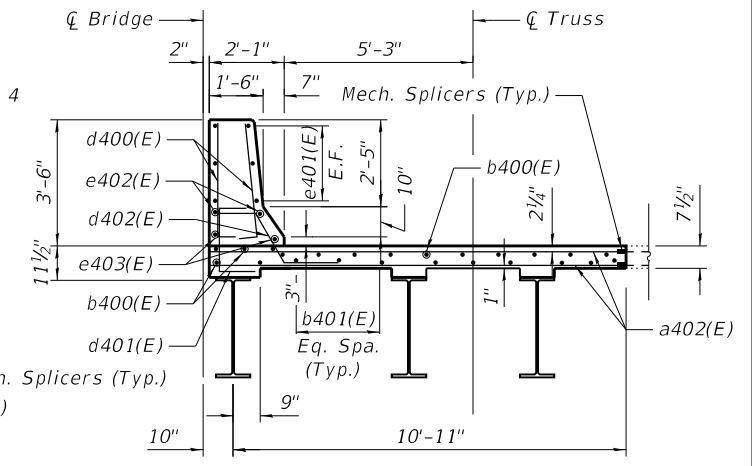
(2 Locations: Inside Truss Southwest Leaf and Northeast Leaf)



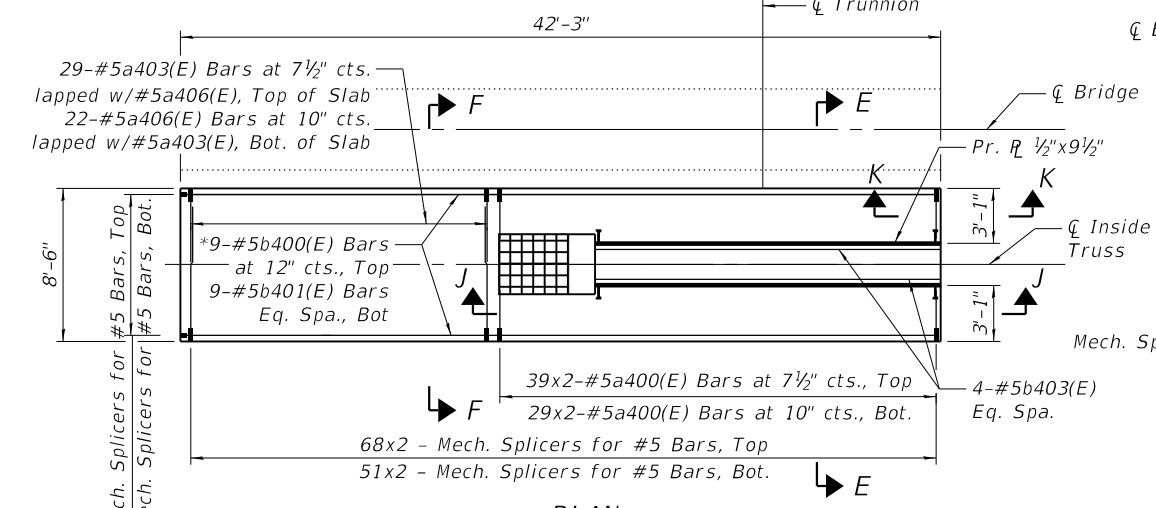
**SECTION B-B**



**SECTION C-C**

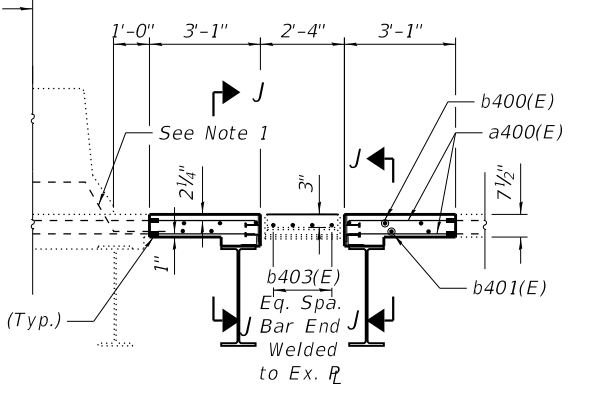


**SECTION D-D**

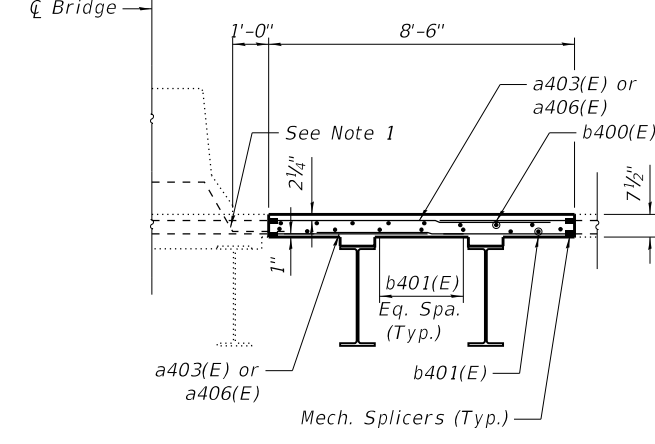


**PLAN**

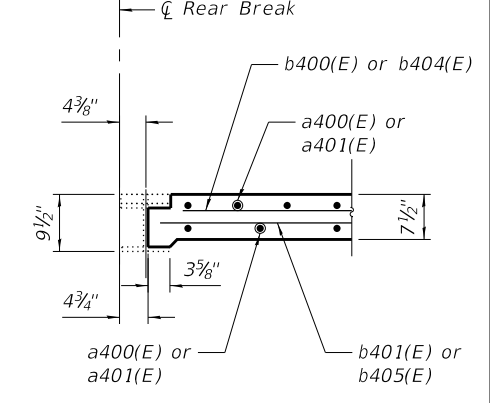
(2 Locations: Inside Truss Northwest Leaf and Southeast Leaf)



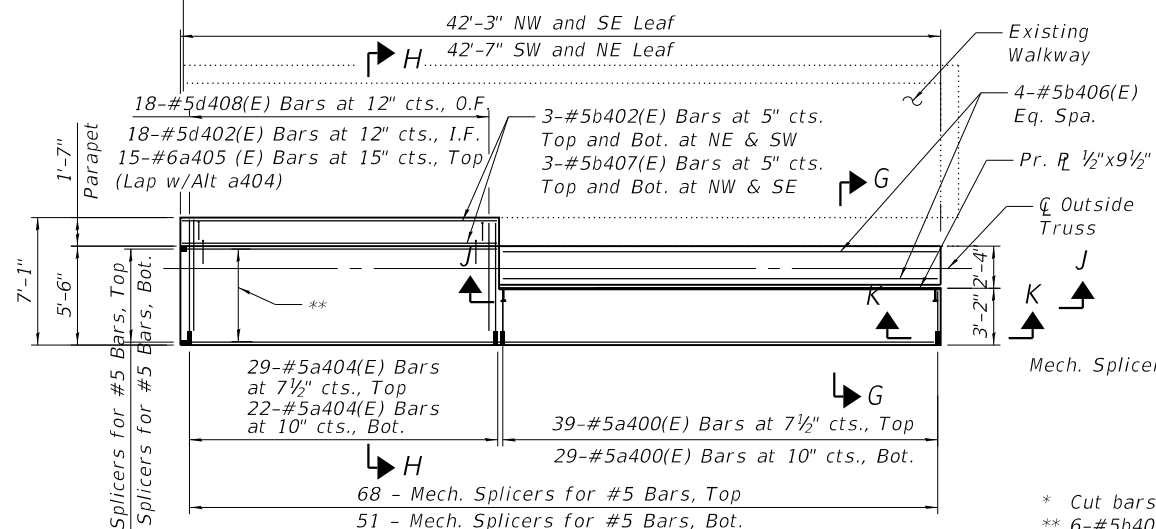
**SECTION E-E**



**SECTION F-F**

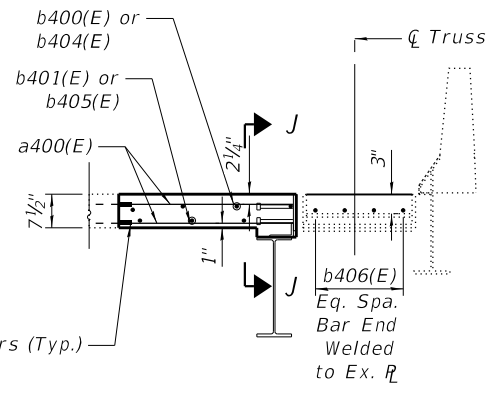


**SECTION K-K**

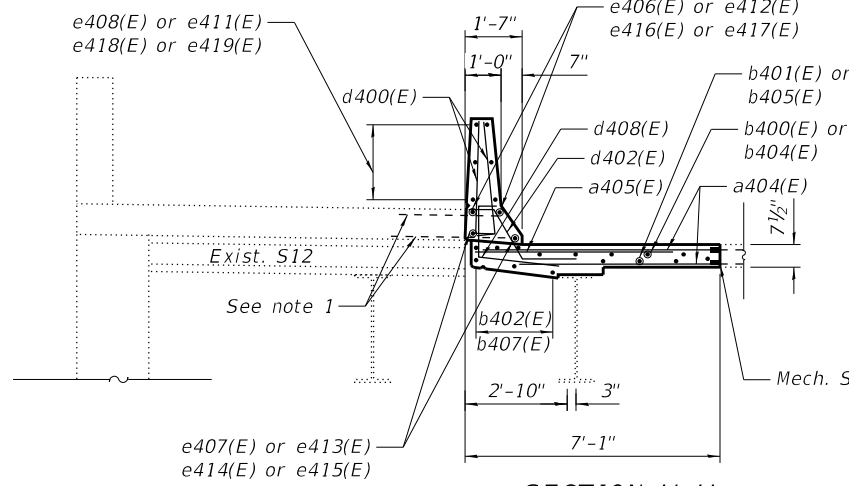


**PLAN**

(4 Locations: Outside Truss Northwest Leaf and Southeast Leaf Outside Truss Northeast Leaf and Southwest Leaf)



**SECTION G-G**



**SECTION H-H**

- \* Cut bars to fit at truss cover
- \*\* 6-#5b400(E) Bars at 12\"/>
- 6-#5b404(E) Bars at 12\"/>
- 4-#5b401(E) Bars Eq. Spa., Bot. at NW&SE.
- 4-#5b405(E) Bars Eq. Spa., Bot. at NE&SW.

**NOTES:**

1. Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Concrete Removal.
2. See Sheet SB-09 for median barrier elevation and Section A-A details.
3. See Sheet SB-09 for Truss Cover details.
4. See Sheet SB-21 for Section K-K.

**LEGEND**

- I.F. Inside Face
- O.F. Outside Face
- E.F. Each Face

MODEL: Default  
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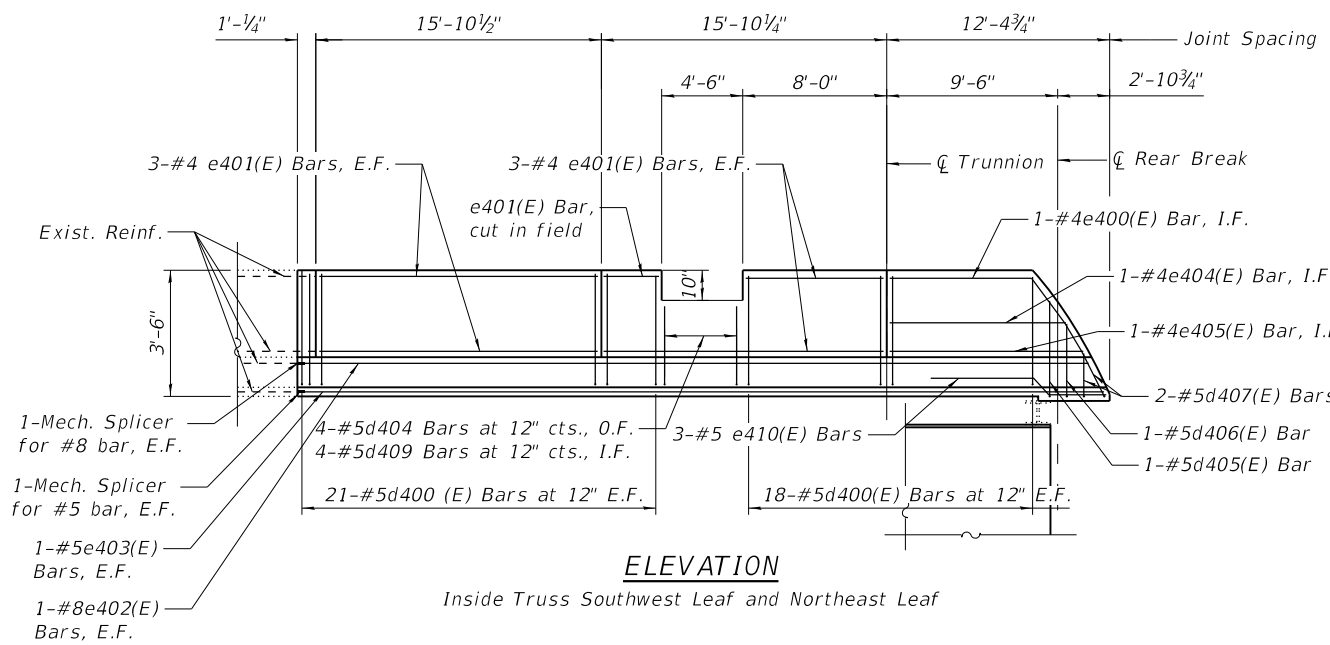
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	CHECKED - TCG	REVISED -
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PLOT DATE =	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK DETAILS 1 OF 2  
SN 016-0202**

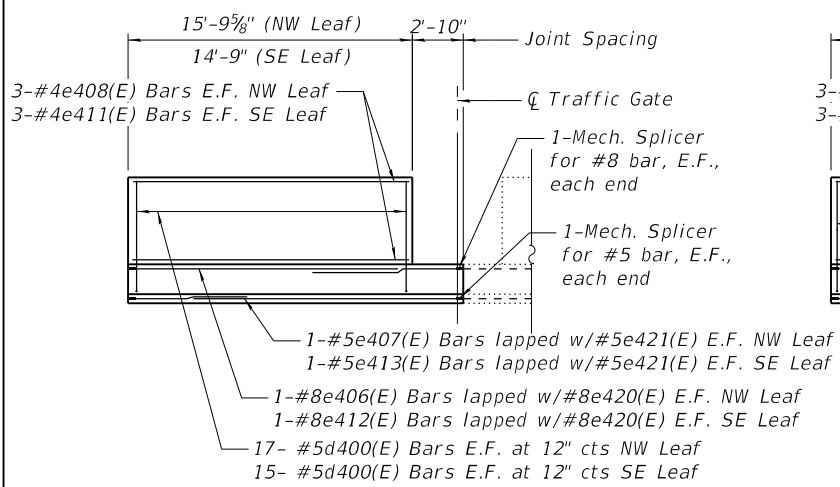
SHEET SB-08 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	87
CONTRACT NO. 62L31				
ILLINOIS		FED. AID PROJECT		



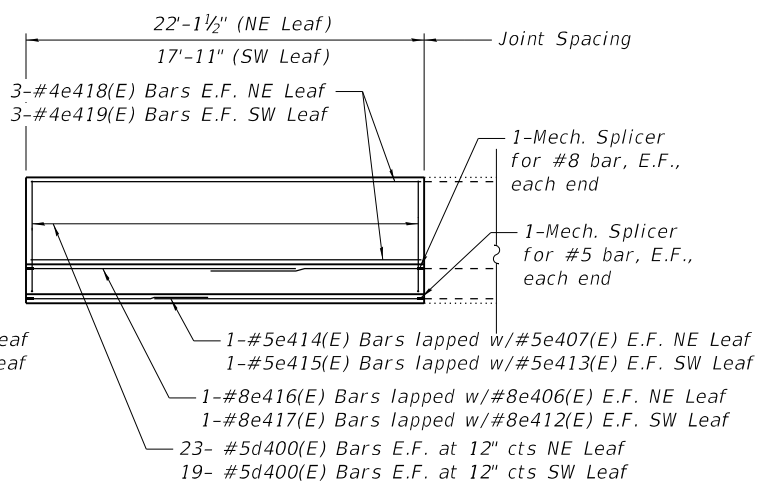
**ELEVATION**

Inside Truss Southwest Leaf and Northeast Leaf



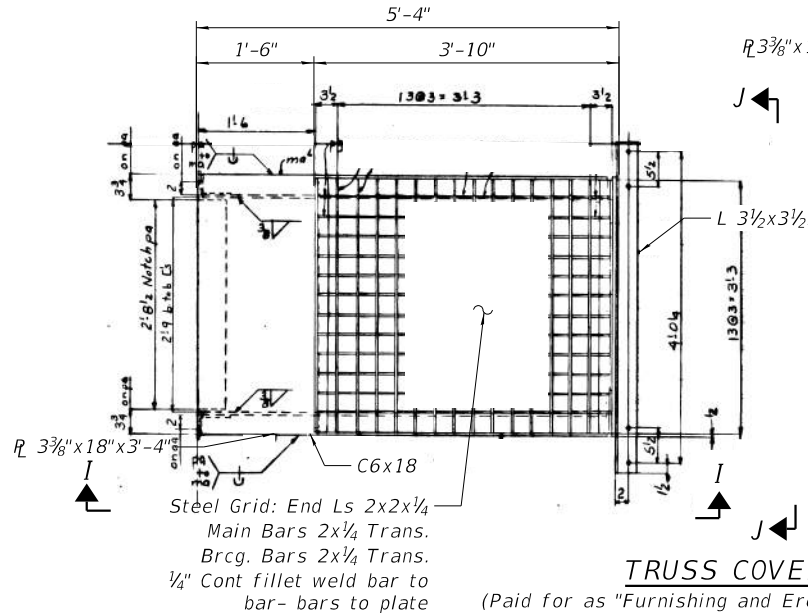
**ELEVATION**

Outside Truss Northwest Leaf and Southeast Leaf



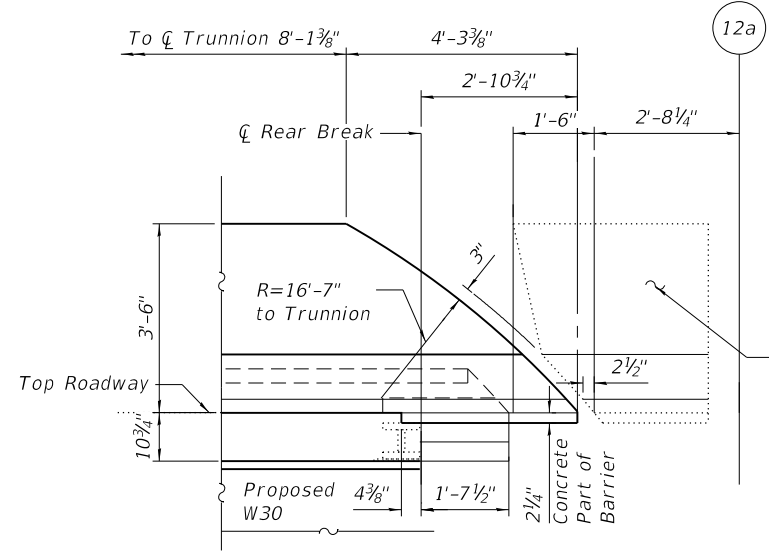
**ELEVATION**

Outside Truss Northeast Leaf and Southwest Leaf

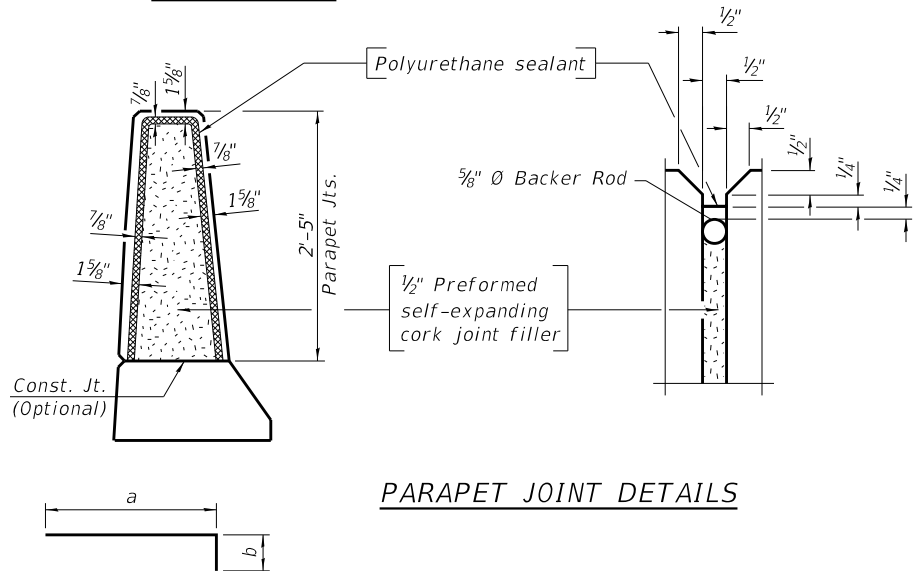


**TRUSS COVER DETAIL**

(Paid for as "Furnishing and Erecting Structural Steel")

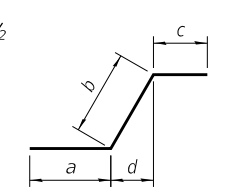


**SECTION A-A**

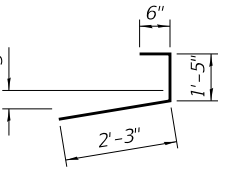


**PARAPET JOINT DETAILS**

**BAR d400(E), d403(E)  
d404(E), d409(E)**

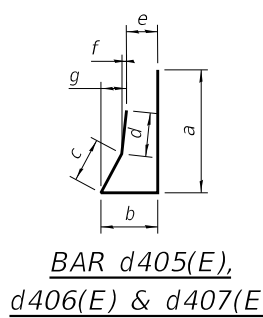


**BAR e409(E)  
BAR e410(E)**

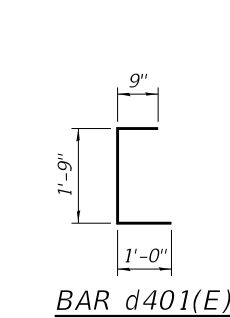


**BAR d408(E)**

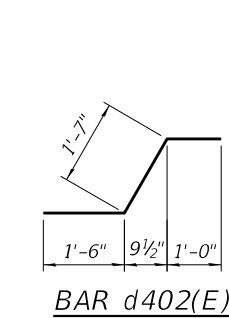
MARK	a	b	c	d	e	f	g
d400(E)	3'-2"	6"	-	-	-	-	-
d403(E)	3'-5"	4"	-	-	-	-	-
d404(E)	2'-7"	6"	-	-	-	-	-
d405(E)	3'-5"	1'-7"	1'-2"	1'-11"	7"	3"	7"
d406(E)	3'-5"	1'-7"	1'-2"	1'-3"	8"	2"	7"
d407(E)	3'-5"	1'-7"	1'-2"	6"	9"	1"	7"
d409(E)	2'-4"	6"	-	-	-	-	-
e409(E)	1'-4"	8 1/2"	4'-10 1/2"	6"	-	-	-
e410(E)	2'-0"	8 1/2"	2'-6 1/2"	6"	-	-	-



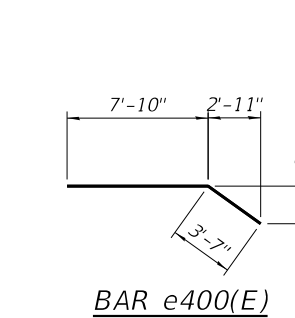
**BAR d405(E),  
d406(E) & d407(E)**



**BAR d401(E)**



**BAR d402(E)**



**BAR e400(E)**

**LAP LENGTH**

#8 - 4'-9"  
#5 - 3'-1"

**LEGEND**

I.F. Inside Face  
O.F. Outside Face  
E.F. Each Face

**BAR SCHEDULE**

Bar	No	SIZE	LENGTH	SHAPE
a400(E)	680	#5	2'-10"	
a401(E)	136	#5	5'-10"	
a402(E)	102	#5	11'-3"	
a403(E)	102	#5	3'-11"	
a404(E)	204	#5	6'-7"	
a405(E)	60	#6	6'-0"	
a406(E)	102	#5	7'-4"	
b400(E)	60	#5	40'-6"	
b401(E)	44	#5	41'-6"	
b402(E)	12	#5	17'-7"	
b403(E)	16	#5	18'-7"	
b404(E)	12	#5	40'-10"	
b405(E)	8	#5	41'-10"	
b406(E)	16	#5	24'-2"	
b407(E)	12	#5	17'-11"	
d400(E)	304	#5	3'-8"	
d401(E)	86	#4	3'-6"	
d402(E)	158	#5	4'-1"	
d403(E)	12	#4	3'-9"	
d404(E)	8	#5	3'-1"	
d405(E)	2	#5	8'-1"	
d406(E)	2	#5	7'-5"	
d407(E)	4	#5	6'-8"	
d408(E)	72	#5	4'-2"	
d409(E)	8	#5	2'-10"	
e400(E)	2	#4	11'-5"	
e401(E)	24	#4	15'-6"	
e402(E)	4	#8	44'-0"	
e403(E)	4	#5	44'-10"	
e404(E)	2	#4	9'-4"	
e405(E)	2	#4	10'-10"	
e406(E)	4	#8	14'-10"	
e407(E)	4	#5	14'-10"	
e408(E)	6	#4	15'-6"	
e409(E)	20	#4	6'-11"	
e410(E)	6	#5	5'-1"	
e411(E)	6	#4	14'-5"	
e412(E)	4	#8	13'-10"	
e413(E)	4	#5	13'-10"	
e414(E)	2	#5	10'-1"	
e415(E)	2	#5	6'-10"	
e416(E)	2	#8	11'-9"	
e417(E)	2	#8	8'-6"	
e418(E)	6	#4	21'-9"	
e419(E)	6	#4	17'-7"	
e420(E)	4	#8	8'-3"	
e421(E)	4	#5	6'-7"	
Concrete Superstructure			Cu. Yd.	86.9
Reinforcement Bars, Epoxy Coated			Pound	18,180

MODEL: Default  
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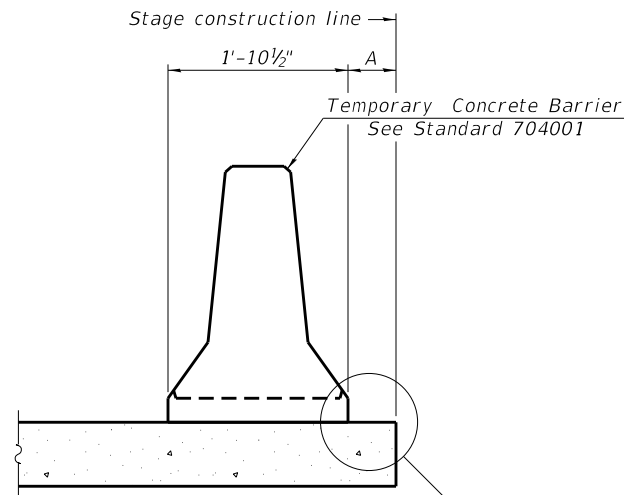


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	DRAWN - IL/AM	REVISED -
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

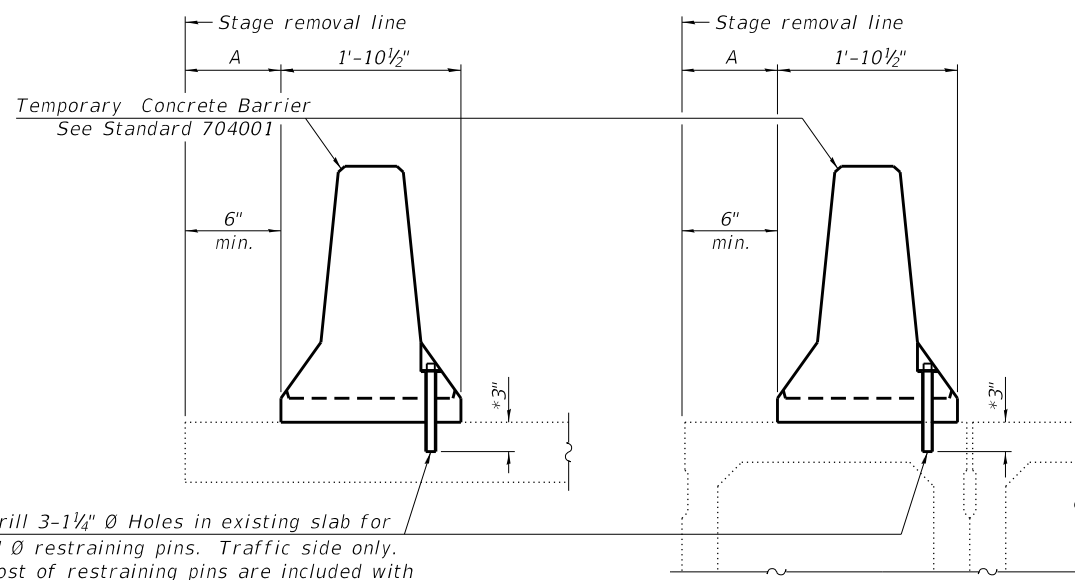
DECK DETAILS 2 OF 2  
SN 016-0202

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	88
CONTRACT NO. 62L31				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



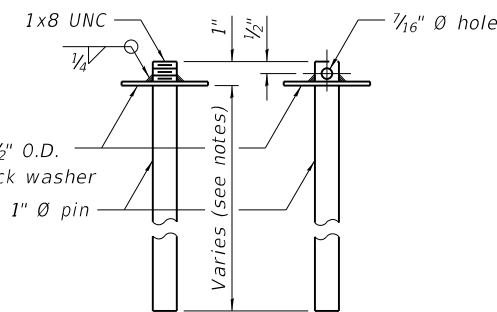
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

EXISTING DECK BEAM

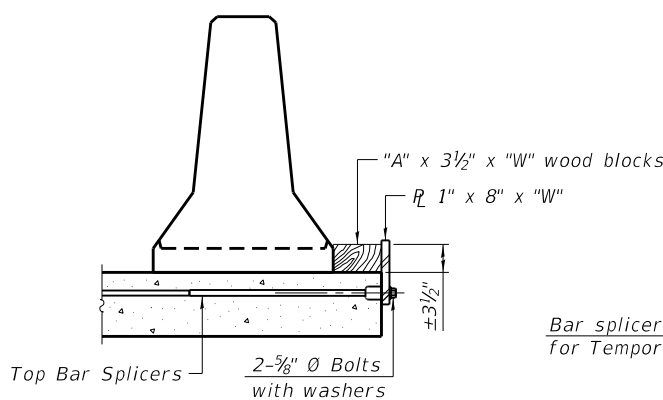
\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

SECTIONS THRU SLAB OR DECK BEAM

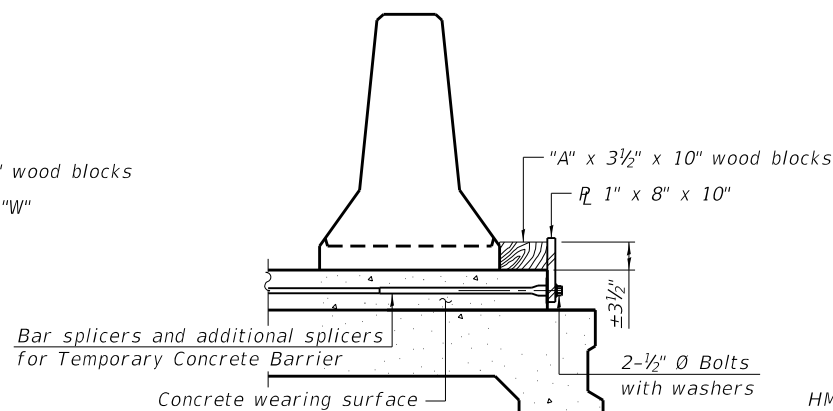


RESTRAINING PIN

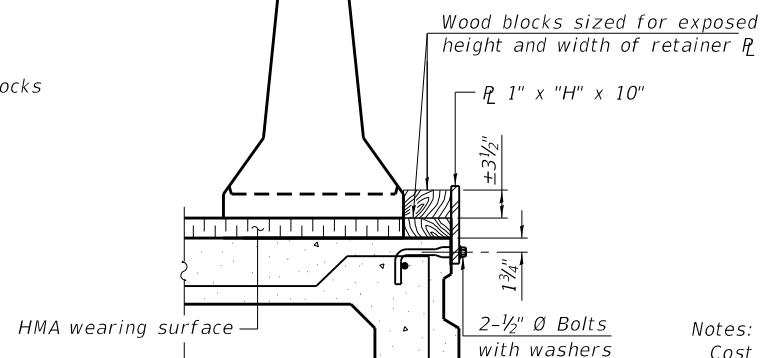
US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer



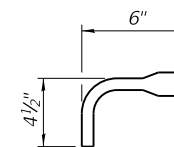
DETAIL I



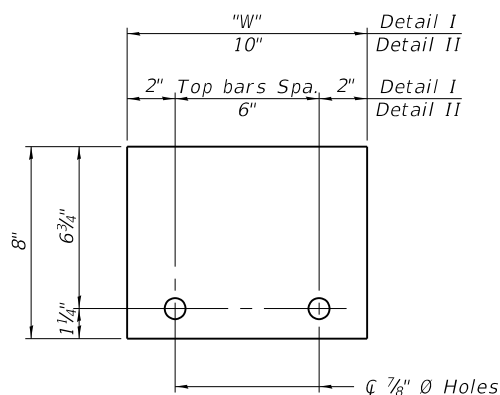
DETAIL II



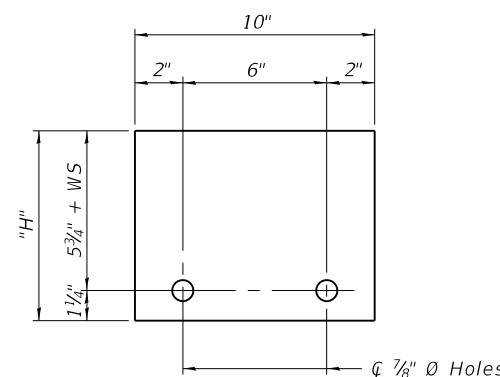
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W" (Detail I and II)



STEEL RETAINER R 1" x "H" x 10" (Detail III)

Notes:  
 Cost of retainer assembly is included with Temporary Concrete Barrier.  
 A retainer assembly shall be located at the approximate  $\bar{C}$  of each temporary concrete barrier.  
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021

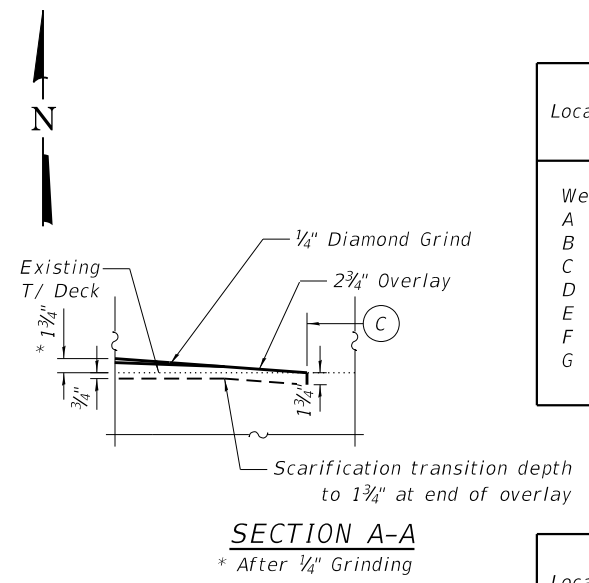
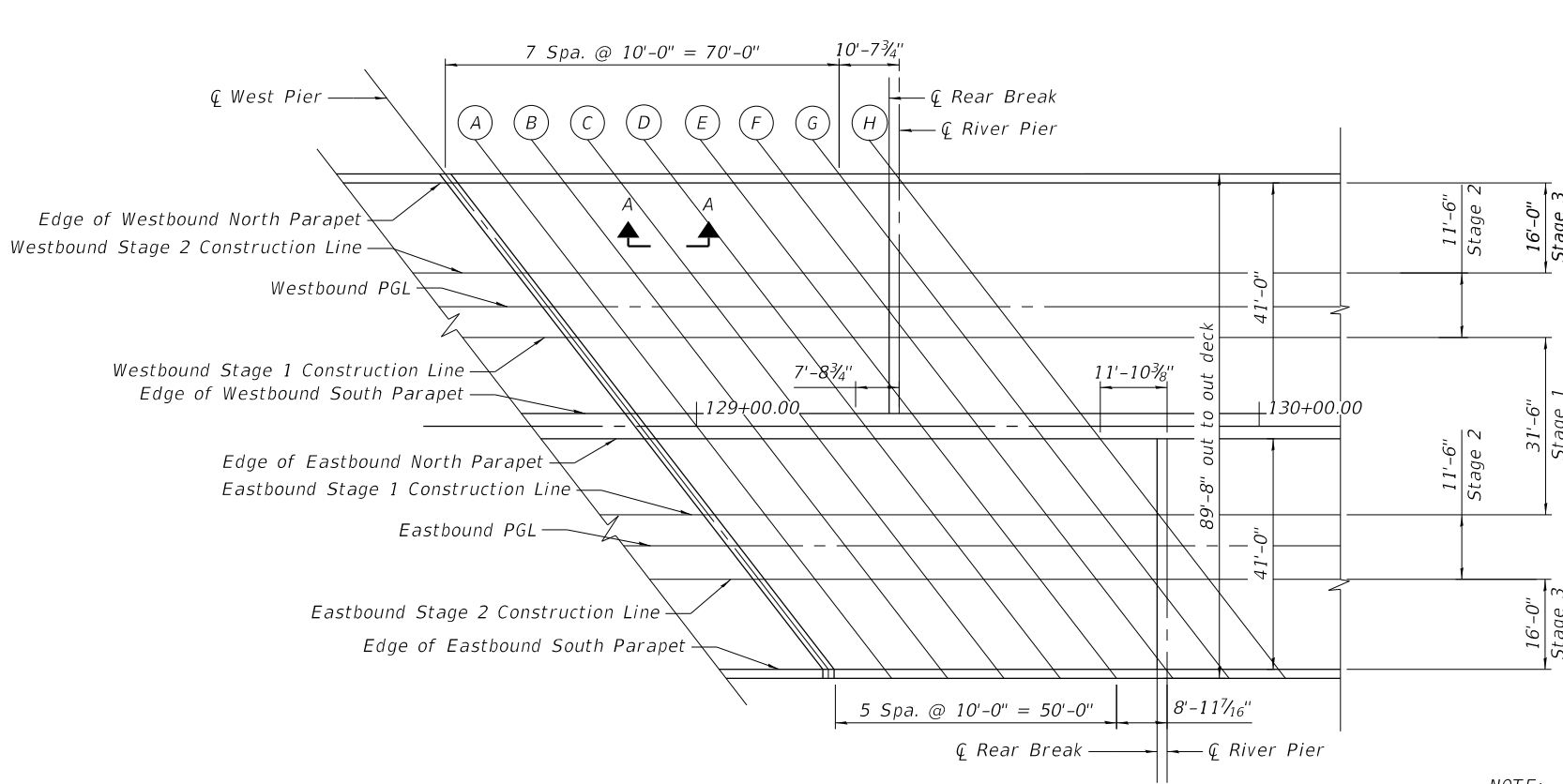
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER  
STRUCTURE NO. 016-0202

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	89
CONTRACT NO. 62L31				

SHEET SB-10 OF SB-48 SHEETS

ILLINOIS FED. AID PROJECT



TOP OF DECK ELEVATIONS PLAN

NOTE:  
Overlay thickness tapers from 0" at line "C" (See Section A-A) to 1 3/4" thickness at West Pier. Transition length: 30'-0".

EDGE OF W.B. NORTH PARAPET

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	128+57.03	-22.00	625.56
A	128+67.03	-22.00	625.58
B	128+77.03	-22.00	625.60
C	128+87.03	-22.00	625.61
D	128+97.03	-22.00	625.68
E	129+07.03	-22.00	625.74
F	129+17.03	-22.00	625.81
G	129+27.03	-22.00	625.87

W.B. STAGE 2 CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	128+69.22	-6.00	625.81
A	128+79.22	-6.00	625.83
B	128+89.22	-6.00	625.84
C	128+99.22	-6.00	625.86
D	129+09.22	-6.00	625.92
E	129+19.22	-6.00	625.99
F	129+29.22	-6.00	626.06

WESTBOUND P.G.L.

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	128+73.80	0.00	625.87
A	128+83.80	0.00	625.89
B	128+93.80	0.00	625.90
C	129+03.80	0.00	625.92
D	129+13.80	0.00	625.99
E	129+23.80	0.00	626.05

W.B. STAGE 1 CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	128+77.99	5.50	625.87
A	128+87.99	5.50	625.89
B	128+97.99	5.50	625.90
C	129+07.99	5.50	625.92
D	129+17.99	5.50	625.98
E	129+27.99	5.50	626.05

EDGE OF W.B. SOUTH PARAPET

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	128+88.79	19.67	625.79
A	128+98.79	19.67	625.81
B	129+08.79	19.67	625.83
C	129+18.79	19.67	625.84
D	129+28.79	19.67	625.91

EDGE OF E.B. NORTH PARAPET

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	128+91.21	-22.00	625.76
A	129+01.21	-22.00	625.78
B	129+11.21	-22.00	625.81
C	129+21.21	-22.00	625.83
D	129+31.21	-22.00	625.91
E	129+41.21	-22.00	625.99
F	129+51.21	-22.00	626.06
G	129+61.21	-22.00	626.14
H	129+71.21	-22.00	626.22

E.B. STAGE 1 CONSTRUCTION LINE

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	129+02.00	-6.00	626.00
A	129+12.00	-6.00	626.03
B	129+22.00	-6.00	626.05
C	129+32.00	-6.00	626.08
D	129+42.00	-6.00	626.16
E	129+52.00	-6.00	626.24
F	129+62.00	-6.00	626.31
G	129+72.00	-6.00	626.39

EASTBOUND P.G.L.

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	129+06.20	0.00	626.07
A	129+16.20	0.00	626.09
B	129+26.20	0.00	626.12
C	129+36.20	0.00	626.14
D	129+46.20	0.00	626.22
E	129+56.20	0.00	626.30
F	129+66.20	0.00	626.38
G	129+76.20	0.00	626.46

E.B. STAGE 2 CONSTRUCTION LINE

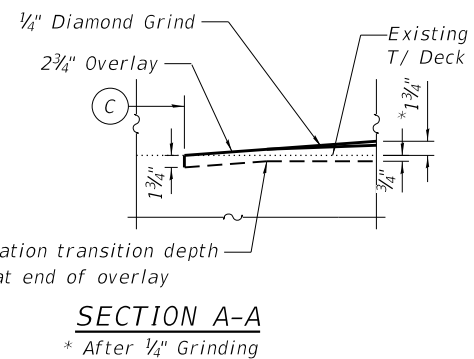
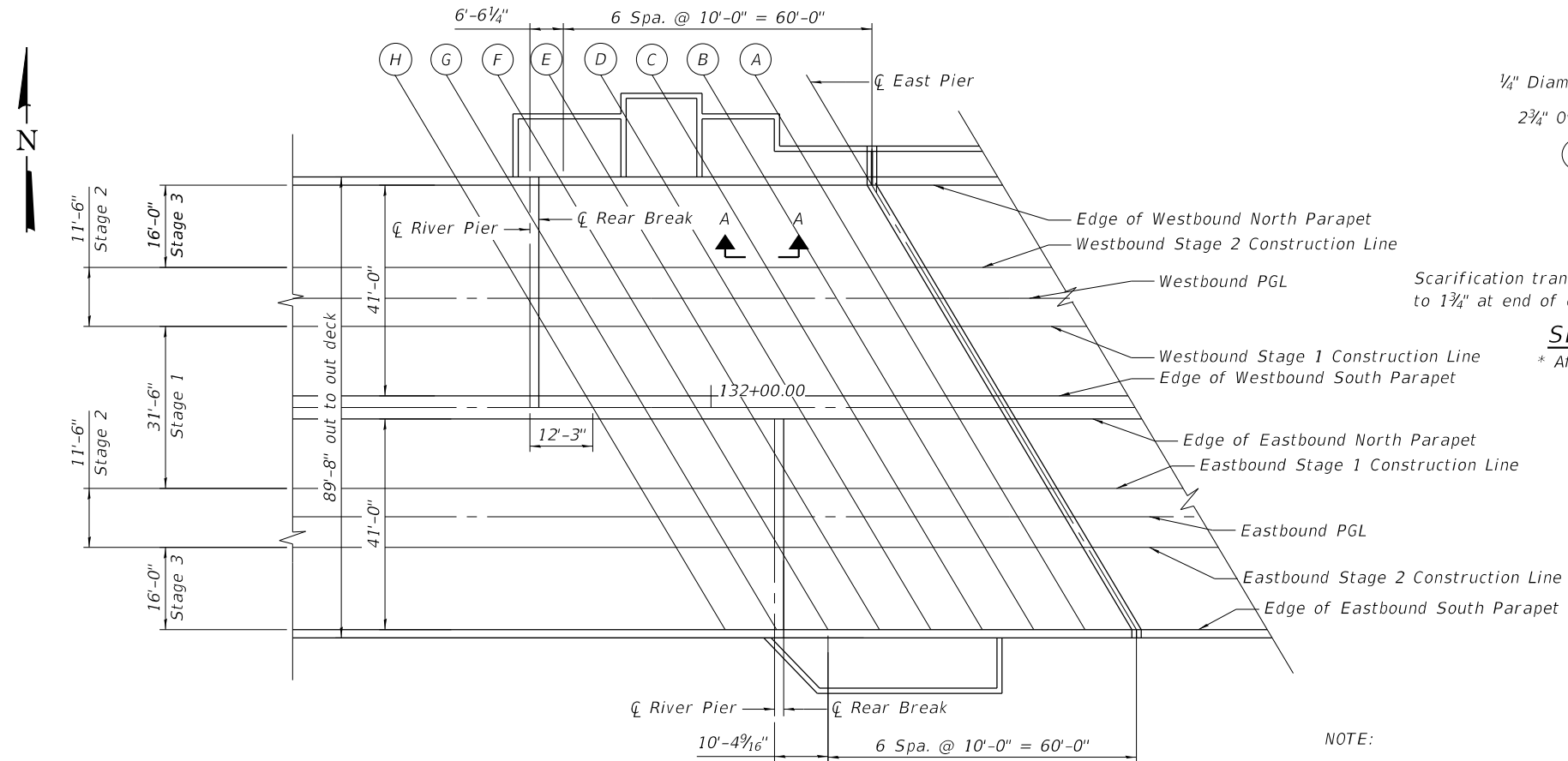
Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	129+10.77	5.50	626.07
A	129+20.77	5.50	626.10
B	129+30.77	5.50	626.12
C	129+40.77	5.50	626.15
D	129+50.77	5.50	626.23
E	129+60.77	5.50	626.31
F	129+70.77	5.50	626.39

EDGE OF E.B. SOUTH PARAPET

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
West Pier	129+22.97	19.67	626.02
A	129+32.97	19.67	626.04
B	129+42.97	19.67	626.07
C	129+52.97	19.67	626.10
D	129+62.97	19.67	626.18
E	129+72.97	19.67	626.26

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**EDGE OF W.B. NORTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+31.26	-22.00	625.96
A	132+41.26	-22.00	625.83
B	132+51.26	-22.00	625.68
C	132+61.26	-22.00	625.55
D	132+71.26	-22.00	625.44
E	132+81.26	-22.00	625.35
F	132+91.26	-22.00	625.24
G	133+01.26	-22.00	625.10

**W.B. STAGE 2 CONSTRUCTION LINE**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+40.83	-6.00	626.05
A	132+50.83	-6.00	625.91
B	132+60.83	-6.00	625.76
C	132+70.83	-6.00	625.63
D	132+80.83	-6.00	625.52
E	132+90.83	-6.00	625.42
F	133+00.83	-6.00	625.29

**TOP OF DECK ELEVATIONS PLAN**

NOTE:  
Overlay thickness tapers from 0" at line "C" (See Section A-A) to 1 3/4" thickness at East Pier. Transition length: 30'-0".

**WESTBOUND P.G.L.**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+44.40	0.00	626.05
A	132+54.40	0.00	625.90
B	132+64.40	0.00	625.76
C	132+74.40	0.00	625.63
D	132+84.40	0.00	625.52
E	132+94.40	0.00	625.41

**W.B. STAGE 1 CONSTRUCTION LINE**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+47.67	5.50	625.99
A	132+57.67	5.50	625.85
B	132+67.67	5.50	625.70
C	132+77.67	5.50	625.57
D	132+87.67	5.50	625.45
E	132+97.67	5.50	625.34

**EDGE OF W.B. SOUTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+56.10	19.67	625.77
A	132+66.10	19.67	625.62
B	132+76.10	19.67	625.48
C	132+86.10	19.67	625.35
D	132+96.10	19.67	625.20

**EDGE OF E.B. NORTH PARAPET**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+57.98	-22.00	625.72
A	132+67.98	-22.00	625.62
B	132+77.98	-22.00	625.49
C	132+87.98	-22.00	625.33
D	132+97.98	-22.00	625.19
E	133+07.98	-22.00	625.03
F	133+17.98	-22.00	624.86
G	133+27.98	-22.00	624.69
H	133+37.98	-22.00	624.48

**E.B. STAGE 1 CONSTRUCTION LINE**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+66.41	-6.00	625.84
A	132+76.41	-6.00	625.73
B	132+86.41	-6.00	625.56
C	132+96.41	-6.00	625.40
D	133+06.41	-6.00	625.25
E	133+16.41	-6.00	625.10
F	133+26.41	-6.00	624.94
G	133+36.41	-6.00	624.76

**EASTBOUND P.G.L.**

Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+69.68	0.00	625.86
A	132+79.68	0.00	625.72
B	132+89.68	0.00	625.55
C	132+99.68	0.00	625.40
D	133+09.68	0.00	625.24
E	133+19.68	0.00	625.10
F	133+29.68	0.00	624.95
G	133+39.68	0.00	624.75

**E.B. STAGE 2 CONSTRUCTION LINE**

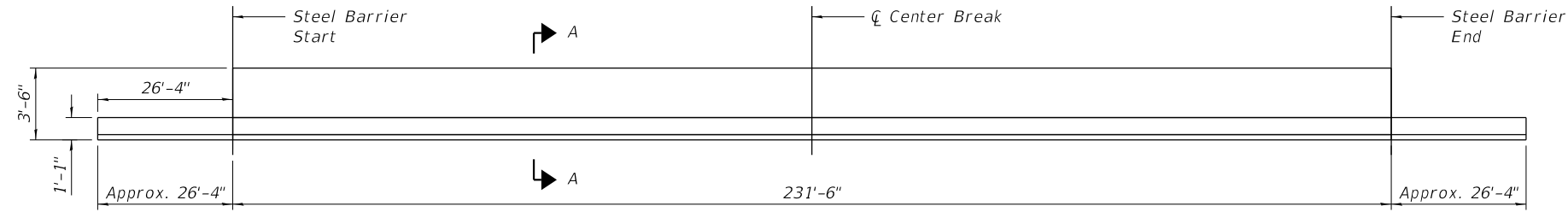
Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+73.25	5.50	625.82
A	132+83.25	5.50	625.65
B	132+93.25	5.50	625.48
C	133+03.25	5.50	625.32
D	133+13.25	5.50	625.15
E	133+23.25	5.50	625.00
F	133+33.25	5.50	624.82

**EDGE OF E.B. SOUTH PARAPET**

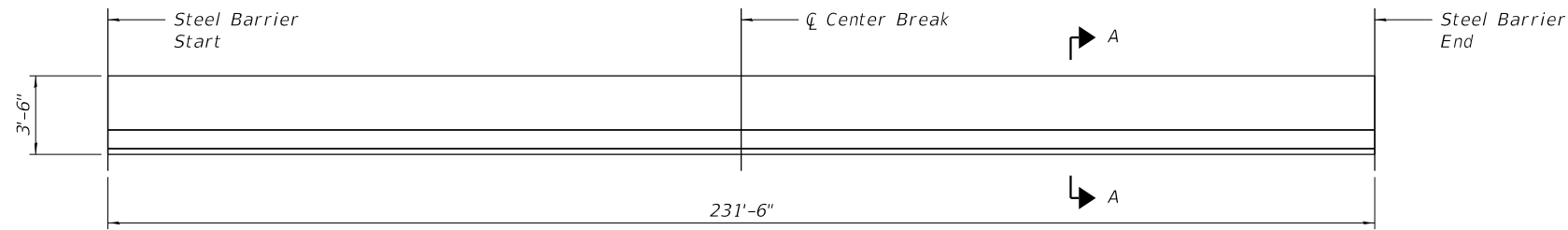
Location	Station	Offset	Theoretical Grade Elevations With Proposed Overlay
East Pier	132+82.77	19.67	625.57
A	132+92.77	19.67	625.39
B	133+02.77	19.67	625.20
C	133+12.77	19.67	625.00
D	133+22.77	19.67	624.81
E	133+32.77	19.67	624.63

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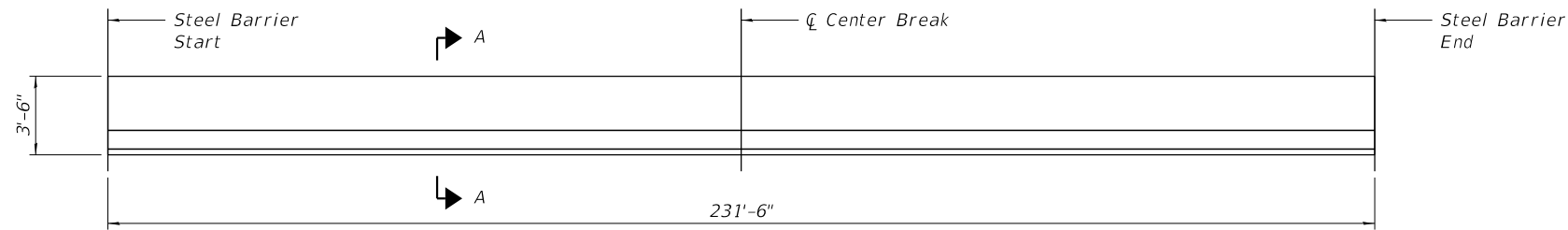
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						SHEET SB-12 OF SB-48 SHEETS			ILLINOIS FED. AID PROJECT	



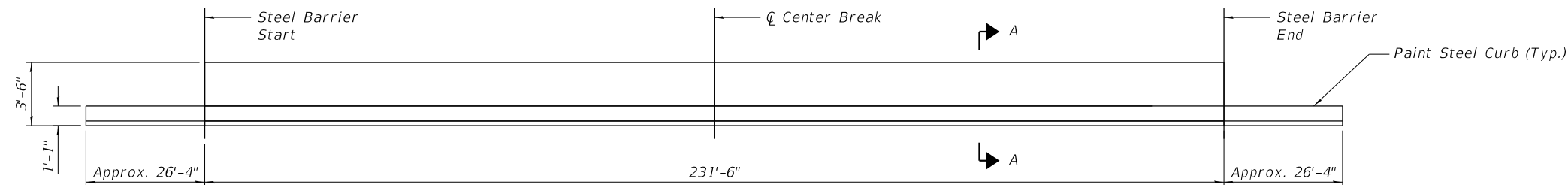
**INSIDE ELEVATION OF PARAPET**  
(North Parapet, Westbound)



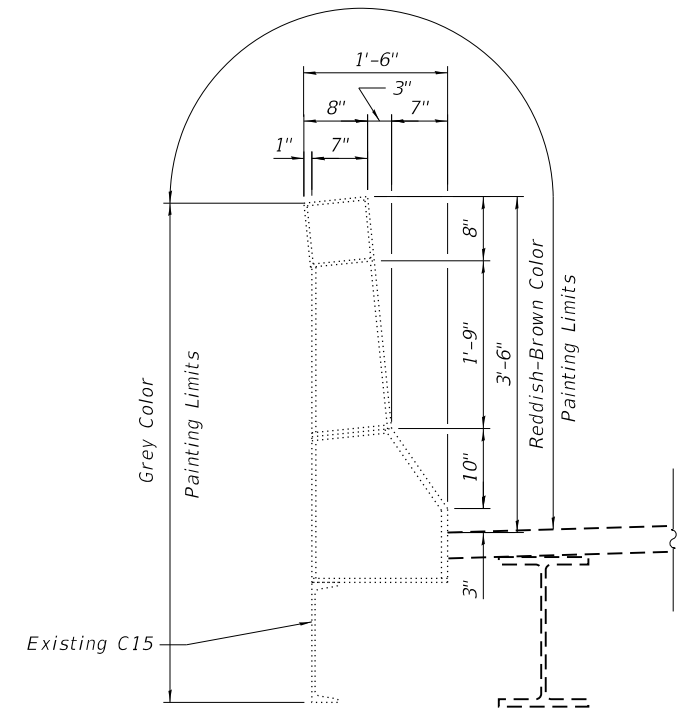
**INSIDE ELEVATION OF PARAPET**  
(South Parapet, Westbound)



**INSIDE ELEVATION OF PARAPET**  
(North Parapet, Eastbound)



**INSIDE ELEVATION OF PARAPET**  
(South Parapet, Eastbound)



**SECTION A-A**

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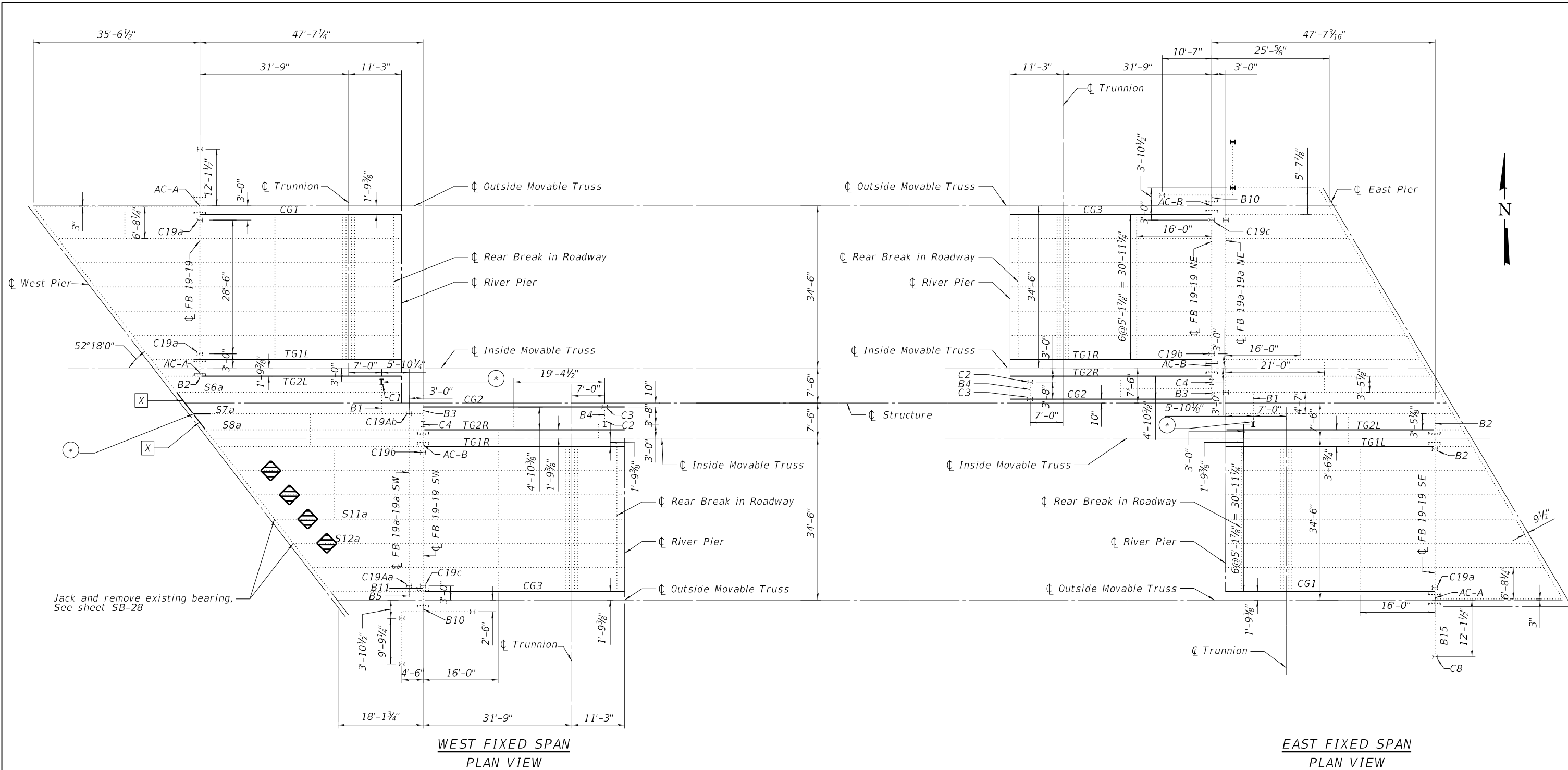
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**BASCULE PARAPET PAINTING DETAILS**  
**SN 016-0202**

SHEET SB-13 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	92
CONTRACT NO. 62L31				
		ILLINOIS FED. AID PROJECT		



**WEST FIXED SPAN  
PLAN VIEW**

**EAST FIXED SPAN  
PLAN VIEW**

**NOTES:**

- Column C1 web and flange are to be repaired using steel plates, see sheet SB-22 for details.
- Stringer S7a beam end is to be repaired using steel plates, see sheet SB-22 for details.
- For CG and TG girder repairs see sheet SB-21 for details.
- Members designated to be removed and replaced shall match existing R, Ls, rivet/bolt holes, and locations. The Contractor shall use existing R's, Ls, and other attachments as templates.
- See sheet SB-31 for "Temporary Shoring and Cribbing" details for fixed deck stringers at the East Pier.
- CG and TG girders shall be hot-dip galvanized. Cost included with Furnishing and Erecting Structural Steel. See special provisions for Hot Dip Galvanizing of Structural Steel.

**LEGEND**

- (\*) Proposed Structural Steel Repair
- ..... Existing Steel
- Proposed Steel
- [X] Proposed Diaphragm Replacement
- [Hatched Box] Suggested Area for Temporary Shoring and Cribbing.

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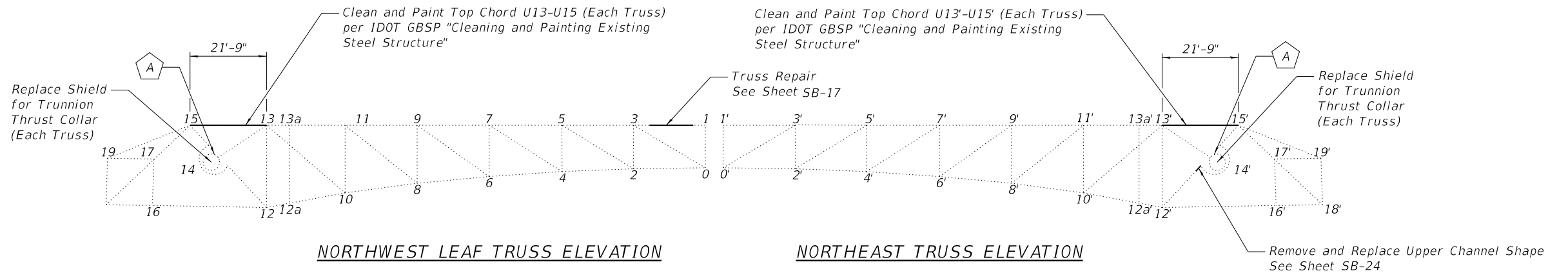
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**FIXED SPANS FRAMING PLAN  
SN 016-0202**

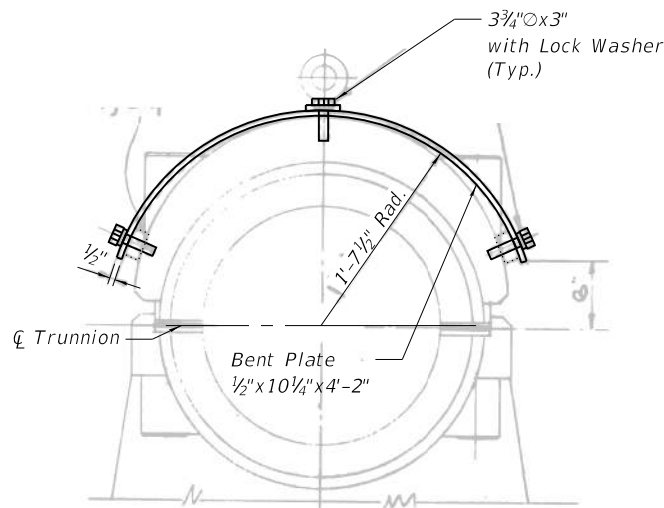
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS		FED. AID PROJECT		

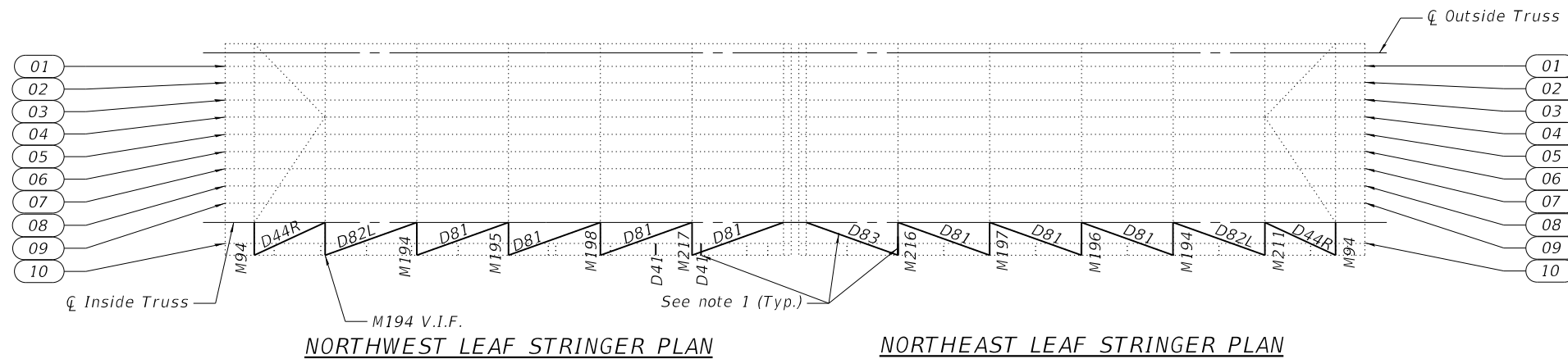


**NORTHWEST LEAF TRUSS ELEVATION**

**NORTHEAST TRUSS ELEVATION**

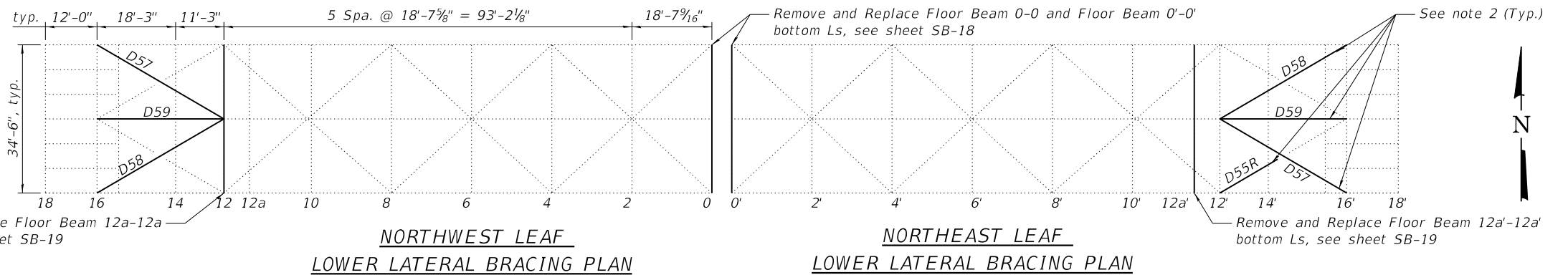


**SHIELD FOR TRUNNION THRUST COLLAR**



**NORTHWEST LEAF STRINGER PLAN**

**NORTHEAST LEAF STRINGER PLAN**



**NORTHWEST LEAF LOWER LATERAL BRACING PLAN**

**NORTHEAST LEAF LOWER LATERAL BRACING PLAN**

**NOTES:**

1. Sidewalk Support Lateral Bracing L's to be removed and replaced, see sheet SB-20 for details.
2. Bottom Chord Lateral Bracing L's to be removed and replaced, see sheet SB-26 for details
3. Weep hole to be drilled into steel plate at both trusses, see sheet SB-25 for details.
4. Sidewalk Support Repair, Top double L's to be removed and replaced, see sheet SB-20 for details.
5. The Diameter of the proposed fasteners must match the size of existing Rivets or Bolts, unless otherwise noted, as required by the existing detail.
6. It is the Contractor's responsibility to take measurements of the existing structure wherever new steel is to be installed or connected into the existing material prior to ordering or fabricating any new steel. The Contractor is responsible for the proper fitting and assembly of all part of his work. The Contractor's shop drawings must indicate which dimensions were obtained by actual field measurements.
7. Contractor shall field verify the required bolt length and length of thread necessary to install all bolts in accordance with the Standard Specifications and Section 8.2.1 of the 2020 RCSC "Specification for Structural Joints using High-Strength Bolts".
8. Bolt and Rivet spacing and sizes must be verified in the field by the Contractor prior to ordering material for fabrication.
9. Members designated to be removed and replaced shall match existing R, L's, rivet/bolt holes, and locations. The Contractor shall use existing R's, L's, and other attachments as templates.
10. Painting truss chord U13-U15 and truss chord U13'-U15' to occur between steel removal and replacement of the curb girders for access.

**LEGEND**

- Weep hole to be drilled into steel plate at both trusses, see sheet SB-23 for details.
- Stringer number
- Existing Steel Member
- Proposed Steel Member

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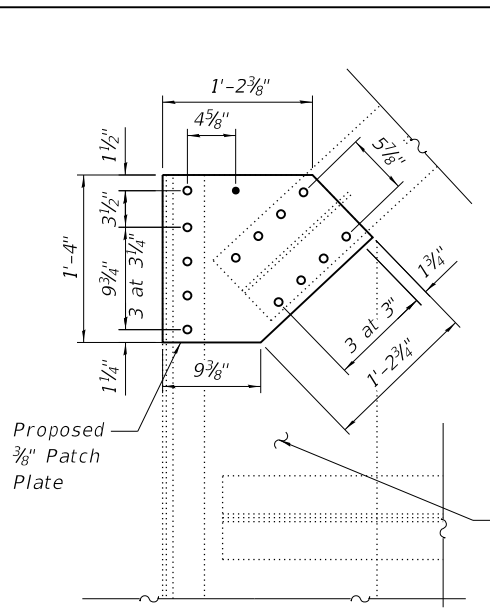
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**NORTH BASCULE FRAMING PLAN  
SN 016-0202**

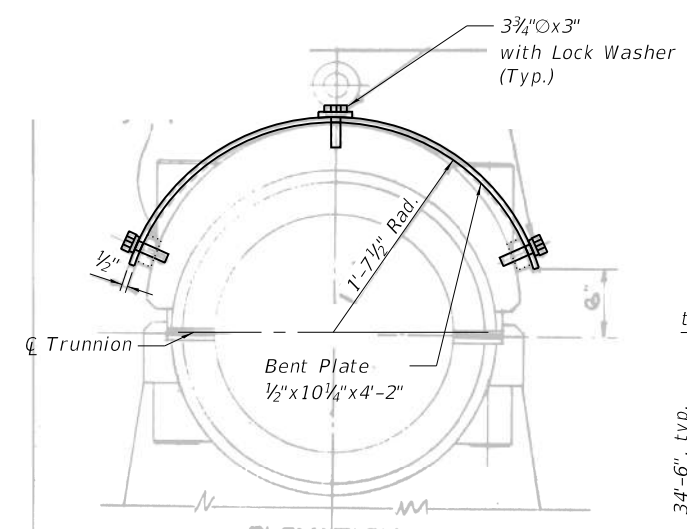
SHEETSB-15 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT				

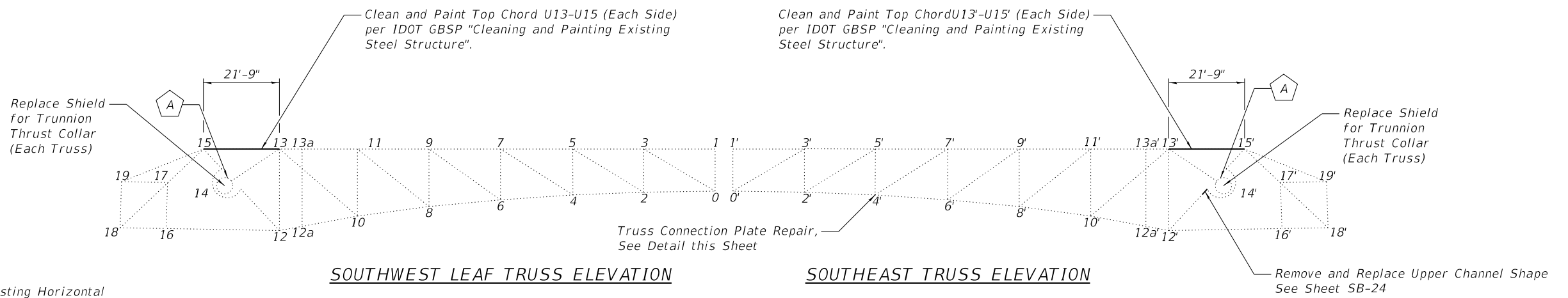
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**TRUSS CONNECTION  
 PLATE REPAIR**

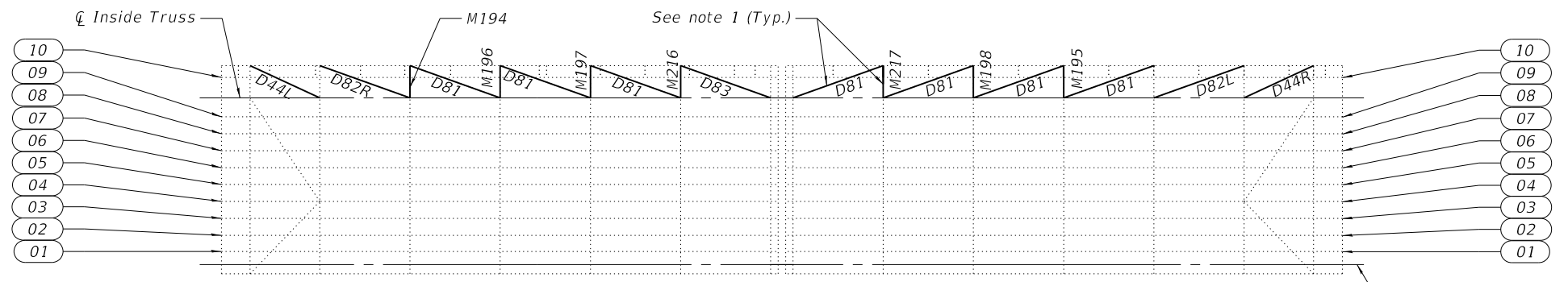


**SHIELD FOR TRUNNION  
 THRUST COLLAR**



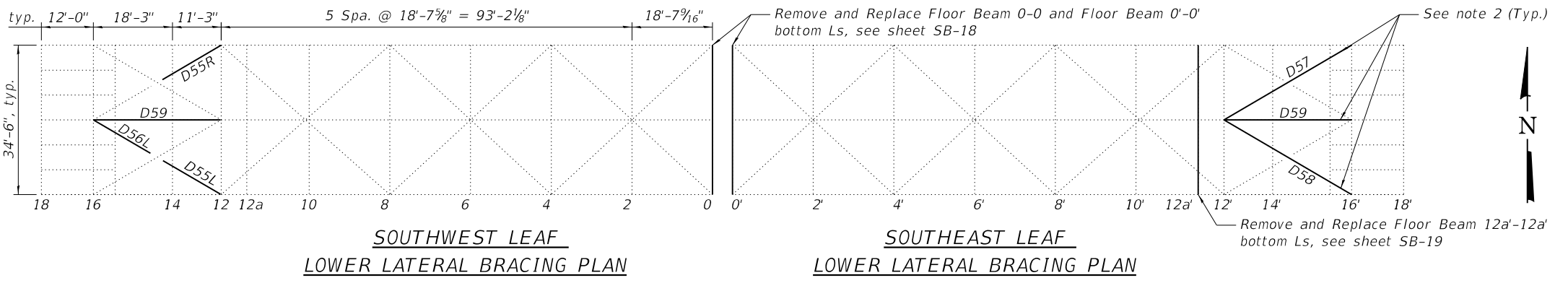
**SOUTHWEST LEAF TRUSS ELEVATION**

**SOUTHEAST TRUSS ELEVATION**



**SOUTHWEST LEAF STRINGER PLAN**

**SOUTHEAST LEAF STRINGER PLAN**



**SOUTHWEST LEAF  
 LOWER LATERAL BRACING PLAN**

**SOUTHEAST LEAF  
 LOWER LATERAL BRACING PLAN**

**NOTES:**

1. Sidewalk Support Lateral Bracing L's to be removed and replaced, see sheet SB-20 for details.
2. Bottom Chord Lateral Bracing L's to be removed and replaced, see sheet SB-26 for details
3. Weep hole to be drilled into steel plate at both trusses, see sheet SB-23 for details.
4. Sidewalk Support Repair, Top double L's to be removed and replaced, see sheet SB-20 for details.
5. The Diameter of the proposed fasteners must match the size of existing Rivets or Bolts, unless otherwise noted, as required by the existing detail.
6. It is the Contractor's responsibility to take measurements of the existing structure wherever new steel is to be installed or connected into the existing material prior to ordering or fabricating any new steel. The Contractor is responsible for the proper fitting and assembly of all part of his work. The Contractor's shop drawings must indicate which dimensions were obtained by actual field measurements.
7. Contractor shall field verify the required bolt length and length of thread necessary to install all bolts in accordance with the Standard Specifications and Section 8.2.1 of the 2020 RCSC "Specification for Structural Joints using High-Strength Bolts".
8. Bolt and Rivet spacing and sizes must be verified in the field by the Contractor prior to ordering material for fabrication.
9. Members designated to be removed and replaced shall match existing R, L's, rivet/bolt holes, and locations. The Contractor shall use existing R's, L's, and other attachments as templates.
10. Proposed connection plate repair steel and associated bolts/fasteners shall be paid for as "Structural Steel Repair".
11. Painting truss chord U13-U15 and truss chord U13'-U15' to occur between steel removal and replacement of the curb girders for access.

**LEGEND**

- ◻ A Weep hole to be drilled into steel plate at both trusses, see sheet SB-23 for details.
- A Stringer number
- ..... Existing Steel Member
- Proposed Steel Member
- New bolt in existing rivet hole. Use existing steel as template for new steel.
- New bolt in field drilled hole through existing steel.



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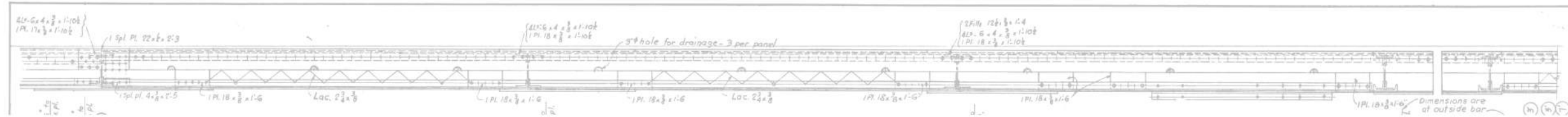
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SOUTH BASCULE FRAMING PLAN  
 SN 016-0202**

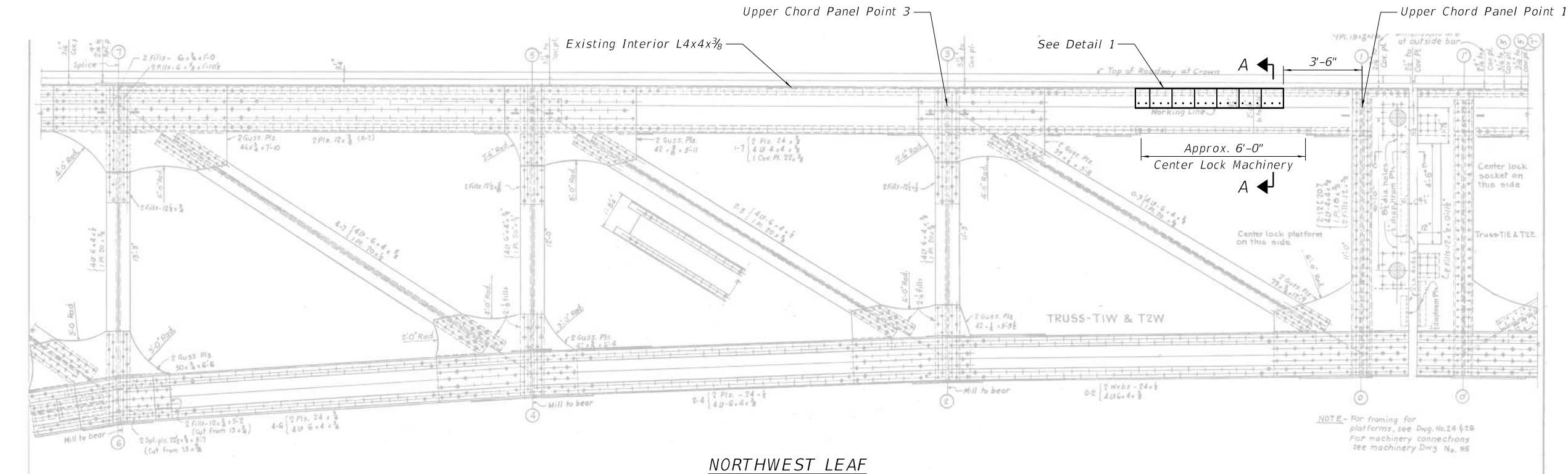
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CONTRACT NO. 62L31				

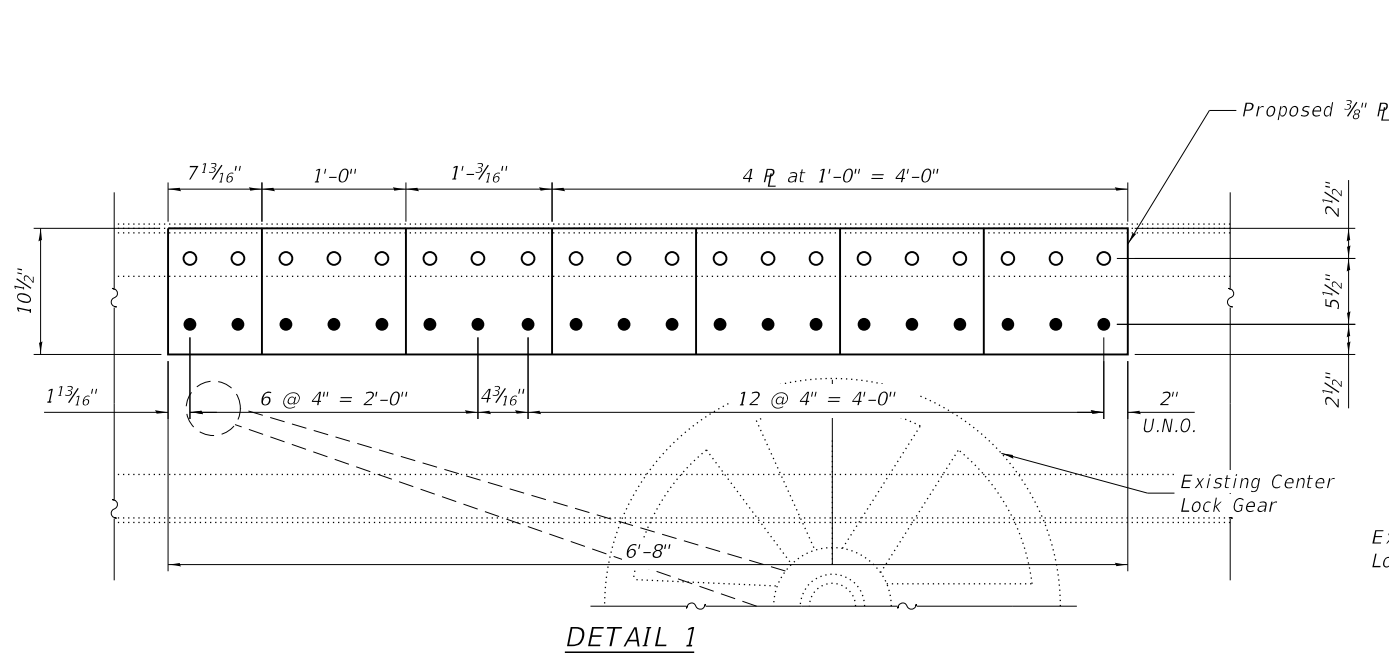
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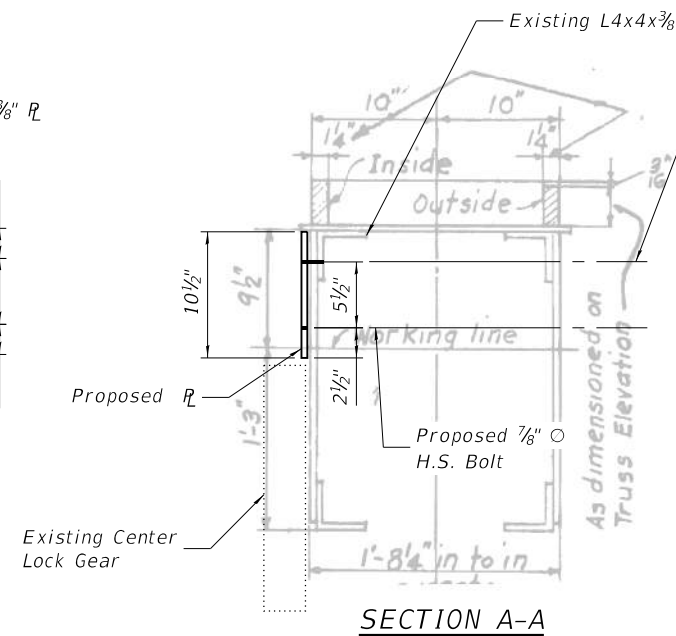
**NORTHWEST LEAF  
NORTH TRUSS UPPER CHORD TOP AND BOTTOM PLAN**



**NORTHWEST LEAF  
NORTH TRUSS INSIDE ELEVATION**



**DETAIL 1**



**SECTION A-A**

Existing rivets to be replaced by 1" O H.S. Bolts

**LEGEND**

- New bolt in existing rivet hole. Use existing steel as template for new steel.
- New bolt in field drilled hole through existing steel.

**NOTES:**

1. Repairs shown are based on field inspection. Conditions in field may have changed. Contractor to verify all components for repair as directed by the Engineer.
2. Repair sequence shall install plates one at a time by removing 3 (max.) rivets progressively and replacing with erection pin/rods. Erection pins shall be replaced with H.S. bolts progressively one at a time. Then field drill the existing web using the proposed repair plate as a template and place 3 (max.) H.S. bolts.
3. All proposed repair plates and associated bolts/fasteners shall be paid for Structural Steel Repair.

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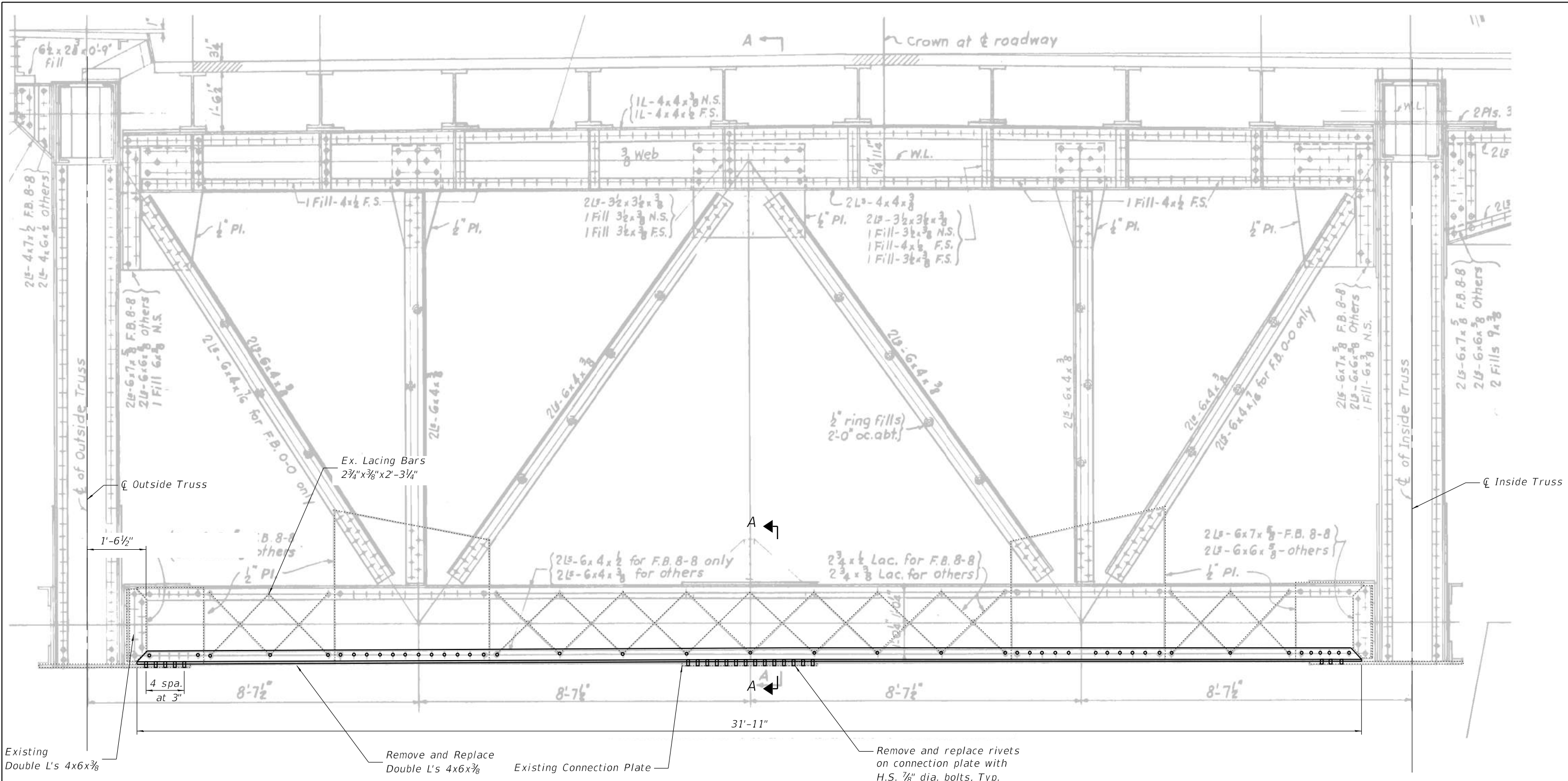
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

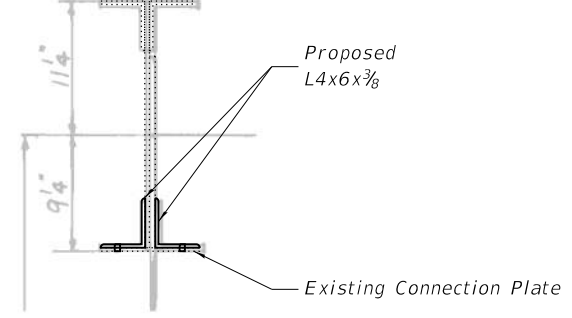
**TRUSS REPAIR NORTHWEST LEAF NORTH TRUSS U1-U3  
SN 016-0202**

SHEET SB-17 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	96
CONTRACT NO. 62L31				
ILLINOIS		FED. AID PROJECT		



**FLOOR BEAM 0-0 ELEVATION**  
(Floor Beam 0'-0' similar)



**LEGEND**

- New bolt in existing rivet hole. Use existing steel as template for new steel.

**NOTES:**

- Steel members to be removed and replaced shall match existing.
- Existing floor beam repair angles removal shall be paid for as "Structural Steel Removal".
- All proposed floor beam repair angles and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".

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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

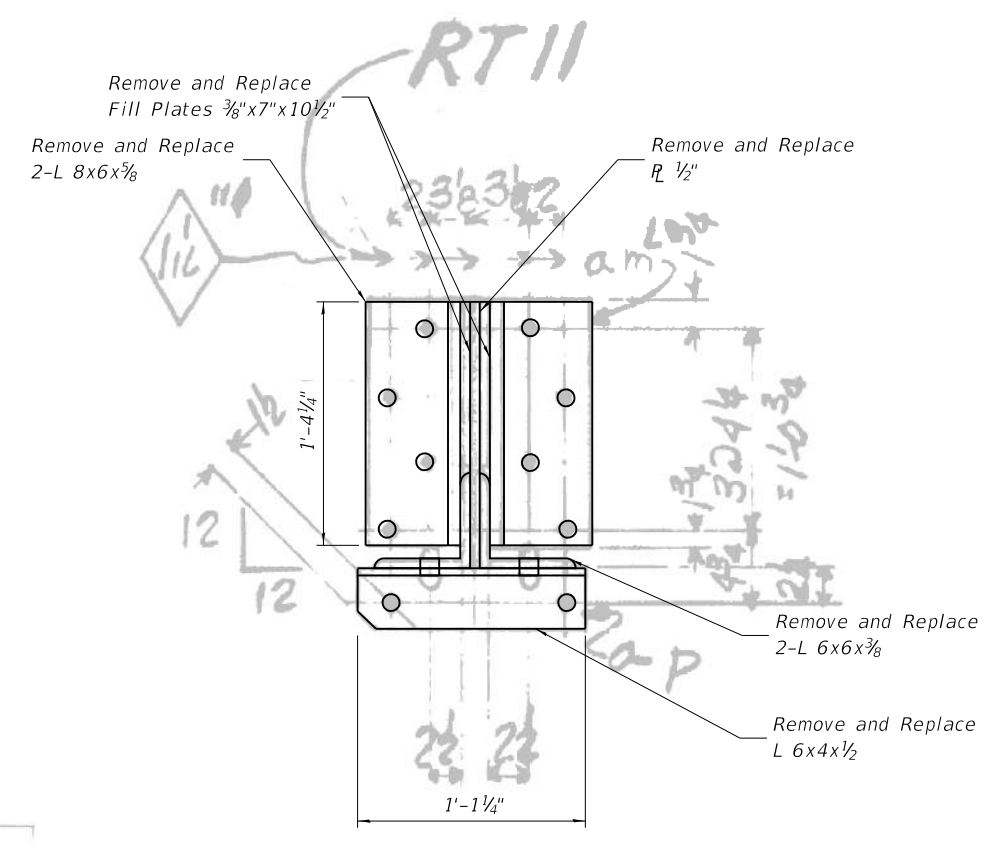
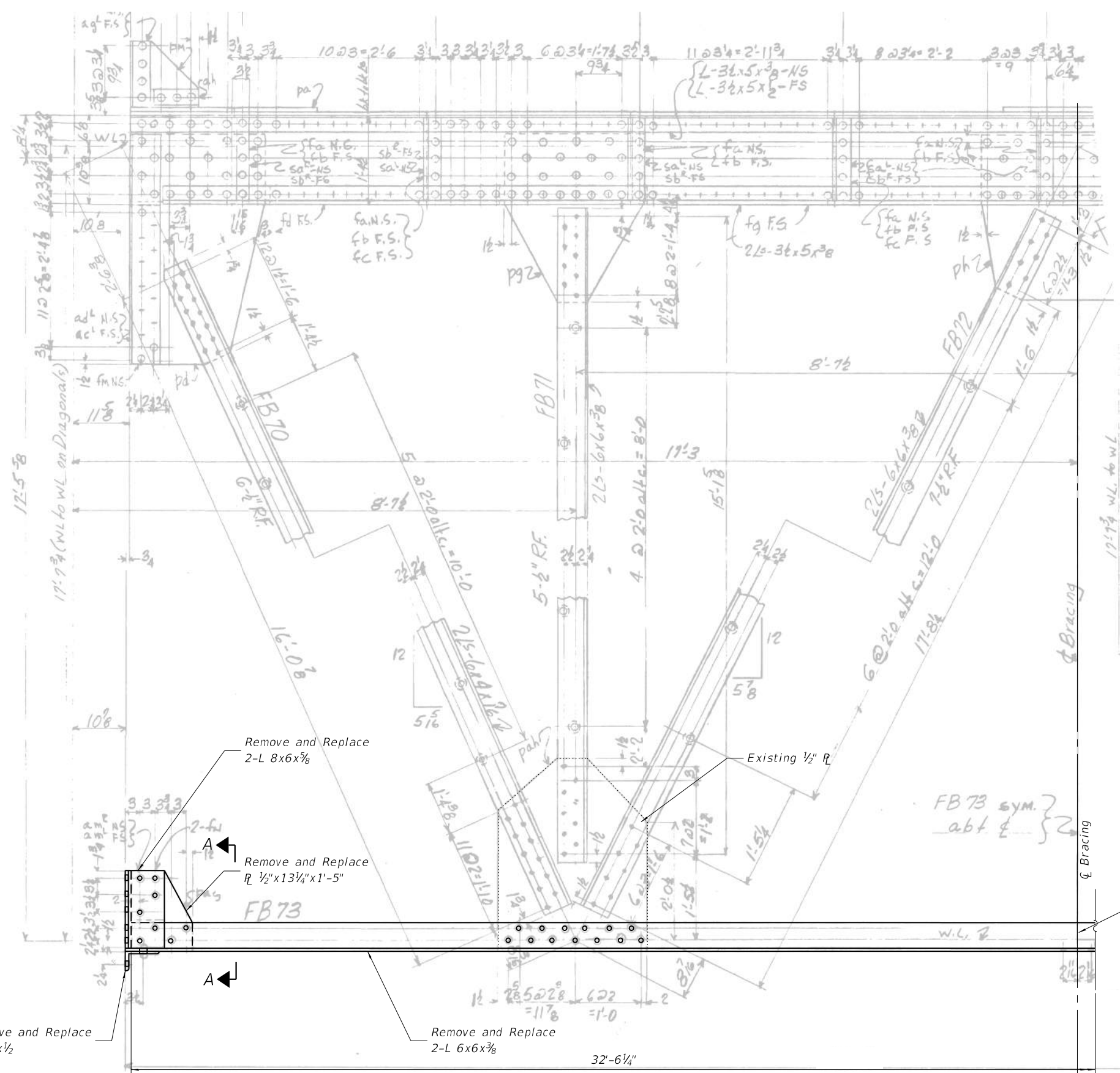
**FLOOR BEAM 0-0 REPAIR**  
**SN 016-0202**

SHEET SB-18 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT

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SECTION A-A

**LEGEND**

- New bolt in existing rivet hole. Use existing steel as template for new steel.

**NOTES:**

1. Steel members to be removed and replaced shall match existing.
2. Existing floor beam repair angles removal shall be paid for as "Structural Steel Removal".
3. All proposed floor beam repair angles and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".

**FLOOR BEAM 12a-12a ELEVATION**  
 (Floor Beam 12a'-12a' similar)

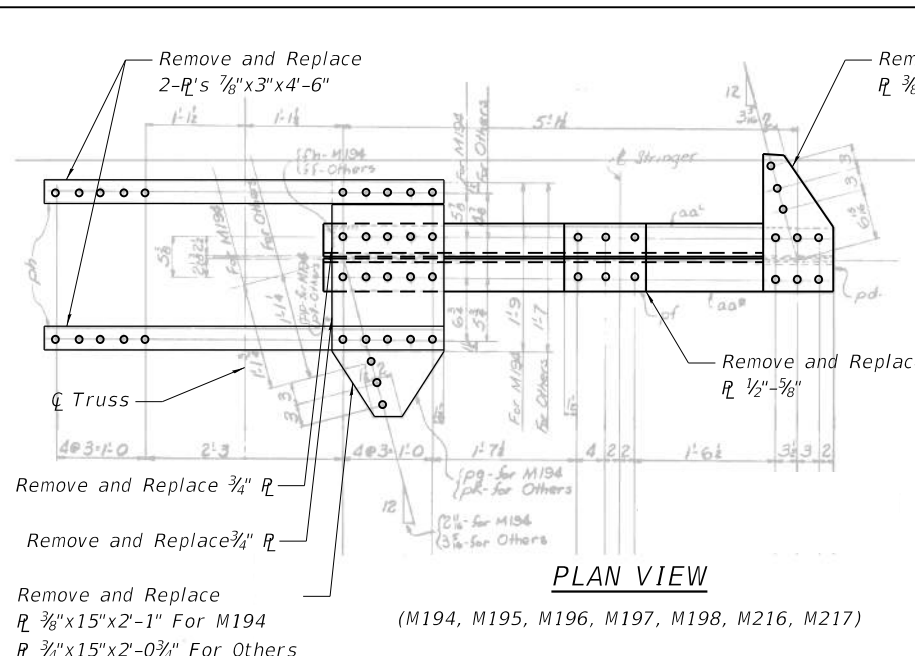
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**FLOOR BEAM 12a-12a REPAIR  
 SN 016-0202**

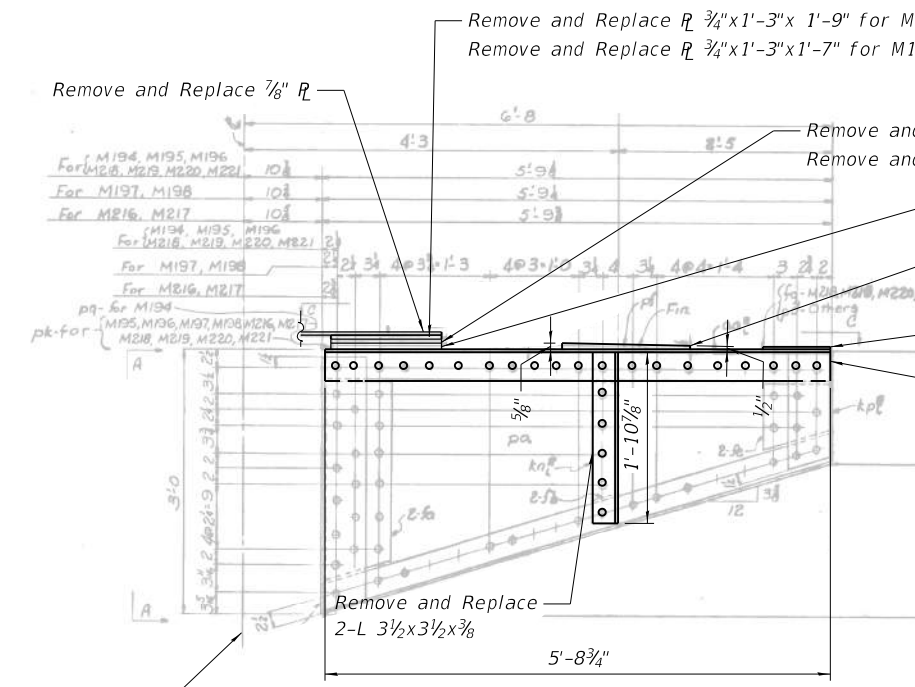
SHEET SB-19 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	98
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

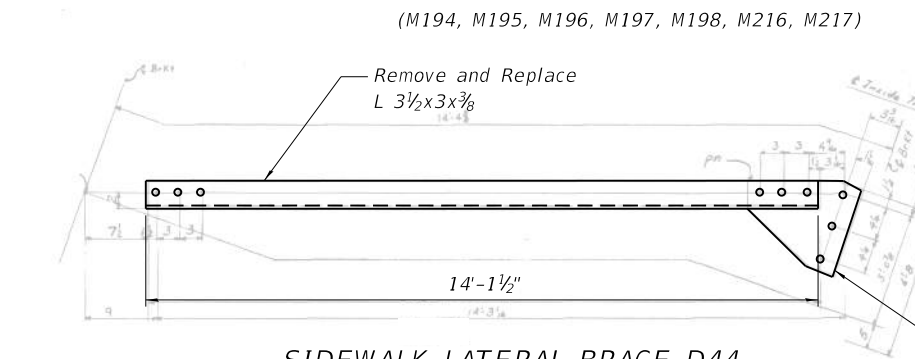




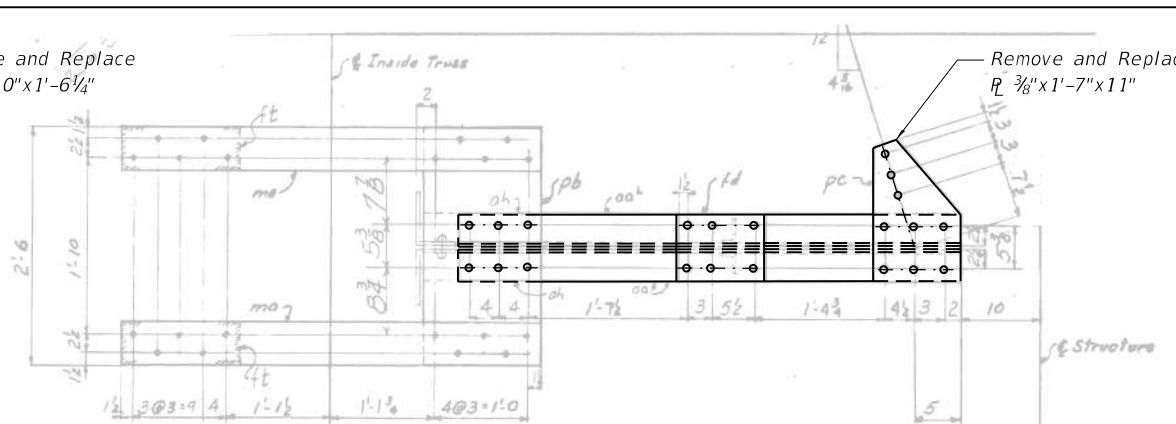
**PLAN VIEW**



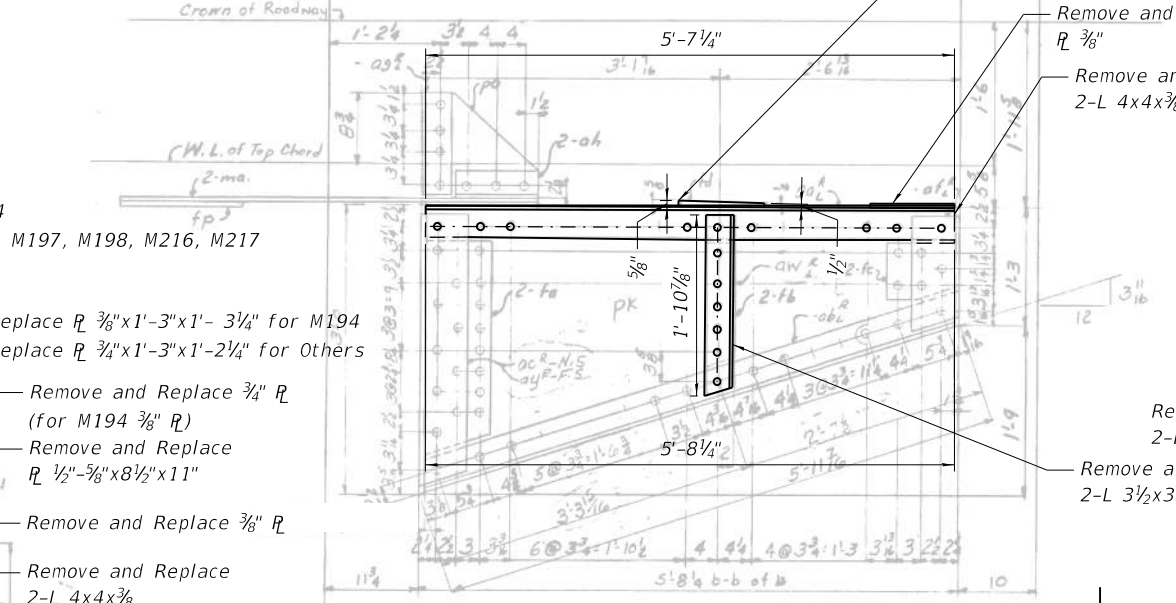
**SIDEWALK SUPPORT**



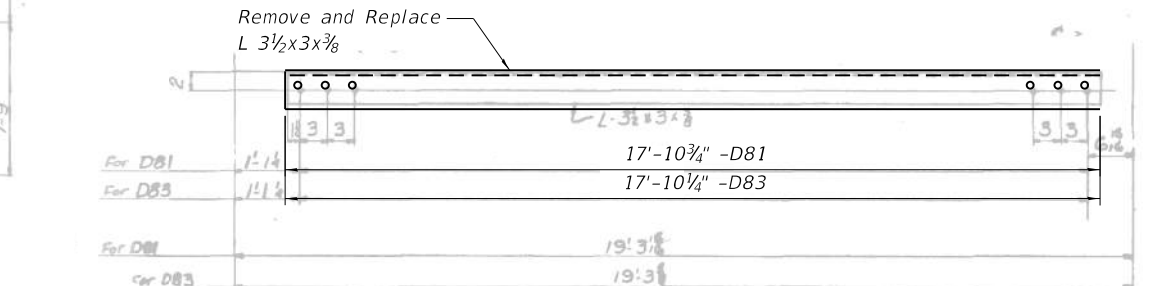
**SIDEWALK LATERAL BRACE D41**



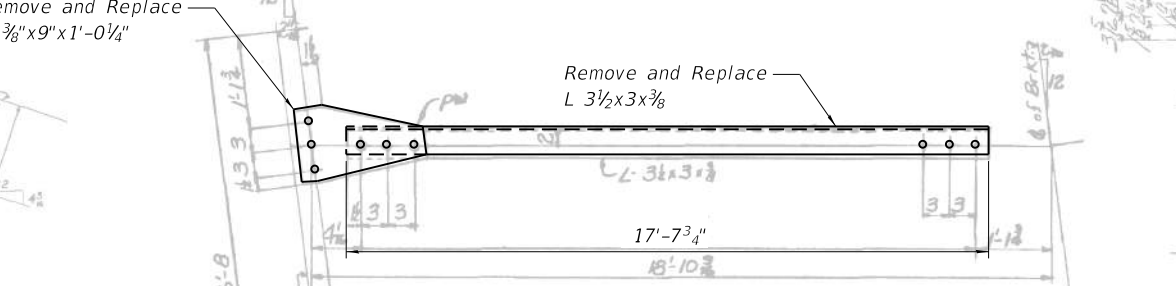
**PLAN VIEW M94**



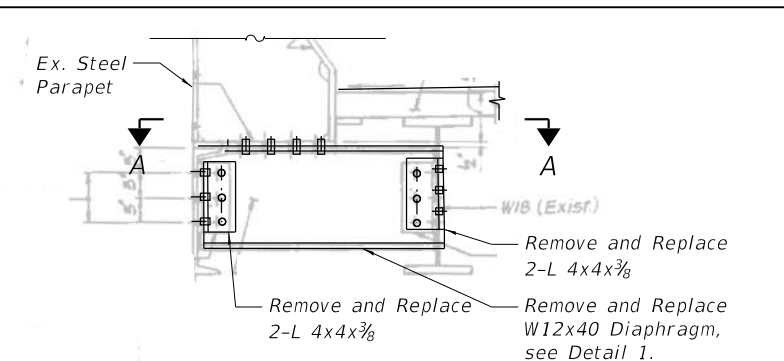
**SIDEWALK SUPPORT M94**



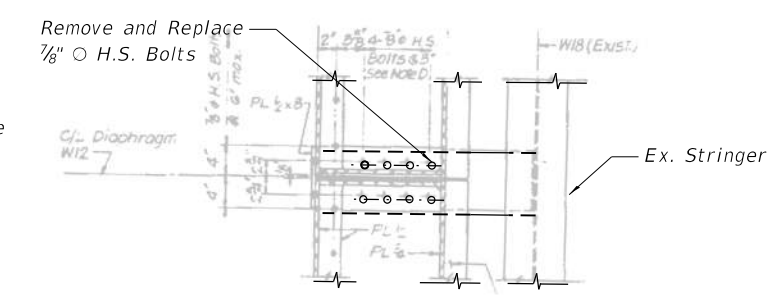
**SIDEWALK LATERAL BRACE D81 & D83**



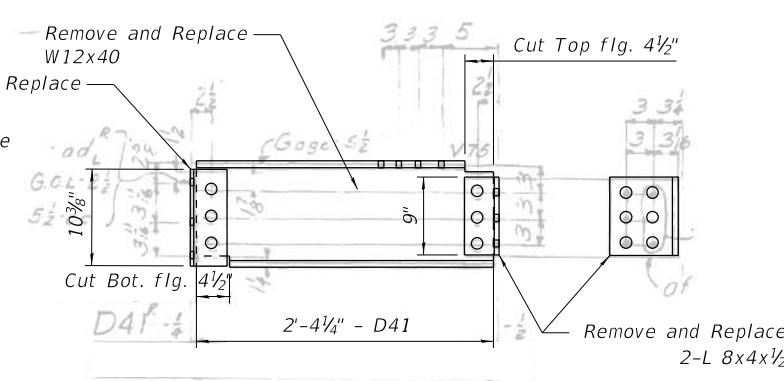
**SIDEWALK LATERAL BRACE D82**



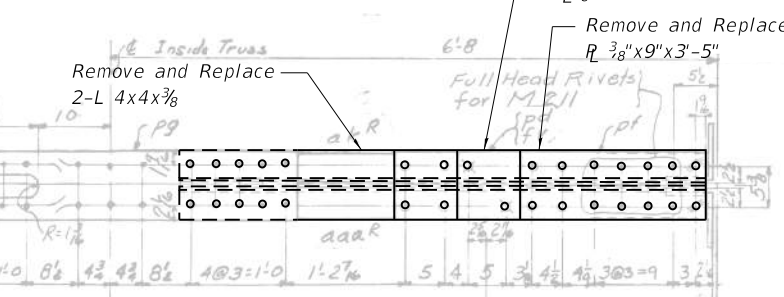
**MEDIAN BARRIER SUPPORT DIAPHRAGM**



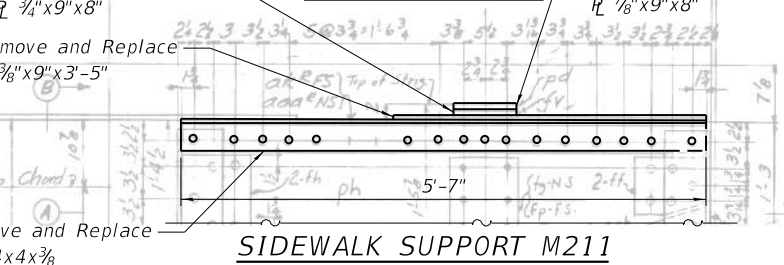
**SECTION A-A**



**DETAIL 1**



**PLAN VIEW M211**



**SIDEWALK SUPPORT M211**

**NOTES:**

1. Steel members to be removed and replaced shall match existing.
2. Existing sidewalk bracket and bracing steel removal shall be paid for as "Structural Steel Removal".
3. All proposed sidewalk bracket and bracing steel and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".

**LEGEND**

- New bolt in existing rivet hole. Use existing steel as template for new steel.

MODEL: Default  
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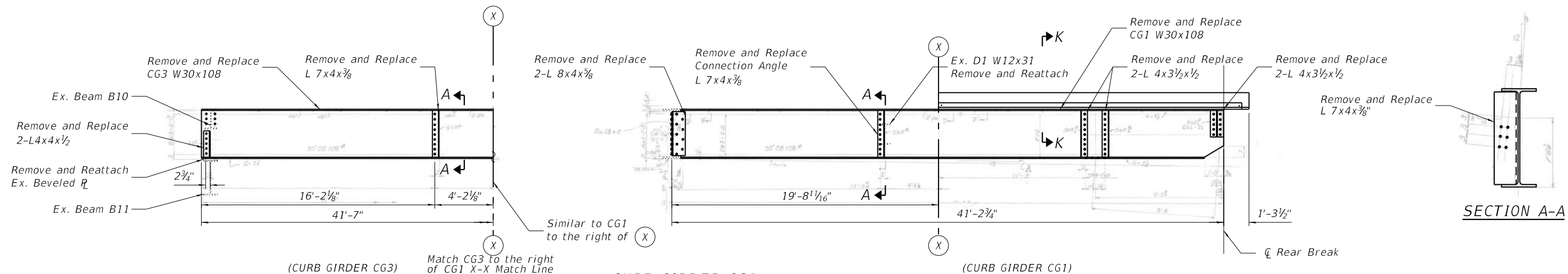


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PLOT DATE =	DRAWN - IL/AM	REVISED -
	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK BRACKET AND INTERIOR LATERAL BRACING REPAIR  
SN 016-0202**

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	99
CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	



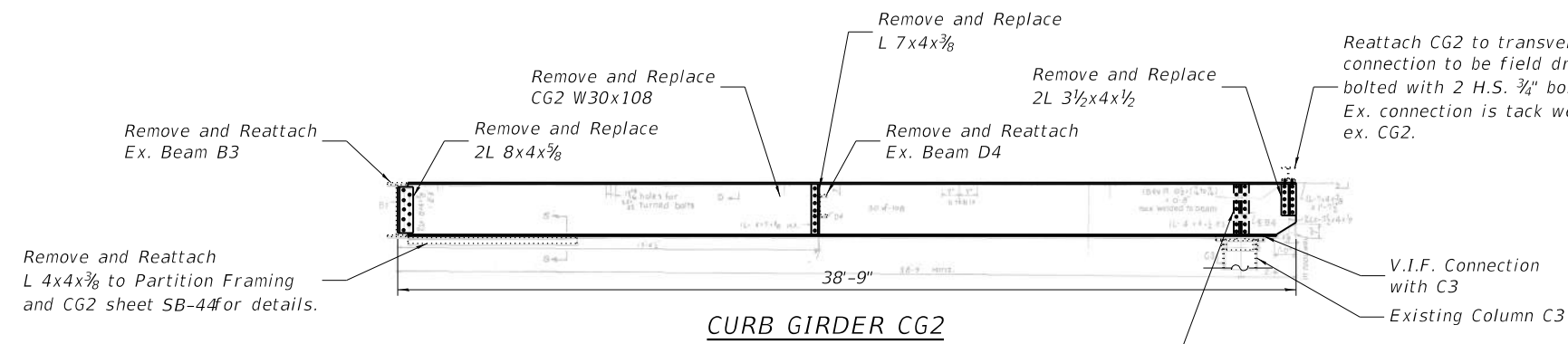
**CURB GIRDER CG1**  
**CURB GIRDER CG3 (SIMILAR OPPOSITE HAND)**

**LEGEND**

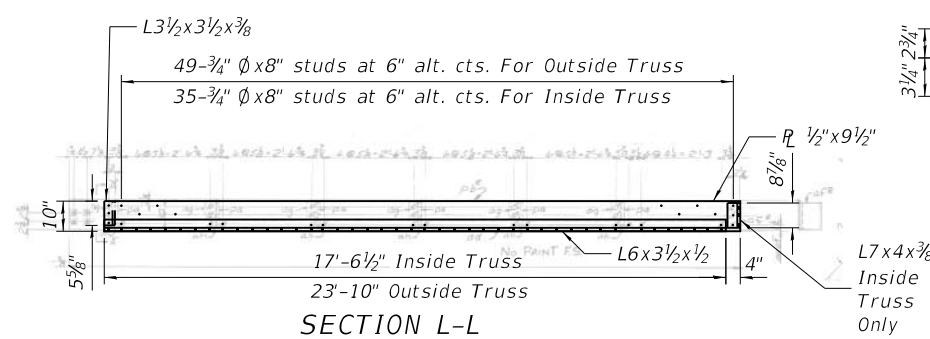
- New bolt in existing rivet hole. Use existing steel as template for new steel.
- New bolt in field drilled hole through existing steel.

**NOTES:**

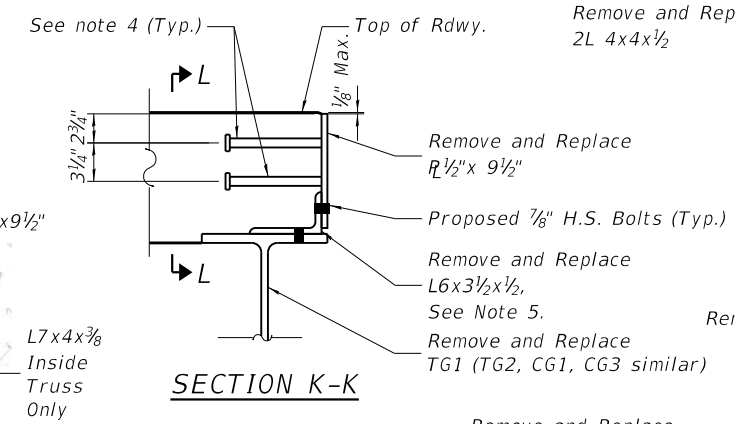
1. Steel members to be removed and replaced shall match existing.
2. Existing curb girder, angle and connection angle removal shall be paid for as "Structural Steel Removal".
3. All proposed curb girders, angles, connection angles and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".
4. 3/4" Øx8" Granular or Solid Flux Filled Headed Studs Conforming to Article 1006.32 of the Standard Specifications Automatically End Welded at 6" Alternating cts. included in the cost of "Furnishing and Erecting Structural Steel".
5. Contractor may omit angle and bolted connection to curb girder if able to attach studded plate to the deck concrete pour directly, based on approval of the Engineer.
6. CG and TG girders shall be hot-dip galvanized. Cost included with Furnishing and Erecting Structural Steel. See Special Provisions for Hot Dip Galvanizing of Structural Steel.
7. Fasteners shall be ASTM F3125 Grade A325 Type 1. Fasteners shall be hot dip galvanized. See Special Provisions for Hot Dip Galvanizing of Structural Steel. Bolts 7/8" dia., holes 1 1/16" dia. unless otherwise noted.



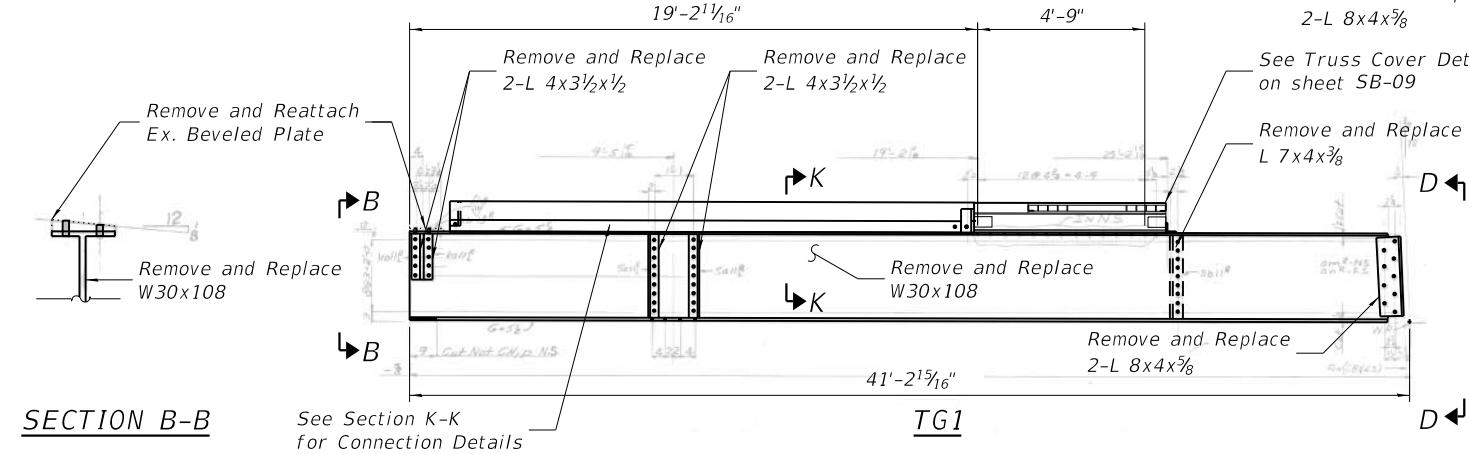
**CURB GIRDER CG2**



**SECTION L-L**

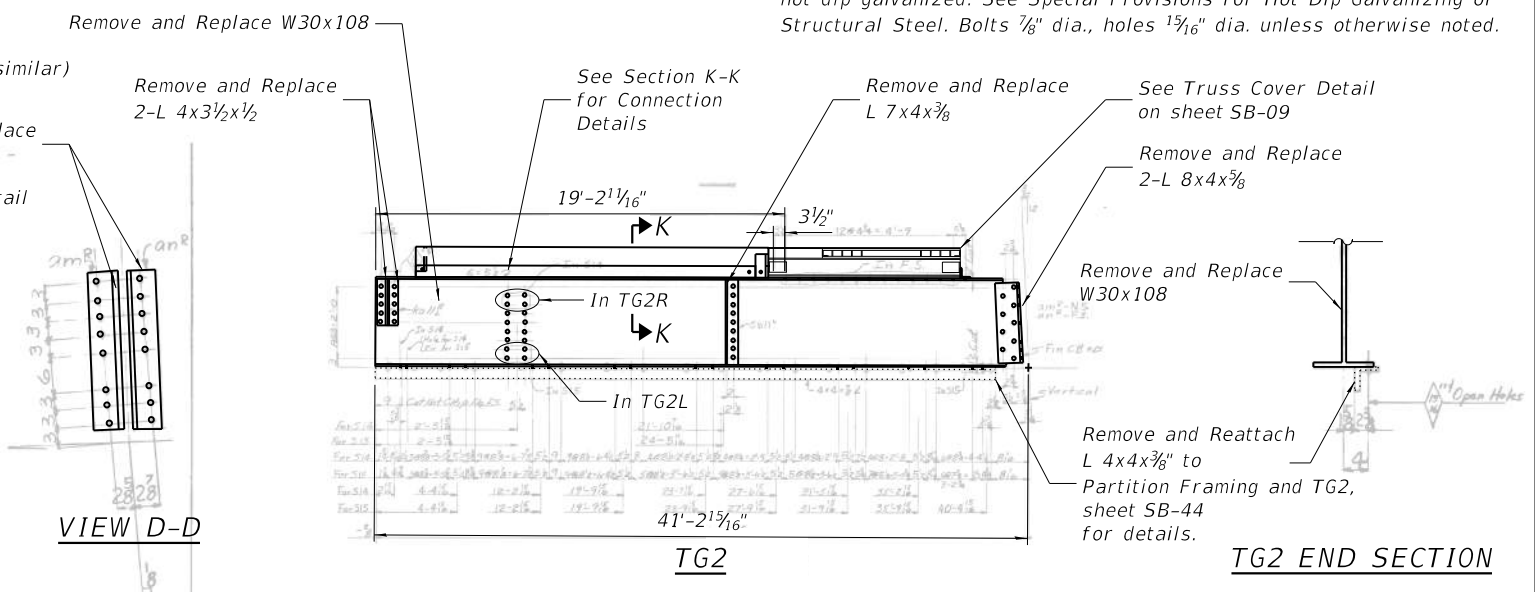


**SECTION K-K**



**SECTION B-B**

**TG1**



**TG2 END SECTION**

**VIEW D-D**

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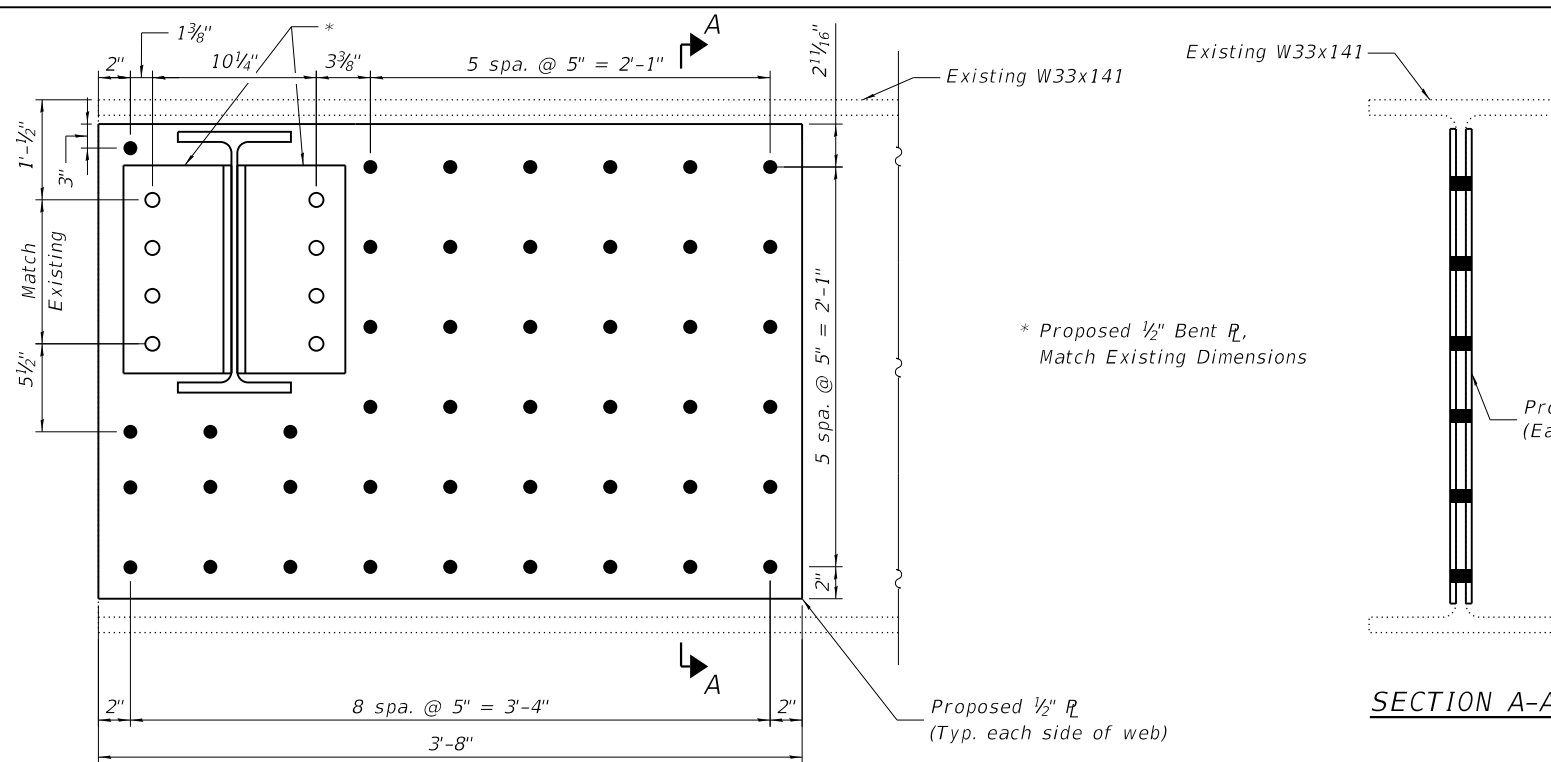
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**CURB GIRDER REPAIRS**  
**SN 016-0202**

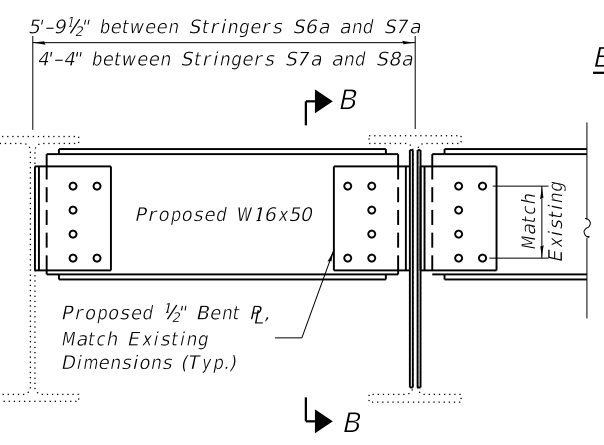
SHEET SB-21 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62L31				

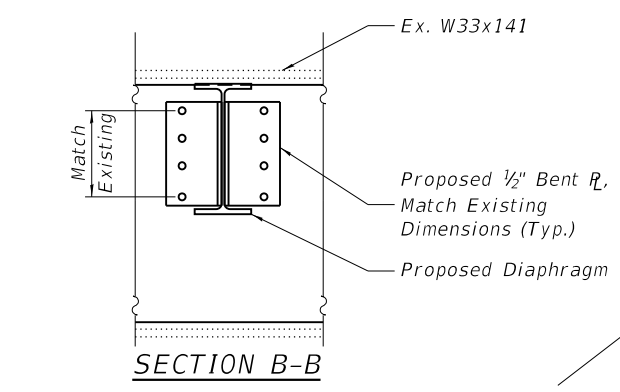
ILLINOIS FED. AID PROJECT



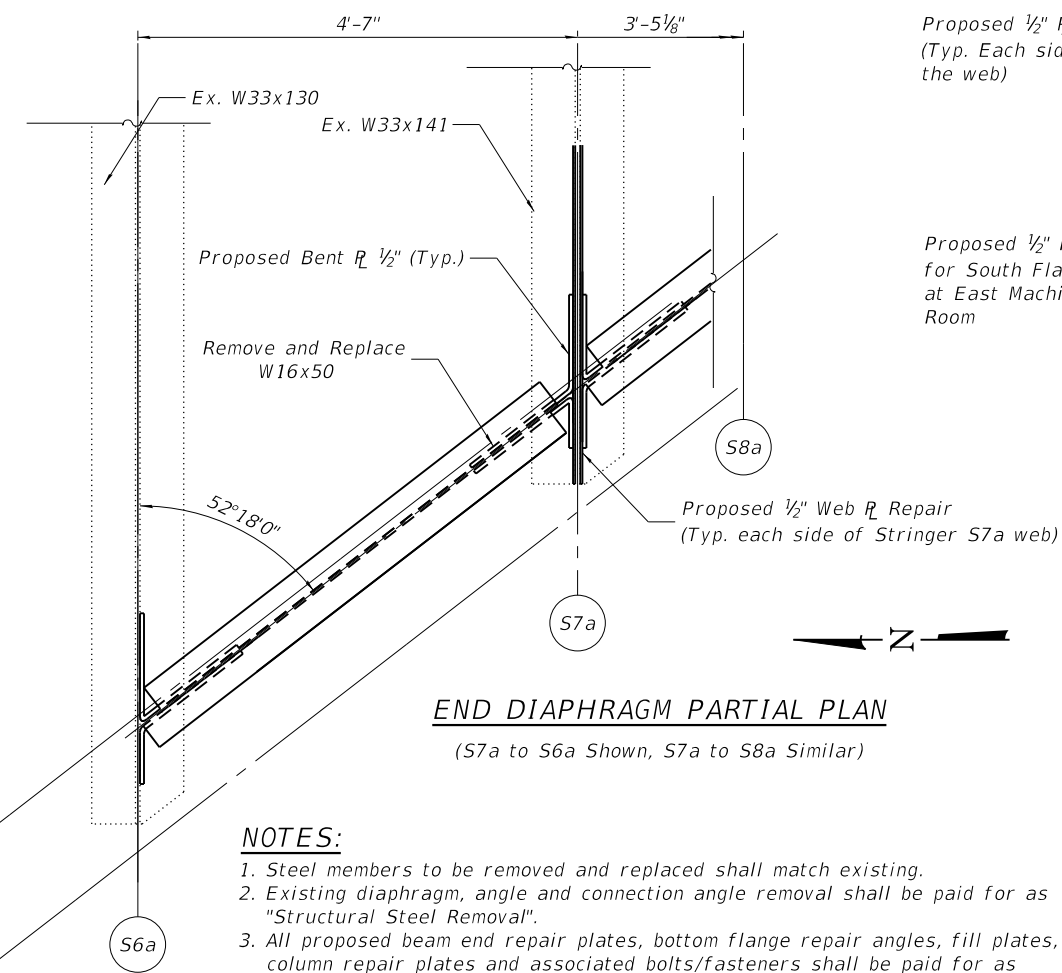
SECTION A-A



PROPOSED DIAPHRAGM ELEVATION  
(S7a to S6a Shown, S7a to S8a Similar)

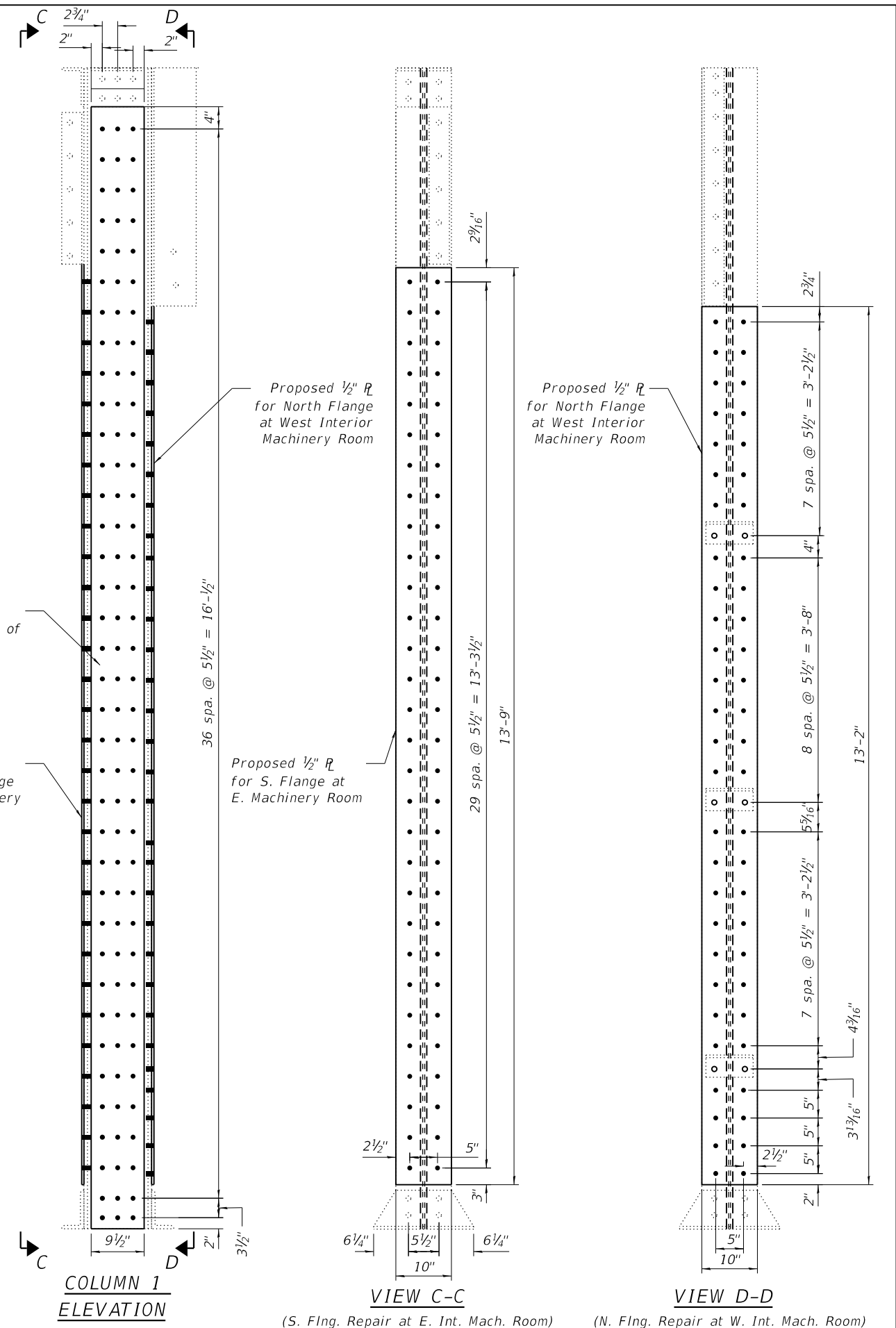


SECTION B-B



END DIAPHRAGM PARTIAL PLAN  
(S7a to S6a Shown, S7a to S8a Similar)

- NOTES:**
1. Steel members to be removed and replaced shall match existing.
  2. Existing diaphragm, angle and connection angle removal shall be paid for as "Structural Steel Removal".
  3. All proposed beam end repair plates, bottom flange repair angles, fill plates, column repair plates and associated bolts/fasteners shall be paid for as "Structural Steel Repair". All proposed diaphragms, connection bent plates and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".



COLUMN 1 ELEVATION

VIEW C-C

VIEW D-D

(S. Flng. Repair at E. Int. Mach. Room) (N. Flng. Repair at W. Int. Mach. Room)

- LEGEND**
- New bolt in existing rivet hole. Use existing steel as template for new steel.
  - New bolt in field drilled hole through existing steel.

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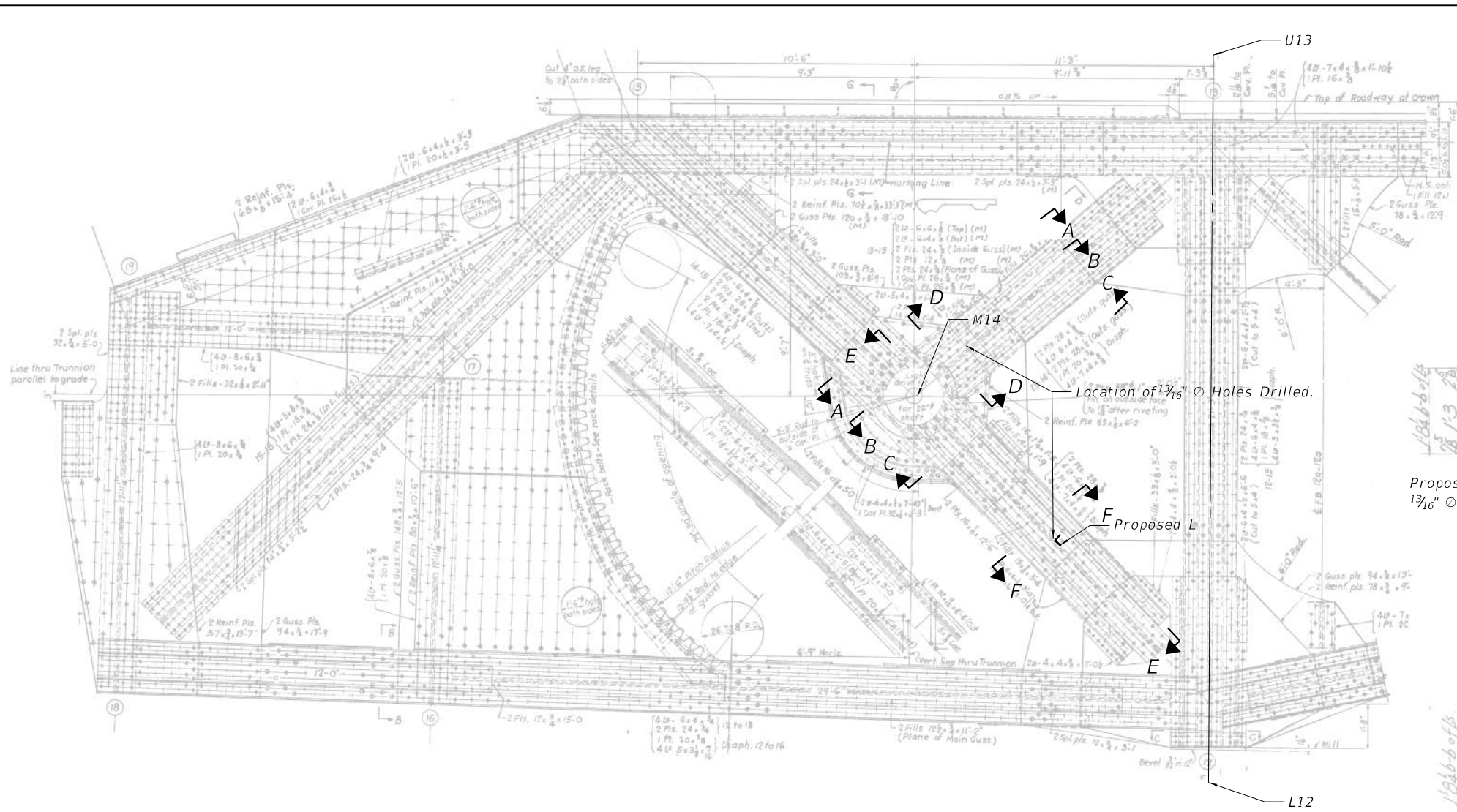
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

COLUMN AND STRINGER REPAIR DETAILS  
SN 016-0202

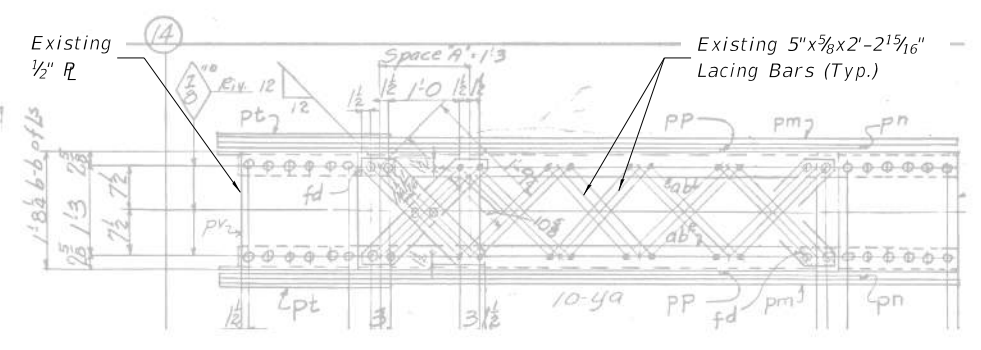
SHEET SB-22 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62L31				

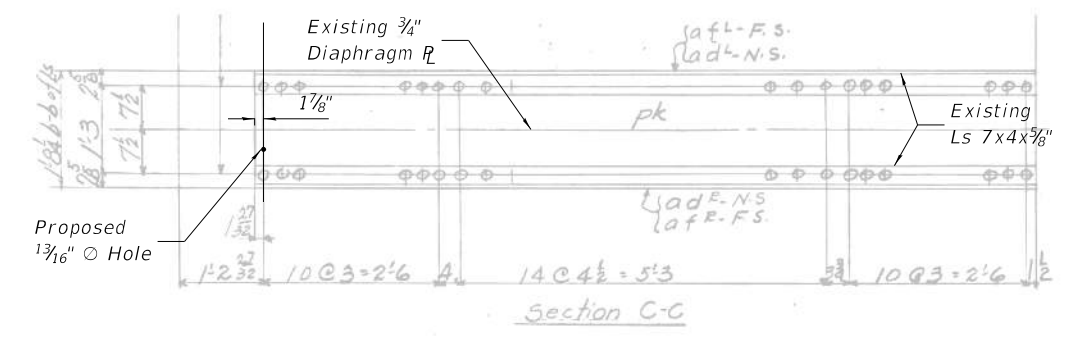
ILLINOIS FED. AID PROJECT



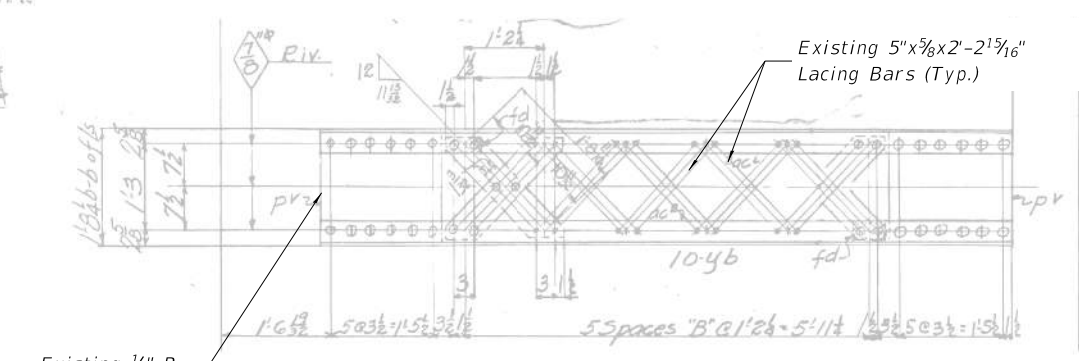
**REAR ARM TRUSS ELEVATION**



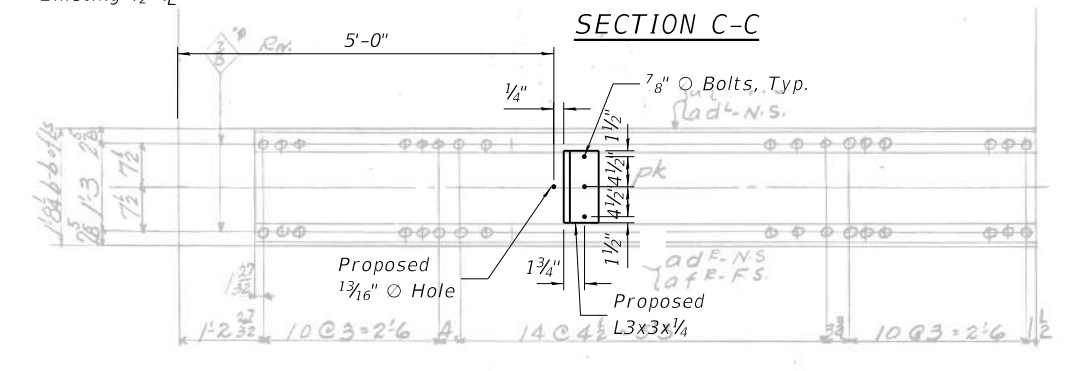
**SECTION A-A**



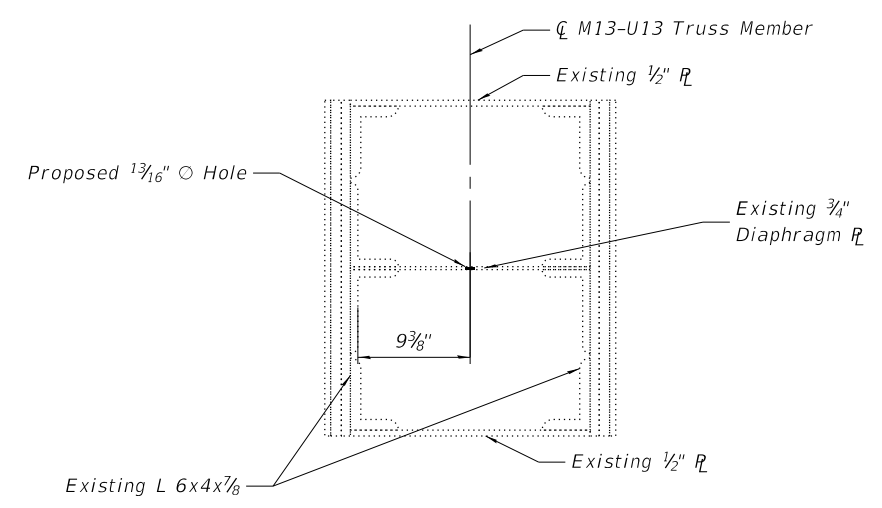
**SECTION B-B**



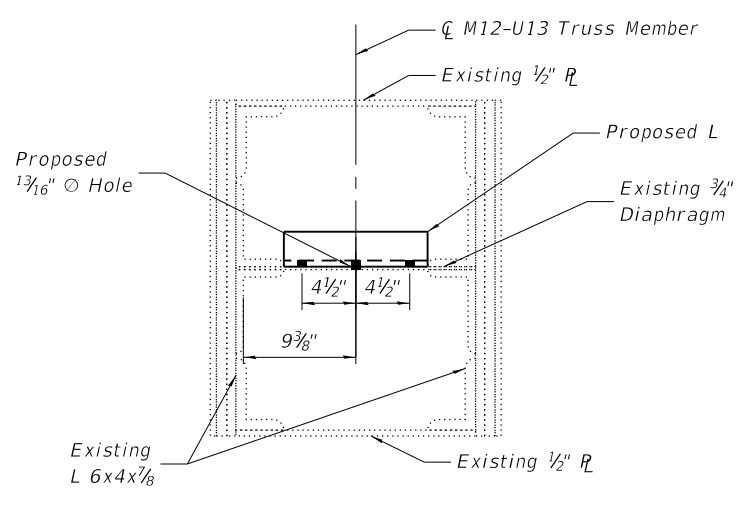
**SECTION C-C**



**SECTION E-E**



**SECTION D-D**



**SECTION F-F**

**LEGEND**

- New bolt in field drilled hole through existing steel.

**NOTES:**

1. Elements of truss member's M14-U13, M14-L12 may require removal and reinstallation due to proposed repair access. Such removal and reinstallation shall be included in the cost of "Holes Drilled".
2. Repaint steel at each "Holes Drilled" location per IDOT GBSP 25 "Cleaning and Painting Existing Steel Structure".
3. Proposed angle and associated bolts shall be paid for as "Furnishing and Erecting Structural Steel".

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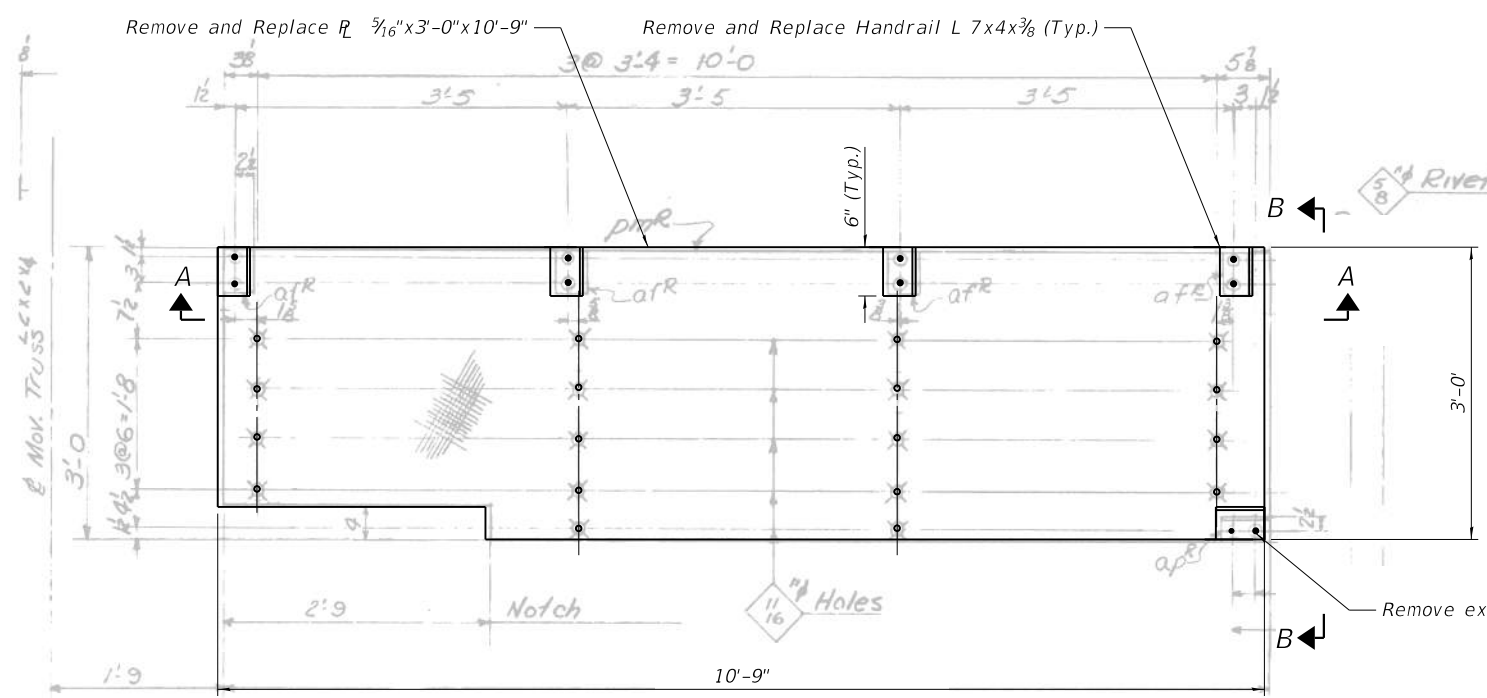
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRUSS REPAIR NODE M14 WEEP HOLE  
SN 016-0202**

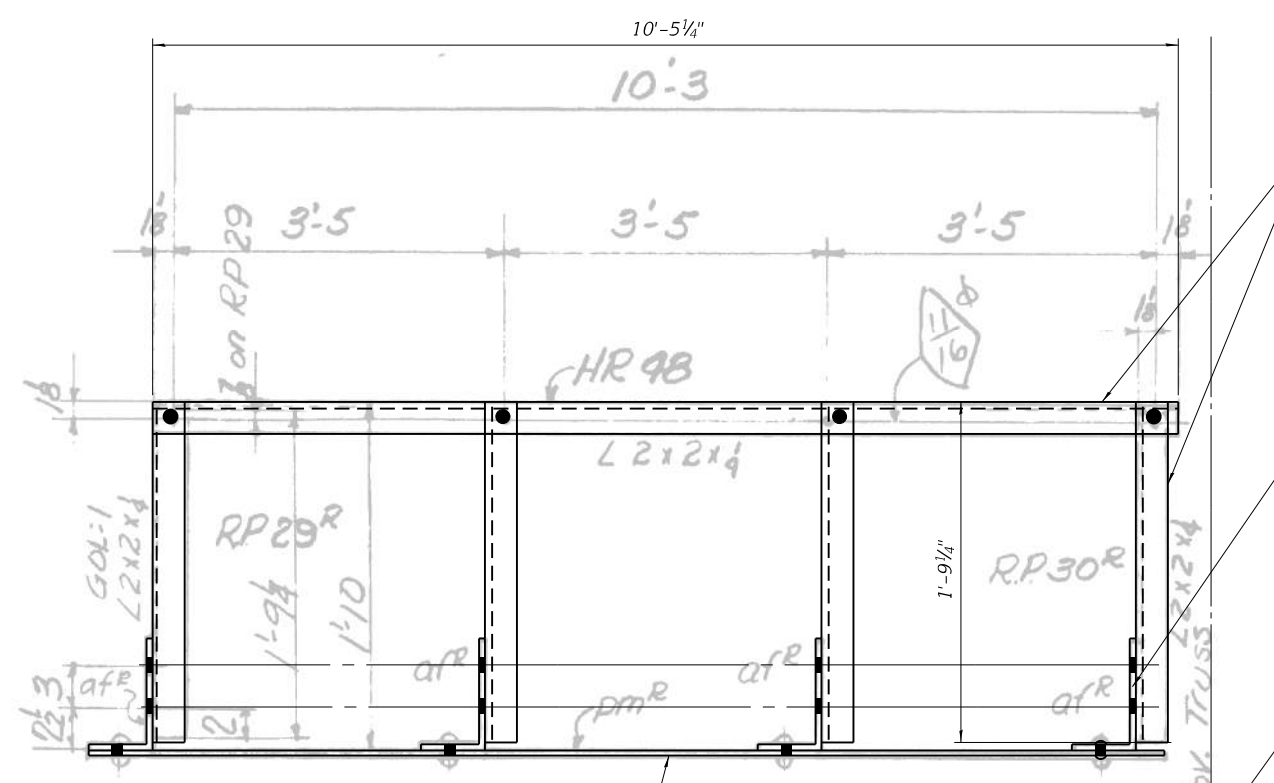
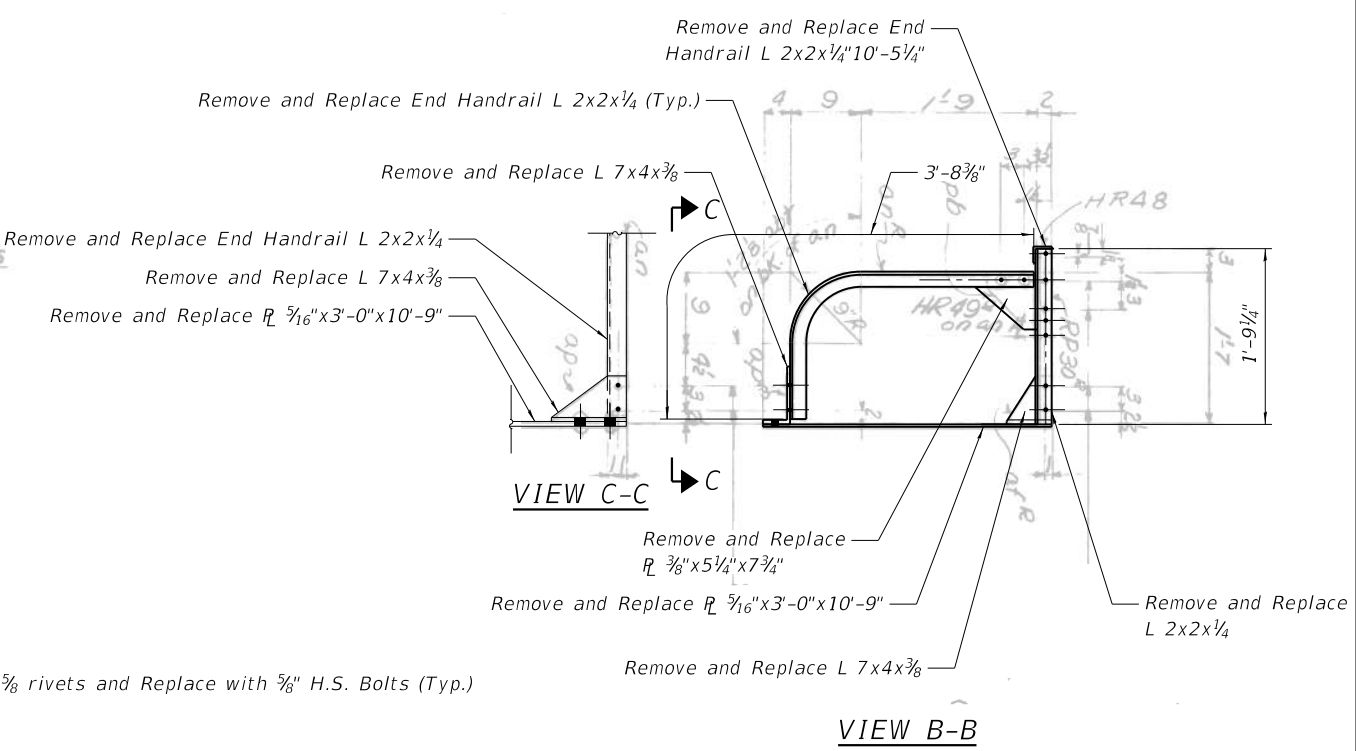
SHEET SB-23 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	102
CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	





LATTICE STRUT WORKING PLATFORM PLAN



SECTION A-A

**LEGEND**

- New bolt in existing rivet hole. Use existing steel as template for new steel.
- New bolt in field drilled hole through existing steel.

**NOTES:**

1. Steel members to be removed and replaced shall match existing.
2. Existing lattice strut working platform steel removal shall be paid for as "Structural Steel Removal".
3. All proposed lattice strut working platform steel and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".

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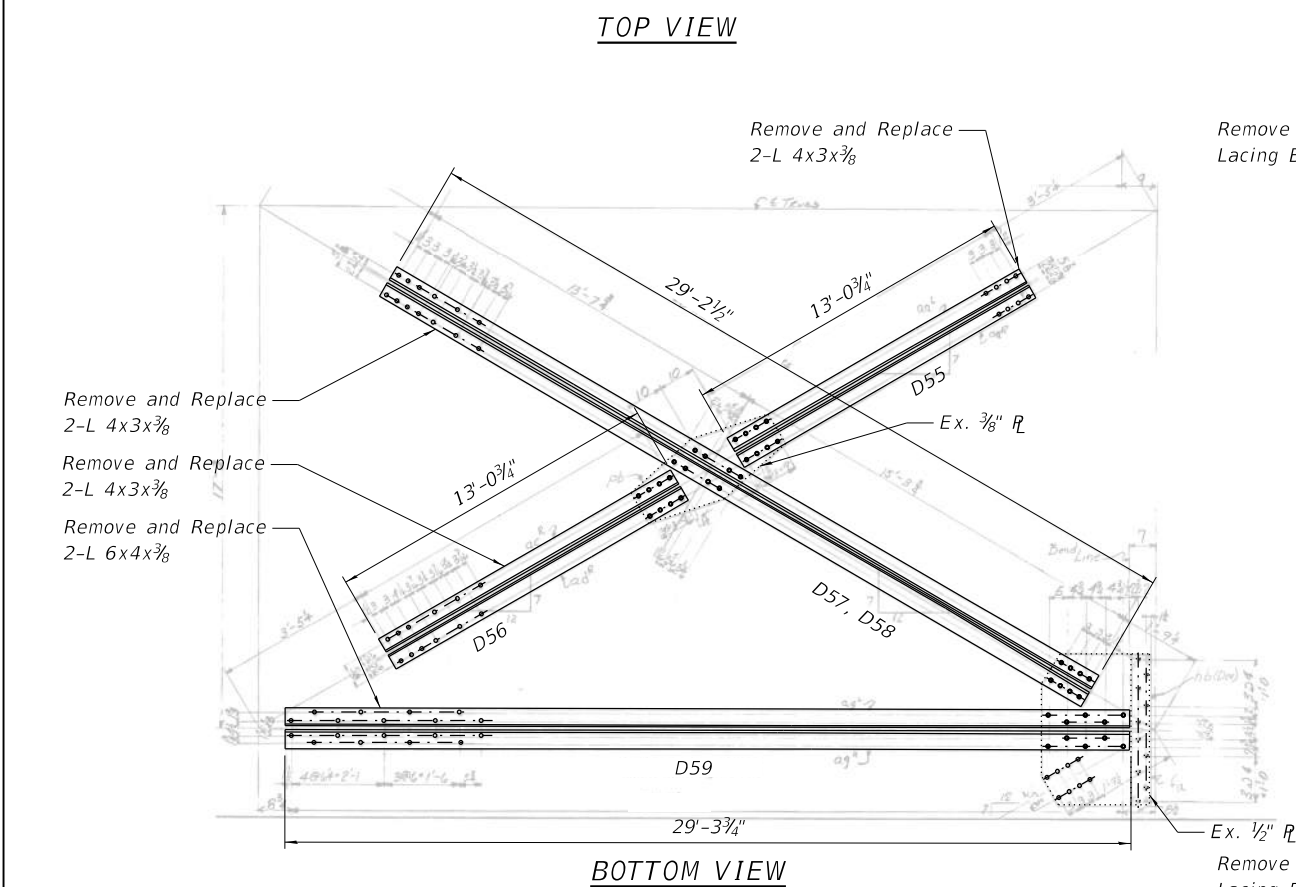
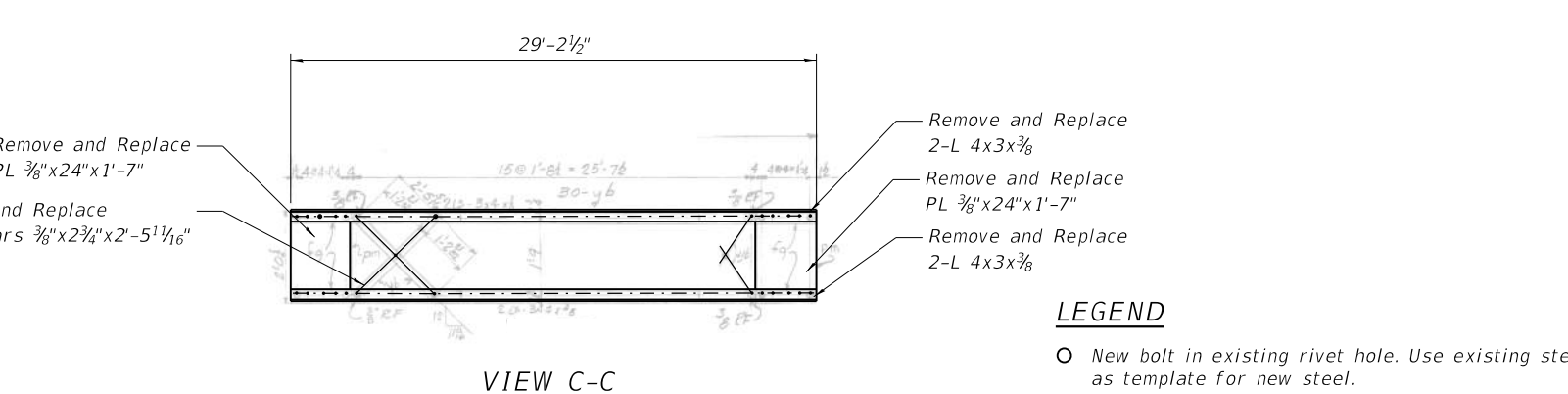
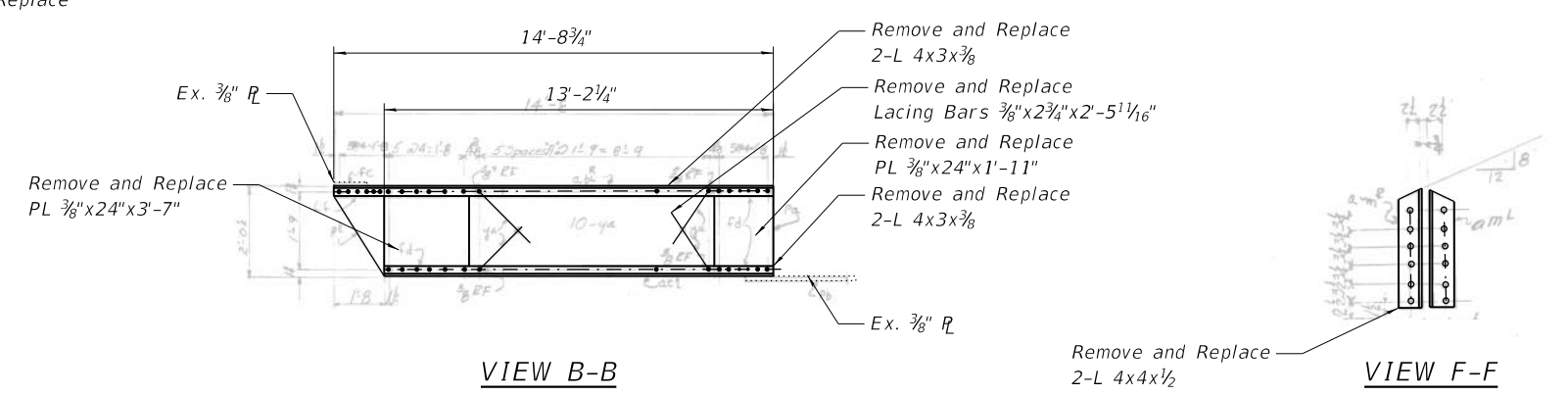
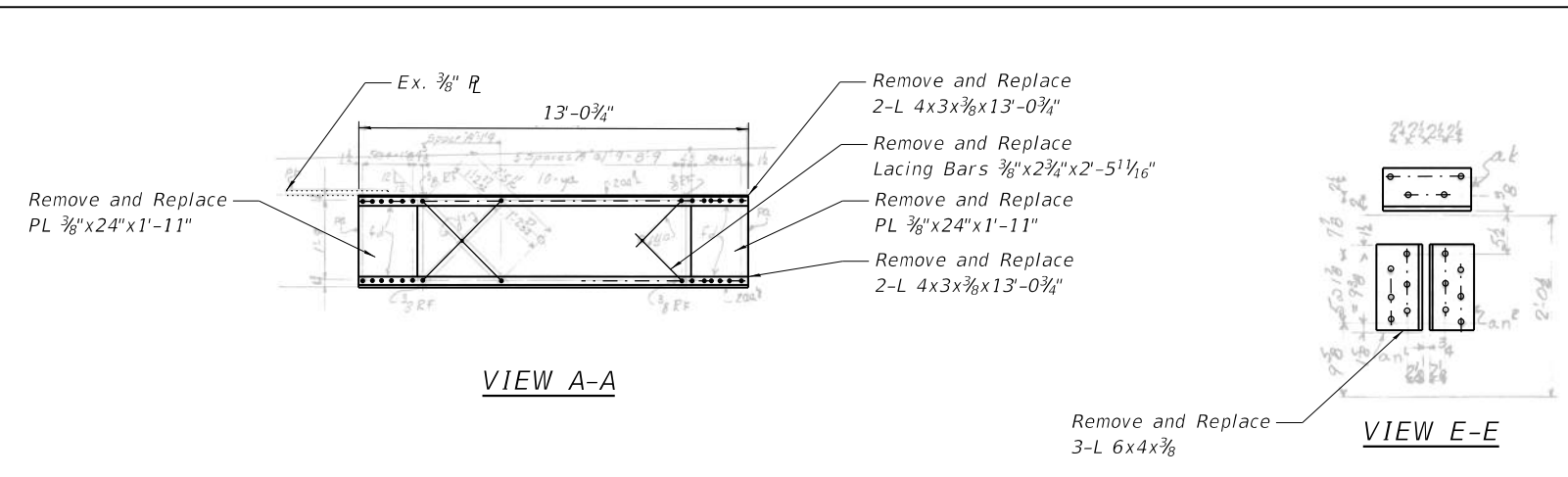
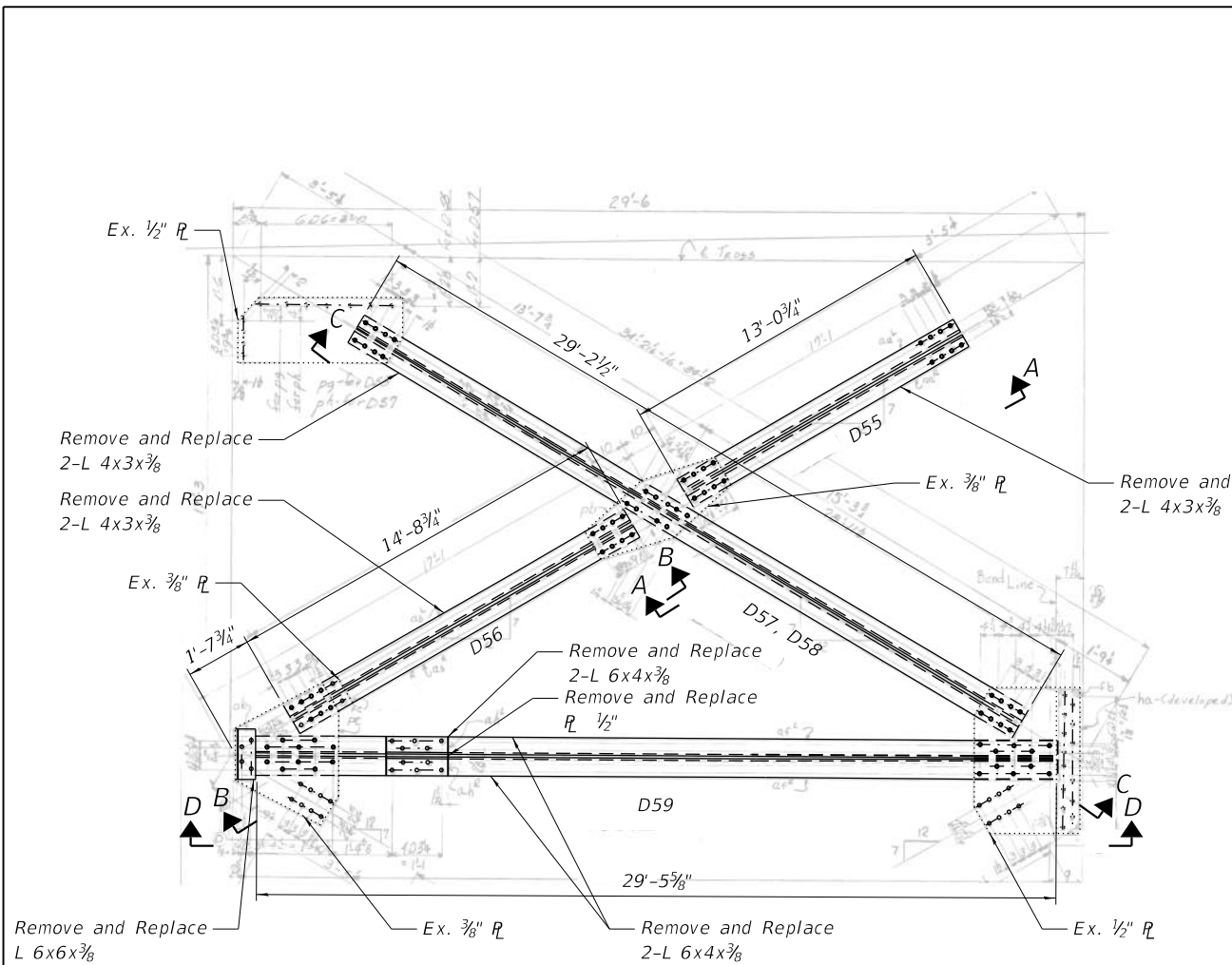
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

LATTICE STRUT L12-M14 WORKING PLATFORM REPAIR DETAILS  
SN 016-0202

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	104
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

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- LEGEND**
- New bolt in existing rivet hole. Use existing steel as template for new steel.
  - New bolt in field drilled hole through existing steel.

- NOTES:**
1. Steel members to be removed and replaced shall match existing.
  2. Existing lateral bracing steel removal shall be paid for as "Structural Steel Removal".
  3. All proposed lateran bracing steel and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".

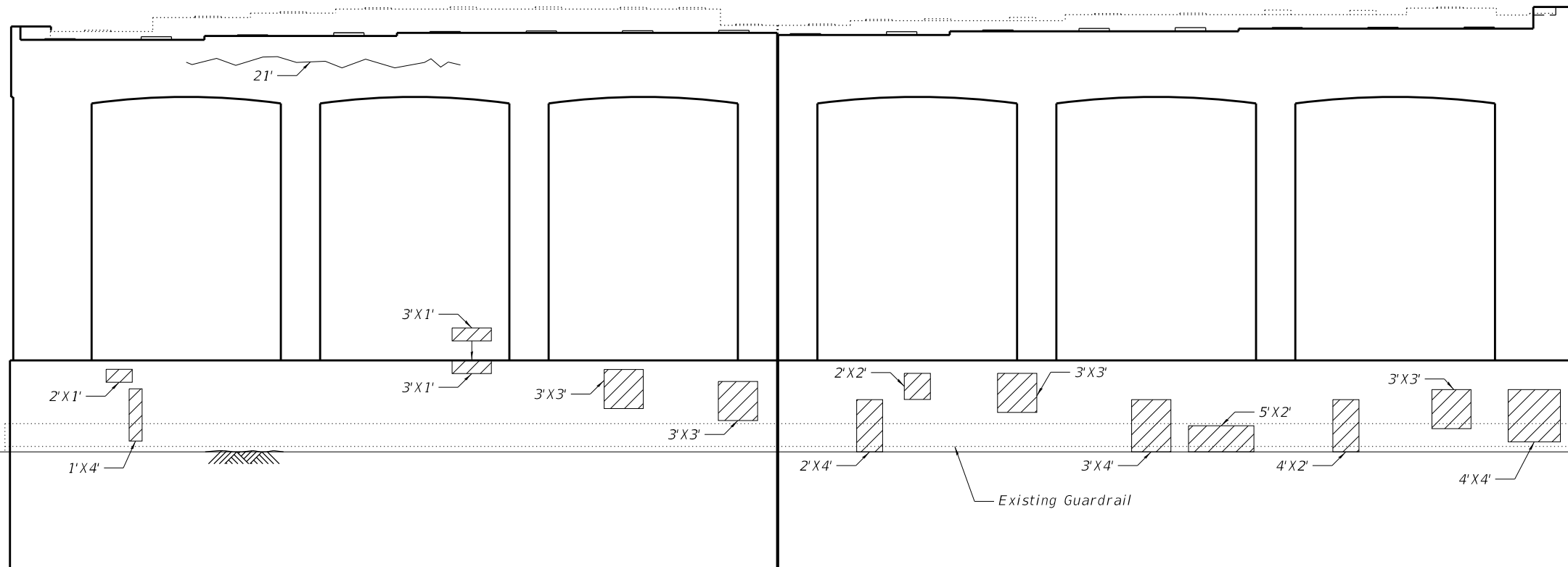
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

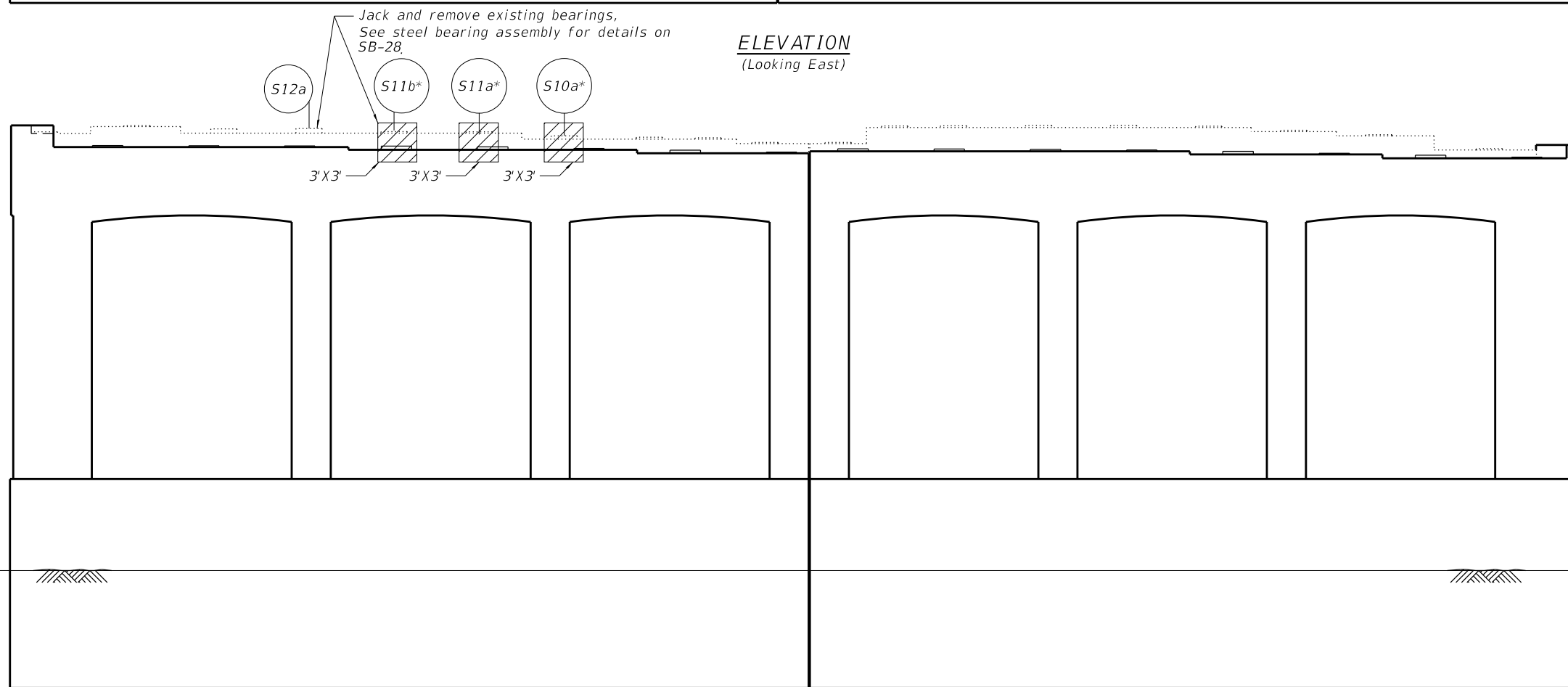
**LATERAL BRACING REPAIRS  
 SN 016-0202**

SHEET SB-26 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	105
CONTRACT NO. 62L31				
ILLINOIS		FED. AID PROJECT		



**ELEVATION**  
(Looking East)






**ELEVATION**  
(Looking West)

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	133
Epoxy Crack Injection	FOOT	21

**LEGEND**

-  Structural Repair of Concrete  
(Depth Equal to or Less Than 5 Inches)
-  Epoxy Crack Injection
-  Location of deteriorated beam/stringer seat

**NOTES:**

1. Repair of the existing pier and bearing pedestals shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer in field.
2. Where existing rebar has lost 25% or more of its original cross section due to corrosion remove additional concrete to expose undamaged rebar. Replace the deteriorated bar with a new bar of the same size and spacing.
3. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
4. Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
5. Removal and reattachment of existing guardrail, if necessary, to perform repair is included in the cost of "Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)".
6. A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
7. "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

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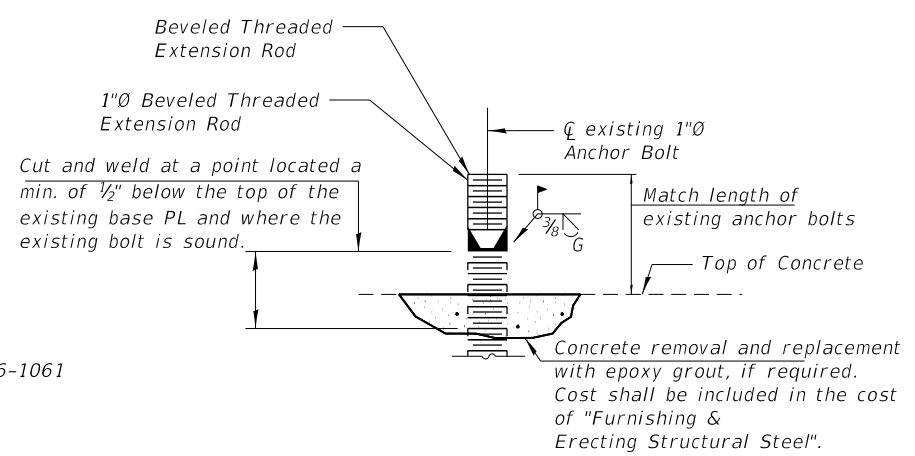
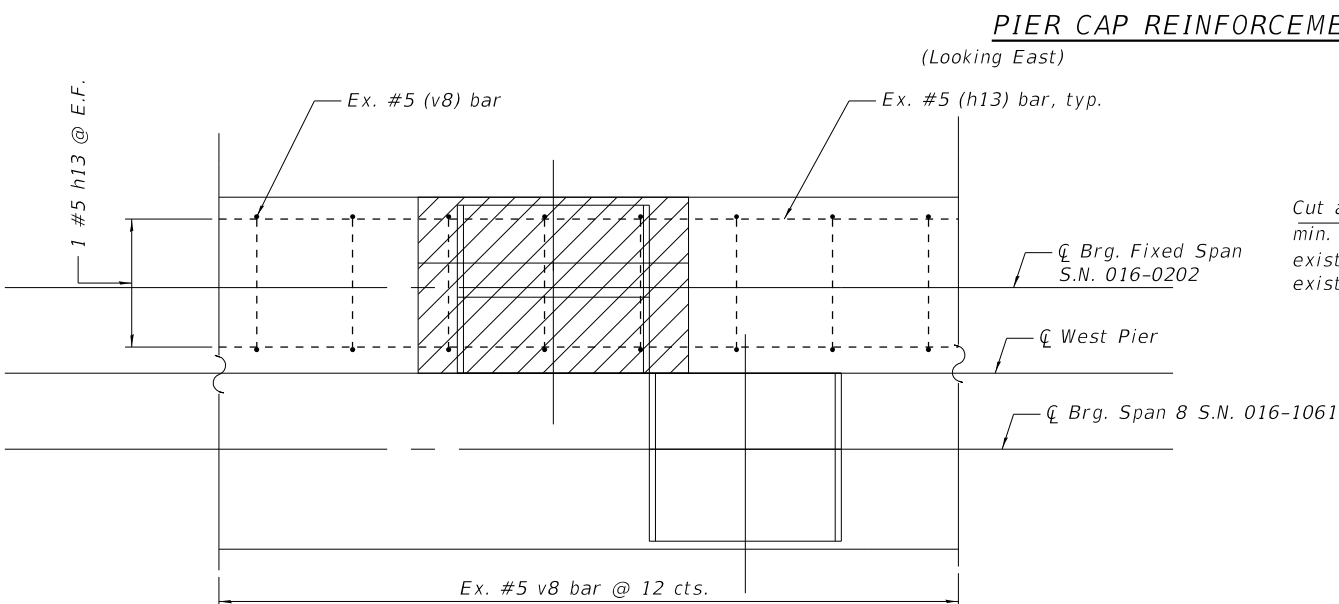
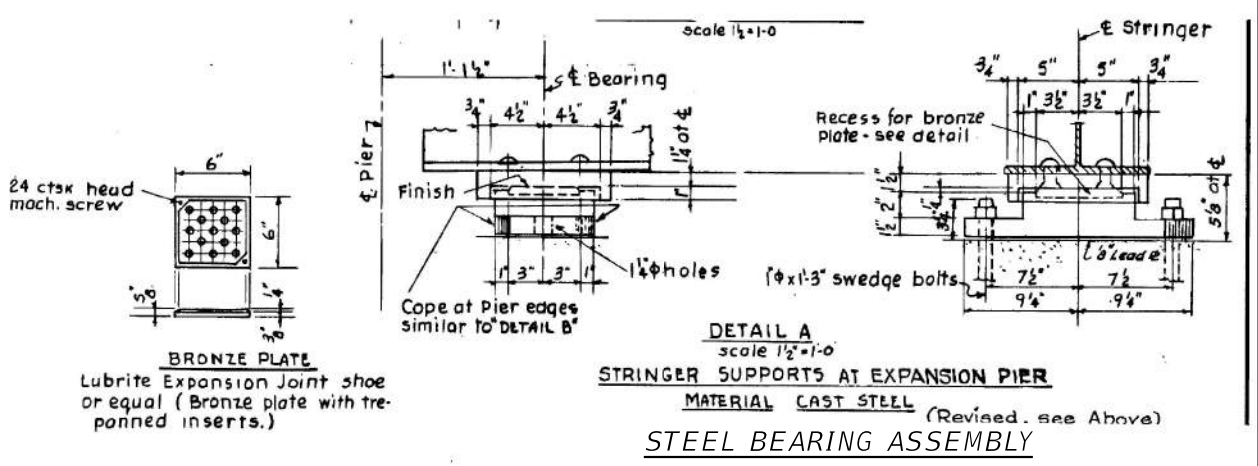
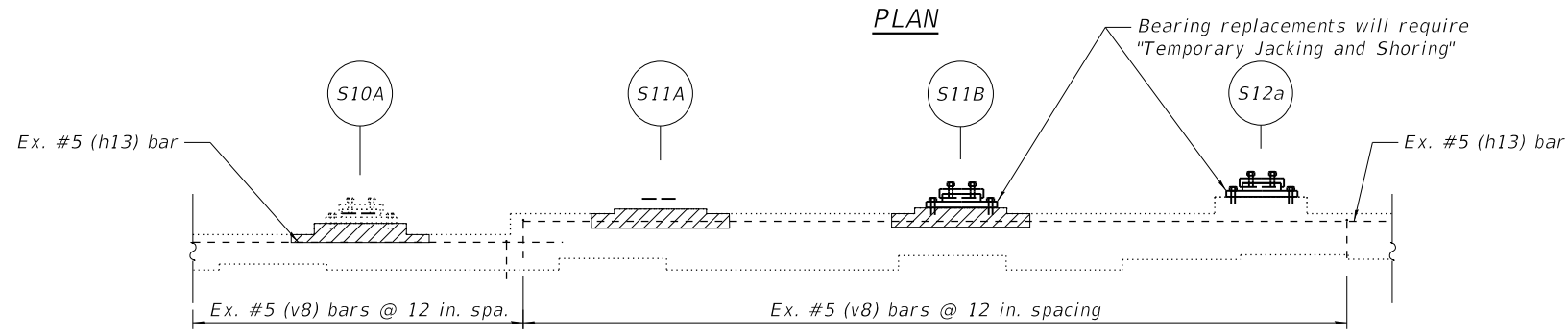
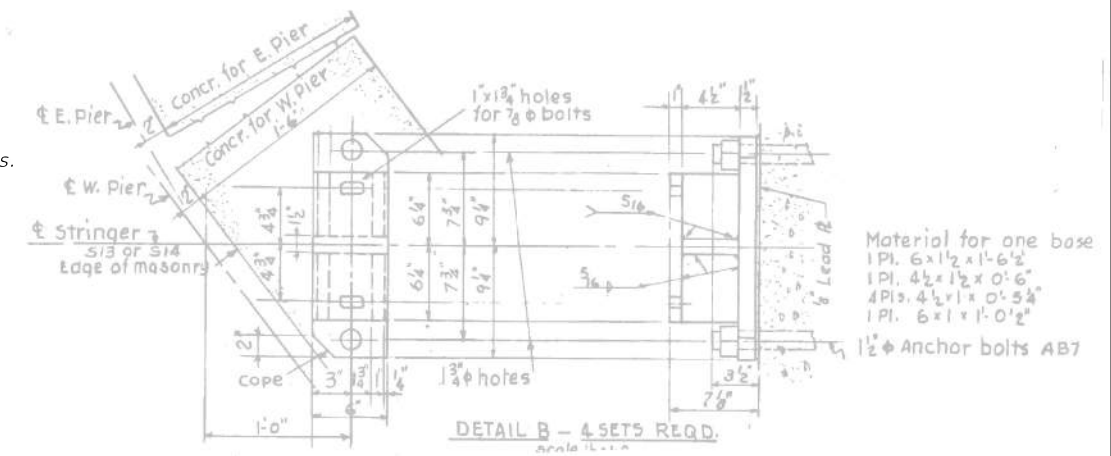
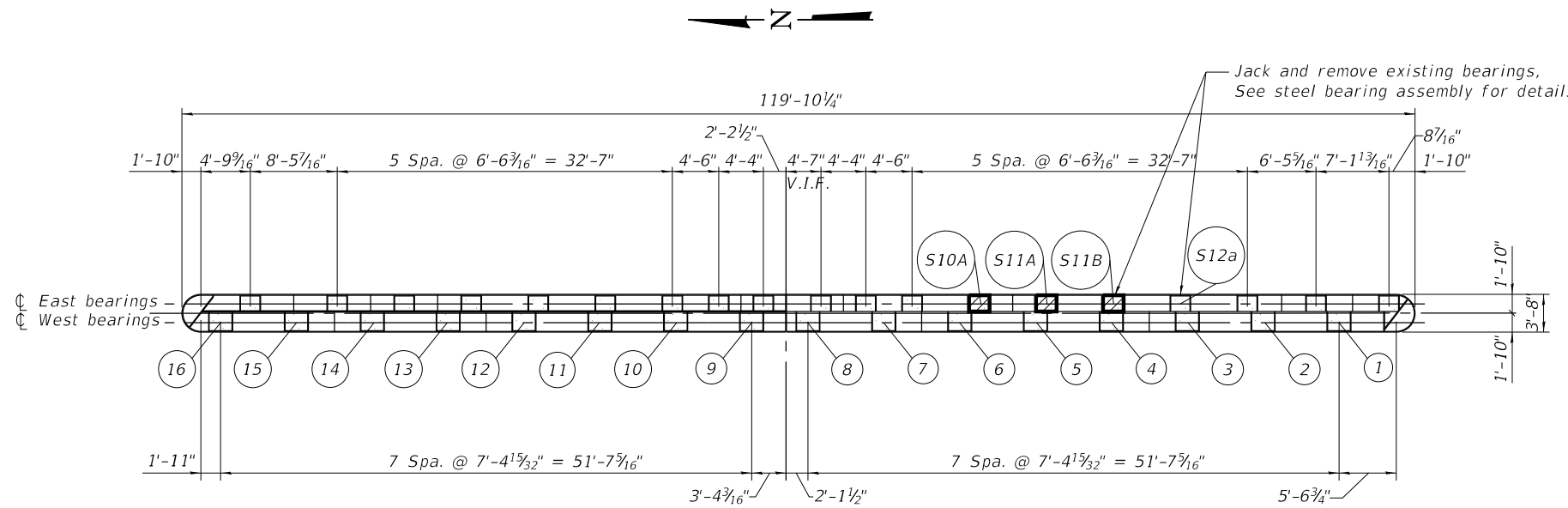
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WEST PIER  
SN 016-0202**

SHEET SB-27 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	106
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				





**ANCHOR BOLT EXTENSION DETAIL**

The furnishing and installation of required Anchor Bolt Extensions shall be included in the cost of "Furnishing and Erecting Structural Steel".

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	15

**GIRDER SERVICE REACTIONS**

	E. Brg.
RDL (K)	29.1
RLL (K)	59.1
RIMP (K)	19.48
RTOTAL (K)	107.68

**LEGEND**

Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)

**NOTES:**

- Refer to sheet SB-27 for additional notes.
- If the concrete deterioration at the bearing pedestal extends underneath existing steel bearing plates, "Temporary Shoring and Cribbing" is to be used to support the related beam while the concrete work is performed.
- The jack capacity shall be designed to support between 50% and 100% greater than the maximum expected dead load and full live load and impact.
- A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
- "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

MODEL: Default  
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USER NAME =	DESIGNED - IL/AM	REVISED -
	CHECKED - TCG	REVISED -
PLOT SCALE =	DRAWN - IL/AM	REVISED -
PLOT DATE =	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**WEST PIER CAP AND BEARING DETAILS  
 SN 016-0202**

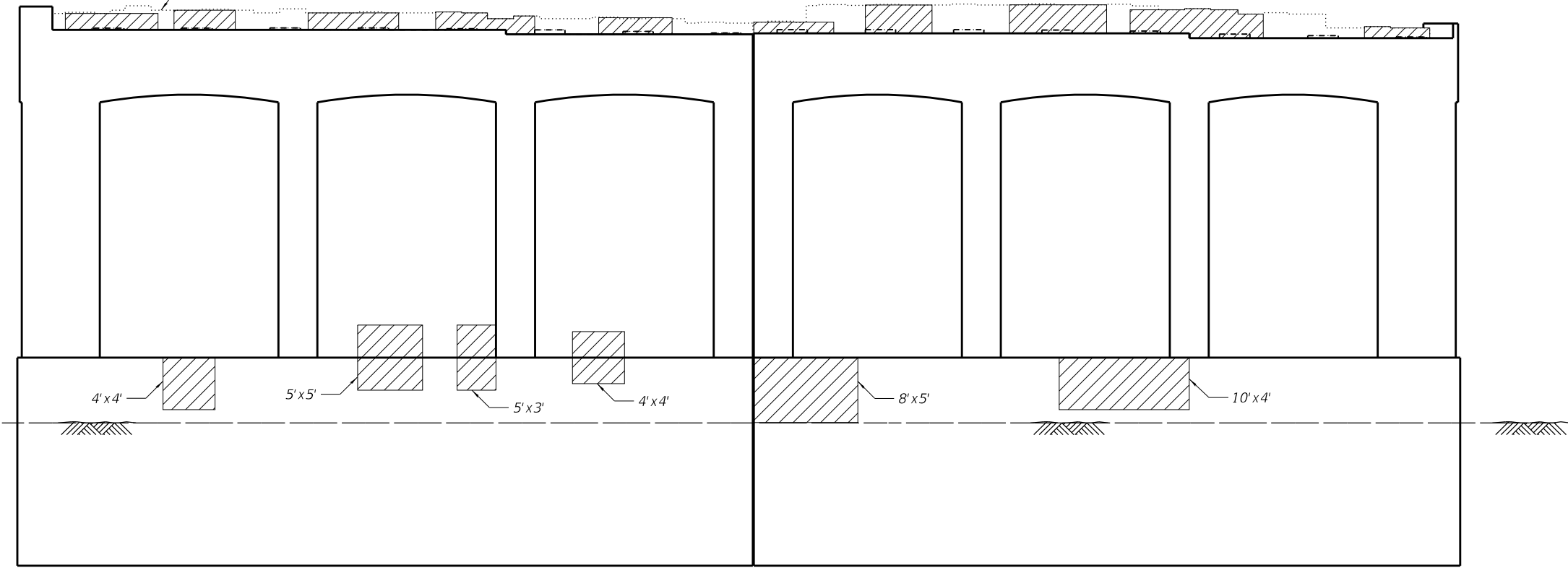
SHEET SB-28 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	107

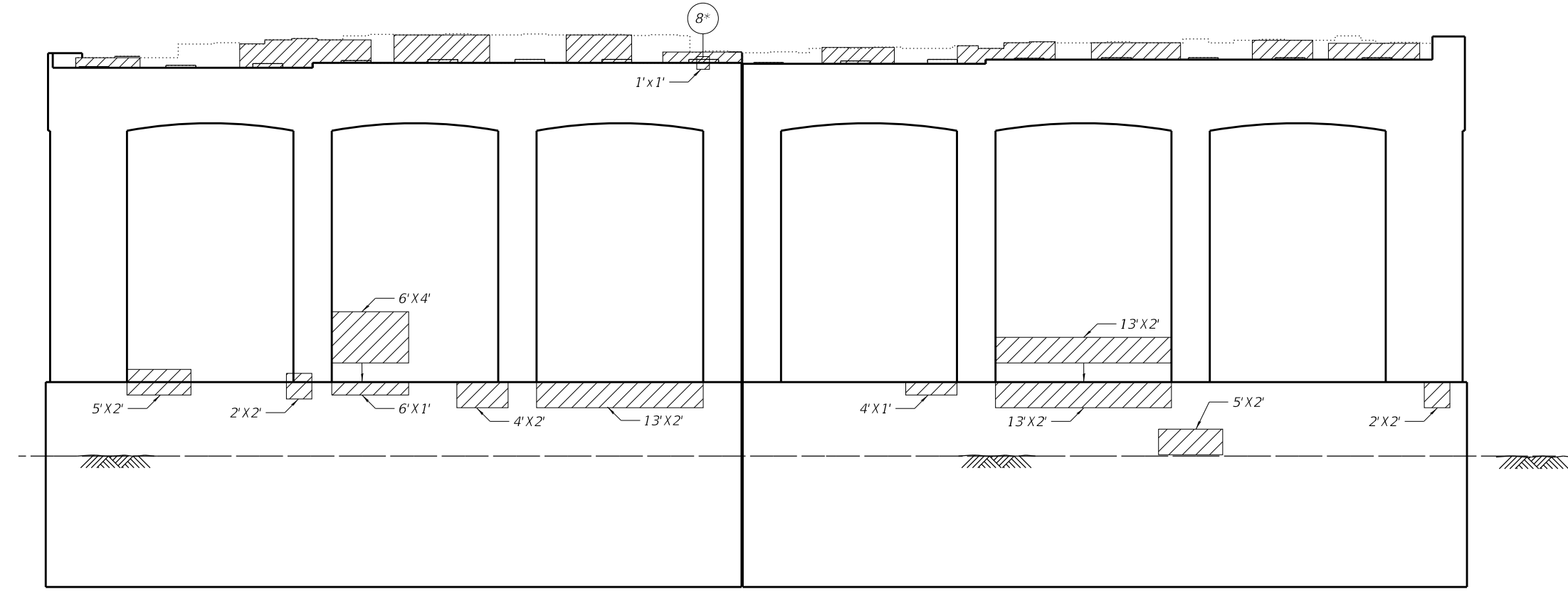
CONTRACT NO. 62L31

ILLINOIS FED. AID PROJECT

Stringers may require "Temporary Jacking and Shoring for repair". See Sheet SB-32 for sections.



**ELEVATION**  
(Looking East)



**ELEVATION**  
(Looking West)

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	301

**LEGEND**

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- Location of deteriorated beam seats

**NOTES:**

1. Repair of the existing pier and bearing pedestals shall include but may not be limited to the areas shown. The actual area to be repaired will be determined by the Engineer in field.
2. Where existing rebar has lost 25% or more of its original cross section due to corrosion remove additional concrete to expose undamaged rebar. Replace the deteriorated bar with a new bar of the same size and spacing.
3. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
4. Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
5. Removal and reattachment of existing guardrail, if necessary, to perform repair is included in the cost of "Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)".
6. See Sheet SB-30 for deteriorated beam seat details.
7. A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
8. "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

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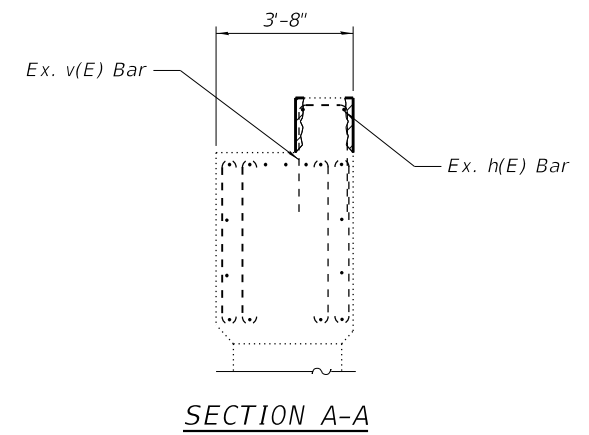
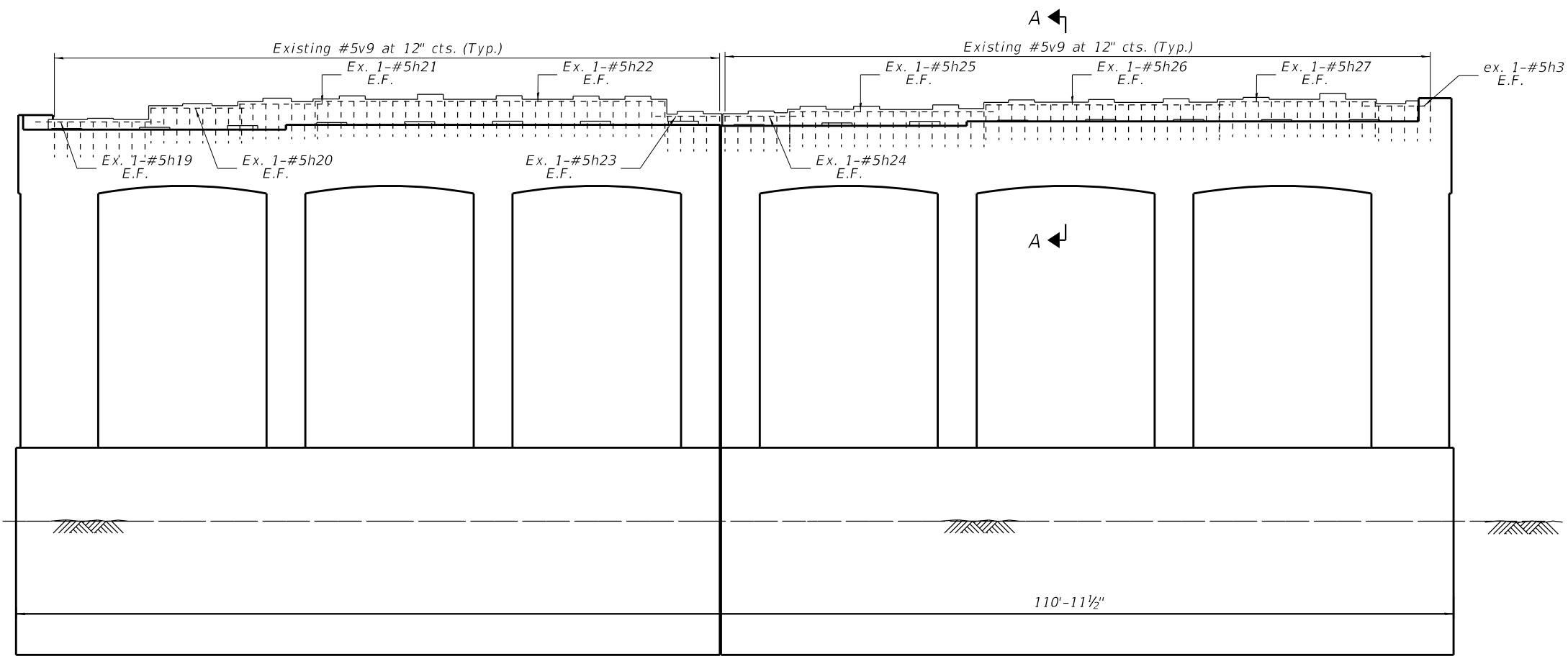
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

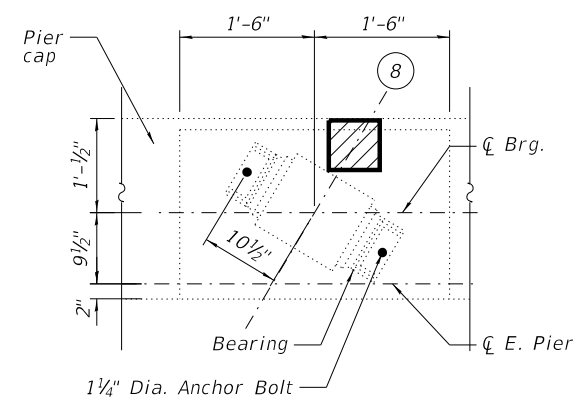
**EAST PIER**  
**SN 016-0202**

SHEET SB-29 OF SB-48 SHEETS

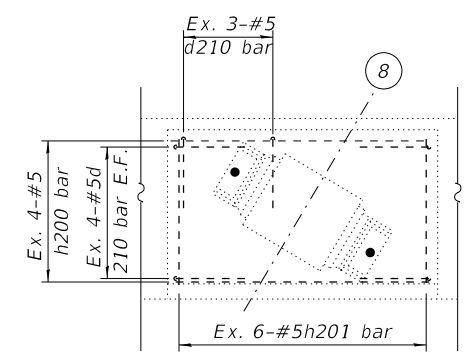
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	108
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



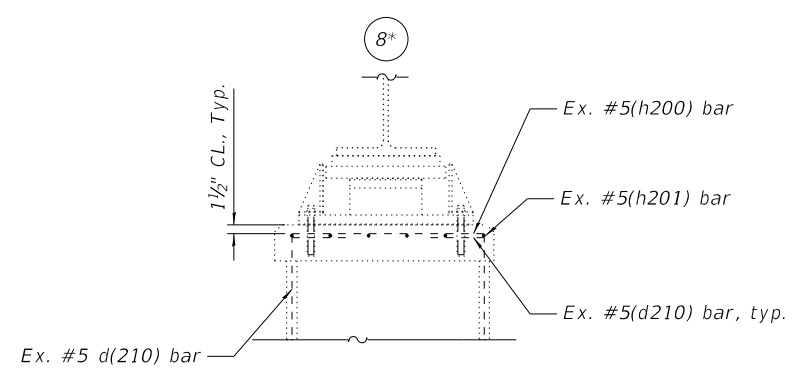
**ELEVATION**  
(Looking West)



**BEARING SEAT PLAN**



**BEARING SEAT REINFORCEMENT PLAN**



**BEARING SEAT ELEVATION**

**GIRDER SERVICE REACTIONS**

	E. Brg.	W. Brg.
R <sub>DL</sub> (K)	25.7	35.59
R <sub>LL</sub> (K)	35.5	61.27
R <sub>IMP</sub> (K)	9.2	20.22
R <sub>TOTAL</sub> (K)	70.4	117.08

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	233

**LEGEND**

Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)

**NOTES:**

1. Refer to sheet SB-29 for additional notes.
2. If the concrete deterioration at the bearing pedestal extends underneath existing steel bearing plates, temporary shoring is to be used to support the related beam while the concrete work is performed.
3. The jack capacity shall be designed to support the dead load and full live load and impact shown in plans.
4. A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.

MODEL: Default  
FILE NAME: P:\Projects\20000\2020\4\DISCIPLINE\Civil\Working\Sheets\SN 016-0202\0160202-42L31-030-E Pier Cap.dgn



USER NAME =	DESIGNED - IL/AM	REVISED -
PLOT SCALE =	CHECKED - TCG	REVISED -
PLOT DATE =	DRAWN - IL/AM	REVISED -
	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EAST PIER CAP DETAILS**  
**SN 016-0202**

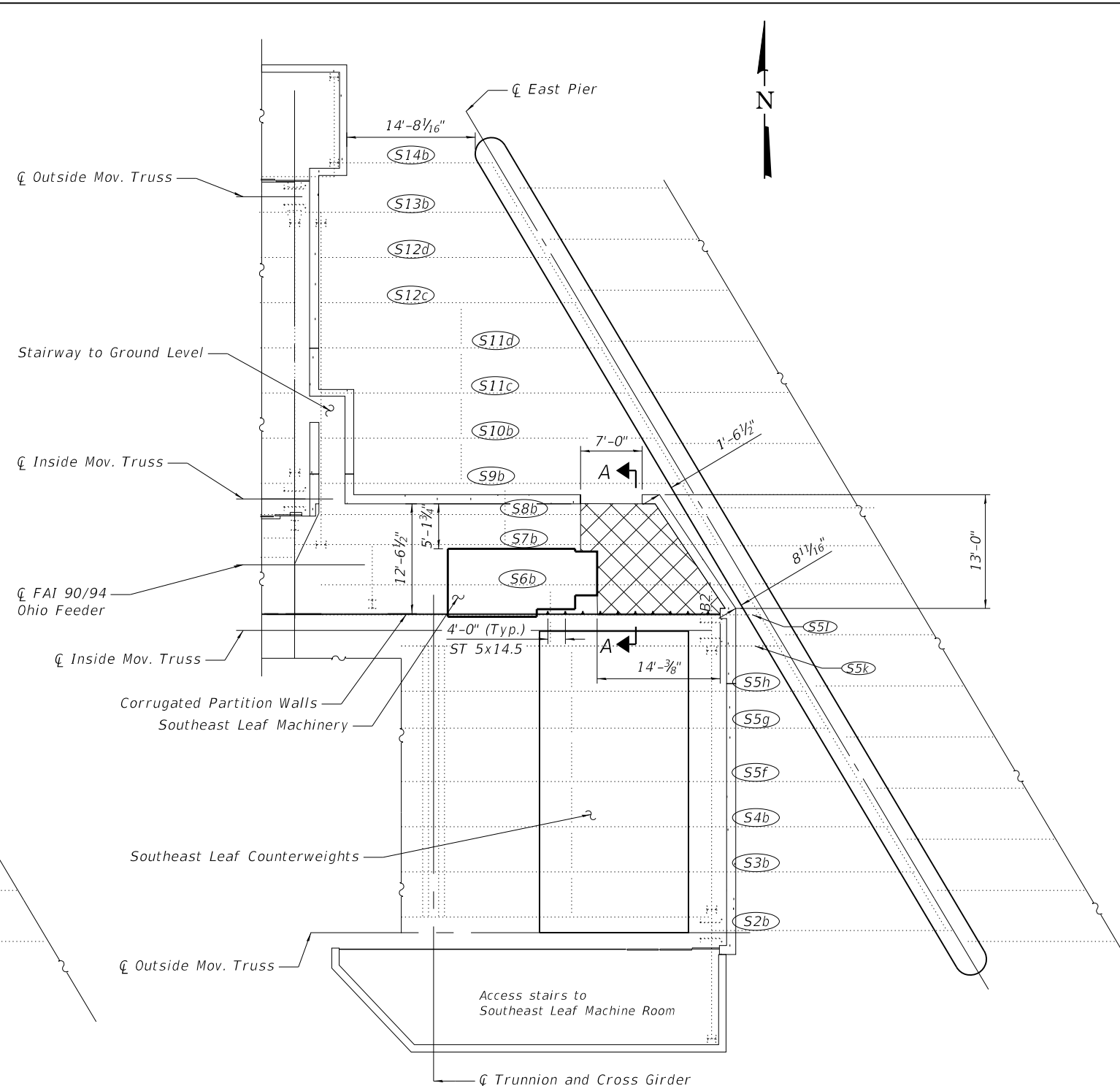
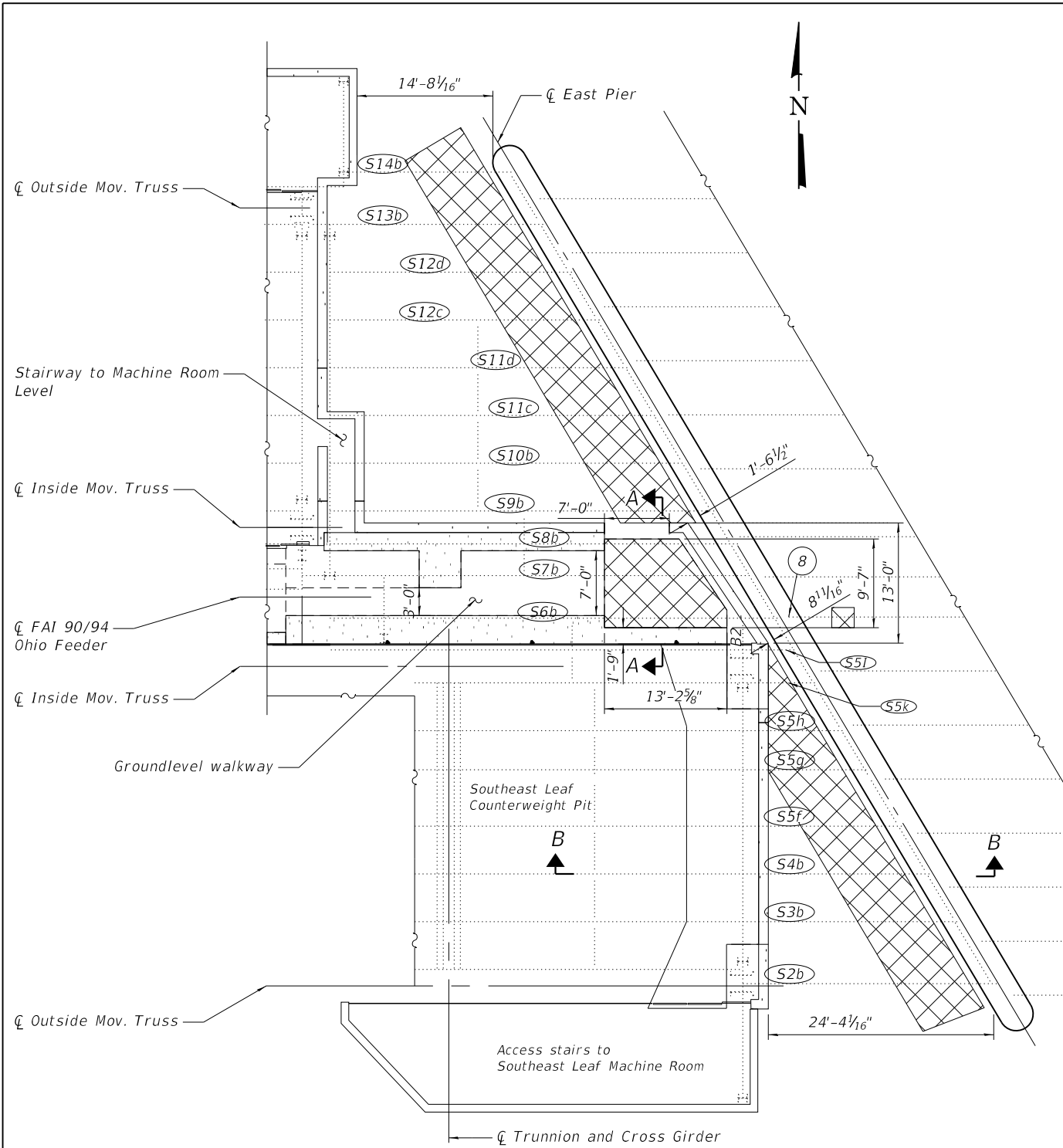
SHEET SB-30 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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
CONTRACT NO. 62L31

ILLINOIS FED. AID PROJECT

MODEL: Ohio Street  
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**LEGEND**

-  Suggested Area for Temporary Shoring and Cribbing.
-  Stringer Number



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PLOT SCALE =	DRAWN - IL/AM	REVISED -
PLOT DATE =	CHECKED - TCG	REVISED -

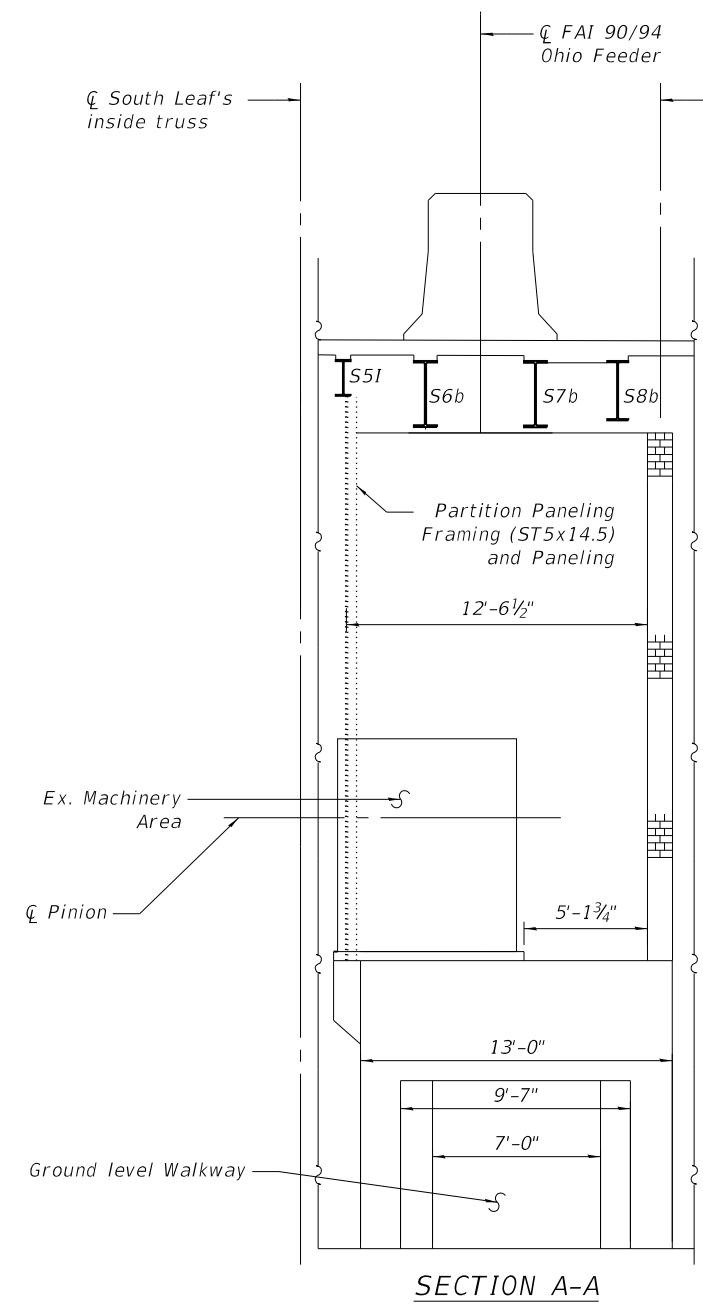
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EAST PIER TEMPORARY SHORING PLAN  
 SN 016-0202**

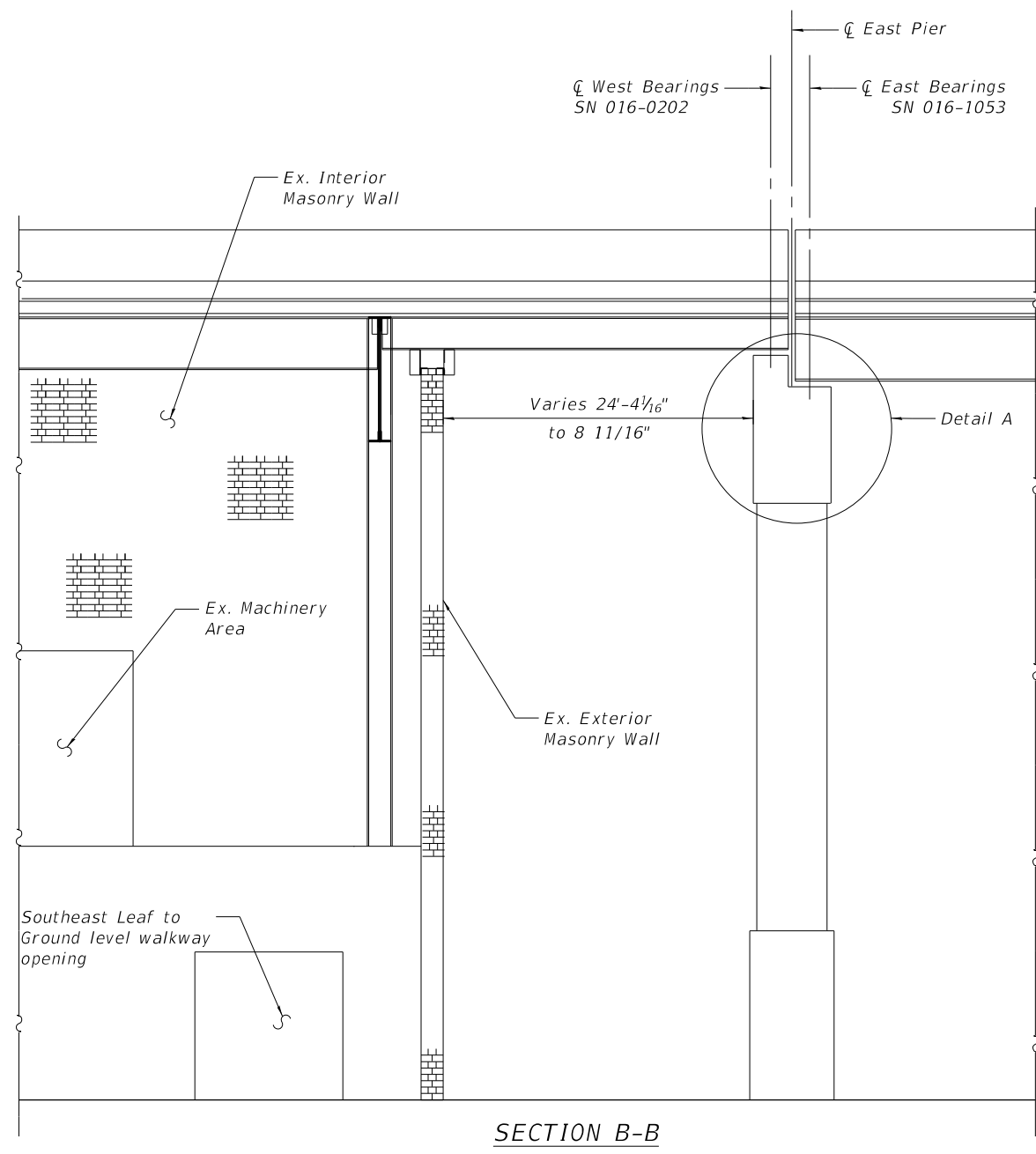
SHEET SB-31 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	110
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

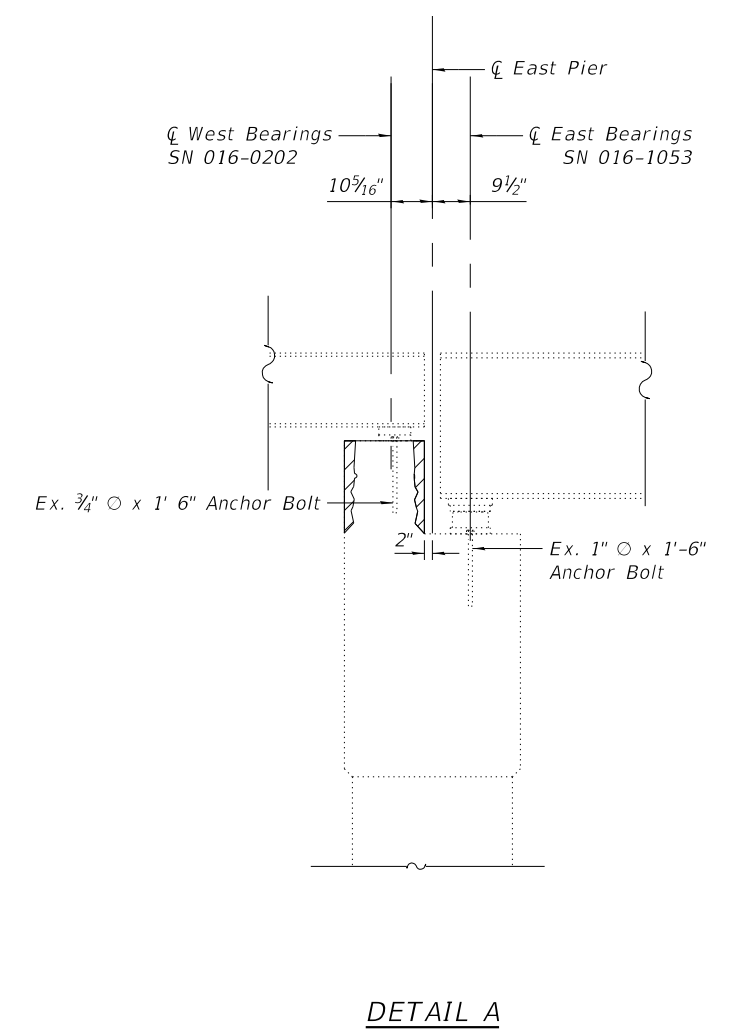
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SECTION A-A



SECTION B-B




DETAIL A

**NOTE:**

"Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

**LEGEND**

 Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)



USER NAME =	DESIGNED - IL/AIM	REVISED -
	CHECKED - TCG	REVISED -
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PLOT DATE =	CHECKED - TCG	REVISED -

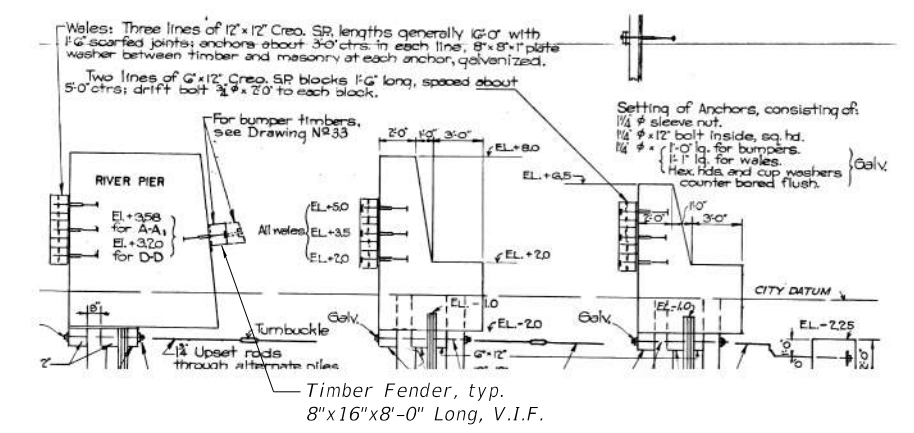
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EAST PIER JACKING AND SHORING SECTIONS  
 SN 016-0202**

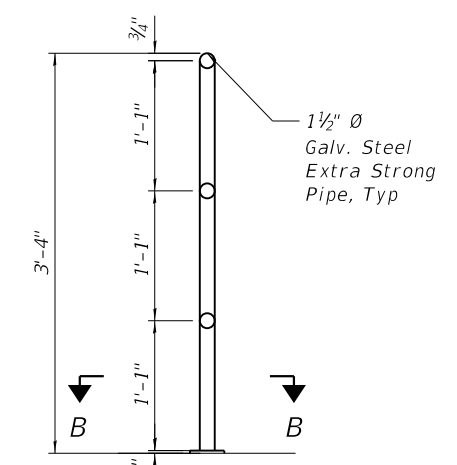
SHEET SB-32 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	111
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				

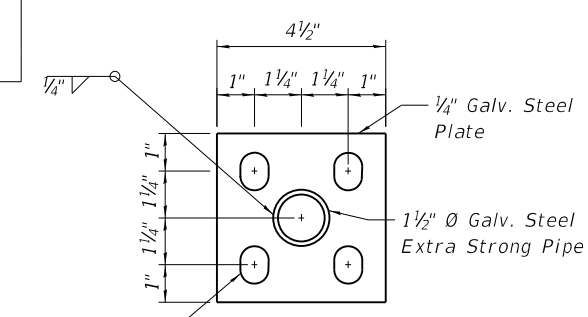
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**TIMBER FENDER AS BUILT DETAILS**  
 (For Information Only)

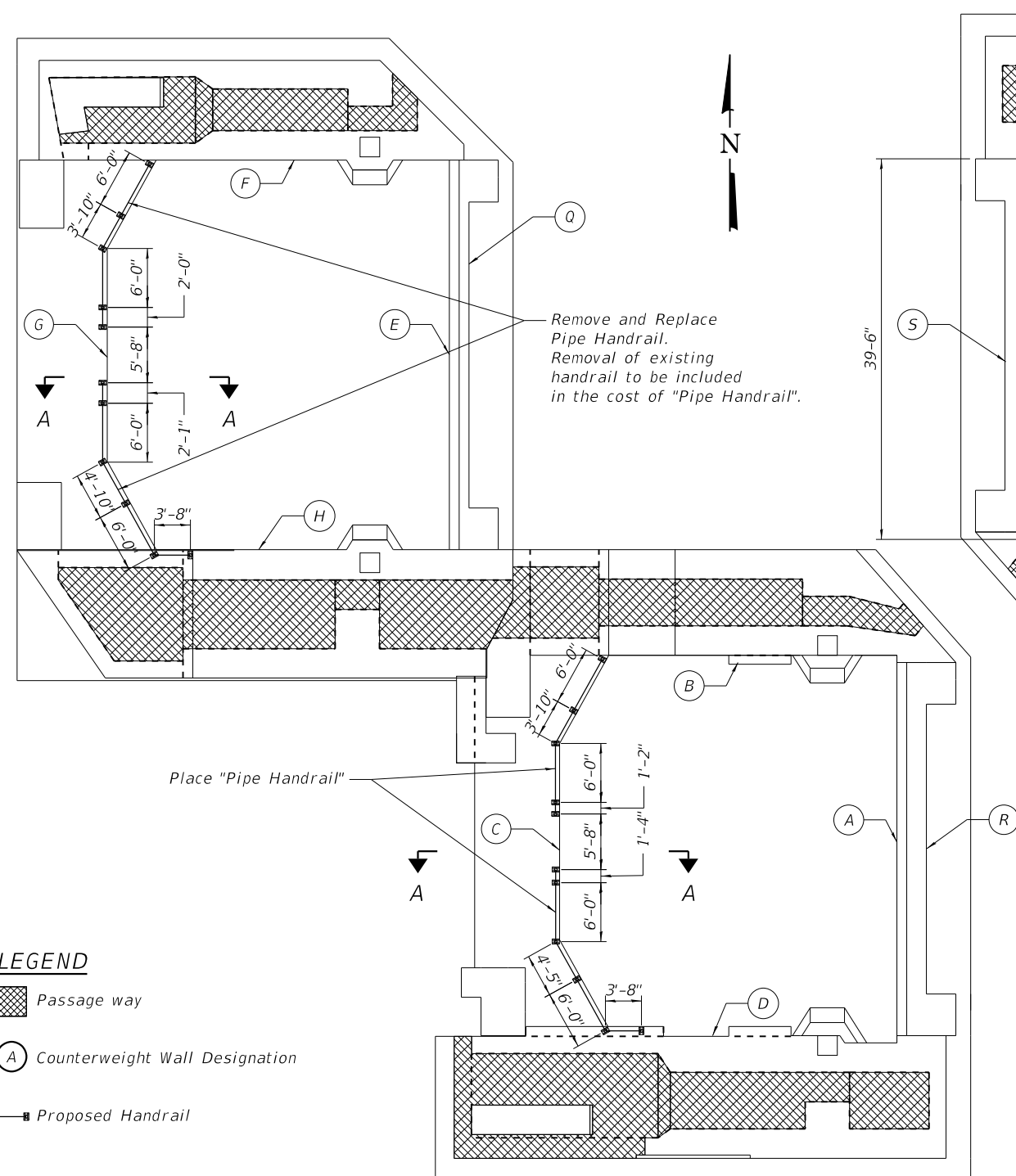


**SECTION A-A**



**SECTION B-B**

1" x 1 1/4" Slotted hole for  
 3/4" Ø Galv. Steel Anchor Rod.  
 Drill and Grout per Manufacture  
 Recommendations.



**PLAN OF UPPER MASONRY**

**NOTES:**

- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
- All debris and foreign materials shall be removed from horizontal surfaces at multiple heights within the counterweight pits. The work to be paid for as "Debris Removal".
- Once debris and foreign materials have been removed within the counterweigh pits, Contractor shall wash all surface areas on the counterweight pits. The work should be paid for as "Bridge Washing No. 1".



USER NAME =	DESIGNED - IL/AM	REVISED -
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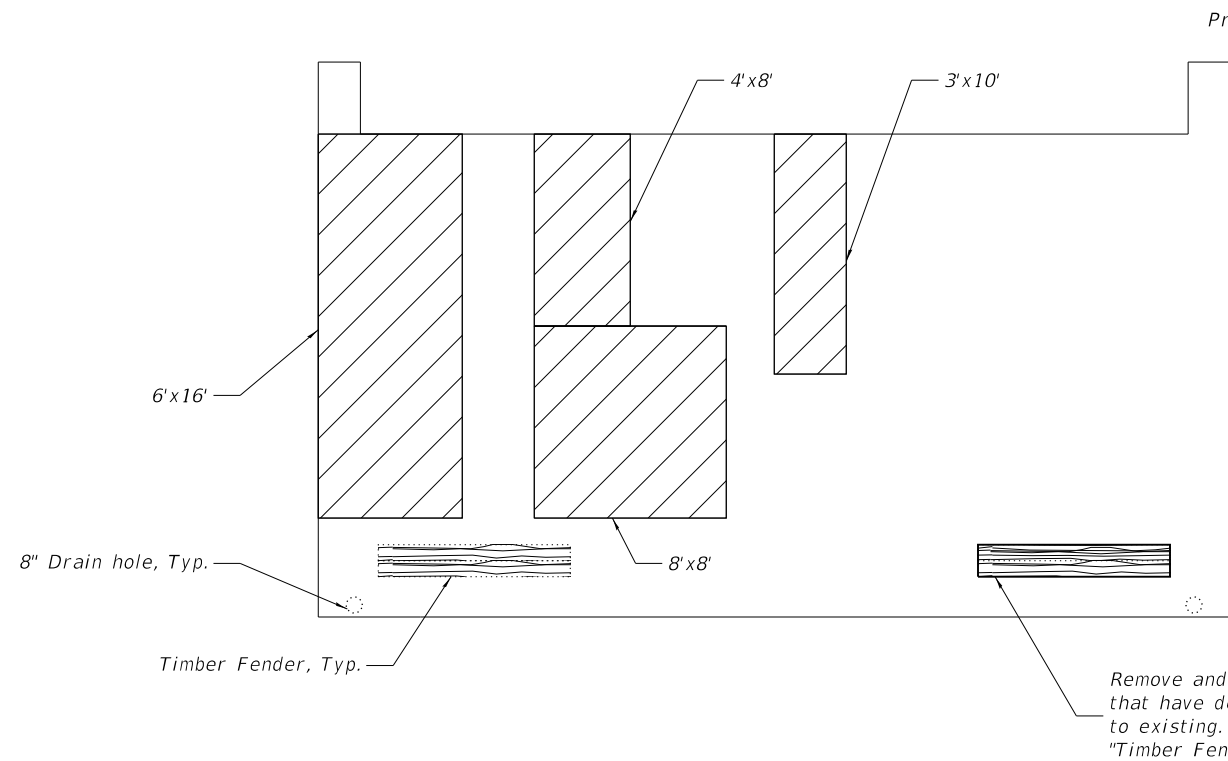
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**COUNTER WEIGHT PIT PLAN  
 SN 016-0202**

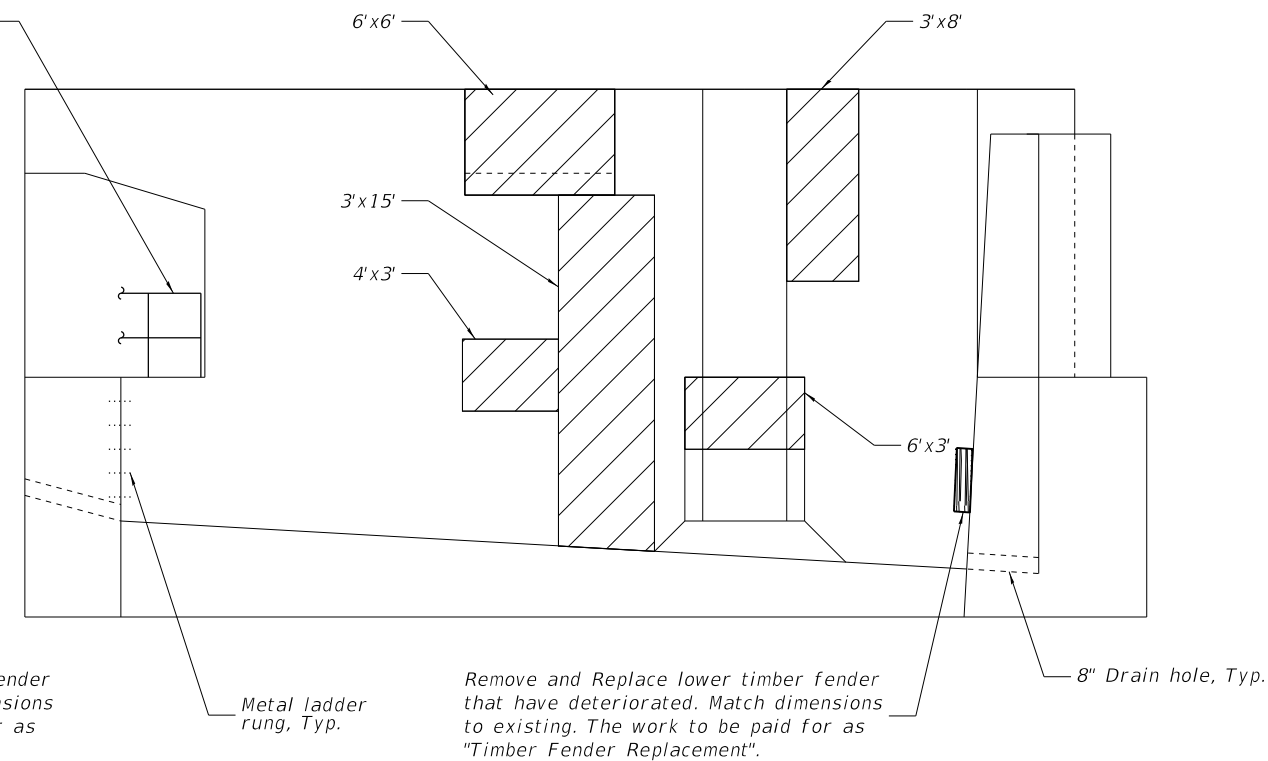
SHEET SB-33 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	

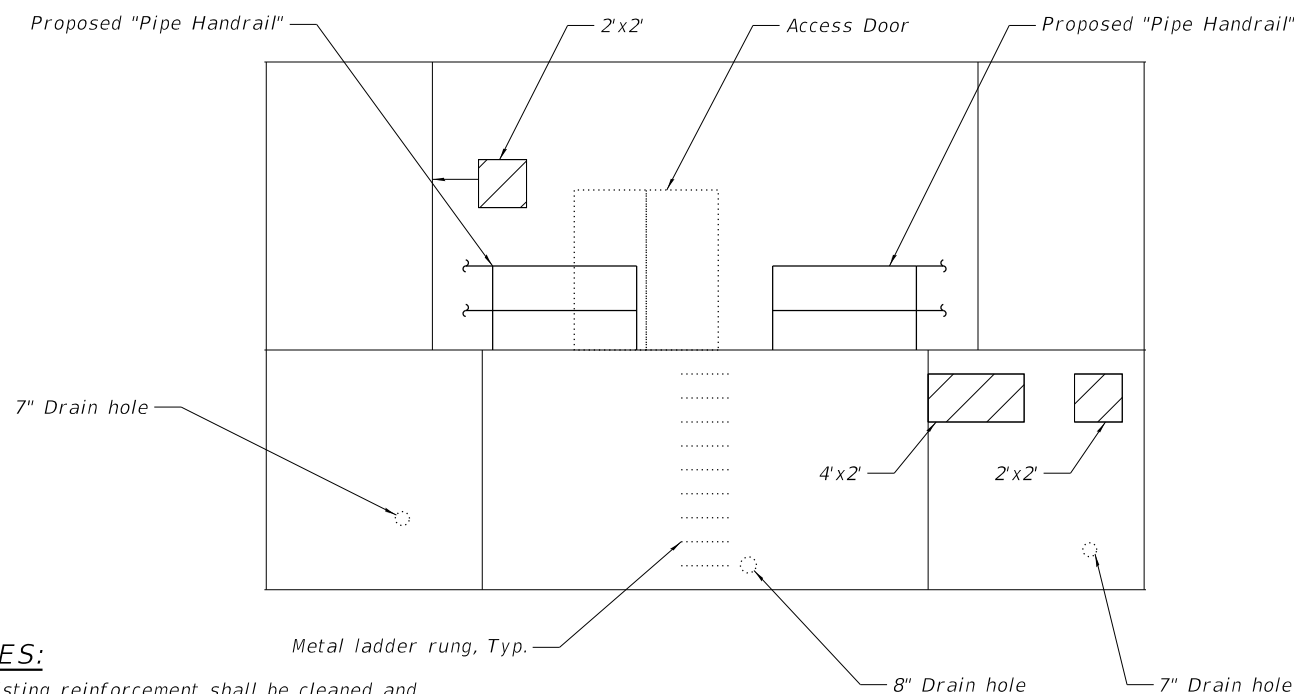
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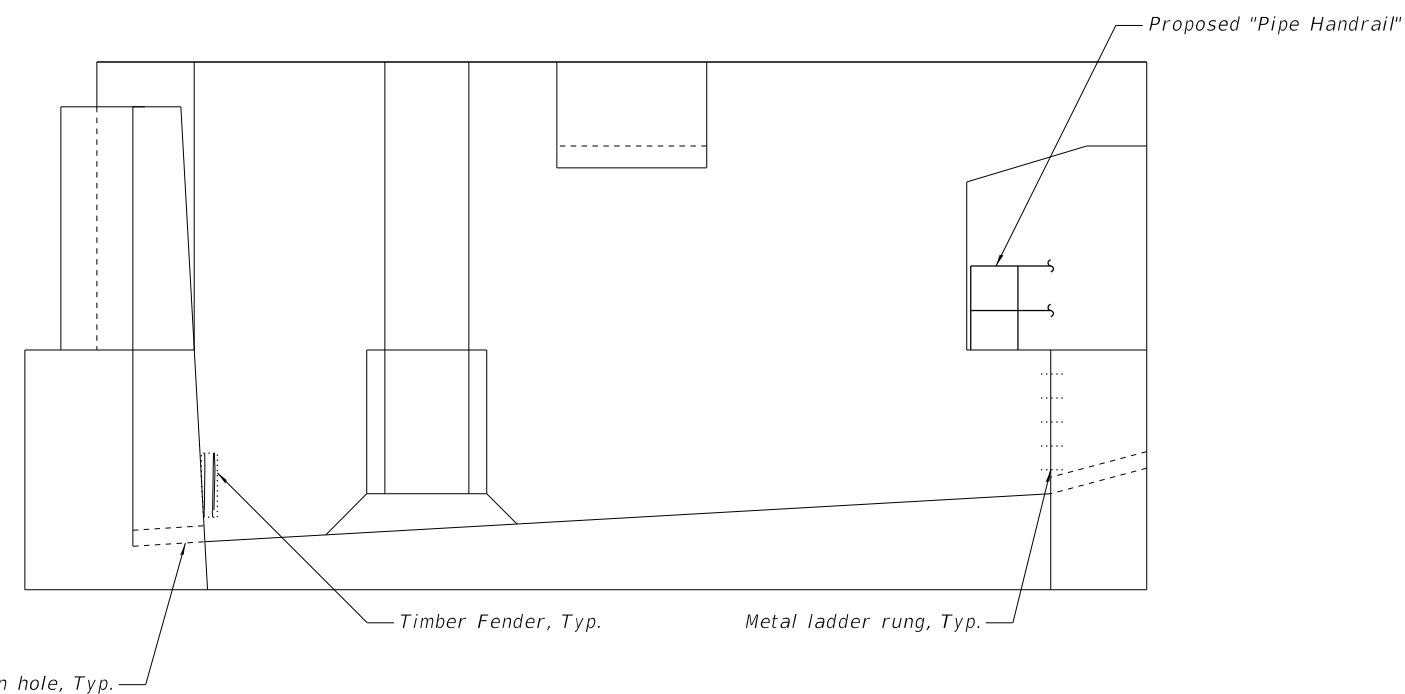
**WALL A ELEVATION**



**WALL B ELEVATION**



**WALL C ELEVATION**



**WALL D ELEVATION**


**NOTES:**

- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
- A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
- "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	324

**LEGEND**

 Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)



USER NAME =	DESIGNED - IL/AIM	REVISED -
	CHECKED - TCG	REVISED -
PLOT SCALE =	DRAWN - IL/AIM	REVISED -
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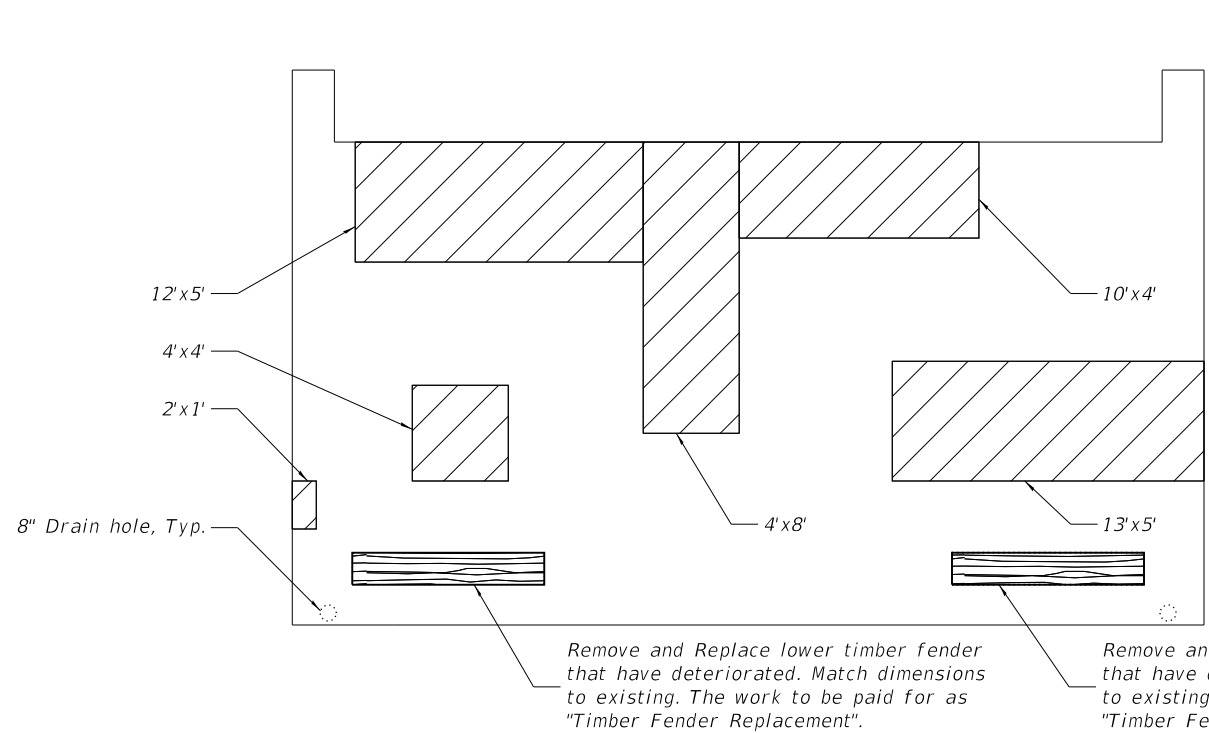
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SOUTHWEST COUNTER WEIGHT PIT ELEVATIONS  
 SN 016-0202**

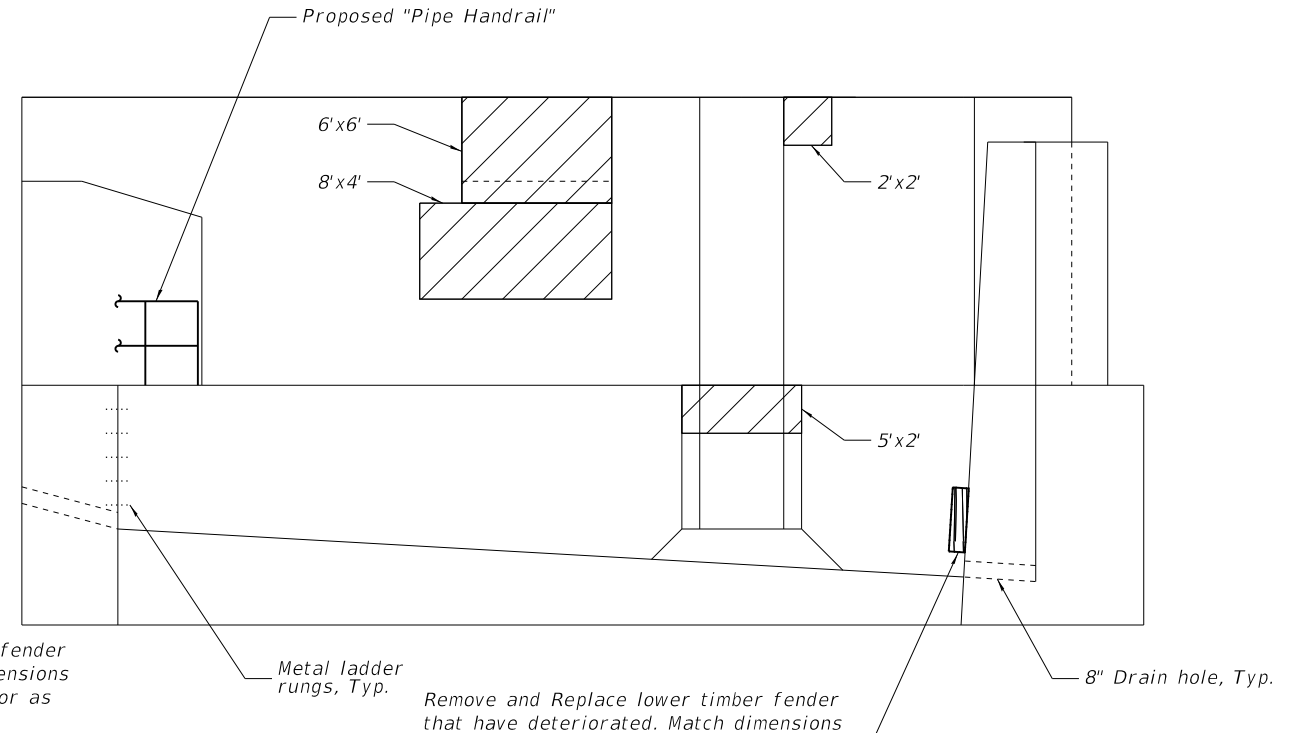
SHEET SB-34 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	113
CONTRACT NO. 62L31				
ILLINOIS		FED. AID PROJECT		

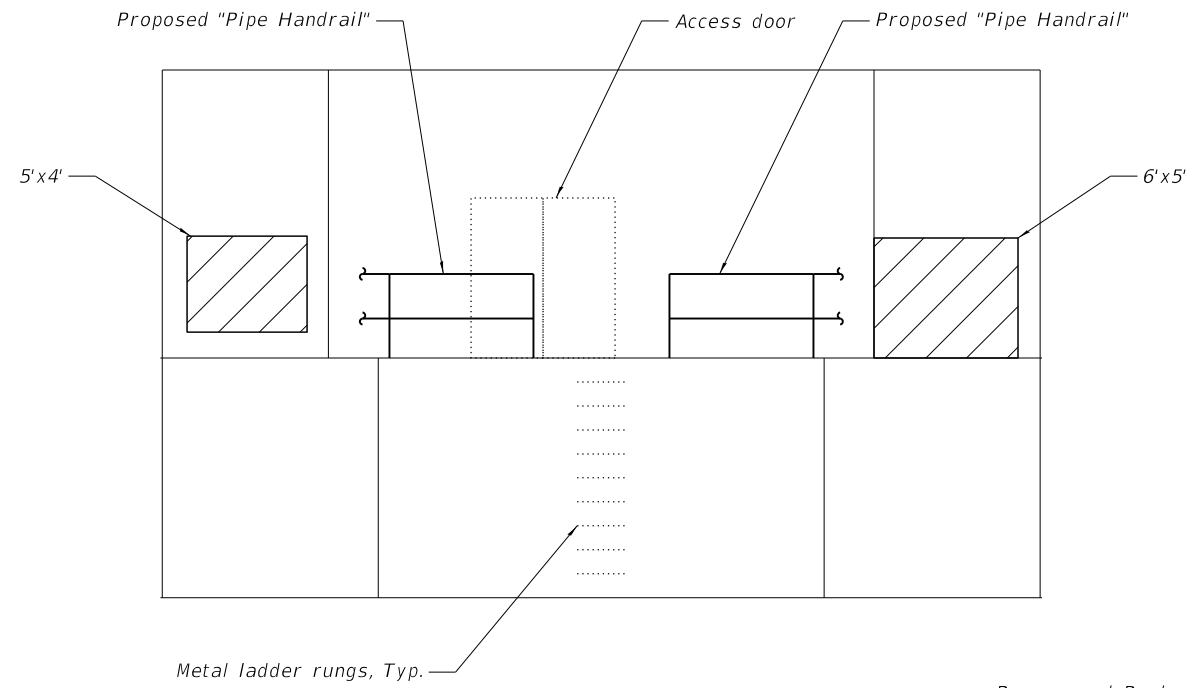
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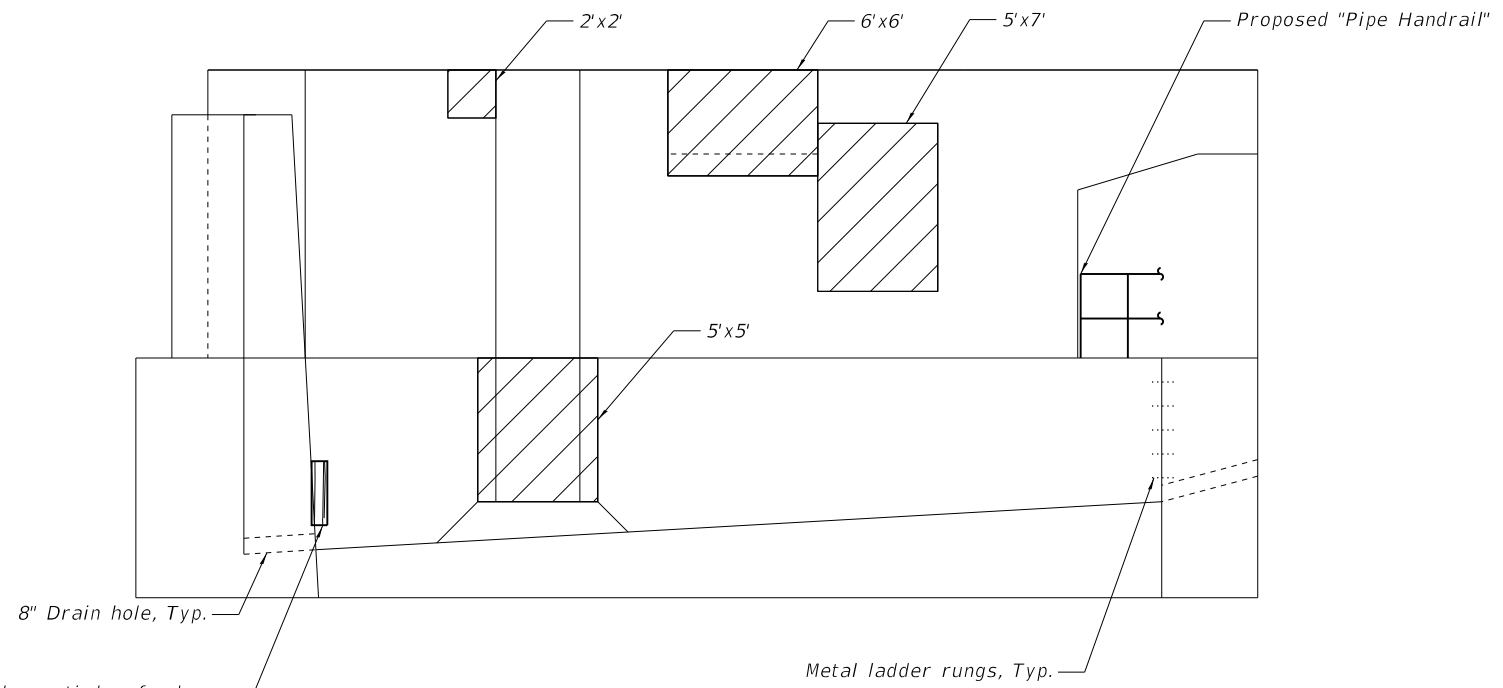
**WALL E ELEVATION**



**WALL F ELEVATION**



**WALL G ELEVATION**



**WALL H ELEVATION**

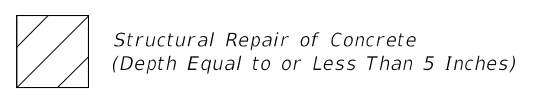
**NOTES:**

- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
- A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
- "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	447

**LEGEND**



USER NAME =	DESIGNED - IL/AM	REVISED -
	CHECKED - TCG	REVISED -
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PLOT DATE =	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

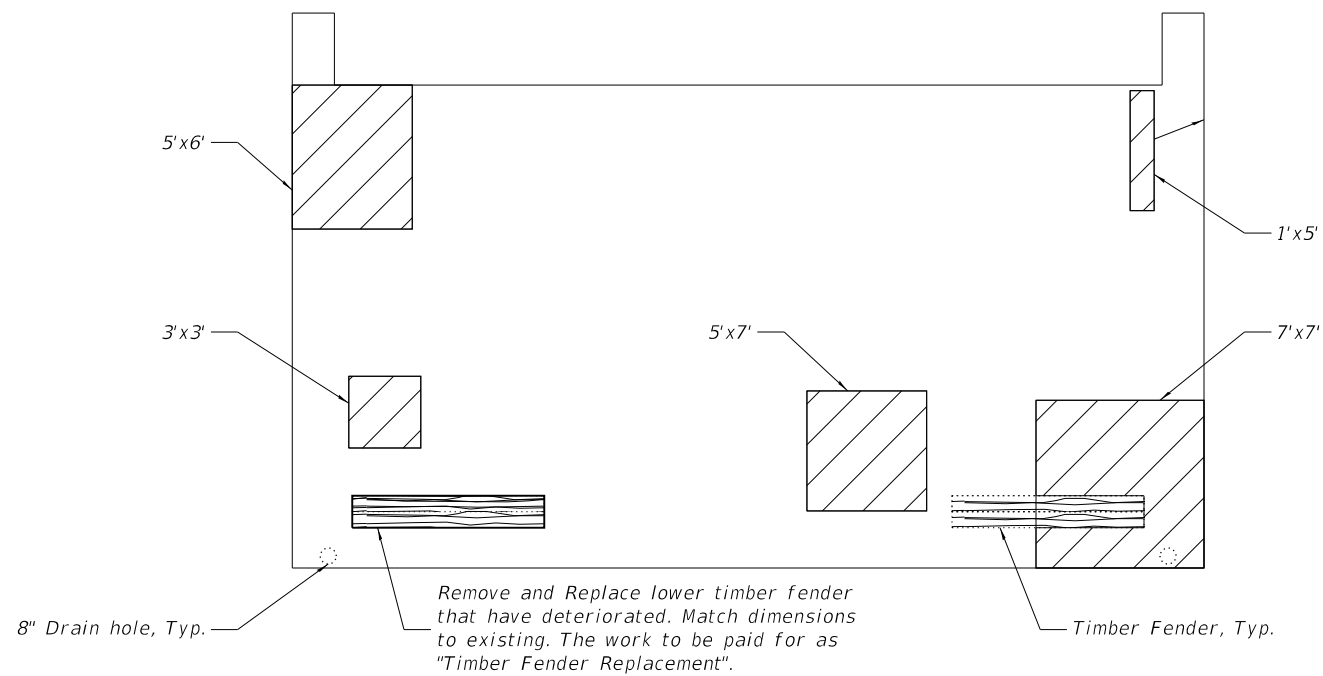
**NORTHWEST COUNTER WEIGHT PIT ELEVATIONS  
 SN 016-0202**

SHEET SB-35 OF SB-48 SHEETS

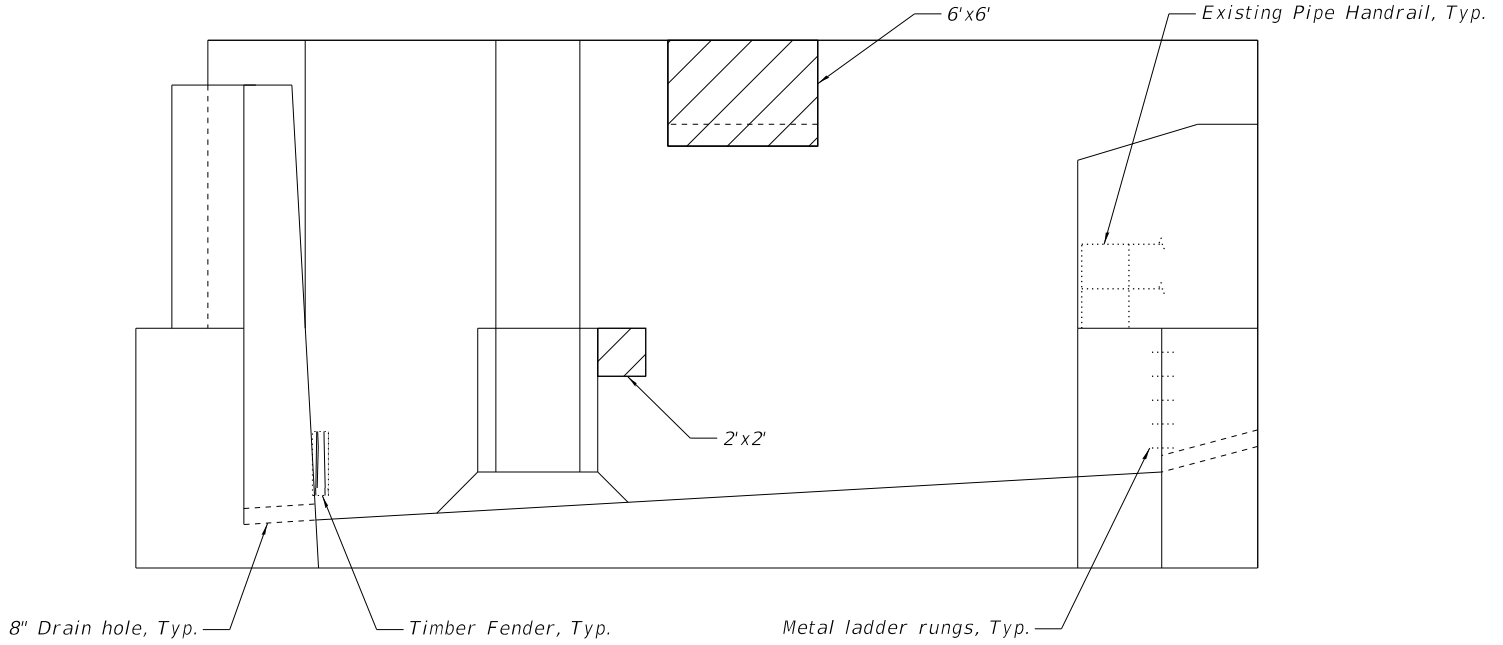
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383	2020-143-BR&BJR	COOK	177	114
CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	



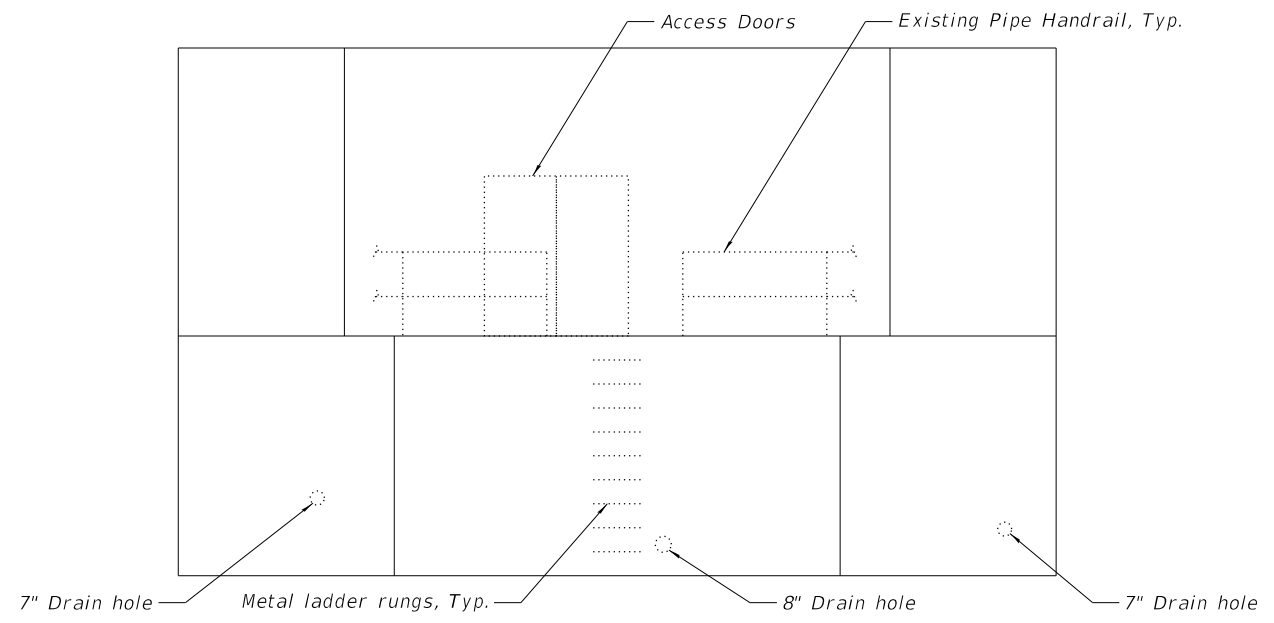
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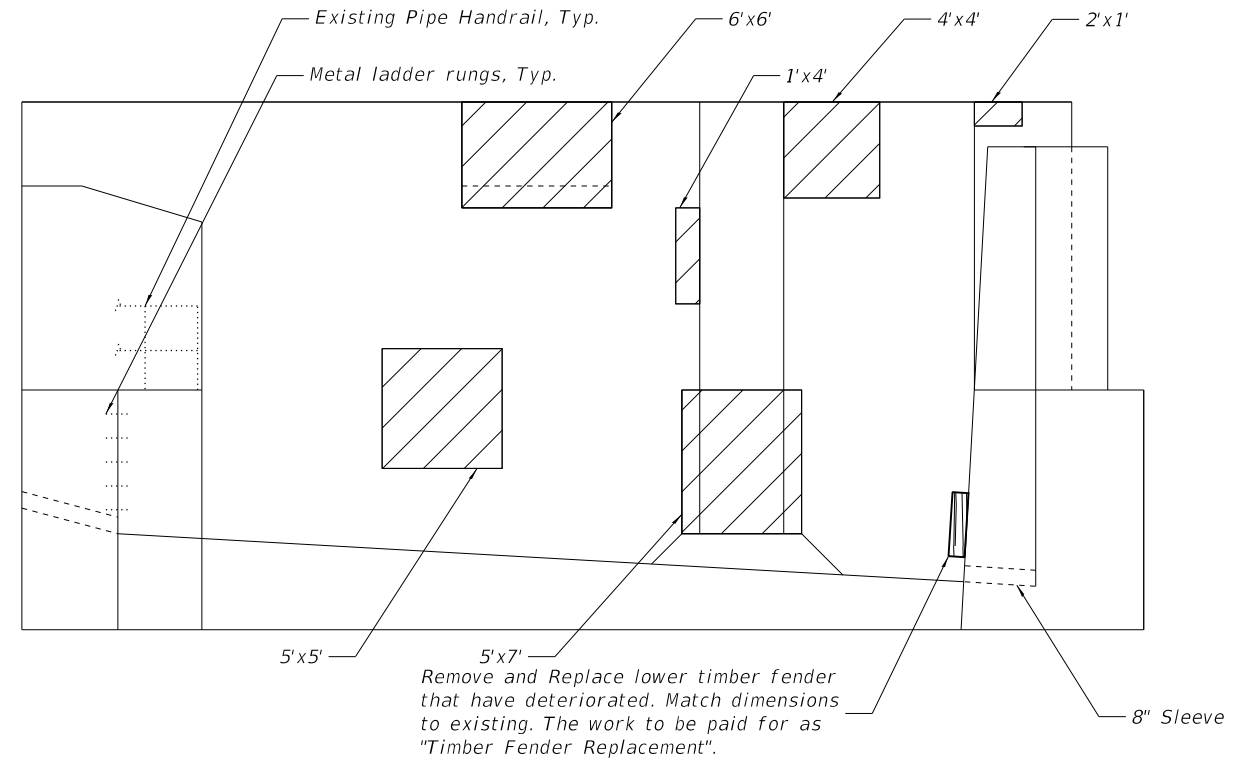
**WALL I ELEVATION**



**WALL J ELEVATION**



**WALL K ELEVATION**



**WALL L ELEVATION**

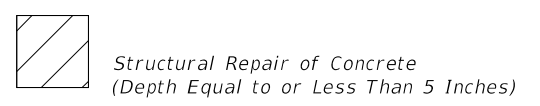
**NOTES:**

- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
- A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
- "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	286

**LEGEND**



USER NAME =	DESIGNED - IL/AM	REVISED -
	CHECKED - TCG	REVISED -
PLOT SCALE =	DRAWN - IL/AM	REVISED -
PLOT DATE =	CHECKED - TCG	REVISED -

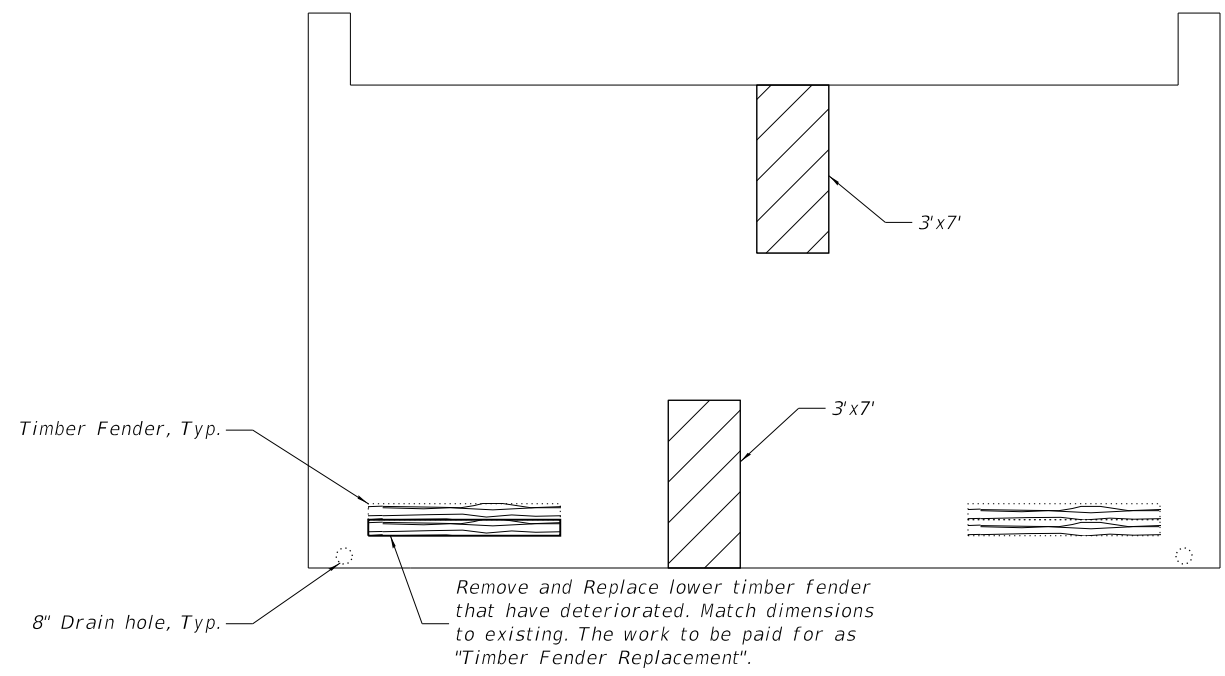
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**NORTHEAST COUNTER WEIGHT PIT ELEVATIONS  
 SN 016-0202**

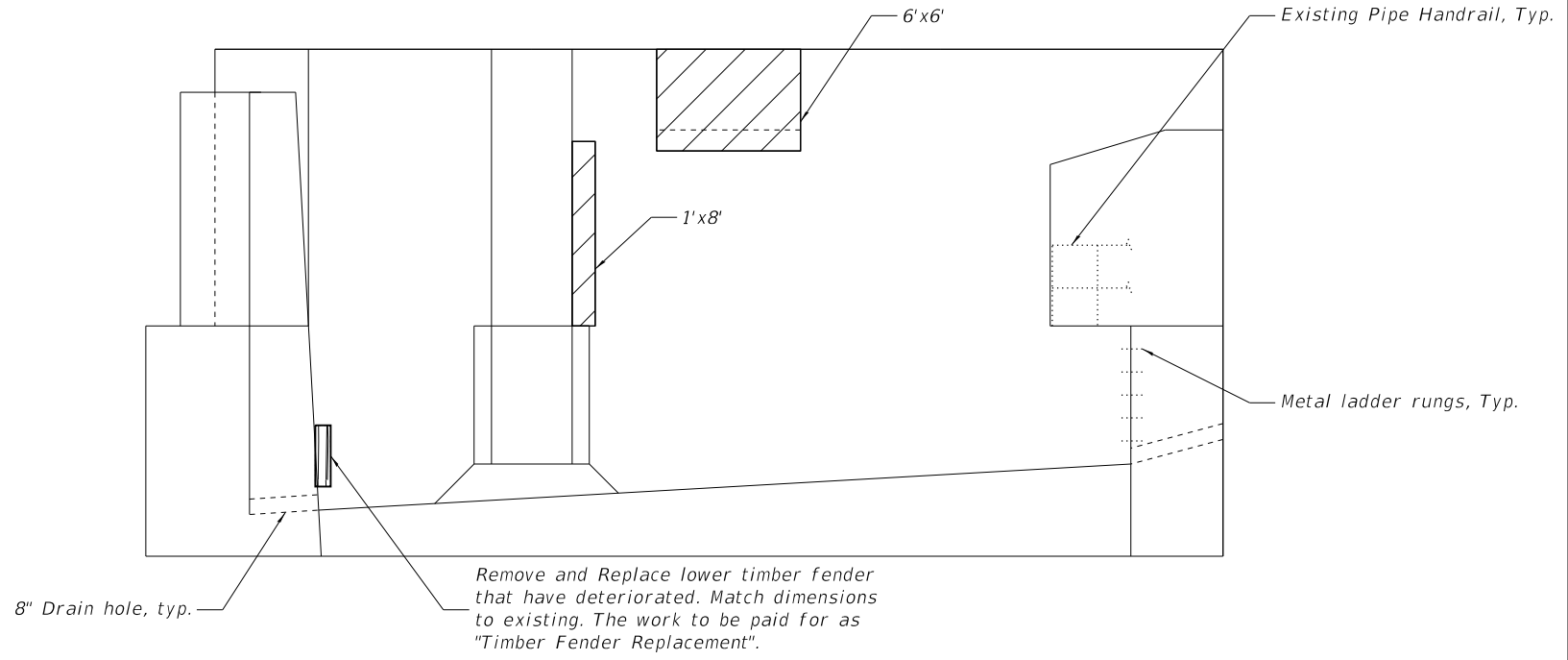
SHEET SB-36 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	115
CONTRACT NO. 62L31				

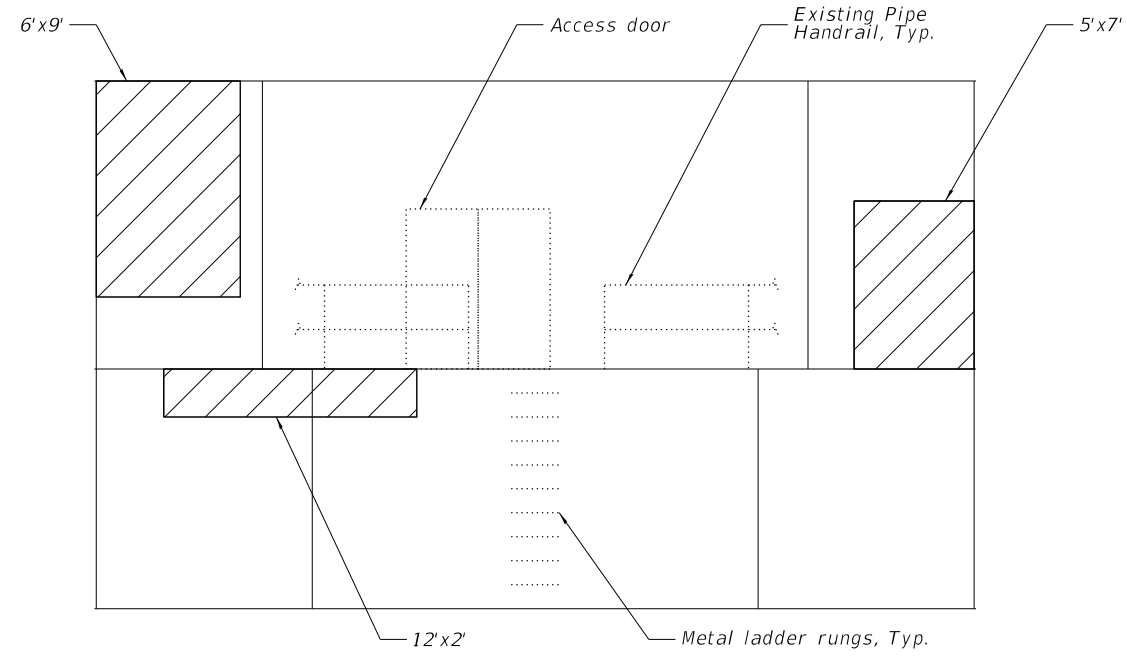
ILLINOIS FED. AID PROJECT



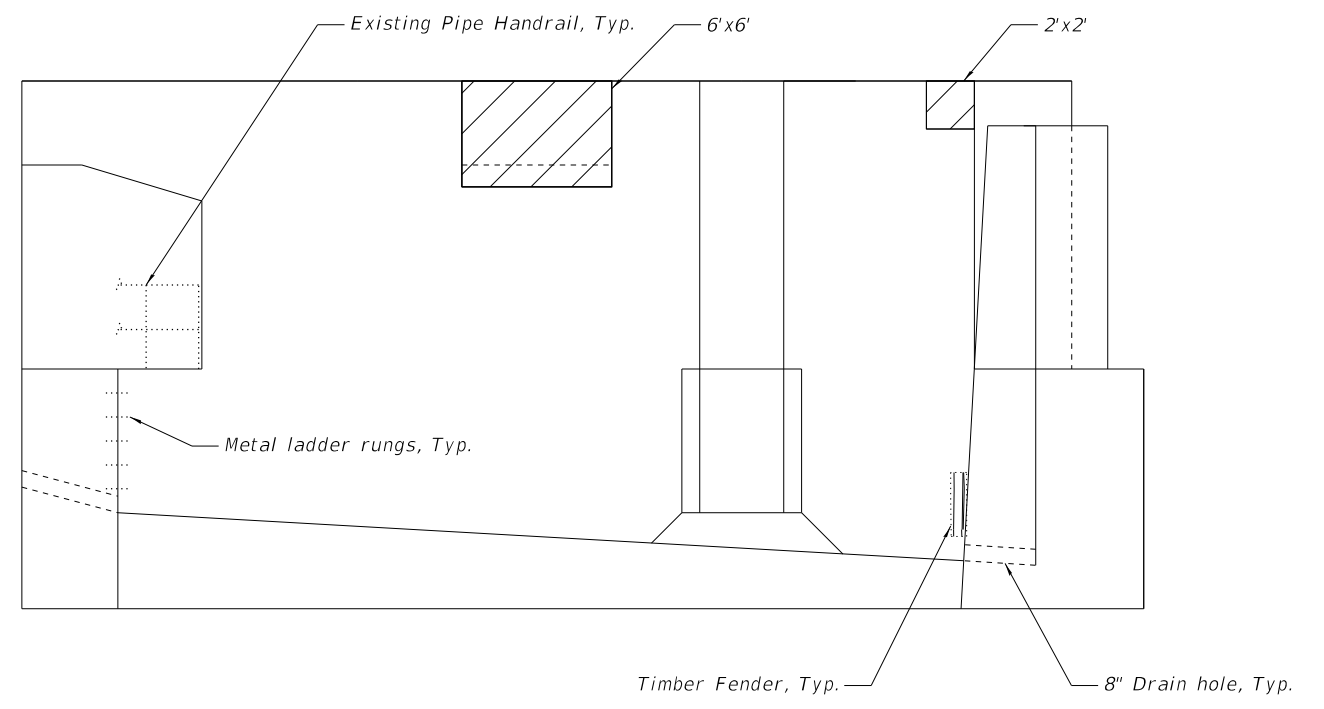
**WALL M ELEVATION**



**WALL N ELEVATION**



**WALL O ELEVATION**



**WALL P ELEVATION**

**NOTES:**

- Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
- A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
- "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	239

**LEGEND**

Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)

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USER NAME =	DESIGNED - IL/AM	REVISED -
	CHECKED - TCG	REVISED -
PLOT SCALE =	DRAWN - IL/AM	REVISED -
PLOT DATE =	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SOUTHEAST COUNTER WEIGHT PIT ELEVATIONS  
SN 016-0202**

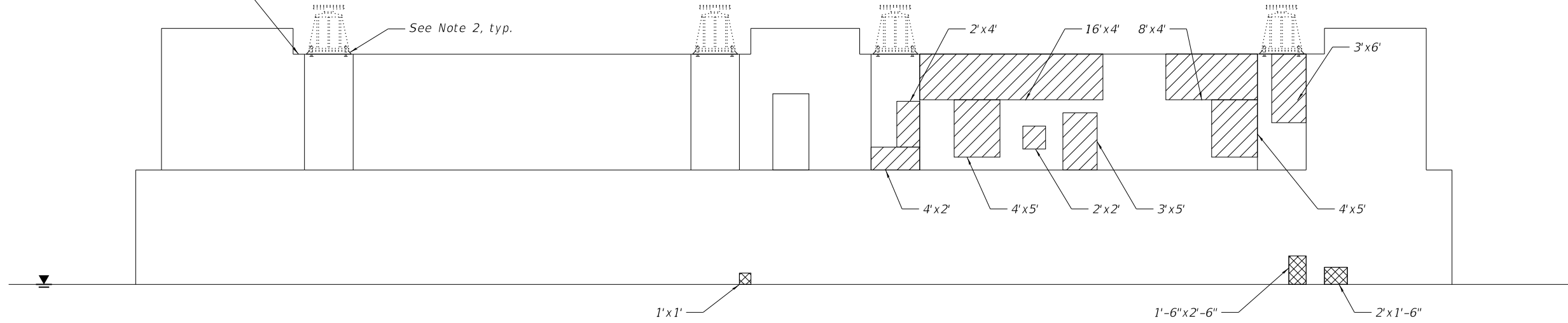
SHEET SB-37 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	116
CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT

4'x3' Area on all Live Load Bearings

See Note 2, typ.



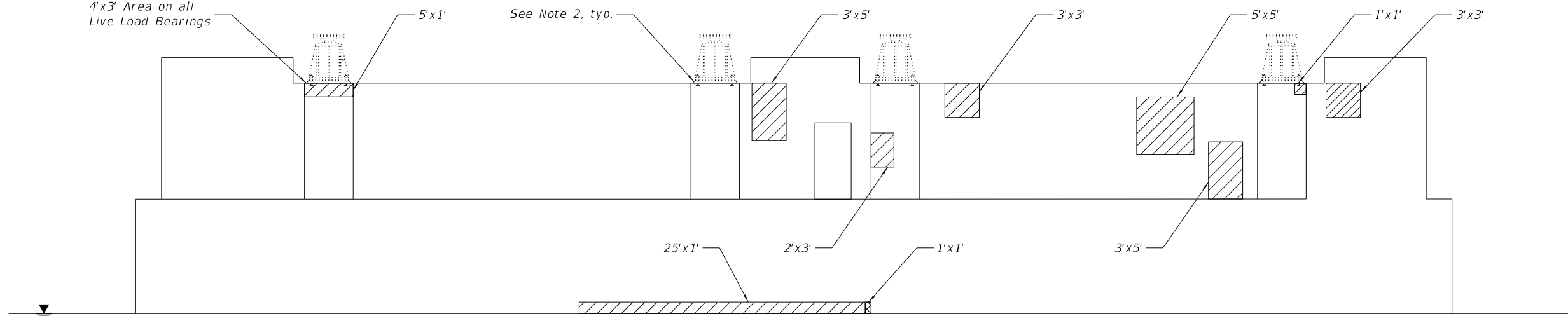
WALL R ELEVATION

WEST RIVER PIER ELEVATION

WALL Q ELEVATION

4'x3' Area on all Live Load Bearings

See Note 2, typ.



WALL S ELEVATION

EAST RIVER PIER ELEVATION

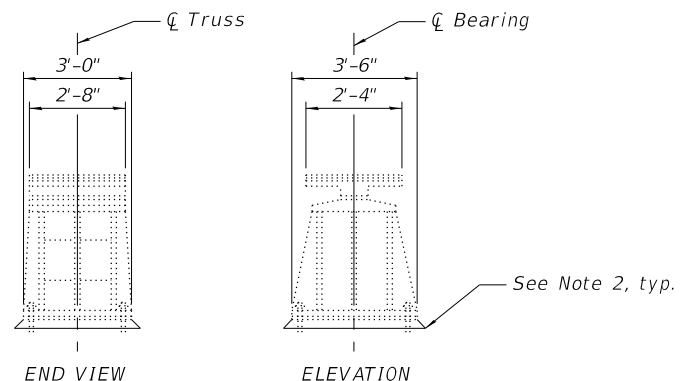
WALL T ELEVATION

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- Structural Repair of Concrete (Depth Greater than or Equal to 5 Inches)

NOTES:

1. Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with "Concrete Removal".
2. Replace missing, loose or deteriorated self-leveling non-shrink grout at all live load bearings. Material per Article 1024.02 of the 2022 IDOT SSRBC. Cost included with "Structural Repair of Concrete (depth equal to or less than 5 inches)." Work to be performed with no live load on the structure.
3. A film forming "Concrete Sealer" shall be applied to the pier concrete repairs.
4. "Structural Repair of Concrete (Depth equal to or less than 5 inches)" and "Structural Repair of Concrete (Depth greater than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.



LIVE LOAD PEDESTAL DETAILS

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	395
Structural Repair of Concrete (Depth greater than 5 inches)	Sq. Ft.	9

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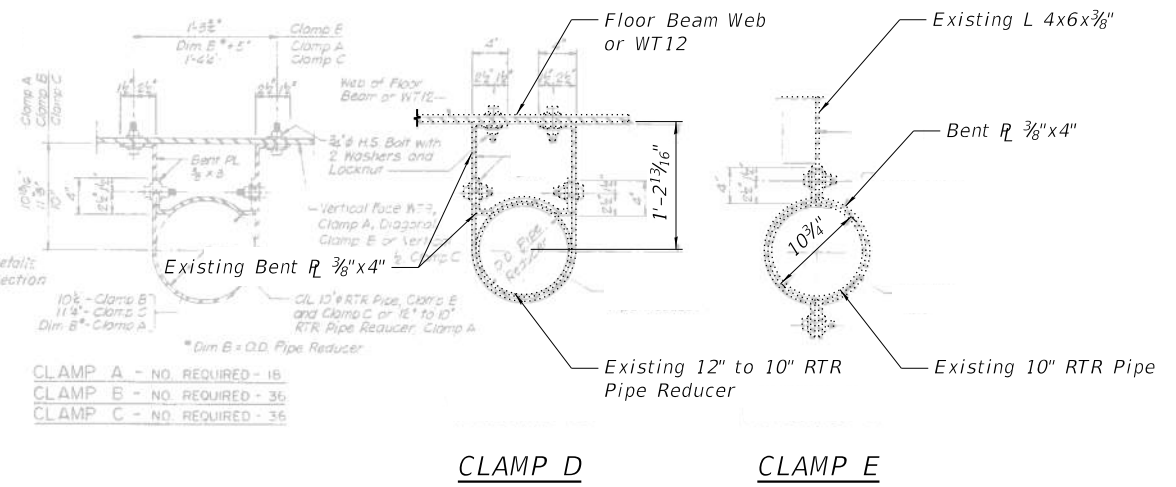
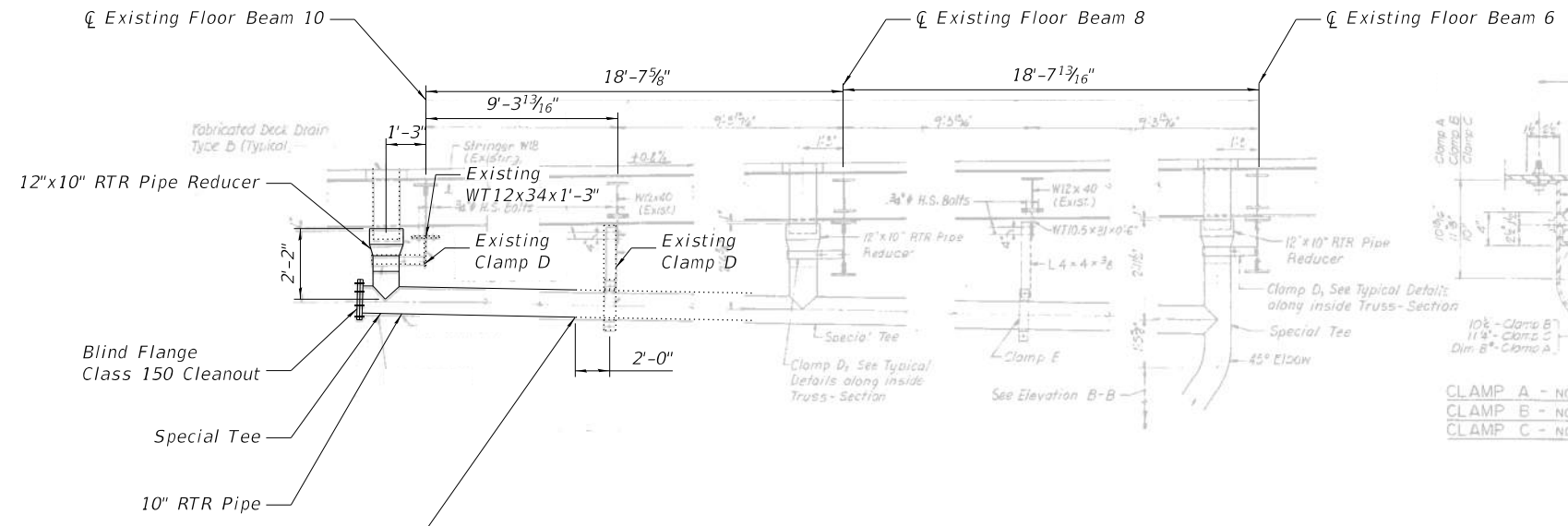
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RIVER FACING COUNTER WEIGHT PIT ELEVATIONS  
SN 016-0202

SHEET SB-38 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	117
CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT



S.E. LEAF DETAILS ALONG INSIDE TRUSS

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Bridge Drainage System Repair	Sq. Ft.	12

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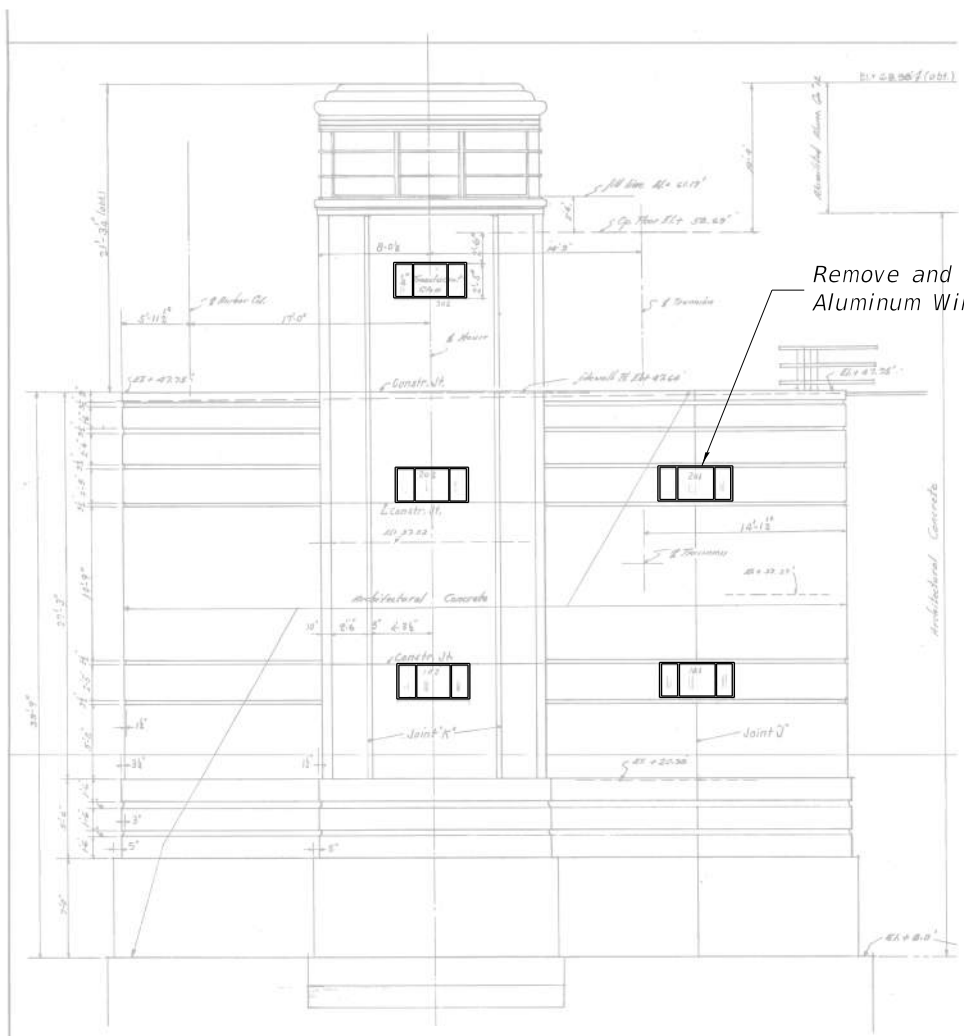
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

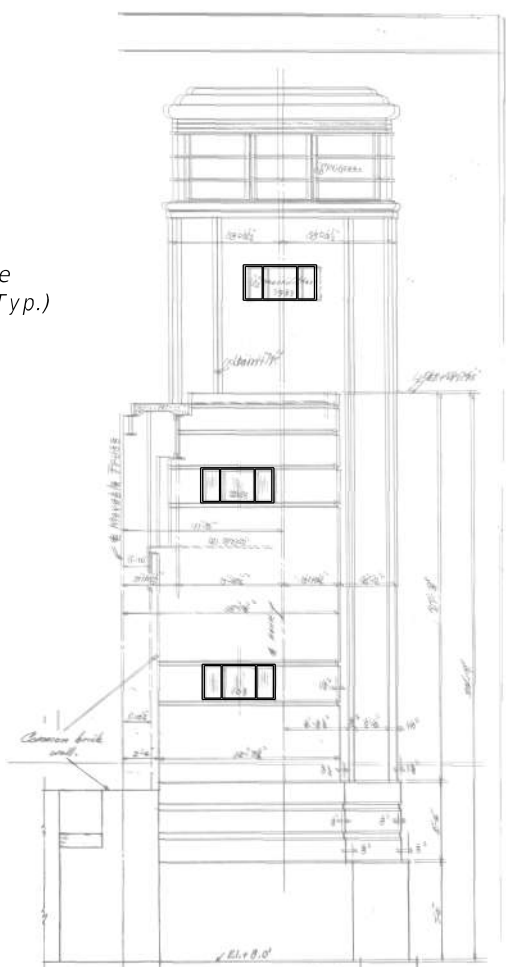
DRAINAGE SYSTEM REPAIRS  
SN 016-0202

SHEET SB-39 OF SB-48 SHEETS

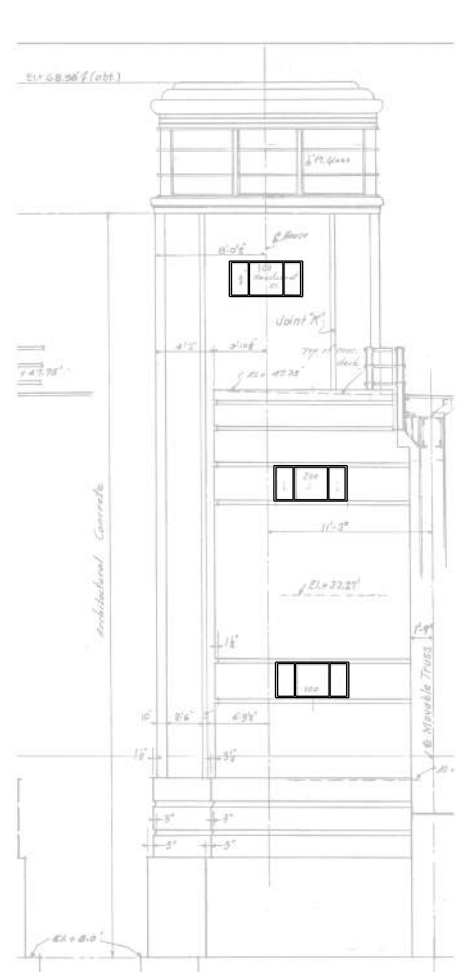
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	118
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



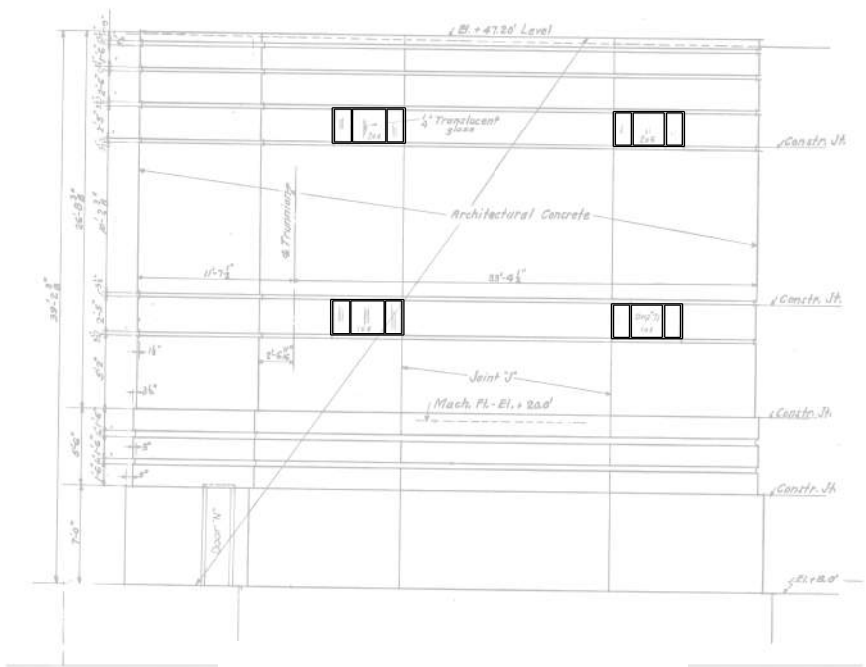
VIEW A-A



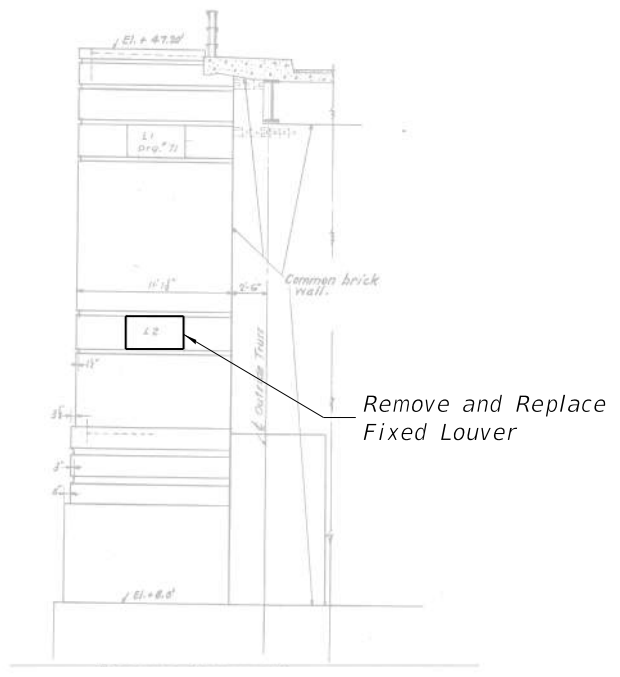
VIEW B-B



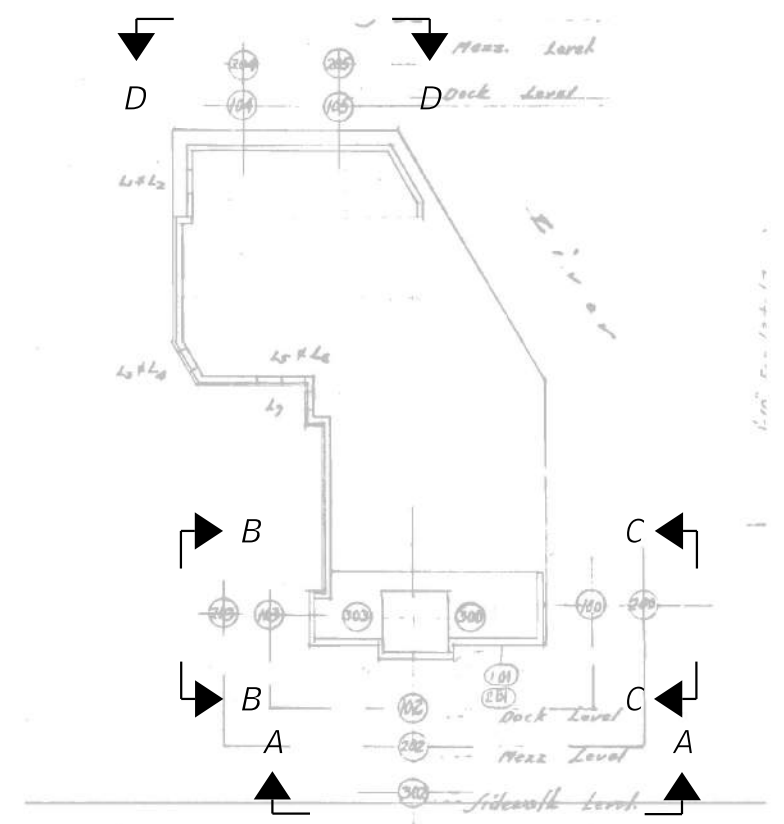
VIEW C-C



VIEW D-D



ENCLOSURE- APPROACH VIEW



KEY PLAN

MODEL: Default  
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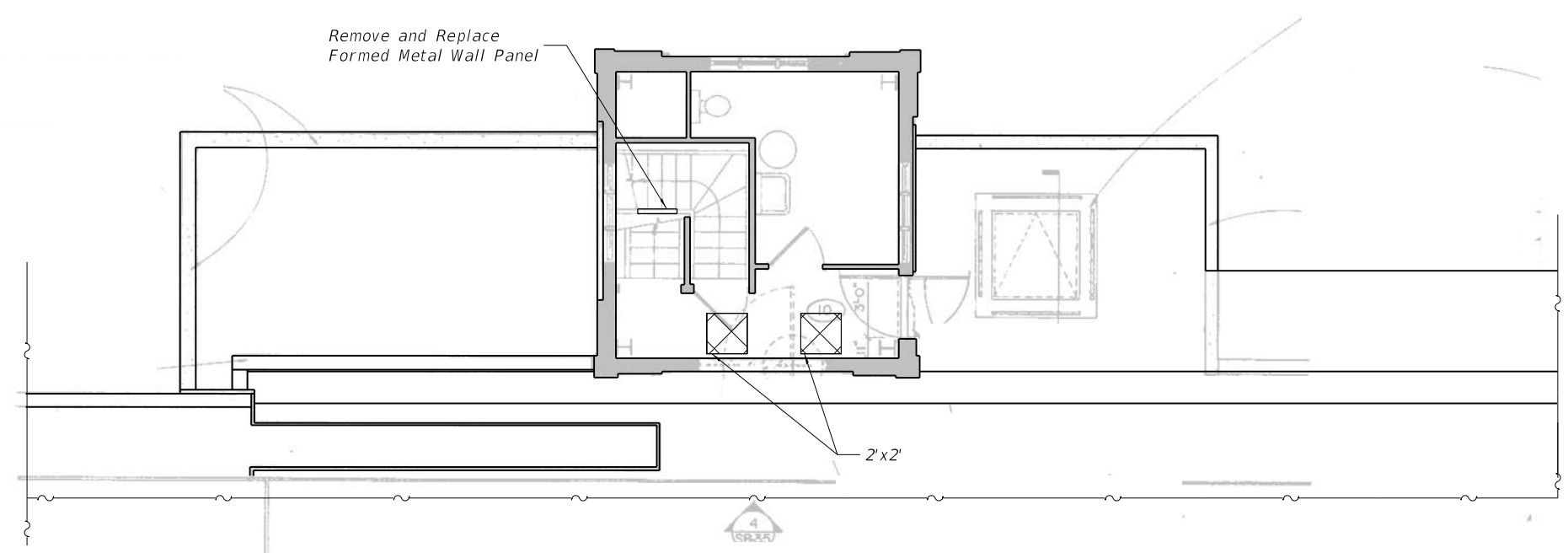
OPERATOR HOUSE ELEVATIONS  
 SN 016-0202

SHEET SB-40 OF SB-48 SHEETS

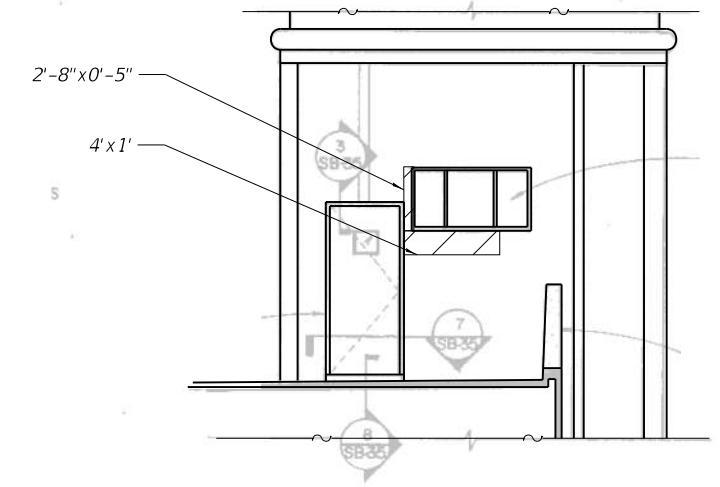
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383	2020-143-BR&BJR	COOK	177	119
CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT

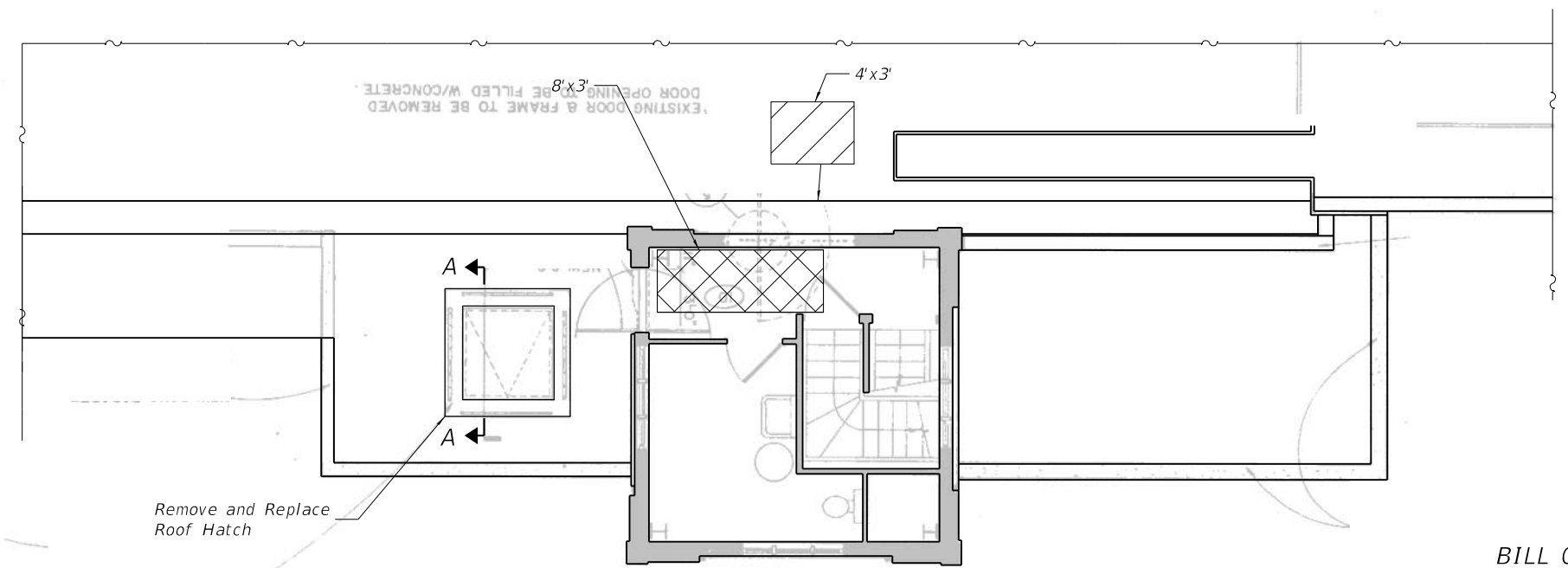
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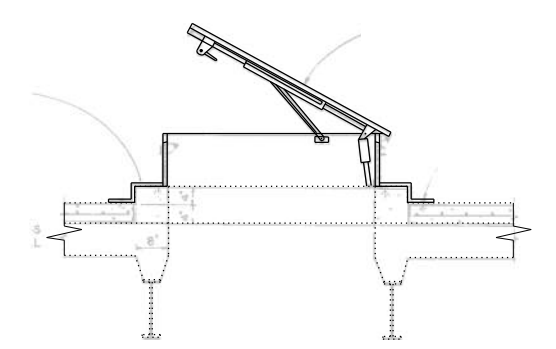
NORTHEAST OPERATORS HOUSE PLAN



ELEVATION-OPERATORS HOUSE



SOUTHWEST OPERATORS HOUSE PLAN





SECTION A-A  
 (All necessary components to reattach to existing roof included in "Roof Hatch")

BILL OF MATERIAL


ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	18

LEGEND

-  Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
-  Deck Slab Repair (Partial)

NOTE:

1. A film forming "Concrete Sealer" shall be applied to the exterior concrete repairs.
2. "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

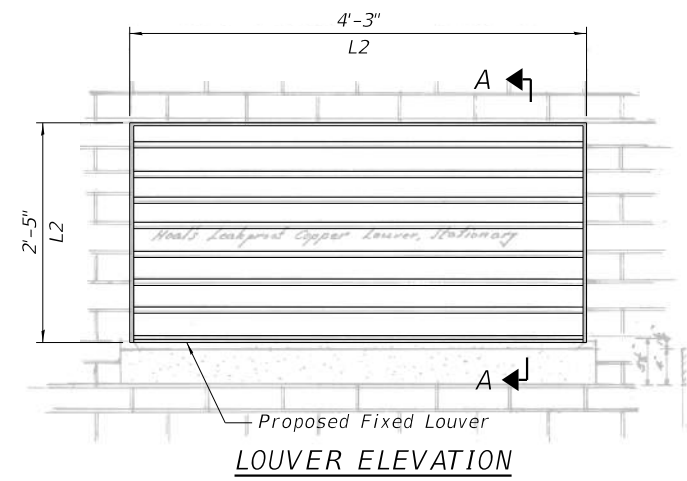
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 DEPARTMENT OF TRANSPORTATION**

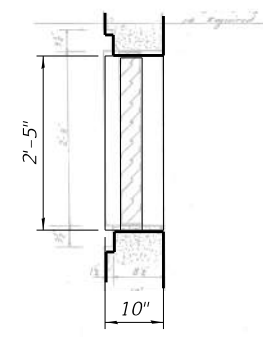
**OPERATOR HOUSE FLOOR AND WALL CONCRETE REPAIRS  
 SN 016-0202**

SHEET SB-41 OF SB-48 SHEETS

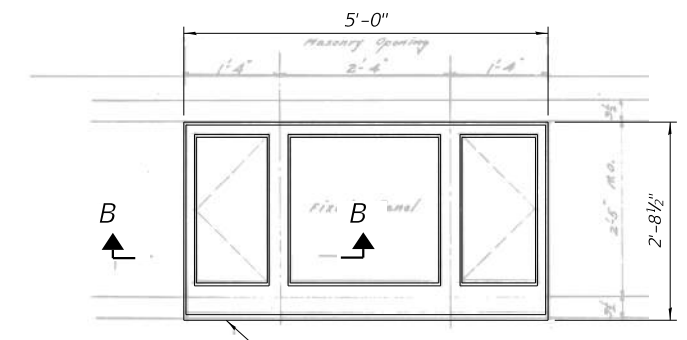
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



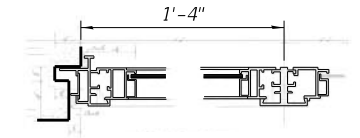
**LOUVER ELEVATION**



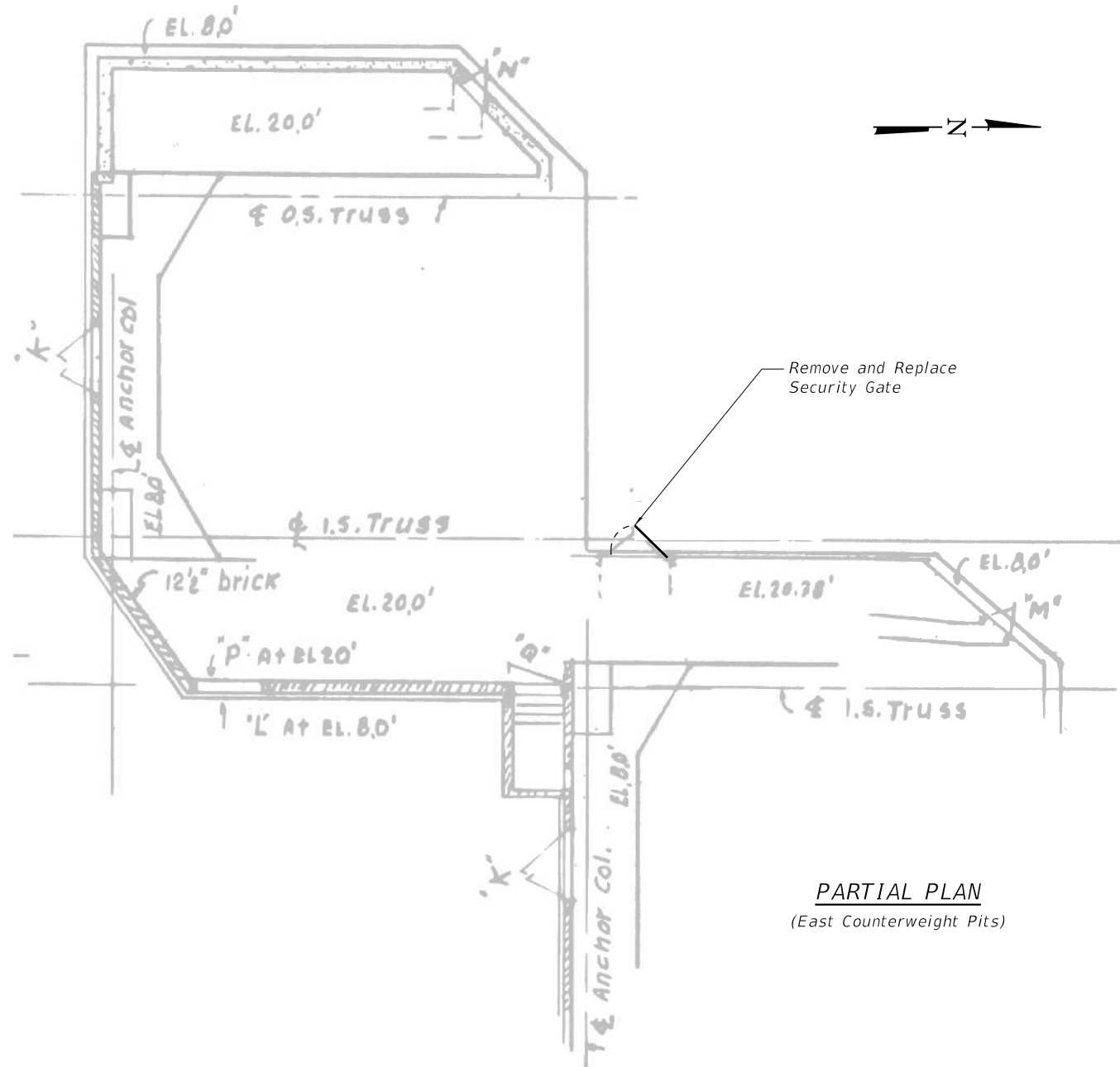
**SECTION A-A**



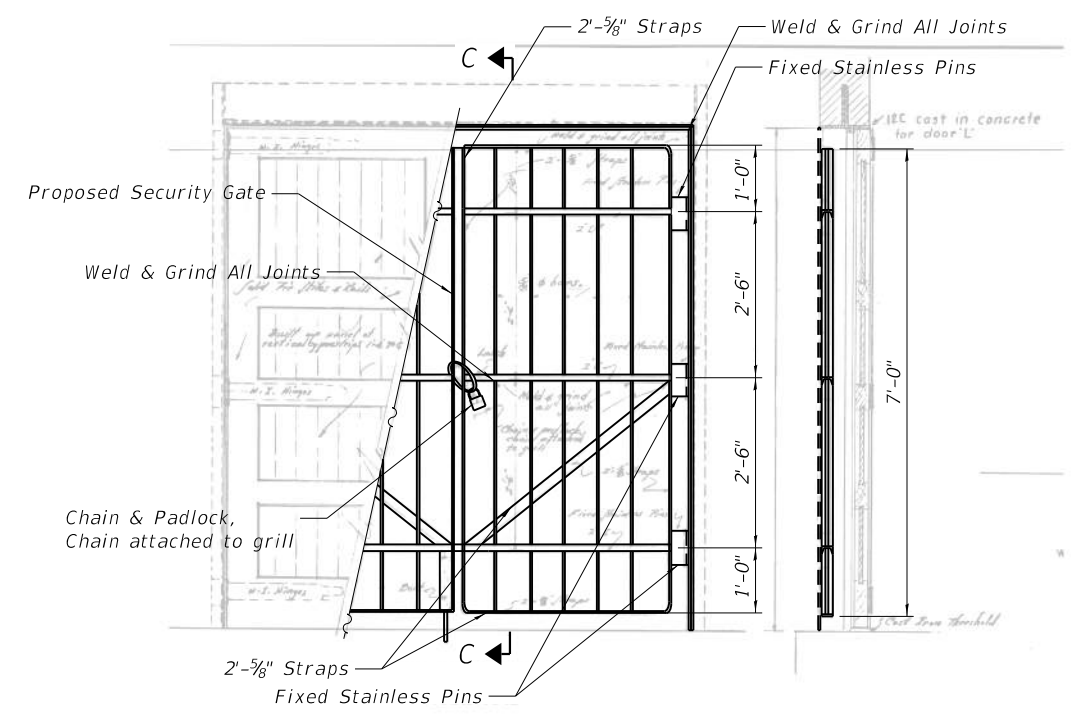
**WINDOW ELEVATION**



**SECTION B-B**



**PARTIAL PLAN**  
(East Counterweight Pits)



**SECURITY GATE ELEVATION**

**SECTION C-C**

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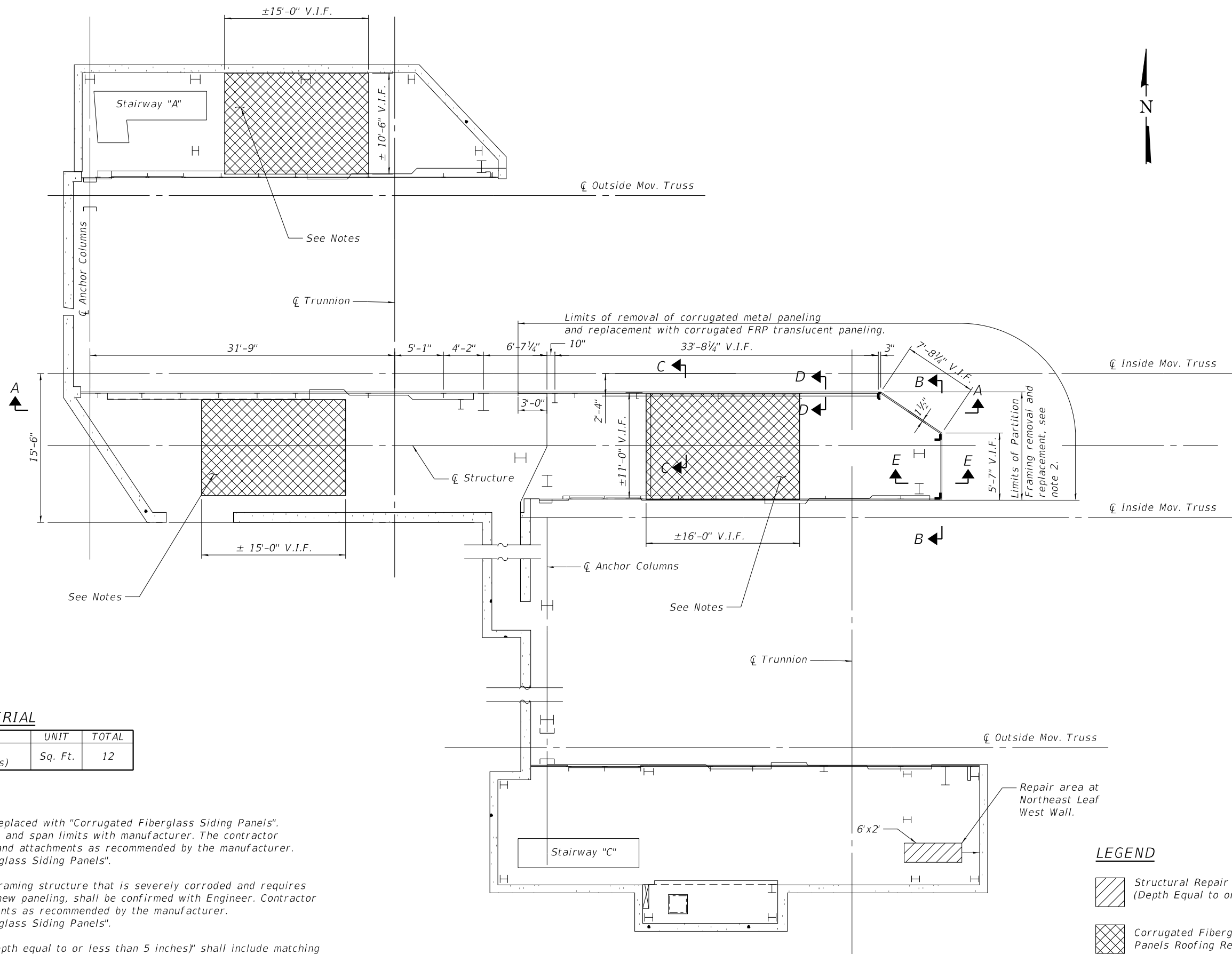
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**OPERATOR HOUSE WINDOWS AND MISC. HARDWARE  
SN 016-0202**

SHEET SB-42 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	121
CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT





**BILL OF MATERIAL**

ITEM	UNIT	TOTAL
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq. Ft.	12

**NOTES:**

1. Corrugated metal paneling to be replaced with "Corrugated Fiberglass Siding Panels". Contractor to field verify dimensions and span limits with manufacturer. The contractor shall furnish and install all hanger and attachments as recommended by the manufacturer. Cost included with "Corrugated Fiberglass Siding Panels".
2. Existing metal paneling girt and framing structure that is severely corroded and requires replacement prior to installation of new paneling, shall be confirmed with Engineer. Contractor to verify fastener spacing requirements as recommended by the manufacturer. Cost included with "Corrugated Fiberglass Siding Panels".
3. "Structural Repair of Concrete (Depth equal to or less than 5 inches)" shall include matching the existing color and texture. Contractor to perform on-site investigation of existing conditions prior to commencement of work.

**LEGEND**

-  Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
-  Corrugated Fiberglass Siding Panels Roofing Replacement

**WEST BRIDGE HOUSE MACHINERY FLOOR PLAN**

(Partition Walls and Details shown, East same by rotation.)

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

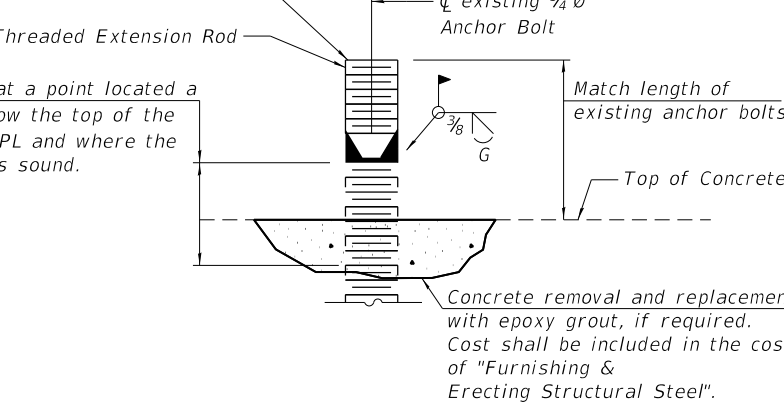
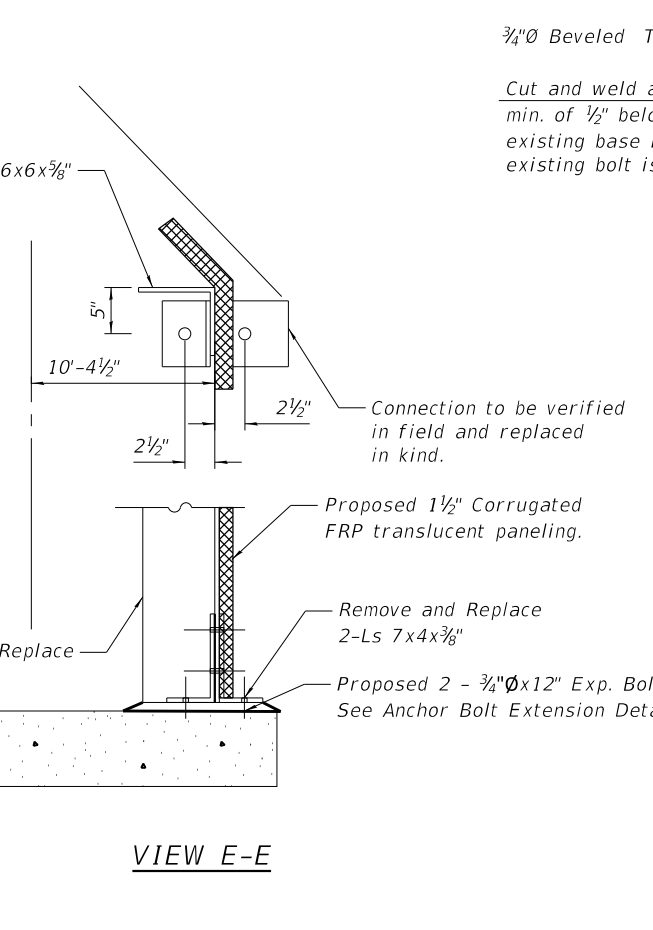
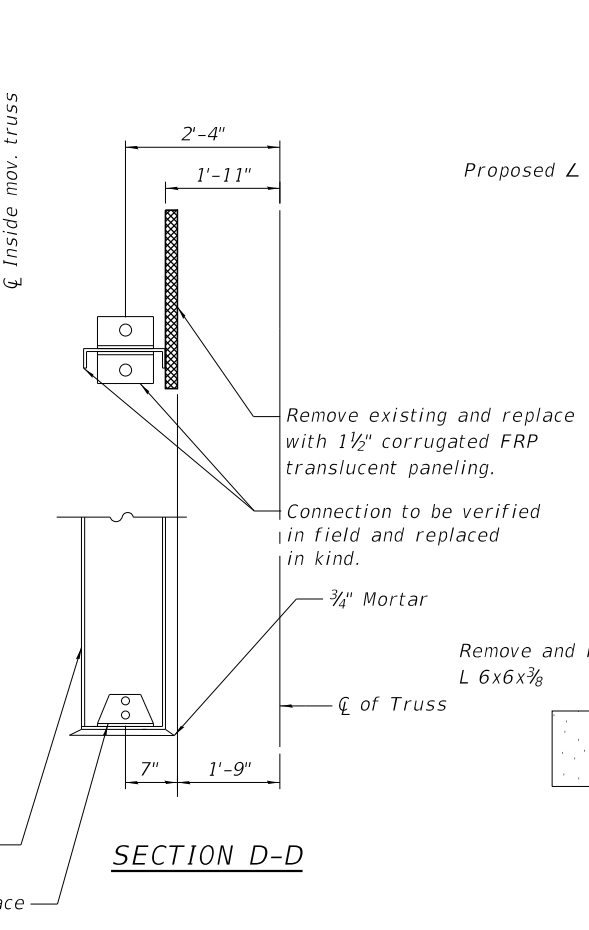
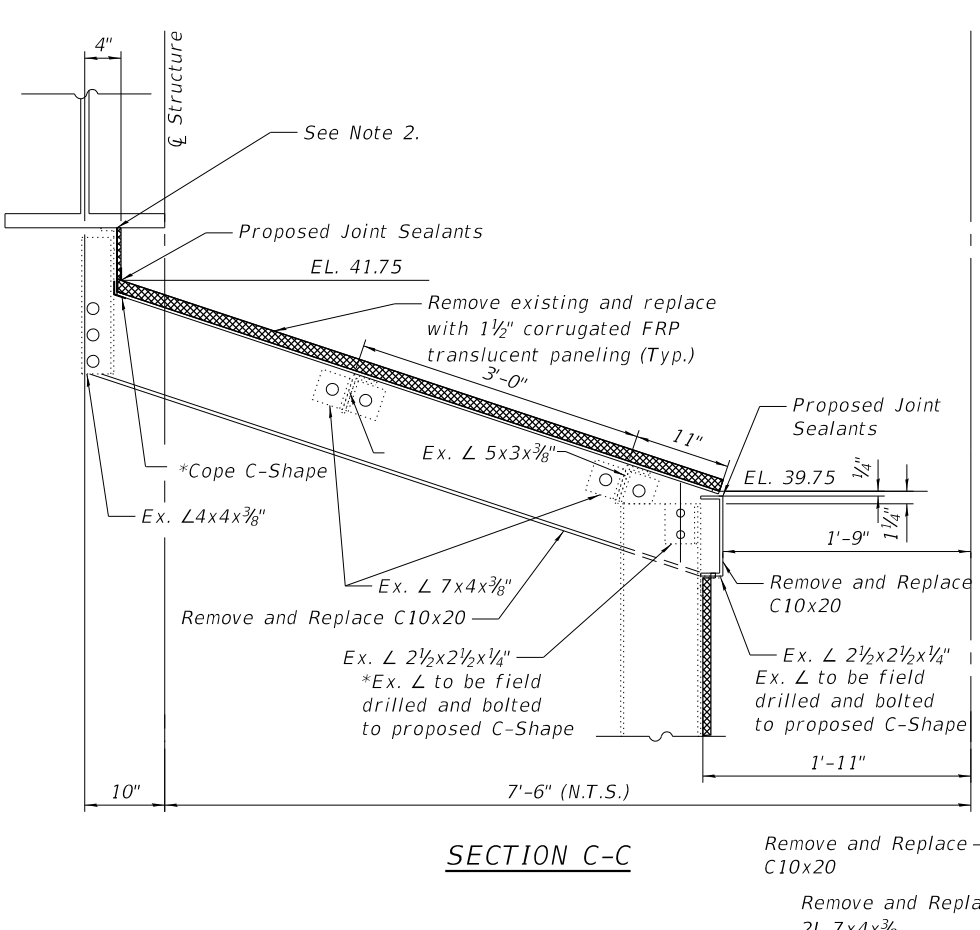
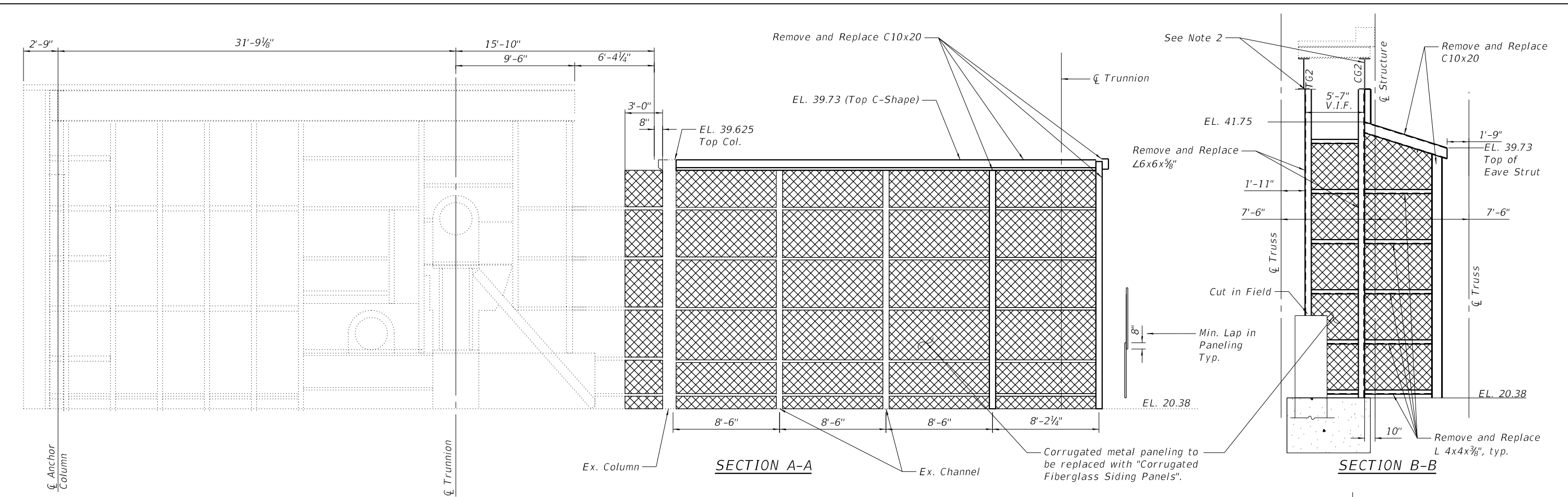
**PIT PARTITION - ENCLOSURE WALLS - PLAN  
SN 016-0202**

SHEET SB-43 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	122
CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



MODEL: Default  
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**ANCHOR BOLT EXTENSION DETAIL**  
 The furnishing and installation of required Anchor Bolt Extensions shall be included in the cost of "Furnishing and Erecting Structural Steel".

- NOTES:**
- Existing metal panning girt and framing structure that is severely corroded and requires replacement prior to installation of new panning, shall be confirmed with Engineer. Contractor to verify fastener spacing requirements as recommended by the manufacturer. Cost of fasteners included with "Corrugated Fiberglass Siding Panels".
  - Existing L's are connected to corrugated panel system and CG2 & TG2. Contractor to determine how to support corrugated panel system during TG2 & CG2 replacement. Support method to be submitted and approved by the Engineer prior to the commencement of work.
  - Steel members to be removed and replaced shall match existing.

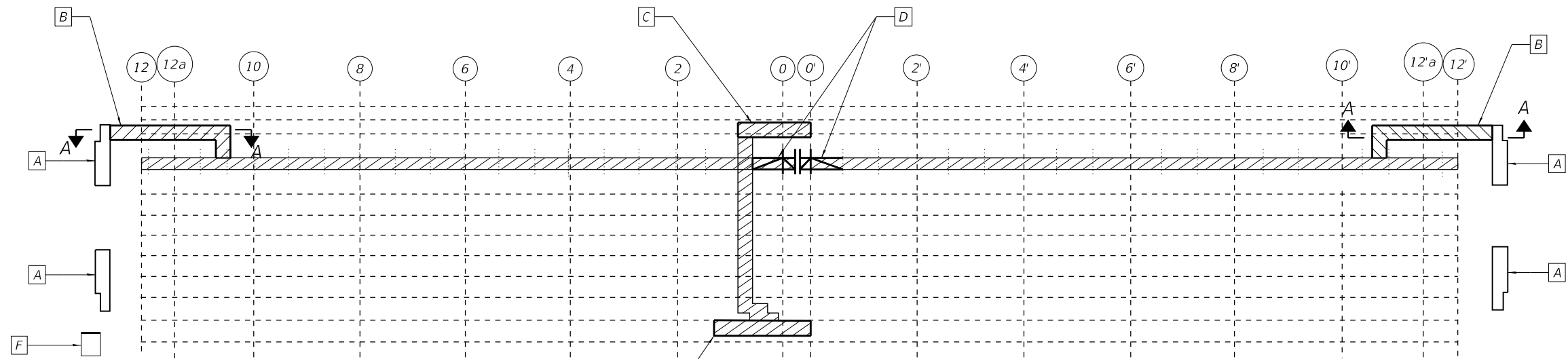
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

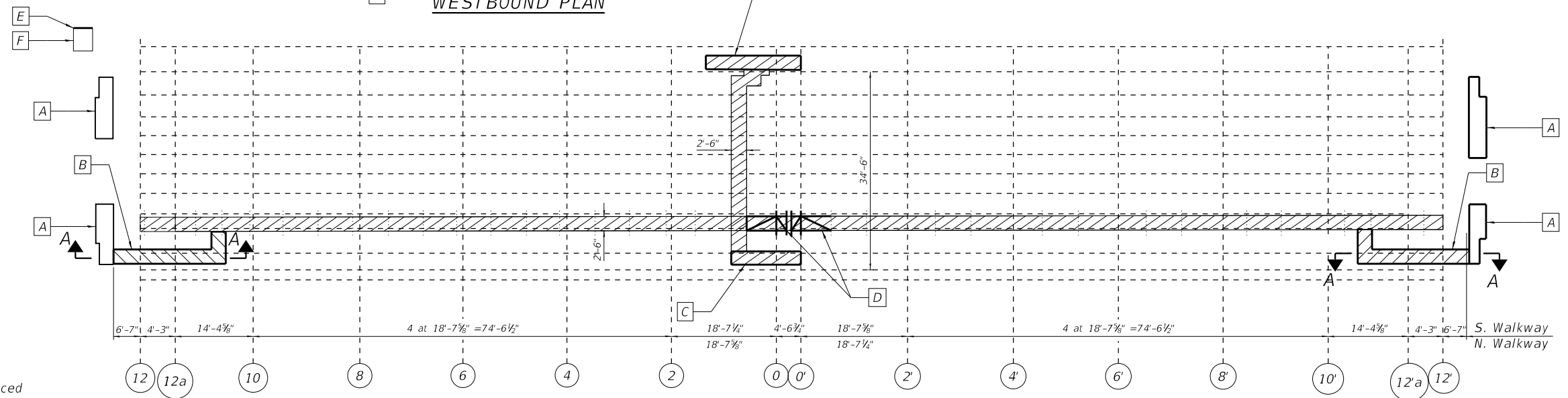
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 SN 016-0202**

SHEET SB-44 OF SB-48 SHEETS

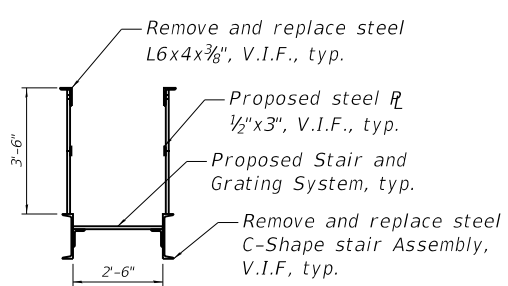
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CONTRACT NO. 62L31				
ILLINOIS FED. AID PROJECT				



WESTBOUND PLAN



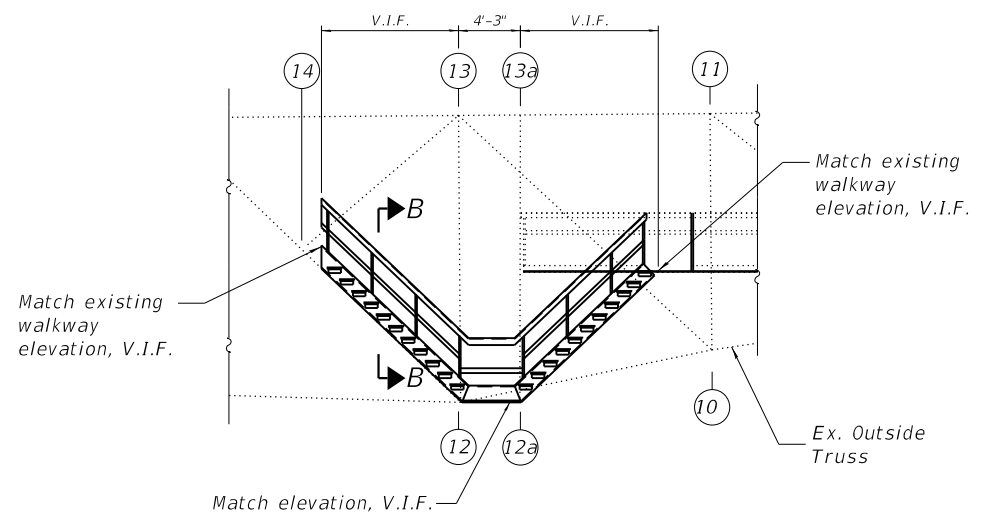
EASTBOUND PLAN



SECTION B-B

**NOTES:**

1. Lattice Strut platforms to be removed and replaced see sheet SB-25 for details.
2. Remove and replace the walkway grating support angles, see sheet SB-46 for details.
3. Center lock lower platforms, supporting angles and handrail posts to be removed and replaced, see sheet SB-47 for details.
4. The Diameter of the proposed fasteners must match the size of existing rivets or bolts, unless otherwise noted, as required by the existing detail.
5. It is the Contractor's responsibility to take measurements of the existing conditions wherever new steel is to be installed or connected into the existing material prior to ordering or fabricating any new steel. The Contractor is responsible for the proper fitting and assembly of all part of his work. The Contractor's shop drawings must indicate which dimensions were obtained by actual field measurements.
6. Contractor shall field verify the required bolt length and length of thread necessary to install all bolts in accordance with the Standard Specifications and Section 8.2.1 of the 2020 RCSC "Specification for Structural Joints using High-Strength Bolts".
7. Bolt and Rivet spacings and sizes must be verified in the field by the Contractor prior to ordering material for fabrication.
8. Repaint existing steel members that the new stair and grating system is being attached to per IDOT GBSP 25 "Cleaning and Painting Existing Steel Structures".
9. Existing C-shape stair assembly removal shall be paid for as "Structural Steel Removal".
10. All proposed C-shape steel stair assembly members and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".



VIEW A-A

**LEGEND**

- Remove and replace existing stair and grating system
- Remove and replace walkway support steel
- Lattice strut platform repairs
- Inspection walkway stairs and landing repairs
- Center lock repairs
- Walkway grating support angle repairs
- Cross girder plate, angle & channel repairs
- Angle & channel repairs

MODEL: Default  
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DEPARTMENT OF TRANSPORTATION**

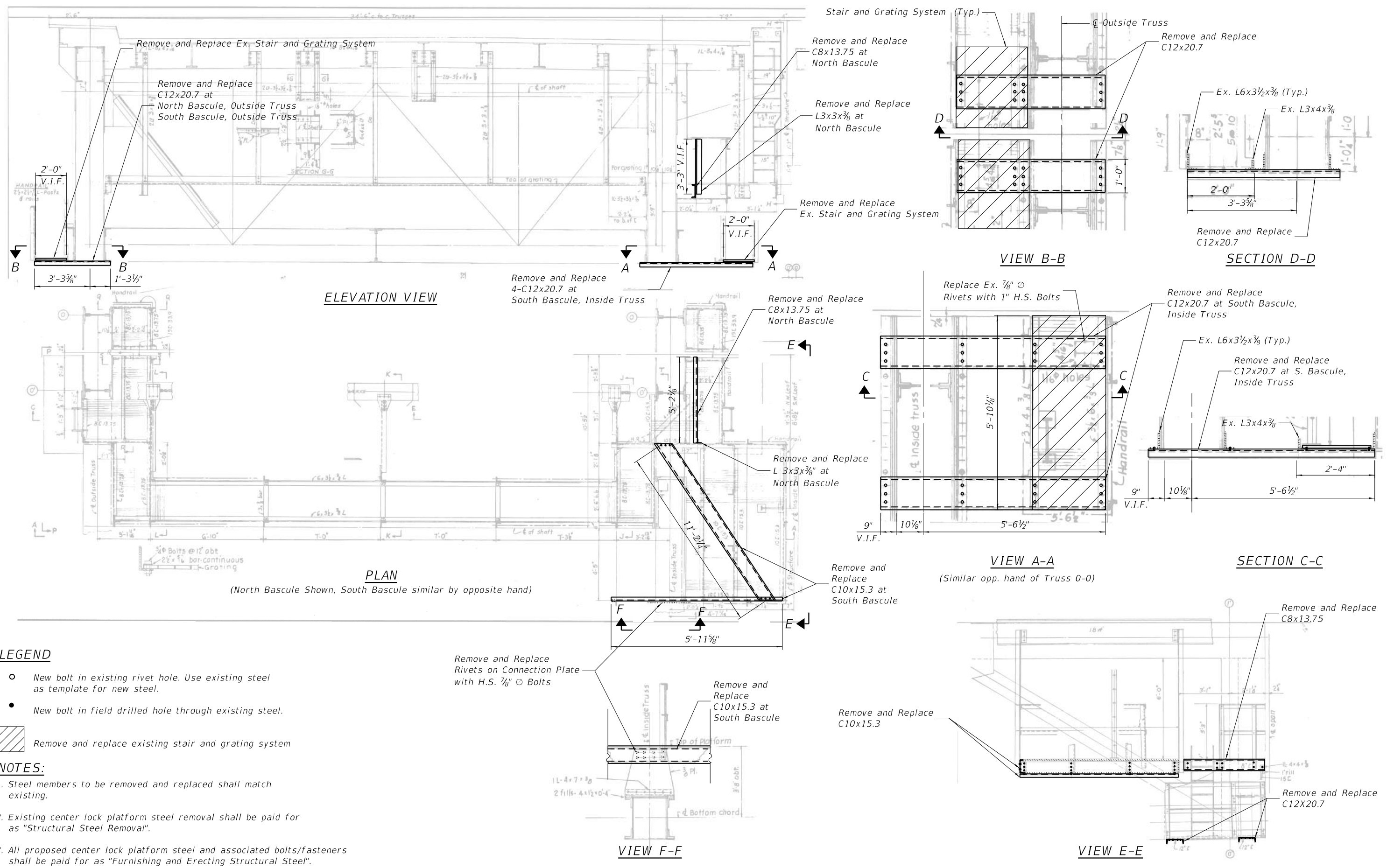
**INSPECTION WALKWAY PLAN  
SN 016-0202**

SHEET SB-45 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	124
CONTRACT NO. 62L31				

ILLINOIS FED. AID PROJECT





**LEGEND**

- New bolt in existing rivet hole. Use existing steel as template for new steel.
- New bolt in field drilled hole through existing steel.
- ▨ Remove and replace existing stair and grating system

**NOTES:**

1. Steel members to be removed and replaced shall match existing.
2. Existing center lock platform steel removal shall be paid for as "Structural Steel Removal".
3. All proposed center lock platform steel and associated bolts/fasteners shall be paid for as "Furnishing and Erecting Structural Steel".

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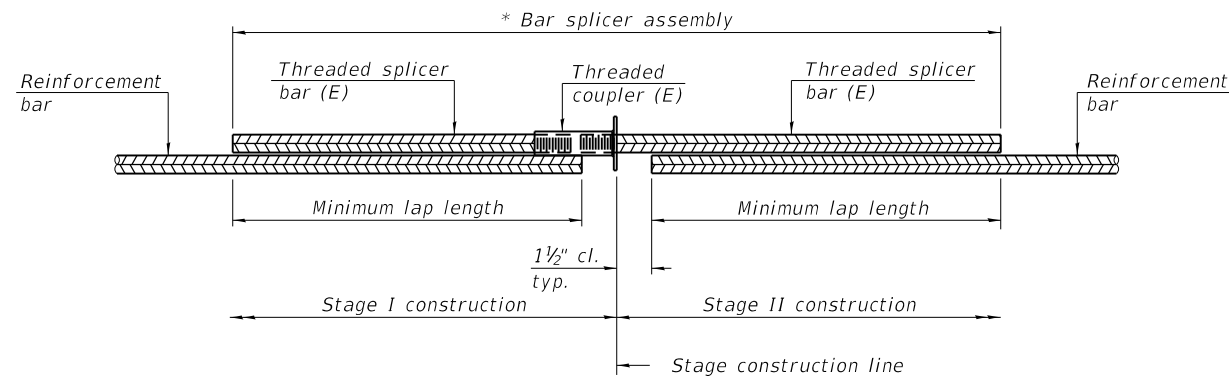
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CENTER LOCK PLATFORM REPAIR DETAILS  
SN 016-0202**

SHEET SB-47 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	126
CONTRACT NO. 62L31				
ILLINOIS		FED. AID PROJECT		

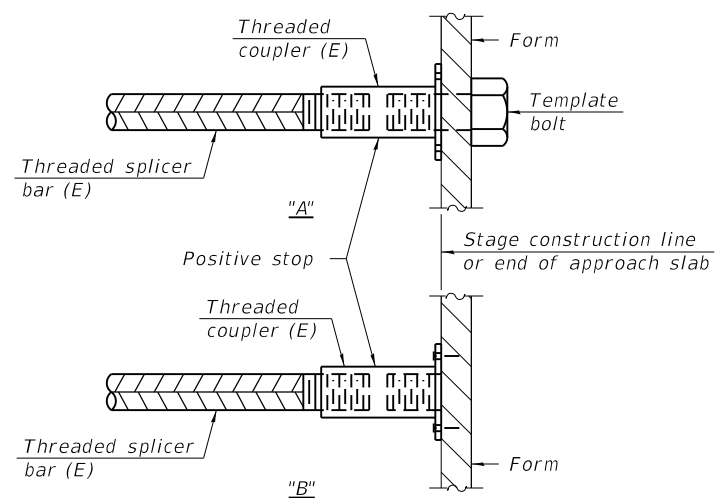


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length

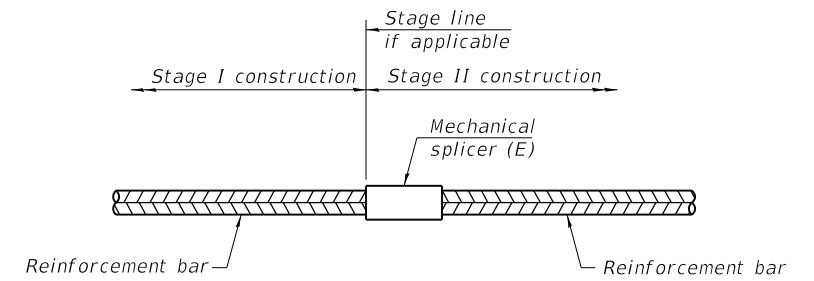


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required
Inside Truss NW Leaf	#5	255
Inside Truss SE Leaf	#5	255
Inside Truss SW Leaf	#5	139
Inside Truss NE Leaf	#5	139
Outside Truss NW Leaf	#5	130
Outside Truss SE Leaf	#5	130
Outside Truss SW Leaf	#5	130
Outside Truss NE Leaf	#5	130
Parapet	#8	16
Parapet	#5	16
Median	#8	4
Median	#5	4

**Notes:**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default  
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BSD-1

1-1-2020



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PLOT DATE =	CHECKED - TCG	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS**  
**SN 016-0202**

SHEET SB-48 OF SB-48 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
383	2020-143-BR&BJR	COOK	177	127
CONTRACT NO. 62L31				
		ILLINOIS	FED. AID PROJECT	