

I-294/I-57 INTERCHANGE COOK COUNTY, ILLINOIS



ENVIRONMENTAL ASSESSMENT ADDENDUM - AMENDMENT

I-57/I-294 Interchange - Updates Based on Value Engineering Studies

FEBRUARY 2010



TABLE OF CONTENTS

Background and History	3
Environmental Assessment Addendum	8
1.0 PURPOSE AND NEED	8
1.1 INTRODUCTION.....	8
2.0 ALTERNATIVES	8
2.3 ALTERNATIVES EVALUATED IN DETAIL.....	8
2.3.2 Preferred Alternative.....	8
3.0 AFFECTED ENVIRONMENT/ENVIRONMENTAL CONSEQUENCES	9
3.1 SOCIAL AND ECONOMIC CONDITIONS	9
3.1.2 Social and Economic Environmental Consequences.....	9
3.2 FARMLAND	9
3.3 AIR QUALITY.....	9
3.4 TRAFFIC NOISE	9
3.5 NATURAL RESOURCES	14
3.5.2 Vegetation and Wildlife Environmental Consequences	14
3.5.4 Threatened and Endangered Species Environmental Consequences.	15
3.5.6 Tree Environmental Consequences.....	15
3.6 WATER RESOURCES AND WATER QUALITY.....	15
3.6.2 Water Resources Environmental Consequences.....	15
3.7 WETLANDS	16
3.8 FLOODPLAIN AND FLOODWAYS	16
3.8.2 Floodway and Floodplain Environmental Consequences.....	16
3.9 HISTORIC AND ARCHAEOLOGICAL RESOURCES	16
3.10 SPECIAL LANDS	16
3.11 SPECIAL WASTE.....	16
3.11.1 Special Waste Affected Environment.....	16
3.12 AESTHETICS	16

3.13 CONSTRUCTION IMPACTS.....16

3.14 INDIRECT (SECONDARY) IMPACTS 16

3.15 CUMULATIVE IMPACTS 16

3.16 PERMITS..... 17

3.17 SUMMARY OF IMPACTS 17

4.0 MITIGATION MEASURES..... 18

4.1 RIGHT-OF-WAY ACQUISITION AND RELOCATIONS 18

4.2 TRAFFIC NOISE 18

4.3 NATURAL RESOURCES 18

 4.3.1 Trees..... 18

 4.3.2 Natural Areas/Nature Preserves/Prairies..... 18

4.4 WATER RESOURCES AND WATER QUALITY.....19

4.5 WETLANDS19

4.6 FLOODPLAINS / FLOODWAYS19

4.7 SPECIAL LANDS19

4.8 SPECIAL WASTE.....19

4.9 AESTHETICS 19

4.10 CONSTRUCTION IMPACTS..... 19

4.11 SUMMARY OF MITIGATION MEASURES..... 19

5.0 COMMENTS AND COORDINATION.....20

5.1 PUBLIC INVOLVEMENT20

APPENDIX B: Correspondence

APPENDIX C: Noise Analysis Results Table

APPENDIX D: Exhibits

D-5 POTENTIALLY IMPACTED PROPERTIES/PROPOSED ROW
ACQUISITION – REVISED.

Background and History

The Environmental Assessment (EA) for the I-294 at I-57 Interchange Project was approved by the Federal Highway Administration (FHWA) on August 20, 2008. The Environmental Assessment Addendum and Finding of No Significant Impact (FONSI) was approved by the FHWA on April 29, 2009.

The purpose of this document is to describe additional work and project updates along the I-294 at I-57 Interchange. The Environmental Assessment for the I-294 at I-57 project was approved for public comment on August 20, 2008 and the public comment period was held from August 25, 2008 to September 26, 2008. The Environmental Assessment Addendum, covering the addition of work along 147th Street and for updates to the I-294 at I-57 project was approved for public comment on January 14, 2009 and the public comment period was held from February 18, 2009 to March 20, 2009.

Subsequently, the Illinois Department of Transportation in conjunction with the Illinois Tollway, has continued to advance the engineering and environmental studies through a Value Engineering process for this project including the I-294 at I-57 system interchange and four surrounding service interchanges (I-57 at 147th St. Interchange, I-57 at 159th St. interchange, I-294 at 147th St. Interchange, and I-294 at US 6 interchange). The additional effort is classified as follows:

- Geometric revisions to the system ramps and CD Road along I-57 to revise the flyover type ramps to outside loop or ‘turbine’ configuration and the addition of Ramp F2 along southbound I-57 and revision to the toll plaza arrangements have been identified to reduce project construction cost and reduce right of way acquisition and property displacement. The addition of Ramp F2 allows greater flexibility in travel patterns in the system interchange.
- IDOT updated environmental studies in the Summer of 2009 for the revision of I-294 at I-57 Interchange Study, and the new work proposed for new Ramp F2. These studies included wetland delineation, threatened and endangered species, and botanical surveys. A Final Preliminary Site Investigation (PSI) for special waste issues was also received.

The intention of this Amendment is to document previously unreported impacts and updates associated with the additional improvements of Ramp F2 and toll plaza changes north of 147th Street on I-294 and note any changes to impacts previously reported in the I-294/I-57 EA and EA Addendum as a result of the subsequent engineering and environmental studies since the September 10, 2008 and February 18, 2009 Public Hearings.

New Project Improvements

The proposed changes include the addition of Ramp F2, allowing traffic from 147th Street to access southbound I-57 via a CD Road, eliminating one ramp connection to mainline I-57, the geometric revision of Ramps C and B to eliminate costly and visually intrusive

flyover ramps and replace them with conventional outside loop or ‘turbine’ configuration ramps. Additional revision includes the relocation of proposed toll plazas from north of 147th Street to north of I-57 on the ramps. Additional right-of-way acquisition will be required to accommodate Ramp F2 and a reduction in right of way acquisition and reduction in the number of impacted residential buildings (single family and multi-family) will be the result of toll plaza relocation. Refer to Appendix D, Exhibit D-5.

The following Table A-1, shows the differences between the three documents:

**Table A-1 – Comparison of Impacts
Environmental Assessment, Addendum and Amendment**

Resource	Environmental Assessment (EA)	EA Addendum	EA Addendum Amendment
Relocations/ROW Impacts	43 residences and 2 businesses require relocation. Approximately 32 acres of land will be required for ROW.	43 residences and 3 businesses require relocation. Approximately 34 acres of land will be required for ROW.	24 residences and 3 businesses require relocation. Approximately 29 acres of land will be required for ROW.
Farmlands	No Impact.	No Change.	No Change.
Air Quality	No Impact.	No Change.	No Change.
Traffic Noise	Noise levels for 245 residences approach or exceed noise abatement criteria.	No Change.	Noise levels for 326 residences approach or exceed noise abatement criteria.
Natural Resources			
Vegetation	No Impact.	No Change.	No Change.
Wildlife	No Impact.	No Change.	No Change.
Threatened and Endangered Species	No Impact.	Two additional State Threatened Species in project area.	No Change.
Trees	9,650 impacted trees	10,410 impacted trees	10,640 impacted trees
Natural Areas/Nature Preserves/Prairies			
Dropseed Prairie	3.1 acres	No Change.	No Change.
Gensburg-Markham	No Impact.	No Change.	No Change.
Markham-East	No Impact.	No Change.	No Change.
Markham-West	12.4 acres	No Change	11.32 acres
Sundrop Prairie	No Impact.	No Change.	No Change.
Paintbrush Prairie	No Impact.	No Change.	No Change.

Resource	Environmental Assessment (EA)	EA Addendum	EA Addendum Amendment
Water Resources and Water Quality			
Midlothian Creek	No Impact.	No Change.	No Change.
Dixie Creek	190 ft. temporary stream impacts loop ramp, 130 feet temporary stream impacts I-294 culvert extension	No Change.	800 ft. stream relocation loop ramp quadrant, 130 ft. temporary stream impacts, I-294 culvert extension
Belaire Creek	30 ft. temporary stream impacts culvert extension	No Change.	No Change.
Unnamed creek	No Impact.	No Change.	No Change.
Calumet Union Ditch	No Impact.	No Change.	No Change.
I-57 Ditch	1,700 ft. southern ditch 4,000 ft. northern ditch	No Change.	4,000 ft. south side of I-57 ditch 1,700 ft. north side of I-57 ditch
Park Creek	140 ft. temporary stream impacts for culvert extension	No Change.	No Change.
Groundwater Resources	Temporary impacts during construction	No Change.	No Change.
Wetlands			
Jurisdictional	Approximately 1.55 acres	Approximately 0 acres	No Change.
Non-Jurisdictional	Approximately 2.26 acres	Approximately 4.293 acres	No Change.
Floodplains/Floodways	Approx. 15.7 acres of 100-year floodplain impacts. An additional approx. 7.4 acres of 100-year floodplain impacts for loop ramp detention. Fill volumes in Phase II.	No Change. No Change. No Change.	No Change. An additional approx. 9.72 acres of 100-year floodplain impacts for loop ramp detention No Change.

Resource	Environmental Assessment (EA)	EA Addendum	EA Addendum Amendment
Historic and Archaeological Resources	No Impact.	No Change.	No Change.
Special Lands			
Section 4(f) Resources			
Memorial Park	No Impact.	No Change.	No Change.
Commissioner's Park	No Impact.	No Change.	No Change.
Posen Park	0.4 acres	No Change.	No Change.
Dropseed Prairie	No Impact.	No Change.	No Change.
G-M (Core)	No Impact.	No Change.	No Change.
G-M (south)	No Impact.	No Change.	No Change.
Ziebell Park	No Impact.	No Change.	No Change.
Sunset Park	No Impact.	No Change.	No Change.
L.E. Donnelly Park	No Impact.	No Change.	No Change.
Country Aire Park	No Impact.	No Change.	No Change.
McNeil Park	No Impact.	No Change.	No Change.
G. Pieterzak Memorial Park	No Impact.	No Change.	No Change.
Belaire Park	No Impact.	No Change.	No Change.
Gospel Center/Dresser Park	No Impact.	No Change.	No Change.
Radio Station Park	No Impact.	No Change.	No Change.
Roesner Park	No Impact.	No Change.	No Change.
Eddie Bard Park	No Impact.	No Change.	No Change.
Pedestrian Overpass	No Impact.	No Change.	No Change.
Section 6 (f) Resources	No Impact.	No Change.	No Change.
Special Waste			
Gasoline Spills	The project has a medium potential to impact three gasoline spill sites with the proposed ROW.	No Change.	No Change.
USTs	The project has a medium potential to impact nine sites with the study area.	No Change.	No Change.
LUSTs	The project has a medium potential to impact three sites with the study area.	No Change.	No Change.

Resource	Environmental Assessment (EA)	EA Addendum	EA Addendum Amendment
Brownfields	The project has a medium potential to impact three sites with the study area.	No Change.	No Change.
Miscellaneous	The project has a medium potential to impact one miscellaneous site with the study area.	No Change.	No Change.
Asbestos	Several buildings were built prior to 1970 and may include asbestos	No Change.	No Change.
Aesthetics	Views from residences and of prairies from interstates	No Change.	No Change.
Construction Impacts	Delays	No Change.	No Change.
Estimated Construction Cost (not including ROW costs)	\$505 Million (2007)	\$579 Million (2009)	\$564 Million (2009)

Environmental Assessment Addendum - Amendment

The Environmental Assessment Addendum – Amendment includes updates to the I-294/I-57 Interchange. The results of the analysis follow the section numbering of the Environmental Assessment document and references to changes in paragraph and sentence refer to the combination of material of the both the original Environmental Assessment and the Environmental Assessment – Addendum documents.

Signature Page: Interstate 294/Interstate 57 Interchange, Cook County, Illinois ENVIRONMENTAL ASSESSMENT

The last paragraph, sentences four, and five should be removed and replaced as follows:

There are 24 residential relocations and 3 business relocations and approximately 29.58 acres of new right-of-way that will be required. The proposed improvements impact approximately 4.293 acres of wetlands and 25.4 acres of floodplains.

1.0 PURPOSE AND NEED

Page 1-4, Section 1.1 Introduction

The second paragraph, last sentence should be removed and replaced as follows:

Both the Illinois Department of Transportation and the Illinois Tollway have committed funding for the engineering, property acquisition, and environmental mitigation as well as for the phased construction of the I-57 at I-294 Interchange Project.

2.0 ALTERNATIVES

Page 2-7, Section 2.3.2.1.2 Half Diamond Interchange at I-294 and 147th Street/Ramp Toll Plaza

The first paragraph, the second and third sentences should be removed and replaced as follows:

Each ramp will have a single lane, with potentially all electronic toll plaza included on the ramp pavement.

Page 2-8, Section 2.3.2.1.3 Collector-Distributor roads along I-294 and I-57

The following paragraph should be added to this section:

The toll plazas for traffic to enter I-294 northbound will be located at the convergence of the I-57 northbound to I-294 northbound ramp (Ramp B) and the I-57 southbound to the I-294 northbound ramp (Ramp D). The toll plazas for traffic to exit I-294 southbound will be located at the divergence of the I-294 southbound CD Road M to the southbound I-57 (Ramp M) and the ramp to I-57

northbound/147th Street (Ramp L). Each ramp will be served by a two-lane toll plaza with one electronic tolling lane and one cash lane.

Page 2-9, Section 2.3.2.2 Cost for the Preferred Alternative

The paragraph should be added to the end of the section:

Subsequent to the Value Engineering review and geometric revisions, an additional Cost Estimate Review analysis yielded a potential projected overall cost of \$564 million for mid-year of construction (2012), not including right-of-way acquisition and relocation costs.

3.0 AFFECTED ENVIRONMENT/ENVIRONMENTAL CONSEQUENCES

The following additional technical documents, investigations, letters, meeting minutes, and memoranda are provided in Appendix B:

- Biological Clearance Concurrence from IDNR, August 12, 2009
- Biological Clearance from IDOT dated August 28, 2009
- Final Preliminary Site Investigation (PSI, by IDOT), October 14, 2009

**Page 3-12, Section 3.1.2 Social and Economic Environmental Consequences/
Preferred Alternative/ROW Acquisition and Relocations**

The second paragraph, third sentence, should be revised as follows:

The estimated ROW needed for the proposed improvements includes 29.58 acres.

The fourth paragraph, first and second sentences, should be revised as follows:

A total of 24 residential buildings and 3 businesses will be relocated as part of this project. The relocation of the 24 residential buildings displaces approximately 49 families.

The fifth paragraph, first and second sentences should be deleted.

Page 3-14, Section 3.2 Farmland

No changes.

Page 3-15, Section 3.3 Air Quality

No changes.

Page 3-21, Section 3.4 Traffic Noise

Delete the entire section 3.4 Traffic Noise and replace with the following:

3.4 Traffic Noise

The FHWA has adopted criteria for evaluating noise impacts associated with federally-funded highway projects and for determining whether such impacts can be mitigated. The FHWA Noise Abatement Criteria (NAC) sets values which, when approached or exceeded, require the consideration of abatement measures. An impact is noted when the predicted noise level approaches, meets, or exceeds the NAC for that land use (Refer to the NAC in Table 3-10). All noise levels determined in this study are one-hour L_{eq} or $L_{eq}(h)$.

Table 3-10: Noise Abatement Criteria (NAC) Hourly A-Weighted Sound Level in Decibels (dBA)

Activity Category	$L_{eq}(h)^1$ (dBA)	Description of Activity Category
A	57 (Exterior)	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	67 (Exterior)	Picnic areas, recreation areas, playgrounds, active sports areas, parks, residences, motels, hotels, schools, churches, libraries, and hospitals.
C	72 (Exterior)	Developed lands, properties, or activities not included in Categories A or B above.
D		Undeveloped lands.
E ²	52 (Interior)	Residences, motels, hotels, public meeting rooms, schools, churches, libraries, hospitals, and auditoriums.

Source: IDOT BDE Manual (2002)

1. $L_{eq}(h)$ – The hourly value of L_{eq} . L_{eq} is the equivalent, steady-state sound level, which in a stated period of time contains the same acoustical energy as the time-varying sound level during the same period. For purposes of measuring or predicting noise levels, a receptor is assumed to be at ear heights, located 5 ft (1.5m) above ground surface.

2. Use of interior noise levels shall be limited to situations where exterior noise levels are not applicable; i.e., where there are no exterior activities to be affected by traffic noise, or where exterior activities are far from or physically shielded from the roadway in a manner that prevents an impact on exterior activities.

Note: The NAC are noise impact thresholds for considering abatement. (Abatement must be considered when predicted traffic noise levels for the design year approach [i.e., are within 1 decibel of] or exceed the noise abatement criteria, or when the predicted traffic noise levels are substantially higher [i.e., are more than 14 decibels greater] than the existing noise level.) The NAC are not attenuation design criteria or targets. The goal of noise abatement measures is to achieve a substantial reduction in future noise level. The reductions may or may not result in future noise levels at or below the NAC.

IDOT regulations further define an impact as an “approaching” level of 1 dBA below the NAC. In addition, IDOT policy defines a traffic noise impact occurs when the noise levels increase more than 14 dBA over existing noise levels. For this study, IDOT noise criteria were used to determine the level of impact.

3.4.1 Traffic Noise Affected Environment

Land uses within the study area include residential, commercial, churches, parks, and recreational facilities. Along I-294 the residential areas are located north of I-57. The residential areas along I-57 are located north of I-294, near the Kedzie crossing, and near the 159th Street interchange. Commercial development is located primarily along 147th Street. The existing traffic noise level is predicted to range between 56 dBA and 75 dBA along I-294 and between 54 dBA and 70 dBA along I-57. Two hundred seventy-nine residences, 1 church, and 6 recreation areas experience traffic noise impacts. No commercial properties experience traffic noise impacts.

3.4.2 Traffic Noise Environmental Consequences

A total of 283 receptors (representing 725 residences, 1 church, 45 commercial units, and 9 recreational areas) were evaluated for noise impacts. Table C-1 in Appendix C presents a summary of the receptors, NAC, and the noise analysis results for the Existing (2003), No-Action (2030), and the Preferred Alternative (2030) conditions. Traffic noise abatement measures were considered for the receptors that approached or exceeded the NAC or receptors that will experience an increase in traffic noise levels of greater than 14 dBA from the existing noise levels. The noise abatement measures were evaluated for being effective at reducing traffic noise levels (at least an 8 dBA decrease) and being reasonable. According to IDOT's Noise Policy, noise abatement measures are economically reasonable if they cost \$24,000 or less per benefited residence (traffic noise levels reduced by at least 5 dBA as a result of a noise barrier). All of the I-294 noise barriers are under the jurisdiction of the Illinois Tollway. The Illinois Tollway has the authority to make the final decisions on construction of these barriers.

No-Action Alternative

Included in the No-Action Alternative are five noise barriers that are being constructed along I-294 as part of the separate I-294 Widening Project (see Figure 3-5 for the locations of the No-Action barriers). Under No-Action conditions, a total of 56 receptors (representing 87 residences, 1 church, 6 recreational areas, and 1 commercial unit) will approach or exceed the NAC. The No-Action traffic noise level is predicted to range between 55 dBA and 73 dBA along I-294 and between 57 dBA and 73 dBA along I-57.

Preferred Alternative

Under the Preferred Alternative, the five No-Action Noise Barriers built under the I-294 Widening project will either be fully or partially removed to construct the I-294/I-57 Interchange. A total of **115 receptors (representing 326 residences, 7 recreation areas, 1 Church, and 1 commercial unit)** will approach or exceed the NAC if the No-Action Noise Barriers along I-294 (which are partially or fully removed) are not replaced. As shown in Appendix C, Table C-1, the Preferred Alternative traffic noise level is predicted to range between **59 dBA and 74 dBA** along I-294 and between **56 dBA and 72 dBA** along I-57.

Seven noise barriers were analyzed along I-57 and six noise barriers were analyzed along I-294. Table 3-11 presents a summary of the noise barriers analyzed. All of the I-294 noise barriers being evaluated are replacing either all or part of a No-Action Noise Barrier. All the evaluated barriers were effective in reducing traffic noise except Barrier 294-1 (maximum noise level reduction of 5 dBA). However, Barrier 294-1 will be considered for reasonableness because they will be replacing existing No-Action Noise Barriers that are being constructed as a part of I-294 Widening Project.

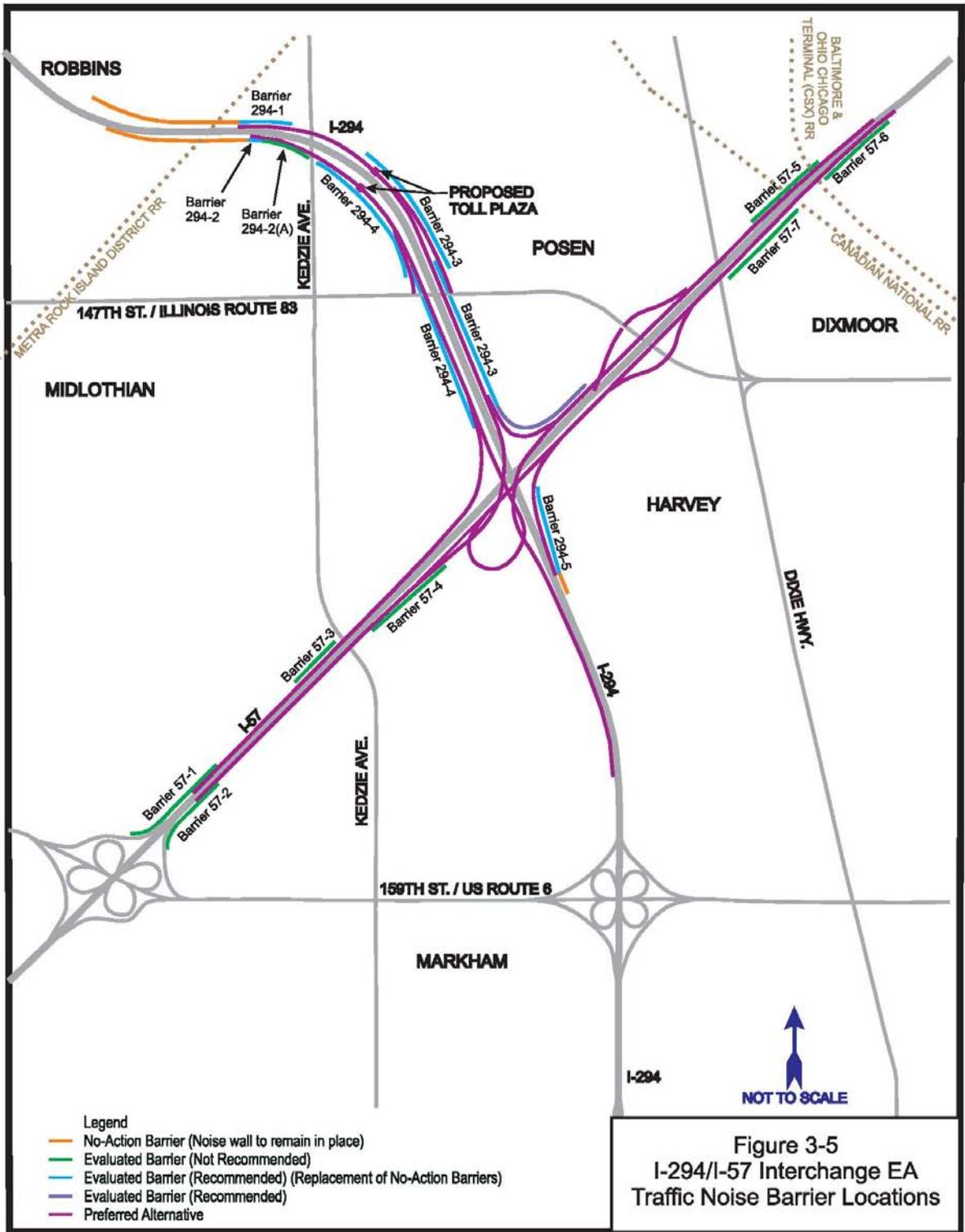
Since the I-294 Noise Barriers are replacing either all or part of the No-Action Noise Barriers, the cost effectiveness is analyzed for both the entire noise barrier and the replacement section. In addition, the cost per benefited receptor was completed for both the receptors that benefit from the entire noise wall and for those receptors that benefit from the replacement section. The analyses were completed for the entire wall to show the full benefits of the extension and the analyses were completed for only the replacement section to present the costs that will be incurred for this project.

All the Noise Barriers along I-294 are reasonable based on the cost per benefited residence, except Barrier 294-2 and Barrier 294-2A, as shown in Table 3-11. Barrier 294-2 and Barrier 294-2A are not considered economically reasonable because they exceed the cost criteria of \$24,000 per benefited receptor. However, the Illinois Tollway has decided to construct Barrier 294-2 because it will replace a 200-foot section of one of the noise barriers constructed as part of the I-294 Widening Project. Barrier 294-2 is the replacement of a section of a No-Action Noise Barrier which protects the actively used baseball diamond in Memorial Park. Since this barrier is not considered reasonable and is not eligible for federal-aid funds, the Illinois Tollway will construct this barrier with Illinois Tollway funds. Barrier 294-2A replaces the same section of the No-Action barrier as Barrier 294-2, but includes an additional 650-foot extension of the noise wall. It is not recommended since the additional length of Barrier 294-2A does not benefit any more receptors than Barrier 294-2.

The **seven** I-57 noise barriers are determined to not be cost-effective and are not recommended. The Preferred Alternative does not include any noise barriers along I-57. A final decision on the installation of the abatement measures will be made upon completion of the project design and the public involvement process.

Page 3-23, Section 3.4 Traffic Noise Figure 3-5

Remove and replace Figure 3-5 with the separate drawing:



Page 3-25 Section 3.4 Traffic Noise. Table 3-11 Noise Barrier Cost Analysis Summary.

Remove and replace Table 3-11 with the following:

Table 3-11: Noise Barrier Cost Analysis Summary								
Barrier	Barrier Height (feet)	Barrier Length (feet)	Barrier Square Footage	Effective in Noise Reduction (achieves 8 dBA reduction)	Cost ¹	Number of Benefited Receptor	Cost per Benefited Receptor	Economically Reasonable
Barrier 57-1	14	2,260	31,640	Yes	\$791,000	15	\$52,700	No
Barrier 57-2	14-16	1,990	30,545	Yes	\$763,700	12	\$63,600	No
Barrier 57-3	19	810	15,390	Yes	\$384,800	2	\$192,400	No
Barrier 57-4	26	2,000	52,000	Yes	\$1,300,000	14	\$92,800	No
Barrier 57-5	13-16	2,425	33,300	Yes	\$832,500	8	\$104,000	No
Barrier 57-6	18-20	1,135	22,260	Yes	\$556,500	6	\$92,800	No
Barrier 57-7	13-15	1,385	20,125	Yes	\$503,100	8	\$62,900	No
Barrier 294-1	16 ²	1,050 ²	48,800/ 16,800 ³	No	\$1,263,900/ \$435,100 ³	134/4 ³	\$9,400/ \$108,800 ³	Yes
Barrier 294-2	20 ²	200 ²	38,000/ 4,000 ³	Yes	\$984,200/ \$103,600 ³	35/1 ³	\$28,100/ \$103,600 ³	No
Barrier 294-2A	14 ²	850 ²	50,000/ 11,900 ³	Yes	\$1,295,000/ \$308,200 ³	35/1 ³	\$37,000/ \$308,200 ³	No
Barrier 294-3	13	6,850	89,050	Yes	\$2,226,200	159	\$14,000	Yes
Barrier 294-4	13-15	5,700	79,600	Yes	\$2,061,700	121	\$17,100	Yes
Barrier 294-5	14-16 ²	1,000 ²	21,650/ 14,500 ³	Yes	\$619,200/ \$414,700 ³	91/91 ³	\$6,800/ \$4,600 ³	Yes

1. The costs of the I- 57 barriers were estimated based on \$25/square foot in accordance with IDOT's Noise Policy. The costs of the I-294 barriers are based on the actual costs of the No-Action Barriers from the I-294 Widening Project (\$25.90 to \$28.60/square foot). Using either IDOT's or the Illinois Tollway's cost per square foot of wall does not alter the economic reasonableness of the noise barriers.

2. Values are for the replacement section only.

3. Entire barrier (including the No-Action Barrier that remains in place and the replacement section)/replacement section only. The total benefited residences include the No-Action benefited residences.

Page 3-26, Section 3.5.2 Vegetation and Wildlife Environmental Consequences/Preferred Alternative

The first and second sentences of the first paragraph should be removed and replaced with the following:

Approximately 29.58 acres of additional ROW from over 150 separate parcels are required to construct the proposed project. Parcel acquisitions range in size from 0.02 acres to 9.72 acres. Refer to Appendix D, Exhibit D-5.

The fourth sentence should be removed and replaced with the following:

The largest parcel (9.72 acres), which occurs within the southern quadrant of the I-294/I-57 crossing, removes woodland and old field habitats.

The fifth sentence should be removed and replaced with the following:

There will be a loss of approximately 29.58 acres of urban wildlife habitat.

Page 3-30, Section 3.5.4 Threatened and Endangered Species Environmental Consequences/Preferred Alternative

The following paragraphs should be inserted at the end of the section:

An additional EcoCAT response from IDNR received August 12, 2009 identified the same Illinois Natural Area Inventory (INAI) sites and Nature Preserves as the EA in the project corridor.

Page 3-31, Section 3.5.6 Trees Environmental Consequences, Preferred Alternative

The following paragraphs should be inserted at the end of the section:

Subsequent to Value Engineering studies, additional areas were identified that will be disturbed during construction. Transect methods have determined that an additional 230 trees will be impacted in the revised area of the interchange. The total number of impacted trees is 10,640.

Page 3-37, Section 3.6.2 Water Resources Environmental Consequences/Preferred Alternative

Dixie Creek

The first sentence should be modified as follows:

The words “two fly-over ramps” should be removed and replaced with “two system connection ramps”.

The following paragraph should be added to end of this section:

Approximately 800 feet of the Dixie Creek channel will be relocated to a new channel further south and east to avoid the proposed loop ramp and the system connection ramps. The relocated Dixie Creek channel will also pass under one of the system connection ramps via a bridge structure.

Bellaire Creek

The first sentence should be modified as follows:

The words “ proposed fly-over ramp” should be removed and replaced with “proposed system connection ramp”.

Page 3-43, Section 3.7 Wetlands

No changes.

Page 3-49, Section 3.8.2 Floodway and Floodplain Environmental Consequences/Preferred Alternate

The first sentence of the last paragraph should be removed and replaced with the following:

Approximately 25.4 acres of impact to the 100-year floodplain occur as a result of the Preferred Alternative.

Page 3-50, 3.9 Historic and Archaeological Resources

No changes.

Page 3-51, Section 3.10 Special Lands

No changes.

Page 3-54, 3.11.1 Special Waste Affected Environment

The following should be inserted at the end of this section:

A Preliminary Site Investigation Report was completed (PSI, October 14, 2009) , by the Illinois Department of Transportation. The PSI provided a review and summary of estimated quantities and costs of special waste and non-special waste disposal items, along with recommendations for Special Provisions.

Page 3-57, Section 3.12 Aesthetics

No changes.

Page 3-57, Section 3.13 Construction Impacts

No changes.

Page 3-58, Section 3.14 Indirect (Secondary) Impacts,

No changes.

Page 3-60, Section 3.15 Cumulative Impacts

No changes.

Page 3-61, Section 3.16 Permits

No changes.

Page 3-61, Section 3.17 Summary of Impacts

The following areas of Table 3-16 should be revised (changes in *bold/italic*):

Resource	No-Action Alternative Impact	Preferred Alternate Impact
Social and Economic		24 Residences and 3 businesses require relocation Approximately 29.58 acres of land will be needed for ROW
Traffic Noise		Noise levels for 326 residences approach or exceed noise abatement criteria
Natural Resources		
Trees		Approximately 10,640 impacted trees
Natural Areas/Nature Preserves/Prairies		
Markham-West ¹		Approximately 11.32 acres
Water Resources and Water Quality		
Dixie Creek		800 ft. stream relocation loop ramp quadrant, 130 ft. temporary stream impacts, I-294 culvert extension
I-57 Ditch		4,000 ft. south side of I-57 ditch 1,700 ft. north side of I-57 ditch
Floodplains/Floodways		Approximately 15.7 acres of 100-year floodplain impacts An additional approximately 9.72 acres of 100-year floodplain/impacts for loop ramp detention basin.

Resource	No-Action Alternative Impact	Preferred Alternate Impact
Estimated Construction Cost (Not including Right-of-Way or Relocation Costs)		\$564 million (2009 dollars)

¹Markham-West lands are owned by The Nature Conservancy as private preservation lands. It is not designated as a Nature Preserve or an Illinois Natural Area Inventory (INAI) site. Portions fall under the Stein Conservation Easement. Refer to Chapter 3 of the Environmental Assessment for further information.

4.0 MITIGATION MEASURES

Page 4-1, Section 4.1 Right of Way Acquisition and Relocations

No text changes.

Page 4-1, Section 4.2 Traffic Noise

No text changes.

Page 4-1, Section 4.3.1. Trees

Remove all paragraphs in the section and replace with the following:

Mitigation of the approximately 10,640 trees that are estimated to be removed will be conducted in accordance with IDOT Departmental Policies (D&E – 18) by IDOT and the ISHTA Criteria for Removal and Replacement of Trees by ISHTA. The actual number of replacement trees may be adjusted during the final design based on the actual number of trees impacted. The potential exists within the project area to work with local natural resource agencies and groups to identify areas where replacement trees can be planted.

Based on the IDOT policy, the number of replacement trees is dependent on if balled and burlapped trees (1:1 replacement ratio) are used or seedlings (3:1 replacement ratio). Therefore, mitigation under the IDOT policy would require 10,640 balled and burlapped tree plantings or 31,920 seedling plantings.

The ISTHA policy mitigates trees based on the size of the tree being replaced with 6 to 12 inch trees being replaced at 1:1, 13 to 29 inch trees at 2:1 and 30 inch and greater being replaced at 3:1. Therefore, mitigation under the ISTHA policy would require approximately 14,808 tree plantings.

Page 4-1, Section 4.3.2 Natural Areas / Nature Preserves / Prairies, Markham-West Prairie

The first sentence should be removed and replaced with the following:

The impact to Markham-West Prairie parcels that are designated for conservation purposes is approximately 9.72 acres.

Page 4-2, Section 4.4 Water Resources and Water Quality

No text changes.

Page 4-3, Section 4.5 Wetlands

No text changes.

Page 4-3, Section 4.6 Floodplains/Floodways

No text changes.

Page 4-3, Section 4.7 Special Lands

No text changes.

Page 4-4, Section 4.8 Special Waste

No text changes.

Page 4-4, Section 4.9 Aesthetics

No text changes.

Page 4-4, Section 4.10 Construction Impacts

No text changes.

Page 4-5, Section 4.11 Summary of Mitigation Measures, Table 4-1

Table 4-1 should be revised as follows (changes in *bold/italic*):

Resource	Impact	Mitigation
Relocations/ROW Impacts	<p>24 residences and 3 businesses require relocation.</p> <p>Approximately 29.58 acres of land will be required for ROW.</p>	<p><i>Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended (1989)</i></p>
Traffic Noise	<p>The noise levels for approximately 326 residences will approach or exceed the noise abatement criteria.</p>	<p>Five noise barriers are proposed to mitigate the traffic noise. These barriers are replacing the No-Action barriers along I-294.</p>
Natural Resources		
Trees	<p>Approximately 10,640 impacted trees</p>	<p>10,640 replacement trees (<i>balled and burlaped, IDOT policy</i>)</p>

Resource	Impact	Mitigation
Natural Areas/Nature Preserves/Prairies		
Markham-West ¹	Approximately <i>11.32</i> acres	2 acres of prairie restoration, enhancement, and/or preservation or land acquisition to be conducted within the Indian Boundary Prairies complex.
Water Resources and Water Quality		
Dixie Creek	<i>800 ft.</i> stream relocation loop ramp quadrant, 130 ft. temporary stream impacts, I-294 culvert extension	Best Management Practices (BMPs) for construction to be determined during final design.
I-57 Ditch	<i>4,000 ft. south side of I-57 ditch</i> <i>1,700 ft. north side of I-57 ditch</i>	Best Management Practices (BMPs) for construction to be determined during final design.
Floodplains/Floodways	Approximately 15.7 acres of 100-year floodplain impacts An additional approximately <i>9.72</i> acres of 100-year floodplain/impacts for loop ramp detention basin.	Coordination with local municipalities and IDNR regarding impacts to the FEMA floodway and floodplain and to obtain required permits.

¹Markham-West lands are owned by The Nature Conservancy as private preservation lands. It is not designated as a Nature Preserve or an Illinois Natural Area Inventory (INAI) site. Portions fall under the Stein Conservation Easement. Refer to Chapter 3 of the Environmental Assessment for further information.

5.0 COMMENTS AND COORDINATION

Page 5-1, 5.1 Public Involvement

The following should be added to the end of this section:

NOTE: Additional Public Involvement material to be added after Public Informational Meeting.

APPENDIX A: List of Acronyms and Abbreviations

No changes.

APPENDIX B: Correspondence

See additional material in separate section.

APPENDIX C: Noise Analysis Results Summary Table

Remove Table C-1 in its' entirety and replace with revised tables with location exhibits.

APPENDIX D: Exhibits.

Remove Exhibit D-1, *Proposed Plans – 147th Street* and replace with Exhibit D-5

Remove Exhibit D-2, *Potentially Impacted Properties*, replace with Exhibit D-5

Remove Exhibit D-3, *Proposed Right-of-Way Acquisition*, replace with Exhibit D-5

New Exhibit D-5 (Sheets 1-20), *Potentially Impacted Properties/Proposed ROW Acquisition*, updates design and preliminary right-of-way information. Exhibit D-5 combines material previously presented in Exhibits D-1, D-2, and D-3.

Appendix B
Correspondence

MEG

Wetlands

Submittal Date: 07/14/2008 Sequence No: 14550
 District: 1 Requesting Agency: DOH Project No:
 Contract #: Job No.: P-91-186-08
 Counties: Cook
 Route: FAI 57 Marked: I-57
 Street: Section:
 Municipality(ies): See add'l info Project Length: km miles
 FromTo (At): @ I-294
 Quadrangle: Blue Island, Harvey Township-Range-Section: T36N, R13E, Sec. 12&13
 Anticipated Design Approval: 02/28/2009 Cleared for Design Approval: 08/28/2009
 Cleared for Letting: 08/28/2009 Mitigation: Yes Mitigation Completed:

Initial Survey and WIE Addendum No:

Initiated	Due Date	Results Received	Wetland Present	District Notified	WIE Requested	WIE Received	Wetland Impacts	Resp to District	Coord Complete
10/02/2008	11/30/2008	12/04/2008	Yes	01/06/2009	Yes	01/20/2009	Yes	01/21/2009	Yes

Comments: Dst. wants wet del asap; 12/30/08: ISTHA responded to draft BRR; 1/23/09: hand delivered standard action letter to Malone; concur (SED)
 Clearances: Cultural: 8/28/2008 Bio 1/12/2009 SW 1/20/2009

Submittal Date: 07/28/2009 Sequence No: 14550 A
 District: 1 Requesting Agency: DOH Project No:
 Contract #: Job No.: P-91-186-08
 Counties: Cook
 Route: FAI 57 Marked: I-57
 Street: Section:
 Municipality(ies): See add'l info Project Length: 0 km 0 miles
 FromTo (At): @ I-294
 Quadrangle: Harvey Township-Range-Section: T36N, R13E, Sec. 12&13
 Anticipated Design Approval: 05/31/2009 Cleared for Design Approval: 08/28/2009
 Cleared for Letting: 08/28/2009 Mitigation: Yes Mitigation Completed:

Initial Survey and WIE Addendum No: A

Initiated	Due Date	Results Received	Wetland Present	District Notified	WIE Requested	WIE Received	Wetland Impacts	Resp to District	Coord Complete
		08/12/2009	No		No				Yes

Comments: Huff & Huff performed wetland delineations (SED)
 Clearances: Cultural: Bio 8/28/2009 SW

Processing: Standard Action

Individual Compensation Plan Required:
 404 Individual Permit Required:

Mitigation Site: Within Project Limi
 Mitigation Basin: In-Basin
 Bank: No
 Accumulation: No

Owner:
 Name:
 Location:
 Size:
 Types:
 Quad:
 Basin:

Processing Comments: ISTHA mitigating on-site within Indian Boundary Prairies per EA; 1/23/09: Malone concur (SED)

Wetland Impact Evaluation

Submittal Date: 01/20/2009 Submitted By:
 Does the project have wetland impacts? Yes Type: Permanent
 Briefly describe the measures considered to avoid and minimize adverse impacts to the wetlands: Side slopes will be constructed as steep as allowable to minimize impacts. Appropriate erosion control measures will be implemented to minimize impacts to adjacent wetlands that are unaffected.
 Summarize briefly why there are no practicable alternatives to the use of the wetland(s): ROW area is needed for expansion of intersection in this area. Adjusting roadway geometry to avoid would require property acquisition and displacement of residents, businesses, etc.
 Wetland mitigation is being proposed: within the project limits (on-site) Reviewed

Memo Date: 08/28/2009 **Memo By:** Susan Dees
Memo: Wetland delineations for Addendum A were conducted by Huff and Huff consultants. They are acceptable to this office. No wetlands were found. Addendum A is thus cleared for construction with respect to wetlands. If there are questions, please contact me at 217/785-0150.
Memo Date: 01/23/2009 **Memo By:** Susan Dees
Memo: The Wetland Impact Evaluation form was received 1/20/09 by this office and is acceptable. The project shall be processed as a Standard Review Action in accordance with the IDOT Interagency Wetlands Action Plan. Mitigation is considered on-site since it shall be performed within one mile of the project area. Wetland Sites #12, #13, #14, and #15 shall be impacted totalling 4.353 acres. All of the impacted wetlands are isolated. Because impacts to Wetlands #12 and #13 are less than 0.5 ac per wetland, they shall likely be mitigated at a ratio of 1.5:1.0 with mitigation acreages 0.344 and 0.146 acres respectively. Because impacts to Wetlands #14 and #15 totally destroy each wetland, they shall likely be mitigated at a ratio of 2.5:1.0, with mitigation acreages of 7.585 and 2.483 acres respectively. Total mitigation acreage may be up to 10.5565 acres. IDOT and ISTHA have separate agreements with IDNR which determine the mitigation ratios for each impact. The minimum mitigation ratio that will be applied to isolated wetland impacts will be 1.5:1.0. The highest ratio that will be applied to isolated wetland impacts, depending on wetland quality, will be 5.5:1.0. ISTHA will be responsible for coordinating all final ratios with IDNR once the design is finalized and prior to the permitting process. Because this project is a Standard Review Action, it shall be coordinated with Mr. Patrick Malone of IDNR Office of Resource Review and Coordination. Upon his concurrence, this project shall be considered cleared for letting with respect to wetlands. If there are questions, please contact me at 217/785-0150.
Memo Date: 01/20/2009 **Memo By:** V. Ruiz
Memo: ISTHA will be responsible for wetland mitigation. Mitigation will occur within The Indian Boundary Prairie Mitigation Site.

Wetland Impacts and Mitigation Required

Site No.	Type	T&E	Nature Preserve	Natural Area	Essential Habitat	Size (acres)	Acres of Impact	Ratio	Acres of Compensation	
12	Wet Shrub	No	No	No	No	0.229	.229	1.5	.344	
Basin 07120003		Quadrangle	Blue Island		FQI	8.2				
Describe the work:		Fill								
13	Wet Mead	No	No	No	No	0.097	.097	1.5	.146	
Basin 07120003		Quadrangle	Blue Island		FQI	3.5				
Describe the work:		Fill								
14	Wet Mead	No	No	No	No	3.034	3.034	2.5	7.585	
Basin 07120003		Quadrangle	Blue Island		FQI	8.5				
Describe the work:		Fill								
15	Forested	No	No	No	No	0.993	.993	2.5	2.483	
Basin 07120003		Quadrangle	Blue Island		FQI	8.8				
Describe the work:										
Total							4.353		10.557	

Mitigation Site Suitability Study

bw

Environmental Survey Request Addendum

A. Project Information Bio Cultural Wetlands Special Waste

Submittal Date: 07/28/2009 Sequence No: 14550 A Project No: _____
District: 1 Requesting Agency: DOH
Contract #: _____ Job No.: P- 91-186-08
Counties: Cook
Route: FAI 57 Marked: I-57
Street: _____ Section: _____
Municipality(ies): See add'l info Project Length: 0 km 0 miles
From To (At): @ I-294
Quadrangle: Harvey Township-Range-Section: T36N, R13E, Sec. 12&13
Anticipated Design Approval: 05/31/2009

B. Reason for Submittal (Check all that apply)

Acquisition of additional ROW or easement Addendum: _____ acres Total Project: _____ acres
 In-Stream Work Stream Name: _____
 Other: Wetland survey to be done by Huff and Huff for addendum A.
 Field Sign Off (Bio & Cultural Only)

C. Addendum Description: Additional ROW required for Value Engineering, revised geometry.

D. Tree Removal?: Don't Know Number?: _____ ha/ _____ acres
Wetland delineation performed by: Consultant End. Species Consultation performed by: BDE

Contact Person: John Baczek	Local Contact Person:
Telephone #: (847) 705-4104 ext.	Telephone #:
Env. Contact: Sam Mead	E-Mail:
Telephone #: 8477054101	Title/Company:

Update Entire Project
 Addendum Only
 Field Sign Off (Bio & Cultural Only) Received in CO

BIOLOGICAL RESOURCES
NO SURVEY OR FURTHER
COORDINATION REQUIRED
8-28-09
Thomas C. Brooks
SIGNED _____ DATE _____



Illinois Department of Natural Resources

One Natural Resources Way Springfield, Illinois 62702-1271
<http://dnr.state.il.us>

Pat Quinn, Governor
Marc Miller, Director

August 12, 2009

Susan Dees
Illinois Department of Transportation-BDE
2300 South Dirksen Parkway
Room 330
Springfield, IL 62764

Re: FAI 57 @ I-294, P-91-186-08, seq. 14550A, Add. A
Project Number(s): 1001356 [14550A]
County: Cook

Dear Applicant:

This letter is in reference to the project you recently submitted for consultation. The natural resource review provided by EcoCAT identified protected resources that may be in the vicinity of the proposed action. The Department has evaluated this information and concluded that adverse effects are unlikely. Therefore, consultation under 17 Ill. Adm. Code Part 1075 is terminated.

This consultation is valid for two years unless new information becomes available that was not previously considered; the proposed action is modified; or additional species, essential habitat, or Natural Areas are identified in the vicinity. If the project has not been implemented within two years of the date of this letter, or any of the above listed conditions develop, a new consultation is necessary.

The natural resource review reflects the information existing in the Illinois Natural Heritage Database at the time of the project submittal, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, you must comply with the applicable statutes and regulations. Also, note that termination does not imply IDNR's authorization or endorsement of the proposed action.

Please contact me if you have questions regarding this review.

Steve Hamer
Division of Ecosystems and Environment
217-785-5500

Applicant: Illinois Department of Transportation-BDE
Contact: Susan Dees
Address: 2300 South Dirksen Parkway
Springfield, IL 62764

IDNR Project #: 1001356
Alternate #: 14550A
Date: 08/11/2009

Project: FAI 57 @ I-294, P-91-186-08, seq. 14550A, Add. A
Address: n/a, n/a

Description: Orig. job for new interchange I-57 @ I-294, etc. cleared by IDNR after extensive coordination; Add. A for addit. unknown quantity of ROW for value engineering & revised geometry. Huff & Huff did wetland delineations and found none.

Natural Resource Review Results

Consultation for Endangered Species Protection and Natural Areas Preservation (Part 1075)

The Illinois Natural Heritage Database shows the following protected resources may be in the vicinity of the project location:

Dropseed Prairie INAI Site
Gensburg - Markham Prairie INAI Site
Markham Prairie - East INAI Site
Paintbrush Prairie INAI Site
Sundrop Prairie INAI Site
Dropseed Prairie Nature Preserve
Gensburg-Markham Prairie Addition Natural Heritage Landmark
Gensburg-Markham Prairie Nature Preserve
Paintbrush Prairie Nature Preserve
Paintbrush Prairie Nature Preserve
Sundrop Prairie Nature Preserve
Alkali Bulrush (*Bolboschoenus maritimus*)
Eastern Prairie Fringed Orchid (*Platanthera leucophaea*)
Eryngium Stem Borer (*Papaipema eryngii*)
Henslow'S Sparrow (*Ammodramus henslowii*)

An IDNR staff member will evaluate this information and contact you within 30 days to request additional information or to terminate consultation if adverse effects are unlikely.

Location

The applicant is responsible for the accuracy of the location submitted for the project.

County: Cook

Township, Range, Section:

36N, 13E, 11 36N, 13E, 12
36N, 13E, 13 36N, 13E, 14



IL Department of Natural Resources Contact

Steve Hamer
217-785-5500
Division of Ecosystems & Environment

Local or State Government Jurisdiction

IL Department of Transportation
Susan Dees
2300 South Dirksen Parkway
Room 330
Springfield, Illinois 62764

Disclaimer

The Illinois Natural Heritage Database cannot provide a conclusive statement on the presence, absence, or condition of natural resources in Illinois. This review reflects the information existing in the Database at the time of this inquiry, and should not be regarded as a final statement on the site being considered, nor should it be a substitute for detailed site surveys or field surveys required for environmental assessments. If additional protected resources are encountered during the project's implementation, compliance with applicable statutes and regulations is required.

Terms of Use

By using this website, you acknowledge that you have read and agree to these terms. These terms may be revised by IDNR as necessary. If you continue to use the EcoCAT application after we post changes to these terms, it will mean that you accept such changes. If at any time you do not accept the Terms of Use, you may not continue to use the website.

1. The IDNR EcoCAT website was developed so that units of local government, state agencies and the public could request information or begin natural resource consultations on-line for the Illinois Endangered Species Protection Act, Illinois Natural Areas Preservation Act, and Illinois Interagency Wetland Policy Act. EcoCAT uses databases, Geographic Information System mapping, and a set of programmed decision rules to determine if proposed actions are in the vicinity of protected natural resources. By indicating your agreement to the Terms of Use for this application, you warrant that you will not use this web site for any other purpose.
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EcoCAT operates on a state of Illinois computer system. We may use software to monitor traffic and to identify unauthorized attempts to upload, download, or change information, to cause harm or otherwise to damage this site. Unauthorized attempts to upload, download, or change information on this server is strictly prohibited by law. Unauthorized use, tampering with or modification of this system, including supporting hardware or software, may subject the violator to criminal and civil penalties. In the event of unauthorized intrusion, all relevant information regarding possible violation of law may be provided to law enforcement officials.

Privacy

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Illinois Department of Transportation

Memorandum

To: Diane O'Keefe Attn: Pete Harmet
From: Charles J. Ingersoll By: Barbara H. Stevens
Subject: Final Preliminary Site Investigation Report
Date: October 14, 2009

Barbara H. Stevens

Refer to: FAI 57 (I-57)
Job No. P91-186-08
I-57 at I-294
Cook County
ISGS #1773 Sequence #14550
Weston5 Work Order #046

Attached is a copy of the completed Preliminary Site Investigation (PSI) Report submitted October 1, 2009 by Weston Solutions regarding the above referenced project. Based on the recommendations of the PSI report, if the District wants to pursue construction in the area of soil contamination, then the Contractor shall be responsible for hiring an Environmental Firm with at least five (5) documented leaking underground storage tanks (LUST) cleanups or that is pre-qualified in hazardous waste by the Department to remediate the soil contamination and monitor for worker protection.

An estimated quantity of potentially non-special waste has been included in the PSI report. The impacted soils would be classified as a non-special waste. The estimated cost associated with contaminated soil is \$32,900 at the Former Xtreme Auto Sales, \$63,900 at the John's Hot Dogs, \$73,100 at Walgreens, \$67,100 at the Citgo Gas Station, \$70,400 at the Valero Gas Station, \$177,700 at the Closed Gasoline Station, \$46,500 at the Dixmoor Fruit & Meat Market, \$85,200 at the Former Marathon Station, \$34,700 at the Harvey Express, and \$8,300 at the U Pull It. All utility companies relocating within the following areas should be notified of the potential soil contamination and the attached special provision shall be included in the contract plans.

1. Station 1005+40 to Station 1006+60 0 to 100 feet RT (John's Hot Dogs, Site 1773-3, 3201 West 147th Street – Midlothian) – non-special waste. Contaminants of concern sampling parameters: BETX, SVOCs, and Arsenic.
2. Station 1005+40 to Station 1006+60 0 to 100 feet LT (Former Xtreme Auto Sales, Site 1773-2, 3202 West 147th Street - Midlothian) – non-special waste. Contaminants of concern sampling parameters: BETX and SVOCs.
3. Station 1006+60 to Station 1007+75 0 to 100 feet LT (CITGO, Site 1773-5, 3144 West 147th Street - Posen) – non-special waste. Contaminants of concern sampling parameters: BETX and Arsenic.
4. Station 1009+80 to Station 1010+90 0 to 90 feet LT (Valero Gasoline Station, Site 1773-6, 3118 West 147th Street - Posen) – non-special waste. Contaminants of concern sampling parameters: BETX and Arsenic.

5. Station 1038+25 to Station 1040+00 0 to 120 feet LT (Closed Gasoline Station, Site 1773-14, 2700 West 147th Street, Posen) – non-special waste. Contaminants of concern sampling parameters: BETX, SVOCs, and Arsenic.
6. Station 6014+00 to Station 6015+50 0 to 80 feet RT (Dixmoor Fruit & Meat Market, Site 1773-26, 14635 Western Avenue - Dixmoor) – non-special waste. Contaminants of concern sampling parameters: BETX and SVOCs.
7. Station 1077+50 to Station 1079+50 0 to 70 feet RT (Former Marathon Station, Site 1773-27, 288 West Sibley Boulevard - Harvey) – non-special waste. Contaminants of concern sampling parameters: BETX and SVOCs.
8. Station 6005+35 to Station 6006+40 0 to 60 feet RT (Harvey Express, Site 1773-33, 14745 Dixie Highway - Harvey) – non-special waste. Contaminants of concern sampling parameters: BETX and SVOCs.
9. Station 1319+80 to Station 1321+00 0 to 160 feet LT (U Pull It, Site 1773-36, 2247 West 141st Street - Dixmoor) – non-special waste. Contaminants of concern sampling parameters: BETX, SVOCs, Chromium, and Manganese.

Some the soils identified above requiring management as a non-special waste can be managed on-site as fill. Although the soil concentrations exceed a residential property's Tier 1 soil remediation objective for the ingestion exposure pathway, they can be utilized on-site as fill because the roadway is not considered a residential property. All storm sewer excavated soils can be placed back into the excavated trench as trench backfill. If the soils cannot be utilized on-site as fill then they must be managed off-site as a non-special waste. The following areas can be managed on-site as fill.

1. Station 1006+60 to Station 1007+75 0 to 100 feet RT (Walgreens, Site 1773-4, 3145 West 147th Street - Posen) – non-special waste. Contaminants of concern sampling parameters: Arsenic.
2. Station 1318+00 to Station 1319+00 0 to 160 feet RT (U Pull It, Site 1773-36, 2247 West 141st Street - Dixmoor) – non-special waste. Contaminants of concern sampling parameters: Manganese.

Any waste generated as a special waste or a waste not certified as a non-special waste from this project should be manifested off-site using the generator number associated with Cook County. **The generator number for Cook County is 0318995023.**

The pay item in the Special Provision should be included in the contact plans with the following quantities.

Pay Item Number	Pay Item	Quantity
66900200	NON-SPECIAL WASTE DISPOSAL.	10,707 cubic yards
66900400	SPECIAL WASTE GROUNDWATER DISPOSAL.	9,600 gallons
66900450	SPECIAL WASTE PLANS AND REPORTS.	Lump Sum
66901000	BACKFILL PLUGS.	70 cubic yards
66900530	SOIL DISPOSAL ANALYSIS.	8 Each

Memorandum
October 14, 2009
Page 3 of 3

It is the opinion of this office in consultation with Chief Council, that the remedial work be documented for potential illegal trespass action. If you have any questions or comments, please contact Steven Gobelman at 217/785-4246.

Attachment

S:\GEN\WPDOCS\GOBELMAN\Districts\Distr1\PSI\Weston5\46psidf.docx

cc: Weston Solutions (w/o attachments)
District Land Acquisition (w/o attachments)
District Utilities Coordinator (w/o attachments)

Appendix C
Noise Analysis Results Summary Table

**Table C-1: Summary of Existing (2003) and Future (2030)
Traffic Noise Levels**

Receptor	Reference Exhibit	IDOT Approach Noise Abatement Criteria	Traffic Noise Level			Increase (Build minus Existing)
			Existing (2003)	No-Action (2030)	Build (2030)	
A-1	Exhibit E-4	71	64	67	66	2
A-2	Exhibit E-4	66	66	69	66	0
A-3	Exhibit E-4	66	63	66	62	-1
A-4	Exhibit E-4	66	65	67	64	-1
A-5	Exhibit E-4	66	64	67	63	-1
A-6	Exhibit E-4	71	65	68	64	-1
A-7	Exhibit E-3	66	67	70	66	-1
A-8	Exhibit E-4	66	63	66	62	-1
A-9	Exhibit E-3	66	62	65	62	0
A-10	Exhibit E-3	66	63	66	64	1
A-11	Exhibit E-3	66	64	66	64	0
A-12	Exhibit E-3	71	64	67	65	1
A-13	Exhibit E-3	66	60	62	62	2
A-14	Exhibit E-3	71	63	65	65	2
A-15	Exhibit E-3	71	64	66	67	3
A-16	Exhibit E-3	66	60	62	62	2
A-17	Exhibit E-3	71	62	64	65	3
A-18	Exhibit E-3	71	59	61	61	2
A-19	Exhibit E-3	71	63	63	63	0
A-20	Exhibit E-3	66	58	59	60	2
A-21	Exhibit E-3	71	65	66	64	-1
A-22	Exhibit E-3	66	58	59	60	2
A-23	Exhibit E-3	66	60	61	60	0
A-24	Exhibit E-3	66	63	63	62	-1
A-25	Exhibit E-3	66	59	59	59	0
B-1	Exhibit E-4	66	62	63	65	3
B-2	Exhibit E-4	71	62	63	68	6
B-3	Exhibit E-4	66	61	63	65	4
B-4	Exhibit E-4	66	59	61	63	4
B-5	Exhibit E-4	66	60	62	62	2
B-6	Exhibit E-4	66	62	64	65	3
B-7	Exhibit E-4	66	61	63	64	3
B-8	Exhibit E-4	66	65	66	68	3
B-9	Exhibit E-4	66	63	65	66	3
B-10	Exhibit E-4	66	61	63	63	2
B-11	Exhibit E-4	66	60	62	63	3
B-12	Exhibit E-4	66	62	64	65	3
B-13	Exhibit E-4	66	61	62	62	1
B-14	Exhibit E-4	66	65	66	66	1
B-15	Exhibit E-4	66	60	62	61	1
B-16	Exhibit E-4	66	63	65	63	0
B-17	Exhibit E-4	66	62	64	63	1
B-18	Exhibit E-4	66	66	68	66	0
B-19	Exhibit E-4	66	61	63	63	2
B-20	Exhibit E-3	66	64	66	65	1
B-21	Exhibit E-3	71	66	68	66	0
B-22	Exhibit E-3	71	62	64	64	2
B-23	Exhibit E-3	71	61	62	63	2

Note: The modeled traffic noise levels are a result of noise generated from I-294, I-57 and 147th Street. Other major roadways, i.e. Dixie Highway, Kedzie Avenue, were not included in the model. Train noise was not included in the model either. Shaded areas represent traffic noise levels that approach (within 1 dBA) or exceed the NAC.

** - Properties that will be relocated by the Interchange Project. Refer Environmental Assessment Report for final list.*

**Table C-1: Summary of Existing (2003) and Future (2030)
Traffic Noise Levels (Continued)**

Receptor	Reference Exhibit	IDOT Approach Noise Abatement Criteria	Traffic Noise Level			Increase (Build minus Existing)
			Existing (2003)	No-Action (2030)	Build (2030)	
B-24	Exhibit E-4	66	60	62	63	3
B-25	Exhibit E-4	66	62	63	64	2
B-26	Exhibit E-4	66	61	62	62	1
B-27	Exhibit E-4	66	64	67	68	4
B-28	Exhibit E-4	66	61	62	63	2
B-29	Exhibit E-4	66	62	64	65	3
C-1	Exhibit E-3	66	61	62	61	0
C-2	Exhibit E-3	66	64	65	63	-1
C-3	Exhibit E-3	66	59	60	60	1
C-4	Exhibit E-3	71	63	64	63	0
C-5	Exhibit E-3	71	66	67	66	0
C-6	Exhibit E-3	71	57	58	60	3
C-7	Exhibit E-3	71	65	67	67	2
C-8	Exhibit E-3	71	64	67	66	2
C-9	Exhibit E-3	66	64	67	67	3
C-10	Exhibit E-3, E-7	66	63	66	66	3
C-11	Exhibit E-3, E-7	66	60	63	63	3
C-12	Exhibit E-3, E-7	66	63	67	68	5
C-13	Exhibit E-3, E-7	66	61	64	64	3
C-14	Exhibit E-3, E-7	66	61	63	65	4
C-15	Exhibit E-3, E-7	66	63	66	67	4
C-16	Exhibit E-3, E-7	66	66	64	68	2
C-17	Exhibit E-3, E-7	66	64	64	67	3
C-18	Exhibit E-3, E-7	66	63	63	66	3
C-19	Exhibit E-3, E-7	66	64	61	67	3
C-20	Exhibit E-3, E-7	66	66	62	69	3
C-21	Exhibit E-3, E-6	66	64	61	67	3
C-22	Exhibit E-6	66	72	64	*	3
C-23	Exhibit E-6	66	64	60	68	4
C-24	Exhibit E-6	66	62	59	65	3
C-25	Exhibit E-6	66	71	63	*	0
C-26	Exhibit E-6	66	64	60	68	4
C-27	Exhibit E-6	66	68	63	70	2
C-28	Exhibit E-6	66	72	64	72	0
C-29	Exhibit E-6	71	70	70	71	1
C-30	Exhibit E-6	66	68	65	68	0
C-31	Exhibit E-6	66	72	71	72	0
C-32	Exhibit E-6	66	75	73	*	-2
C-33	Exhibit E-6	66	70	63	72	2
C-34	Exhibit E-6	66	62	59	69	7
C-35	Exhibit E-6	66	66	61	71	5
C-36	Exhibit E-3, E-6	66	67	62	71	4
C-37	Exhibit E-3, E-6	66	67	62	70	3
C-38	Exhibit E-3, E-6	66	71	64	*	3
C-39	Exhibit E-3, E-6	66	63	61	66	3
C-40	Exhibit E-3, E-6	66	66	62	68	2
C-41	Exhibit E-3, E-6	66	65	62	68	3
C-42	Exhibit E-3, E-6	66	68	64	70	2
C-43	Exhibit E-3, E-6	66	68	64	71	3

*Note: The modeled traffic noise levels are a result of noise generated from I-294, I-57 and 147th Street. Other major roadways, i.e. Dixie Highway, Kedzie Avenue, were not included in the model. Train noise was not included in the model either. Shaded areas represent traffic noise levels that approach (within 1 dBA) or exceed the NAC. * - Properties that will be relocated by the Interchange Project. Refer Environmental Assessment Report for final list.*

**Table C-1: Summary of Existing (2003) and Future (2030)
Traffic Noise Levels (Continued)**

Receptor	Reference Exhibit	IDOT Approach Noise Abatement Criteria	Traffic Noise Level			Increase (Build minus Existing)
			Existing (2003)	No-Action (2030)	Build (2030)	
C-44	Exhibit E-3, E-6	66	63	67	68	5
C-45	Exhibit E-3, E-6	66	62	66	66	4
C-46	Exhibit E-3, E-6	66	61	64	64	3
D-1	Exhibit E-3	66	59	60	62	3
D-2	Exhibit E-3	66	58	59	62	4
D-3	Exhibit E-3	66	57	58	61	4
D-4	Exhibit E-3	66	57	58	61	4
D-5	Exhibit E-3	66	60	62	63	3
D-6	Exhibit E-3	66	60	62	63	3
D-7	Exhibit E-3	66	59	61	63	4
D-8	Exhibit E-3	66	59	60	62	3
D-9	Exhibit E-3	71	63	65	66	3
E-1	Exhibit E-2	71	61	64	63	2
E-2	Exhibit E-2	66	58	61	62	4
E-3	Exhibit E-2	66	60	63	65	5
E-4	Exhibit E-2	66	57	59	61	4
E-5	Exhibit E-2	66	59	61	64	5
E-6	Exhibit E-2	66	67	71	72	5
E-7	Exhibit E-2	66	59	63	65	6
E-8	Exhibit E-2	66	57	61	63	6
E-9	Exhibit E-1	66	60	63	63	3
E-10	Exhibit E-1	66	69	71	70	1
E-11	Exhibit E-1	66	62	64	64	2
E-12	Exhibit E-1	66	66	68	67	1
E-13	Exhibit E-1	66	60	63	63	3
E-14	Exhibit E-1	66	62	64	64	2
E-15	Exhibit E-1	66	64	66	66	2
E-16	Exhibit E-1	66	54	56	56	2
E-17	Exhibit E-1	66	69	71	70	1
E-18	Exhibit E-1	66	70	73	71	1
F-1	Exhibit E-1	66	69	70	70	1
F-2	Exhibit E-1	66	67	68	67	0
F-3	Exhibit E-1	66	65	66	65	0
F-4	Exhibit E-1	66	61	62	61	0
F-5	Exhibit E-1	66	59	62	61	2
F-6	Exhibit E-1	66	59	61	61	2
F-7	Exhibit E-2	66	62	64	65	3
F-8	Exhibit E-2	66	60	62	63	3
F-9	Exhibit E-2	66	63	65	66	3
F-10	Exhibit E-2	66	59	61	63	4
F-11	Exhibit E-2	66	59	61	63	4
F-12	Exhibit E-2	66	64	65	66	2
F-13	Exhibit E-2	66	59	61	63	4
F-14	Exhibit E-7	66	63	64	70	7
F-15	Exhibit E-7	66	64	66	66	2
F-16	Exhibit E-8	66	66	68	68	2

Note: The modeled traffic noise levels are a result of noise generated from I-294, I-57 and 147th Street. Other major roadways, i.e. Dixie Highway, Kedzie Avenue, were not included in the model. Train noise was not included in the model either.

Shaded areas represent traffic noise levels that approach (within 1 dBA) or exceed the NAC.

** - Properties that will be relocated by the Interchange Project. Refer Environmental Assessment Report for final list.*

**Table C-1: Summary of Existing (2003) and Future (2030)
Traffic Noise Levels (Continued)**

Receptor	Reference Exhibit	IDOT Approach Noise Abatement Criteria	Traffic Noise Level			Increase (Build minus Existing)
			Existing (2003)	No-Action (2030)	Build (2030)	
F-17	Exhibit E-2	66	67	68	67	0
G-1	Exhibit E-1	66	69	71	70	1
G-2	Exhibit E-1	66	65	67	66	1
G-3	Exhibit E-1	66	63	65	64	1
G-4	Exhibit E-1	66	60	62	62	2
G-5	Exhibit E-1	66	59	62	61	2
G-6	Exhibit E-1	66	61	63	62	1
G-7	Exhibit E-1	66	59	61	62	3
H-1	Exhibit E-1	66	60	61	63	3
H-2	Exhibit E-1	66	65	66	67	2
H-3	Exhibit E-1	66	60	62	63	3
H-4	Exhibit E-1	66	62	64	64	2
H-5	Exhibit E-1	71	70	71	70	0
H-6	Exhibit E-1	66	65	66	67	2
H-7	Exhibit E-1	66	60	61	63	3
I-1	Exhibit E-5	66	72	62	65	-7
I-2	Exhibit E-5	66	69	60	65	-4
I-3	Exhibit E-5	66	72	62	64	-8
I-4	Exhibit E-5	66	69	60	64	-5
I-5	Exhibit E-5	66	65	57	63	-2
I-6	Exhibit E-5	66	65	58	61	-4
I-7	Exhibit E-5	66	71	62	62	-9
I-8	Exhibit E-5	66	69	60	62	-7
I-9	Exhibit E-5	66	71	62	61	-10
I-10	Exhibit E-5	66	68	59	61	-7
I-11	Exhibit E-5	66	70	61	61	-9
I-12	Exhibit E-5	66	68	60	61	-7
I-13	Exhibit E-5	66	66	58	61	-5
I-14	Exhibit E-5	66	69	60	*	-
I-15	Exhibit E-5	66	68	59	64	-4
I-16	Exhibit E-5	66	65	59	64	-1
I-17	Exhibit E-5	66	67	67	65	-2
I-18	Exhibit E-6	66	67	67	*	-
I-19	Exhibit E-6	66	71	70	*	-
I-20	Exhibit E-6	66	69	68	66	-2
I-21	Exhibit E-6	66	68	67	67	-1
I-22	Exhibit E-6	66	63	60	67	3
I-23	Exhibit E-6	66	64	59	66	2
I-24	Exhibit E-6	66	61	58	63	2
I-25	Exhibit E-6	66	71	64	*	-
I-26	Exhibit E-6	66	65	61	66	1
I-27	Exhibit E-6	66	69	62	69	-1
I-28	Exhibit E-6	66	71	63	*	-
I-29	Exhibit E-6	66	70	63	67	-3
I-30	Exhibit E-6	66	69	65	68	-1
I-31	Exhibit E-6	66	73	71	*	-
I-32	Exhibit E-6	66	73	72	*	-
I-33	Exhibit E-6	71	70	70	70	0

Note: The modeled traffic noise levels are a result of noise generated from I-294, I-57 and 147th Street. Other major roadways, i.e. Dixie Highway, Kedzie Avenue, were not included in the model. Train noise was not included in the model either.

Shaded areas represent traffic noise levels that approach (within 1 dBA) or exceed the NAC.

** - Properties that will be relocated by the Interchange Project. Refer Environmental Assessment Report for final list.*

**Table C-1: Summary of Existing (2003) and Future (2030)
Traffic Noise Levels (Continued)**

Receptor	Reference Exhibit	IDOT Approach Noise Abatement Criteria	Traffic Noise Level			Increase (Build minus Existing)
			Existing (2003)	No-Action (2030)	Build (2030)	
I-34	Exhibit E-6	66	63	62	63	0
I-35	Exhibit E-6	66	65	64	65	0
J-1	Exhibit E-3 , E-7	66	66	67	65	1
J-2	Exhibit E-3 , E-7	66	64	65	65	1
J-3	Exhibit E-3 , E-7	66	63	64	64	1
J-4	Exhibit E-3 , E-7	66	61	62	63	2
J-5	Exhibit E-3 , E-7	66	66	65	65	-1
J-6	Exhibit E-3 , E-7	66	64	63	64	0
J-7	Exhibit E-3 , E-7	66	66	64	65	-1
J-8	Exhibit E-3 , E-7	66	64	63	64	0
J-9	Exhibit E-3 , E-7	66	65	62	65	0
J-10	Exhibit E-3 , E-7	66	68	64	68	0
J-11	Exhibit E-3 , E-7	66	69	64	70	1
J-12	Exhibit E-3 , E-7	66	66	62	67	1
J-13	Exhibit E-7	66	64	61	65	1
J-14	Exhibit E-7	66	71	64	71	0
J-15	Exhibit E-7	66	67	63	69	2
J-16	Exhibit E-7	66	65	62	66	1
J-17	Exhibit E-7	66	73	65	74	1
J-18	Exhibit E-7	66	68	63	69	1
J-19	Exhibit E-7	66	66	63	67	1
J-20	Exhibit E-7	66	69	68	70	1
K-1	Exhibit E-5	66	65	60	61	-4
K-2	Exhibit E-5	66	63	58	61	-2
K-3	Exhibit E-5	66	68	67	61	-7
K-4	Exhibit E-5	66	68	65	60	-8
K-5	Exhibit E-5	66	66	63	59	-7
K-6	Exhibit E-5	66	68	66	62	-6
K-7	Exhibit E-5	66	67	65	61	-6
K-8	Exhibit E-5	66	67	65	61	-6
K-9	Exhibit E-5	66	70	72	74	4
K-10	Exhibit E-5 , E-6	66	69	69	69	0
K-11	Exhibit E-6	66	65	66	69	4
K-12	Exhibit E-6	66	62	57	68	6
K-13	Exhibit E-6	66	58	55	65	7
K-14	Exhibit E-6	66	59	55	66	7
K-15	Exhibit E-6	66	56	54	63	7
K-16	Exhibit E-6	66	57	55	64	7
K-17	Exhibit E-6	66	57	55	62	5
K-18	Exhibit E-6	66	62	61	66	3
K-19	Exhibit E-6	66	68	63	69	1
K-20	Exhibit E-6	66	66	63	68	2
K-21	Exhibit E-6	66	71	65	71	0
K-22	Exhibit E-6	66	63	61	65	2
K-23	Exhibit E-6	66	65	64	67	2
K-24	Exhibit E-6	66	69	67	69	-1
K-25	Exhibit E-6	71	68	68	67	-1

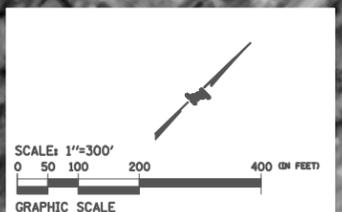
*Note: The modeled traffic noise levels are a result of noise generated from I-294, I-57 and 147th Street. Other major roadways, i.e. Dixie Highway, Kedzie Avenue, were not included in the model. Train noise was not included in the model either. Shaded areas represent traffic noise levels that approach (within 1 dBA) or exceed the NAC.
* - Properties that will be relocated by the Interchange Project. Refer Environmental Assessment Report for final list.*

**Table C-1: Summary of Existing (2003) and Future (2030)
Traffic Noise Levels (Continued)**

Receptor	Reference Exhibit	IDOT Approach Noise Abatement Criteria	Traffic Noise Level			Increase (Build minus Existing)
			Existing (2003)	No-Action (2030)	Build (2030)	
K-26	Exhibit E-6	66	67	68	66	-1
K-27	Exhibit E-6	66	56	56	65	9
K-28	Exhibit E-6	66	57	56	65	8
K-29	Exhibit E-6	66	58	56	65	7
K-30	Exhibit E-6	66	62	57	63	6
K-31	Exhibit E-6	66	65	58	64	6
K-32	Exhibit E-6	66	64	58	69	6
K-33	Exhibit E-6	66	67	60	71	4
K-34	Exhibit E-6	66	67	59	71	4
K-35	Exhibit E-6	66	66	58	70	4
K-36	Exhibit E-6	66	65	61	69	4
K-37	Exhibit E-6	66	68	63	70	2
K-38	Exhibit E-6	66	60	56	67	7
K-39	Exhibit E-6	66	63	58	69	6
K-40	Exhibit E-6	66	59	57	66	7
K-41	Exhibit E-6	66	58	57	64	7
K-42	Exhibit E-6	66	58	56	66	8
L-1	Exhibit E-6	66	65	64	65	0
L-2	Exhibit E-6	71	69	70	68	-1
L-3	Exhibit E-6	66	68	66	68	0
L-4	Exhibit E-6	66	73	68	71	-2
L-5	Exhibit E-6	66	70	65	71	-1
L-6	Exhibit E-6	66	69	63	69	0
L-7	Exhibit E-6	66	63	60	65	2
L-8	Exhibit E-6	66	64	60	66	2
L-9	Exhibit E-6	66	68	63	69	1
L-10	Exhibit E-6	66	71	64	72	1
L-11	Exhibit E-6	66	69	63	71	2
L-12	Exhibit E-6	66	65	60	68	3
L-13	Exhibit E-6	66	62	58	65	3
L-14	Exhibit E-6	66	68	62	69	1
L-15	Exhibit E-6	66	70	64	72	2
L-16	Exhibit E-6	66	63	59	66	3
L-17	Exhibit E-6	66	67	62	69	2
L-18	Exhibit E-6	66	71	64	74	3
L-19	Exhibit E-6	66	62	58	66	4
L-20	Exhibit E-6	66	65	60	68	3
L-21	Exhibit E-6	66	69	63	72	3
L-22	Exhibit E-6	66	67	62	70	3
L-23	Exhibit E-6	66	63	59	66	3
L-24	Exhibit E-3 , E-7	66	65	61	68	3
L-25	Exhibit E-3 , E-7	66	60	57	64	4
L-26	Exhibit E-3, E-7	66	62	59	65	3
L-27	Exhibit E-7	66	67	62	71	4
L-28	Exhibit E-7	66	65	61	68	3

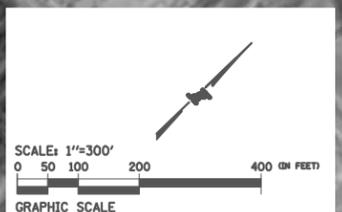
Note: The modeled traffic noise levels are a result of noise generated from I-294, I-57 and 147th Street. Other major roadways, i.e. Dixie Highway, Kedzie Avenue, were not included in the model. Train noise was not included in the model either. Shaded areas represent traffic noise levels that approach (within 1 dBA) or exceed the NAC.

** - Properties that will be relocated by the Interchange Project. Refer Environmental Assessment Report for final li*



Legend	
●	Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
●	Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project
X-X	Traffic Noise Receptor ID
XX dBA	Traffic Noise Levels Measured in the Field

Exhibit B-1
I-294-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 1 of 7



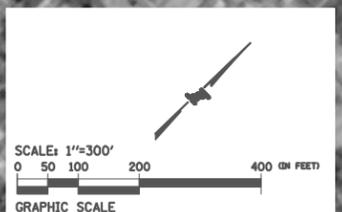
Legend

- Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
- Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project

X-X	Traffic Noise Receptor ID
60 dBA	F-1
69 dBA	F-9

XX dBA Traffic Noise Levels Measured in the Field

Exhibit B-2
I-294-I-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 2 of 7



Legend	
●	Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
●	Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project
<u>X-X</u>	Traffic Noise Receptor ID
<u>XX dBA</u>	Traffic Noise Levels Measured in the Field

Exhibit B-3
I-294I-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 3 of 7

← See Exhibit B-3

Dixie Highway

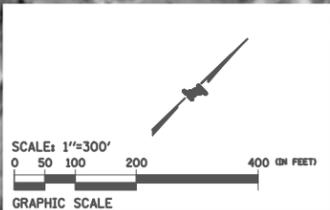
Grand Truck Railroad

I-57

B-18
67 dBA

B-16
64 dBA

B-27
68 dBA

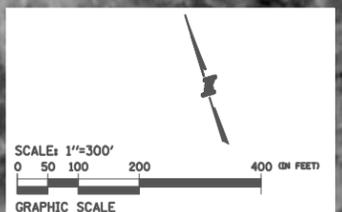


Legend

- Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
- Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project

X-X	Traffic Noise Receptor ID
XX dBA	Traffic Noise Levels Measured in the Field

Exhibit B-4
I-294-I-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 4 of 7



Legend	
●	Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
●	Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project
X-X	Traffic Noise Receptor ID
XX dBA	Traffic Noise Levels Measured in the Field

Exhibit B-5
I-294I-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 5 of 7

← See Exhibit B-5

See Exhibit B-7 →

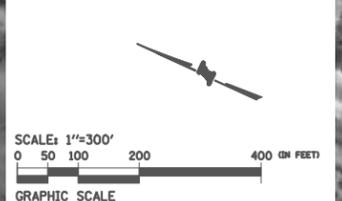


Legend

- Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
- Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project

X-X	Traffic Noise Receptor ID
XX dBA	Traffic Noise Levels Measured in the Field

Exhibit B-6
I-294I-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 6 of 7



FAHNS AVENUE

I-57

I-294

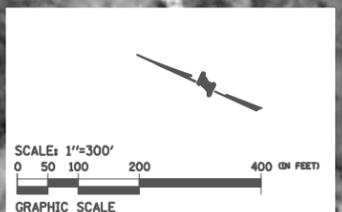
C-14
62 dBA

C-12
66 dBA

J-10
69 dBA

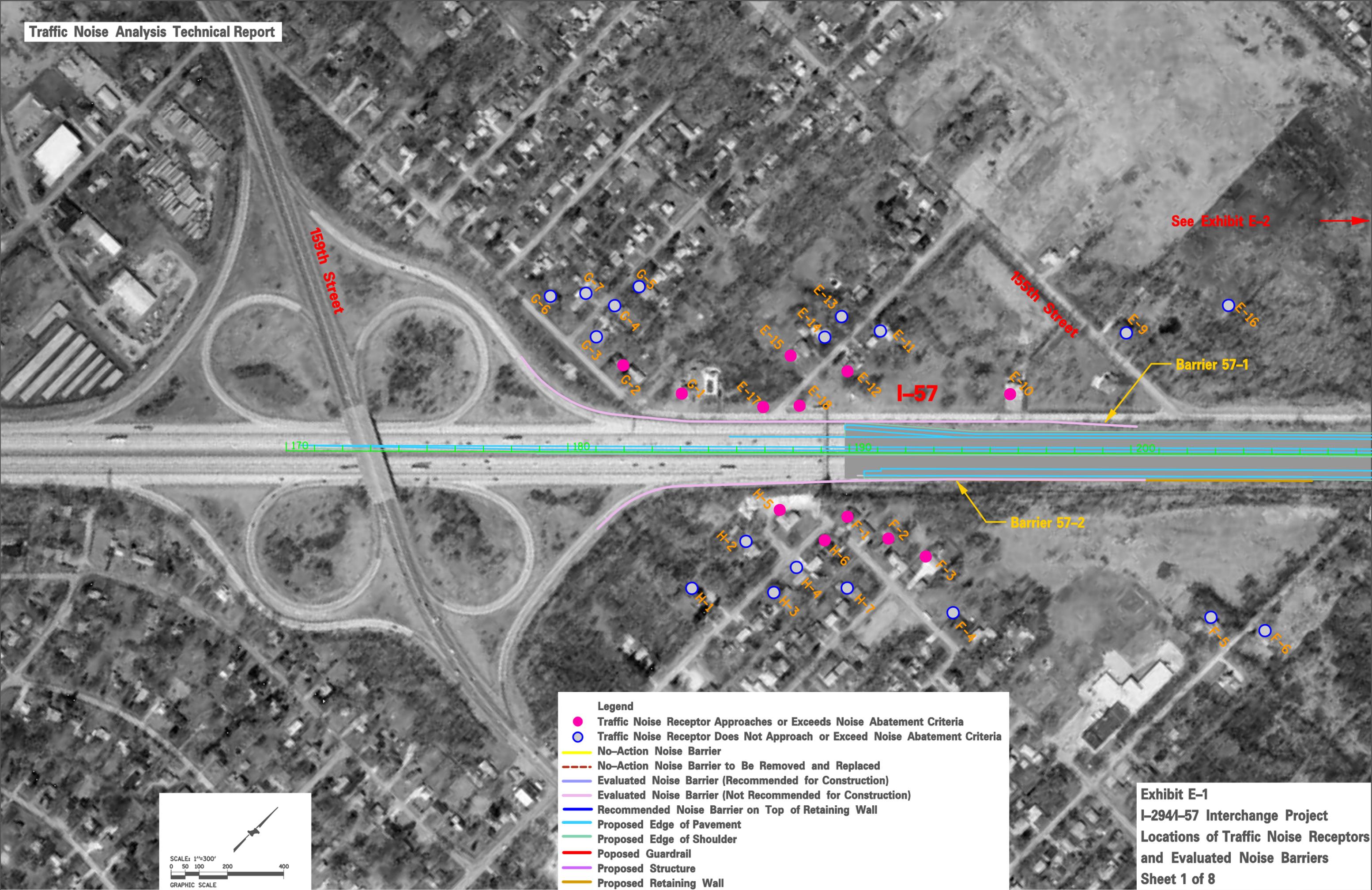
J-14
72 dBA

See Exhibit B-6



Legend	
●	Traffic Noise Receptor Location Measured by HDR Engineering, Inc.
●	Traffic Noise Receptor Location Measured as Part of the I-294 Widening Project
X-X	Traffic Noise Receptor ID
XX dBA	Traffic Noise Levels Measured in the Field

Exhibit B-7
I-294I-57 Interchange Project
Locations of Traffic Noise Receptors
with Field Noise Measurement
Sheet 7 of 7



See Exhibit E-2

159th Street

155th Street

I-57

Barrier 57-1

Barrier 57-2

Legend

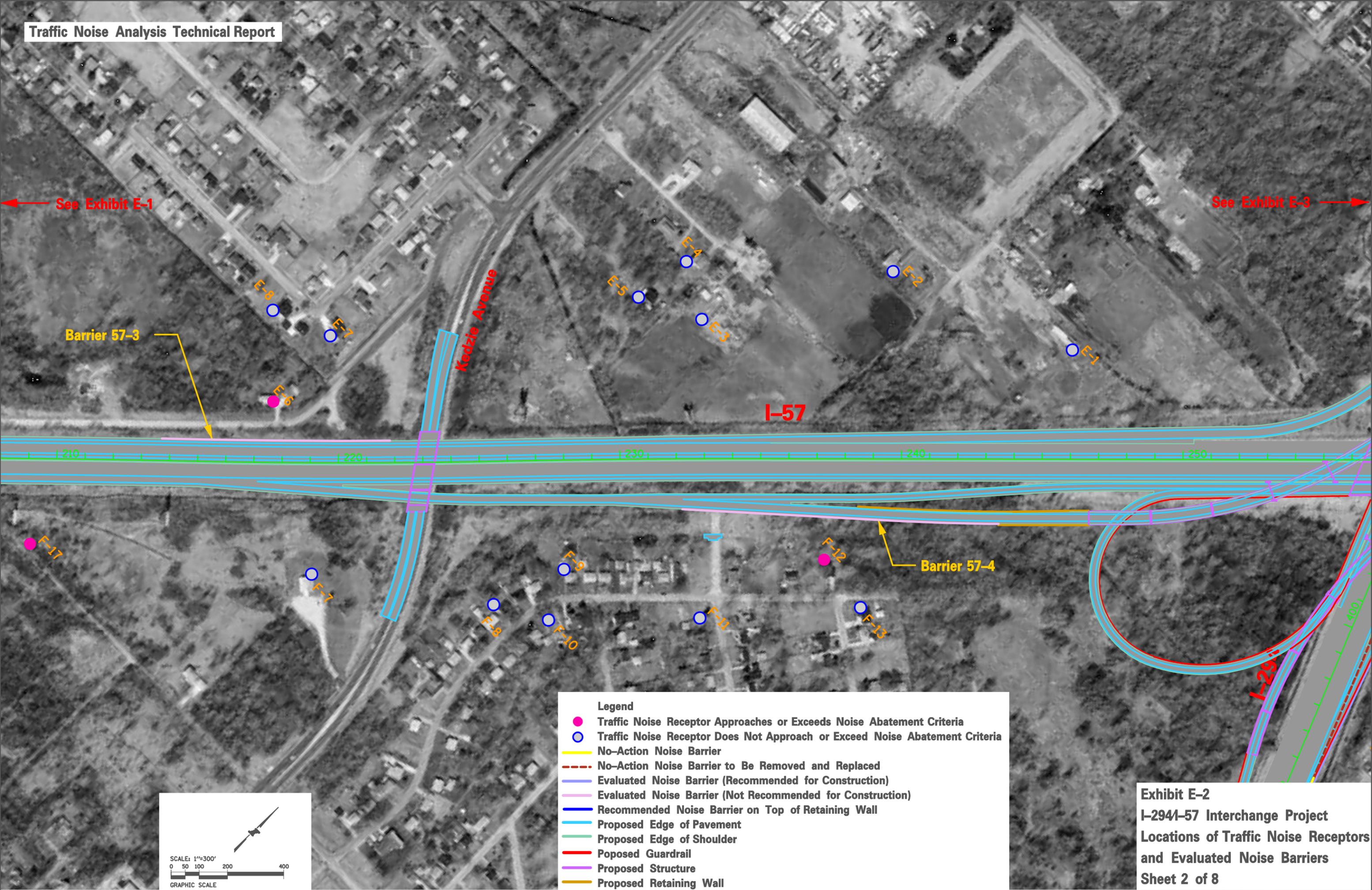
- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
- Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
- No-Action Noise Barrier
- - - No-Action Noise Barrier to Be Removed and Replaced
- Evaluated Noise Barrier (Recommended for Construction)
- Evaluated Noise Barrier (Not Recommended for Construction)
- Recommended Noise Barrier on Top of Retaining Wall
- Proposed Edge of Pavement
- Proposed Edge of Shoulder
- Proposed Guardrail
- Proposed Structure
- Proposed Retaining Wall

SCALE: 1"=300'

0 50 100 200 400

GRAPHIC SCALE

Exhibit E-1
I-294I-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
 Sheet 1 of 8



See Exhibit E-1

See Exhibit E-3

Barrier 57-3

I-57

Kedzie Avenue

Barrier 57-4

- Legend**
- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
 - Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
 - No-Action Noise Barrier
 - - - No-Action Noise Barrier to Be Removed and Replaced
 - Evaluated Noise Barrier (Recommended for Construction)
 - Evaluated Noise Barrier (Not Recommended for Construction)
 - Recommended Noise Barrier on Top of Retaining Wall
 - Proposed Edge of Pavement
 - Proposed Edge of Shoulder
 - Proposed Guardrail
 - Proposed Structure
 - Proposed Retaining Wall

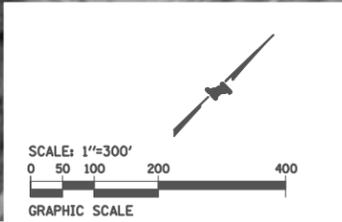
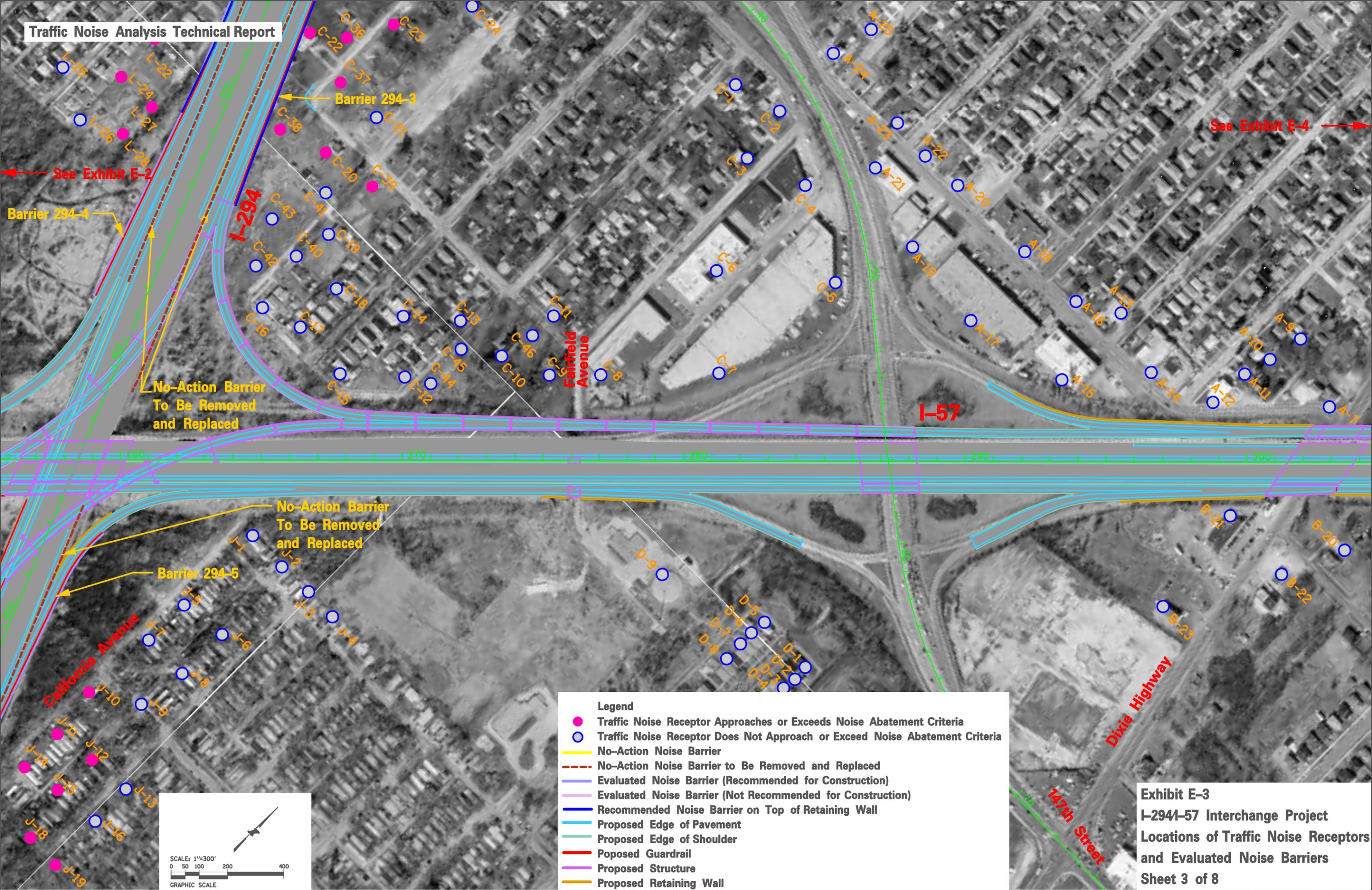


Exhibit E-2
I-294I-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
 Sheet 2 of 8



See Exhibit E-4

See Exhibit E-2

Legend

- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
- Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
- No-Action Noise Barrier
- - - No-Action Noise Barrier to Be Removed and Replaced
- Evaluated Noise Barrier (Recommended for Construction)
- Evaluated Noise Barrier (Not Recommended for Construction)
- Recommended Noise Barrier on Top of Retaining Wall
- Proposed Edge of Pavement
- Proposed Edge of Shoulder
- Proposed Guardrail
- Proposed Structure
- Proposed Retaining Wall

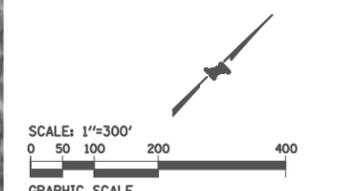
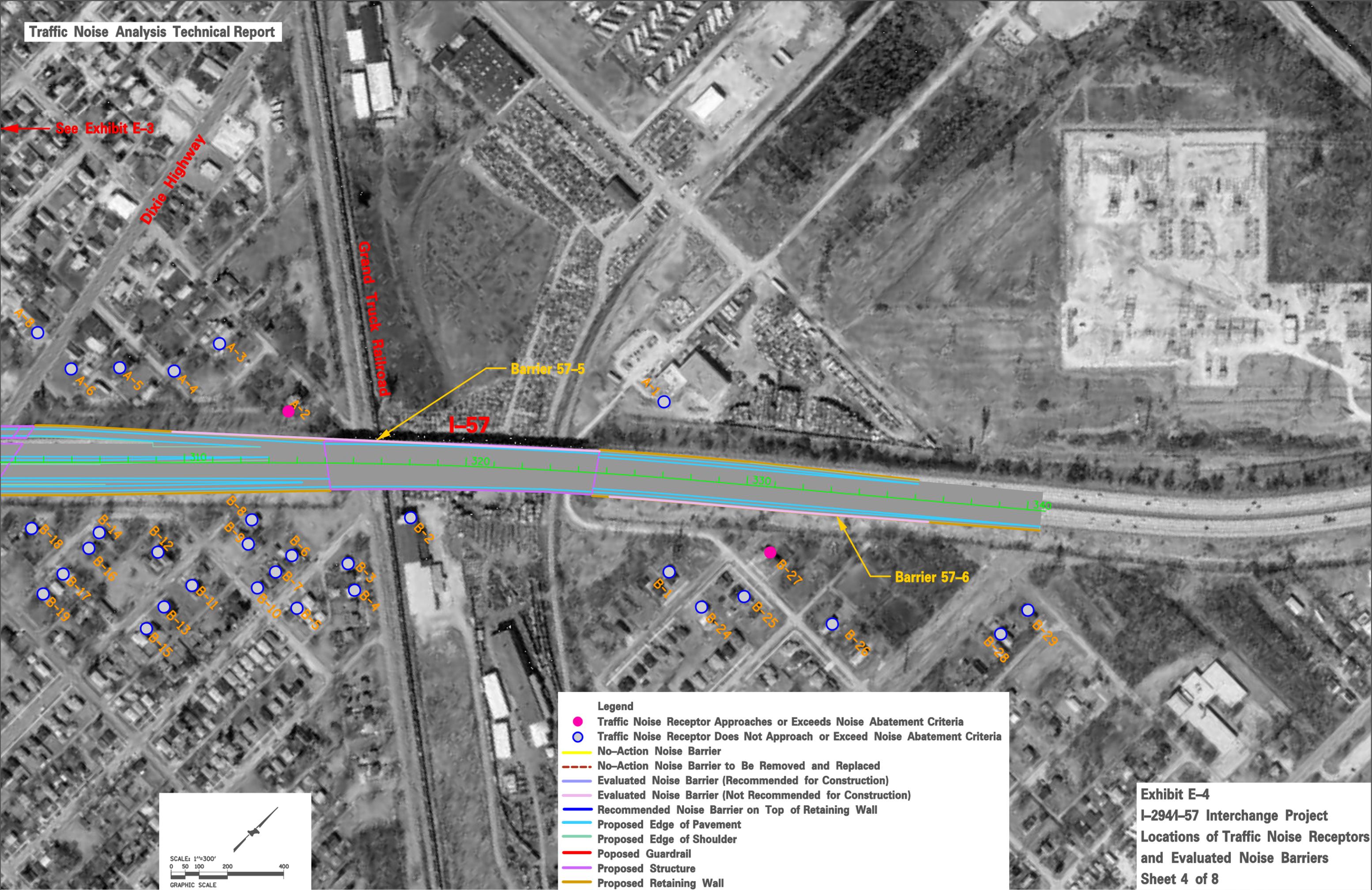


Exhibit E-3
I-294-I-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
 Sheet 3 of 8



See Exhibit E-3

Dixie Highway

Grand Truck Railroad

I-57

Barrier 57-5

Barrier 57-6

Legend

- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
- Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
- No-Action Noise Barrier
- - - No-Action Noise Barrier to Be Removed and Replaced
- Evaluated Noise Barrier (Recommended for Construction)
- Evaluated Noise Barrier (Not Recommended for Construction)
- Recommended Noise Barrier on Top of Retaining Wall
- Proposed Edge of Pavement
- Proposed Edge of Shoulder
- Proposed Guardrail
- Proposed Structure
- Proposed Retaining Wall

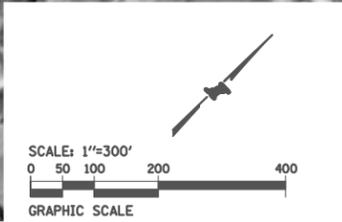
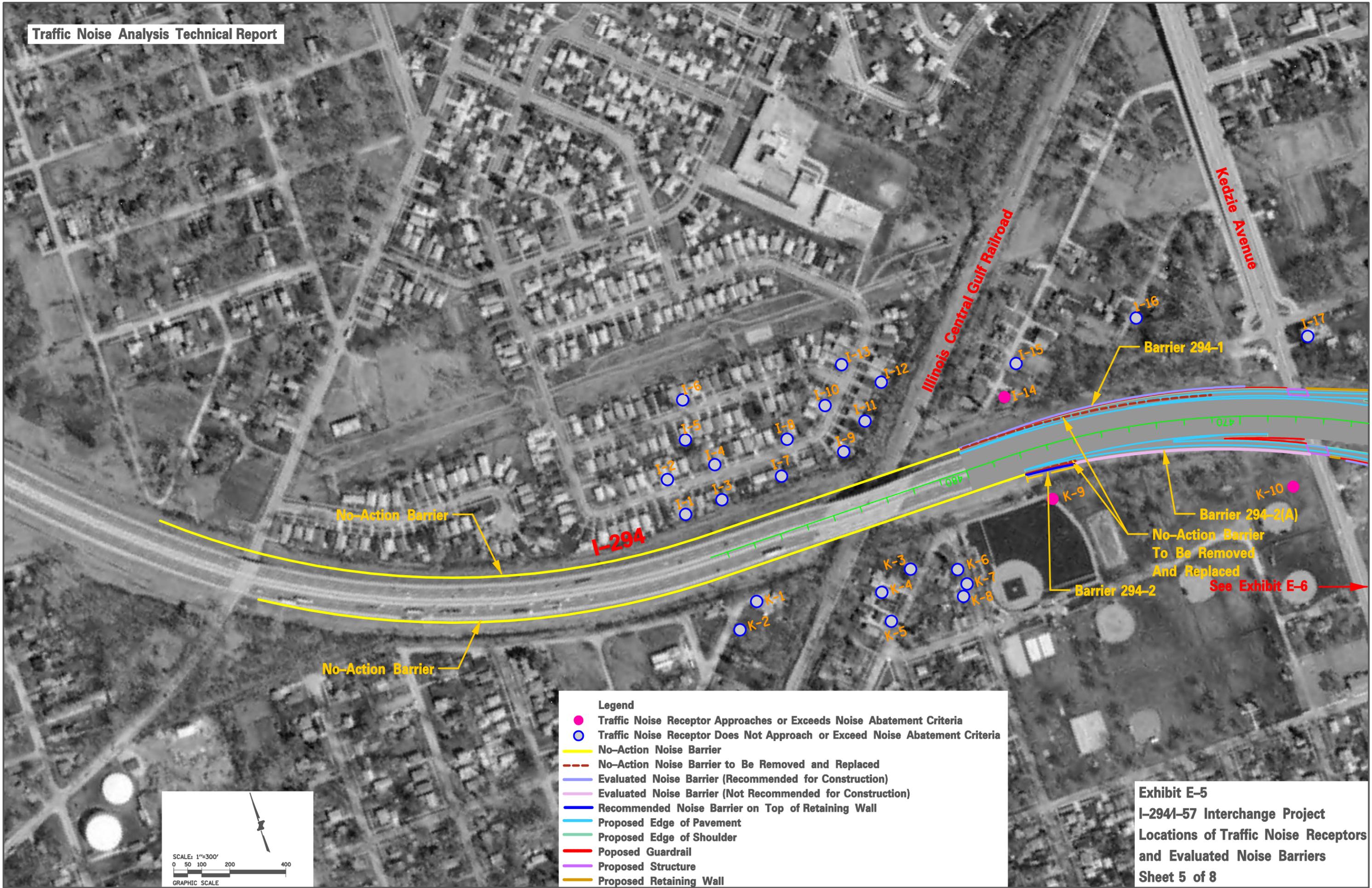


Exhibit E-4
I-294-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
 Sheet 4 of 8



Legend

- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
- Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
- No-Action Noise Barrier
- - - No-Action Noise Barrier to Be Removed and Replaced
- Evaluated Noise Barrier (Recommended for Construction)
- Evaluated Noise Barrier (Not Recommended for Construction)
- Recommended Noise Barrier on Top of Retaining Wall
- Proposed Edge of Pavement
- Proposed Edge of Shoulder
- Proposed Guardrail
- Proposed Structure
- Proposed Retaining Wall

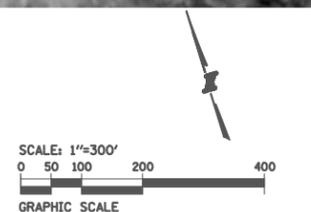


Exhibit E-5
I-294-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
Sheet 5 of 8



← See Exhibit E-5

See Exhibit E-7 →

No-Action Barrier
To Be Removed
And Replaced

Barrier 294-3

Barrier 294-4

147th Street

145th Street

Legend

- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
- Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
- No-Action Noise Barrier
- - - No-Action Noise Barrier to Be Removed and Replaced
- Evaluated Noise Barrier (Recommended for Construction)
- Evaluated Noise Barrier (Not Recommended for Construction)
- Recommended Noise Barrier on Top of Retaining Wall
- Proposed Edge of Pavement
- Proposed Edge of Shoulder
- Proposed Guardrail
- Proposed Structure
- Proposed Retaining Wall

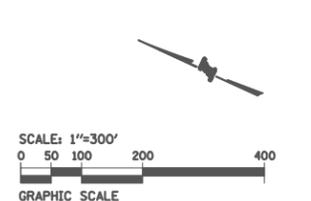
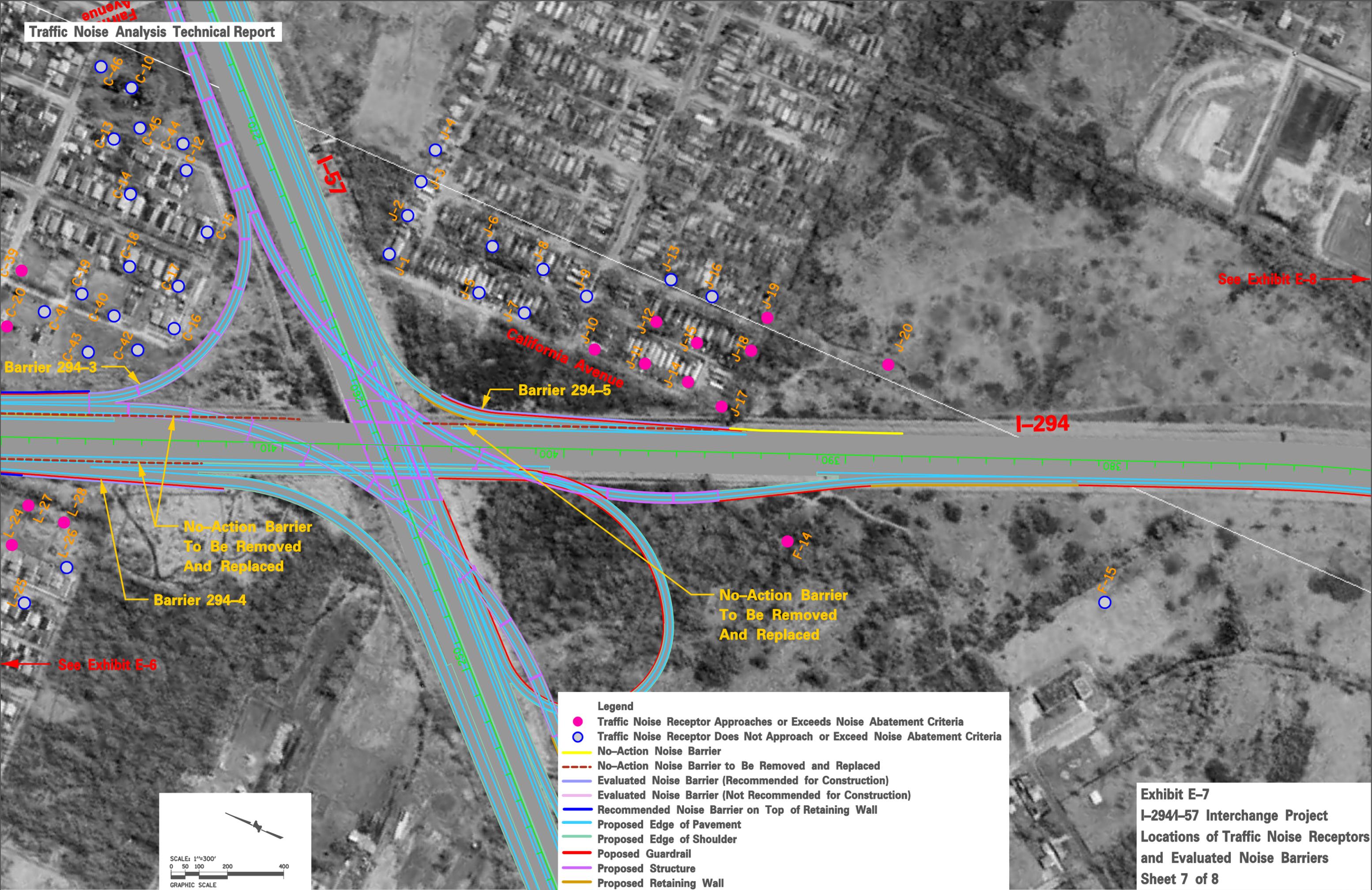


Exhibit E-6
I-294-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
Sheet 6 of 8



See Exhibit E-8

No-Action Barrier
To Be Removed
And Replaced

No-Action Barrier
To Be Removed
And Replaced

- Legend**
- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
 - Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
 - No-Action Noise Barrier
 - - - No-Action Noise Barrier to Be Removed and Replaced
 - Evaluated Noise Barrier (Recommended for Construction)
 - Evaluated Noise Barrier (Not Recommended for Construction)
 - Recommended Noise Barrier on Top of Retaining Wall
 - Proposed Edge of Pavement
 - Proposed Edge of Shoulder
 - Proposed Guardrail
 - Proposed Structure
 - Proposed Retaining Wall

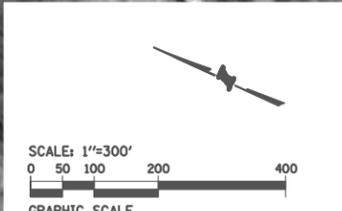
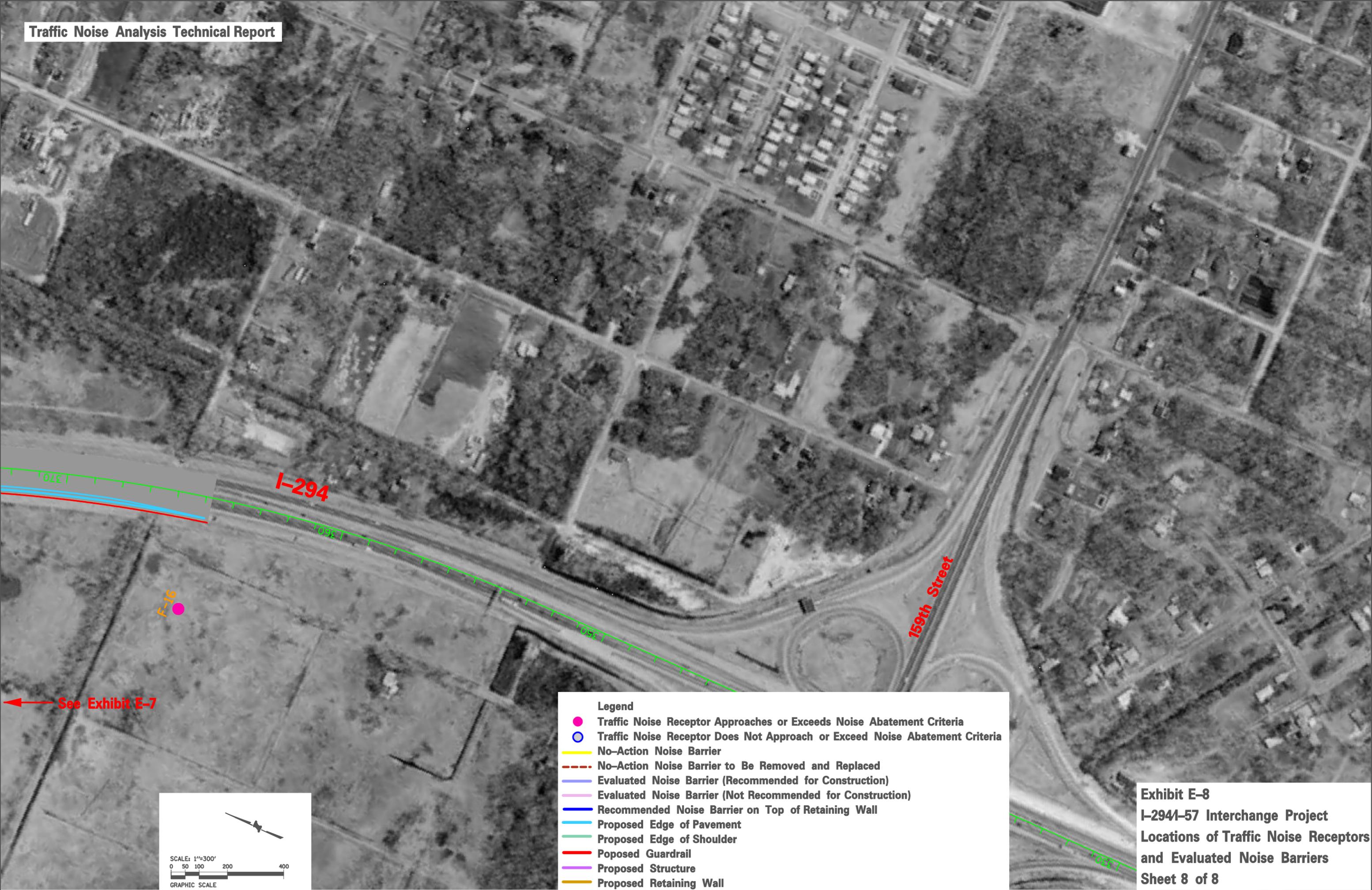


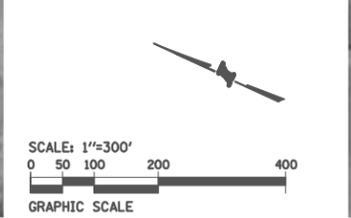
Exhibit E-7
I-294-I-57 Interchange Project
Locations of Traffic Noise Receptors
and Evaluated Noise Barriers
Sheet 7 of 8

See Exhibit E-6



Legend

- Traffic Noise Receptor Approaches or Exceeds Noise Abatement Criteria
- Traffic Noise Receptor Does Not Approach or Exceed Noise Abatement Criteria
- No-Action Noise Barrier
- - - No-Action Noise Barrier to Be Removed and Replaced
- Evaluated Noise Barrier (Recommended for Construction)
- Evaluated Noise Barrier (Not Recommended for Construction)
- Recommended Noise Barrier on Top of Retaining Wall
- Proposed Edge of Pavement
- Proposed Edge of Shoulder
- Proposed Guardrail
- Proposed Structure
- Proposed Retaining Wall



Appendix D
Exhibits



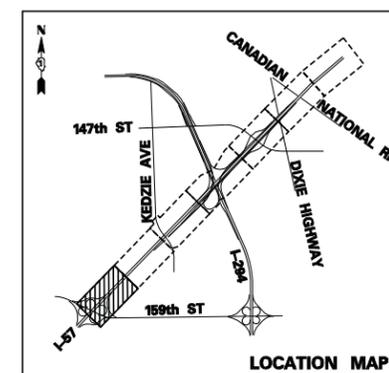
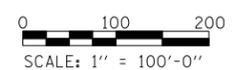
SB IMPROVEMENT ENDS
I-57 STA. 1173+90.00

NB IMPROVEMENT BEGINS
I-57 STA. 1191+44.22

MATCHLINE STA. 1196+00 - SEE SHEET 2

LEGEND:

- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
- PROPERTY ACQUISITION AREA
- IMPACTED STRUCTURE



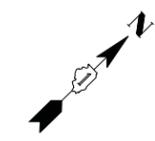
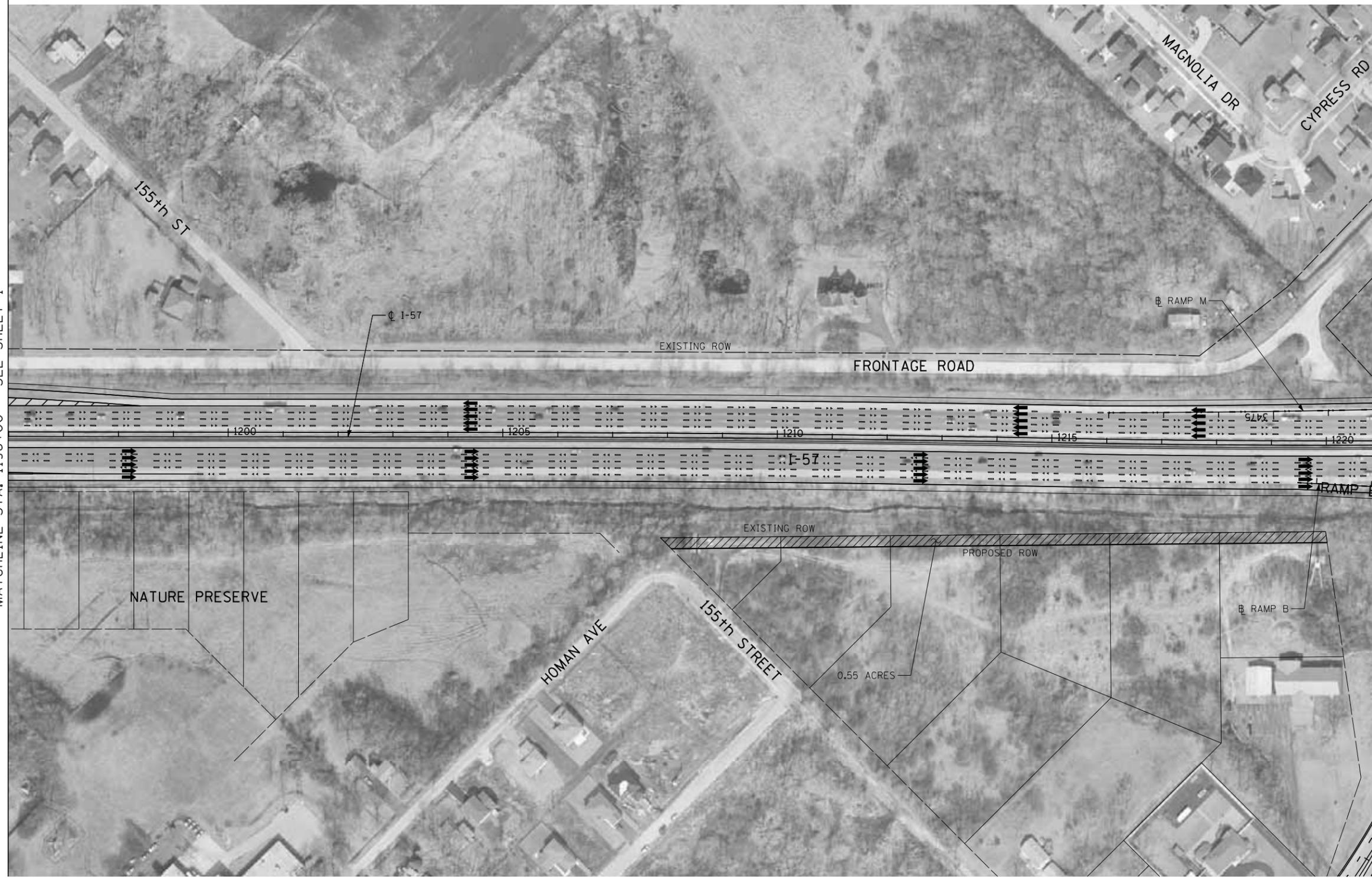
NOTES:

1. THE RIGHT OF WAY AND PROPERTY LINES SHOWN ON THE PLANS WERE OBTAINED FROM VARIOUS SOURCES AND ARE NOT BASED ON FIELD PROPERTY SURVEYS. THE FINAL STATIONS, OFFSETS AND AREAS OF POTENTIAL PROPERTY ACQUISITION CAN AND WILL VARY FROM THOSE SHOWN AND MUST BE FIELD VERIFIED BY PROPERTY SURVEY FROM A REGISTERED ILLINOIS LAND SURVEYOR.

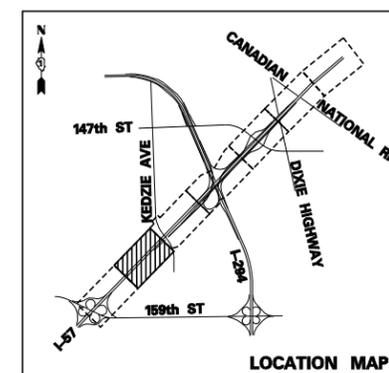
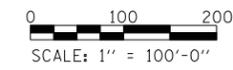
TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-294 AT I-57 INTERCHANGE PROJECT D-5	F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 1	
	PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -			SCALE: 1" = 100' SHEET NO. 1 OF 20 SHEETS STA. 1170+00 TO STA. 1196+00					
	PLOT DATE = 12/30/2009	DATE = 12/31/2008	REVISED -			CONTRACT NO.					
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT											

MATCHLINE STA. 1196+00 - SEE SHEET 1

MATCHLINE STA. 1221+00 - SEE SHEET 3



- LEGEND:**
- XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE



TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE - 12/31/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

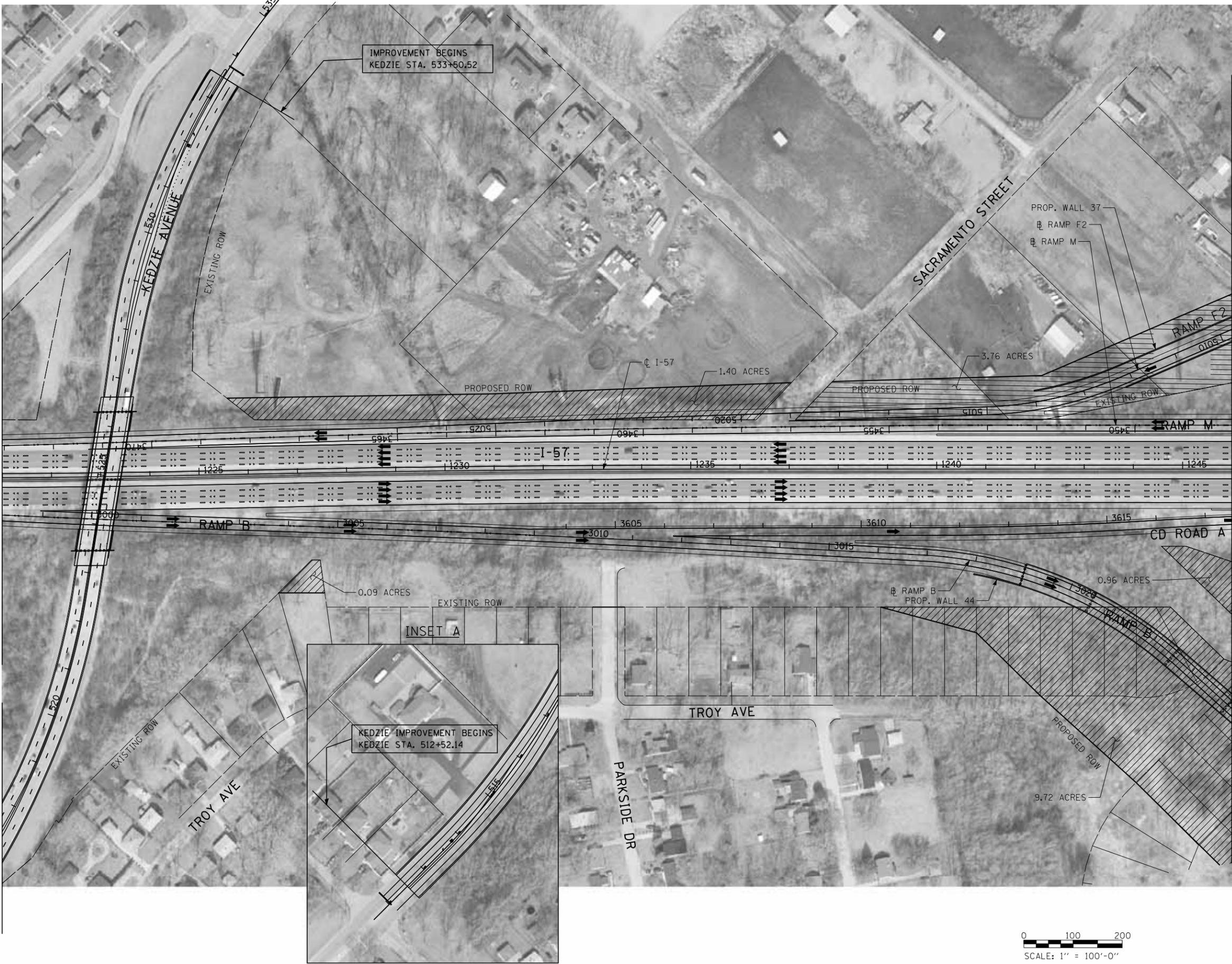
I-294 AT I-57 INTERCHANGE PROJECT				D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION				
SCALE: 1" = 100'	SHEET NO. 2 OF 20 SHEETS	STA. 1196+00	TO STA. 1221+00	

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	2
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MATCHLINE STA. 1221+00 - SEE SHEET 2

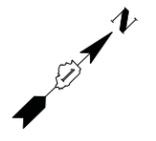
MATCHLINE (SEE INSET A)

MATCHLINE STA. 1246+00 - SEE SHEET 4

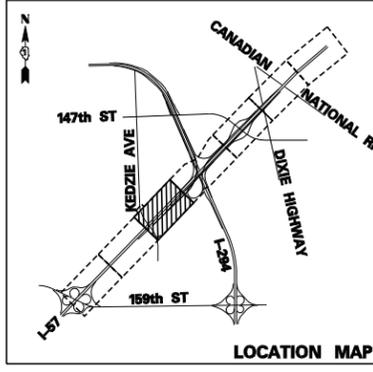
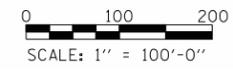


IMPROVEMENT BEGINS
KEDZIE STA. 533+50.52

KEDZIE IMPROVEMENT BEGINS
KEDZIE STA. 512+52.14



- LEGEND:**
- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE



TYLIN INTERNATIONAL	USER NAME = *USERS*	DESIGNED -	REVISED -
	PLOT SCALE = *SCALE*	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE = 12/31/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 100'	SHEET NO. 3 OF 20 SHEETS	STA. 1221+00 TO STA. 1246+00

F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 3
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

LEGEND:

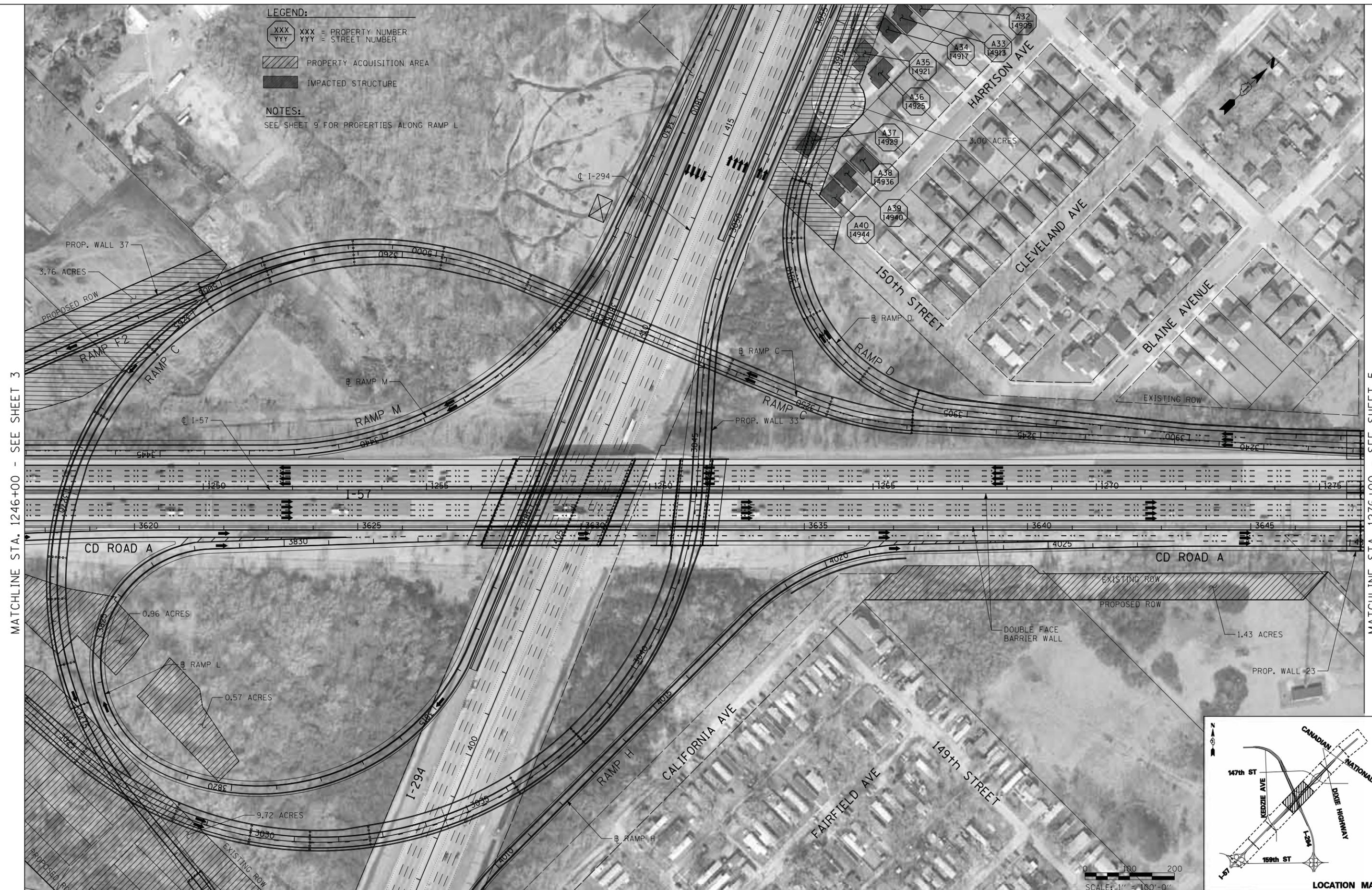
XXX
YYY = PROPERTY NUMBER
YYY = STREET NUMBER

PROPERTY ACQUISITION AREA

IMPACTED STRUCTURE

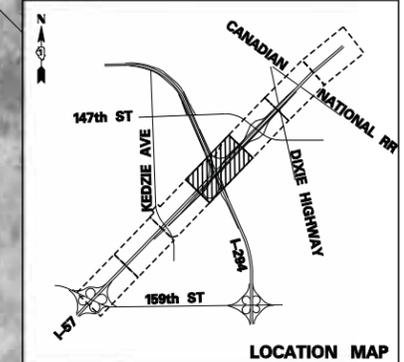
NOTES:

SEE SHEET 9 FOR PROPERTIES ALONG RAMP L



MATCHLINE STA. 1246+00 - SEE SHEET 3

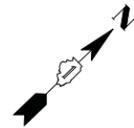
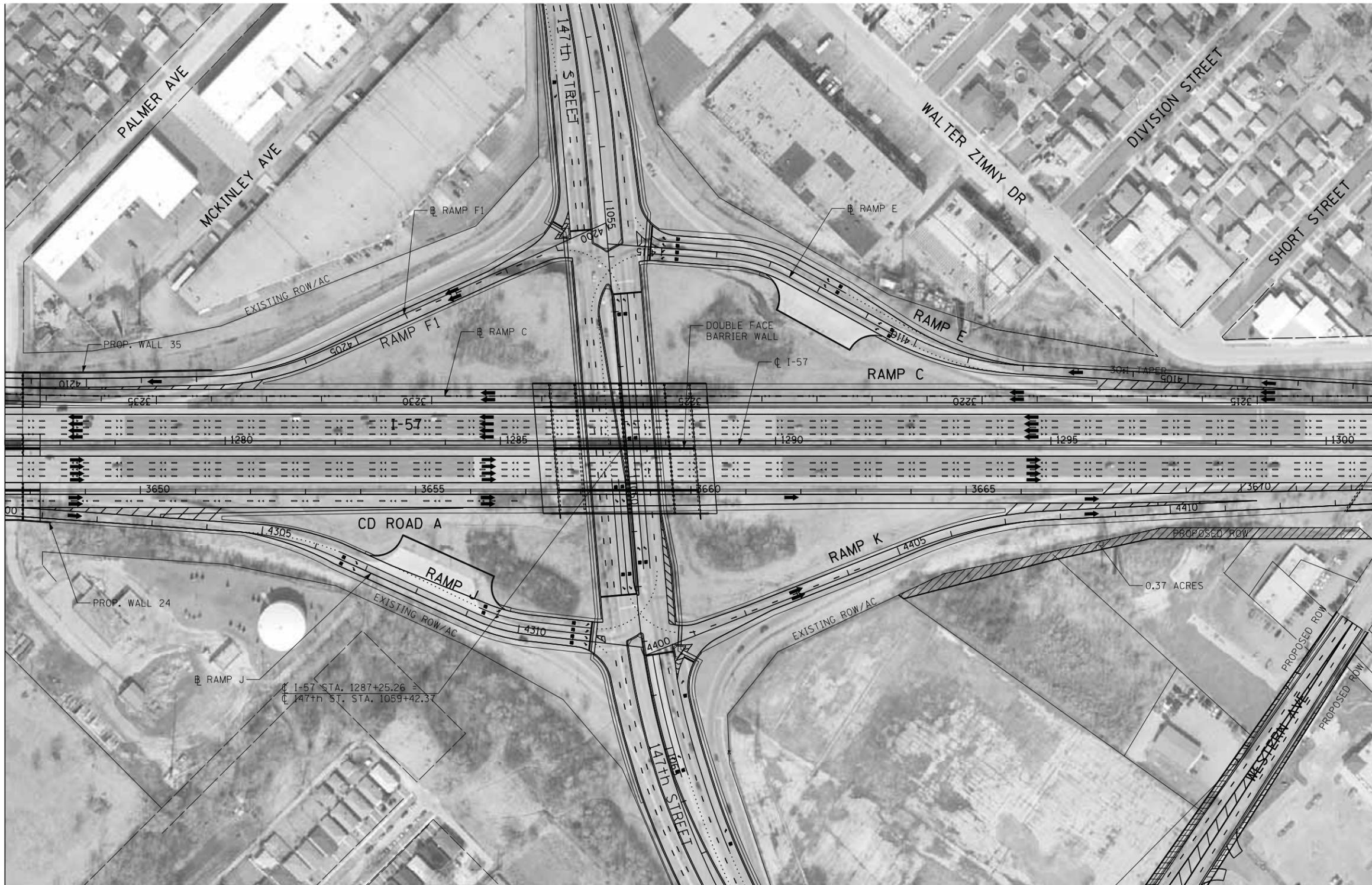
MATCHLINE STA. 1276+00 - SEE SHEET 5



TYLIN INTERNATIONAL USER NAME = \$USERS PLOT SCALE = \$SCALE\$ PLOT DATE = 12/30/2009	DESIGNED - DRAWN - CHECKED - DATE = 12/31/2008	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			I-294 AT I-57 INTERCHANGE PROJECT D-5 POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION			F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 4
	SCALE: 1" = 100' SHEET NO. 4 OF 20 SHEETS STA. 1246+00 TO STA. 1276+00						CONTRACT NO.						
	FEDERAL ROAD DIST. NO. ILLINOIS FED. AID PROJECT												

MATCHLINE STA. 1276+00 - SEE SHEET 4

MATCHLINE STA. 1310+00 - SEE SHEET 6

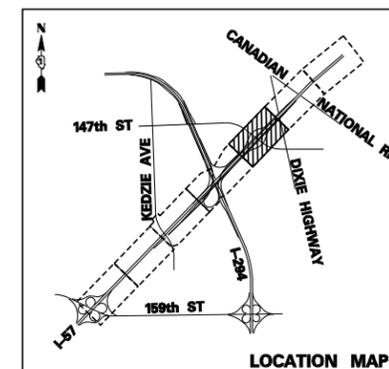
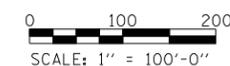


LEGEND:

-  XXX = PROPERTY NUMBER
YYY = STREET NUMBER
-  PROPERTY ACQUISITION AREA
-  IMPACTED STRUCTURE

NOTES:

SEE SHEET 20 FOR PROPERTIES ALONG WESTERN AVE.



TYLIN INTERNATIONAL

USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
PLOT DATE = 12/30/2009	CHECKED -	REVISED -
	DATE = 12/31/2008	REVISED -

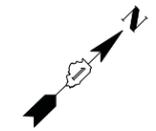
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 100'	SHEET NO. 5 OF 20 SHEETS	STA. 1276+00 TO STA. 1301+00

F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 5
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

MATCHLINE STA. 1310+00 - SEE SHEET 5

MATCHLINE STA. 1326+00 - SEE SHEET 7



LEGEND:

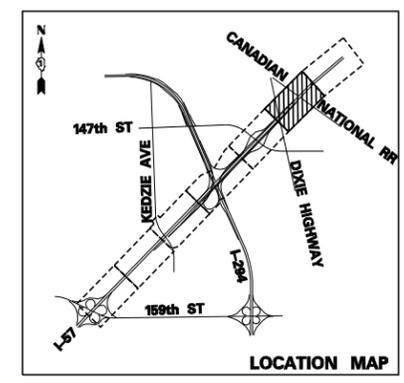
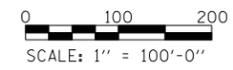
XXX
YYY XXX = PROPERTY NUMBER
 YYY = STREET NUMBER

PROPERTY ACQUISITION AREA

IMPACTED STRUCTURE

NOTES:

SEE SHEET 20 FOR PROPERTIES ALONG DIXIE HIGHWAY

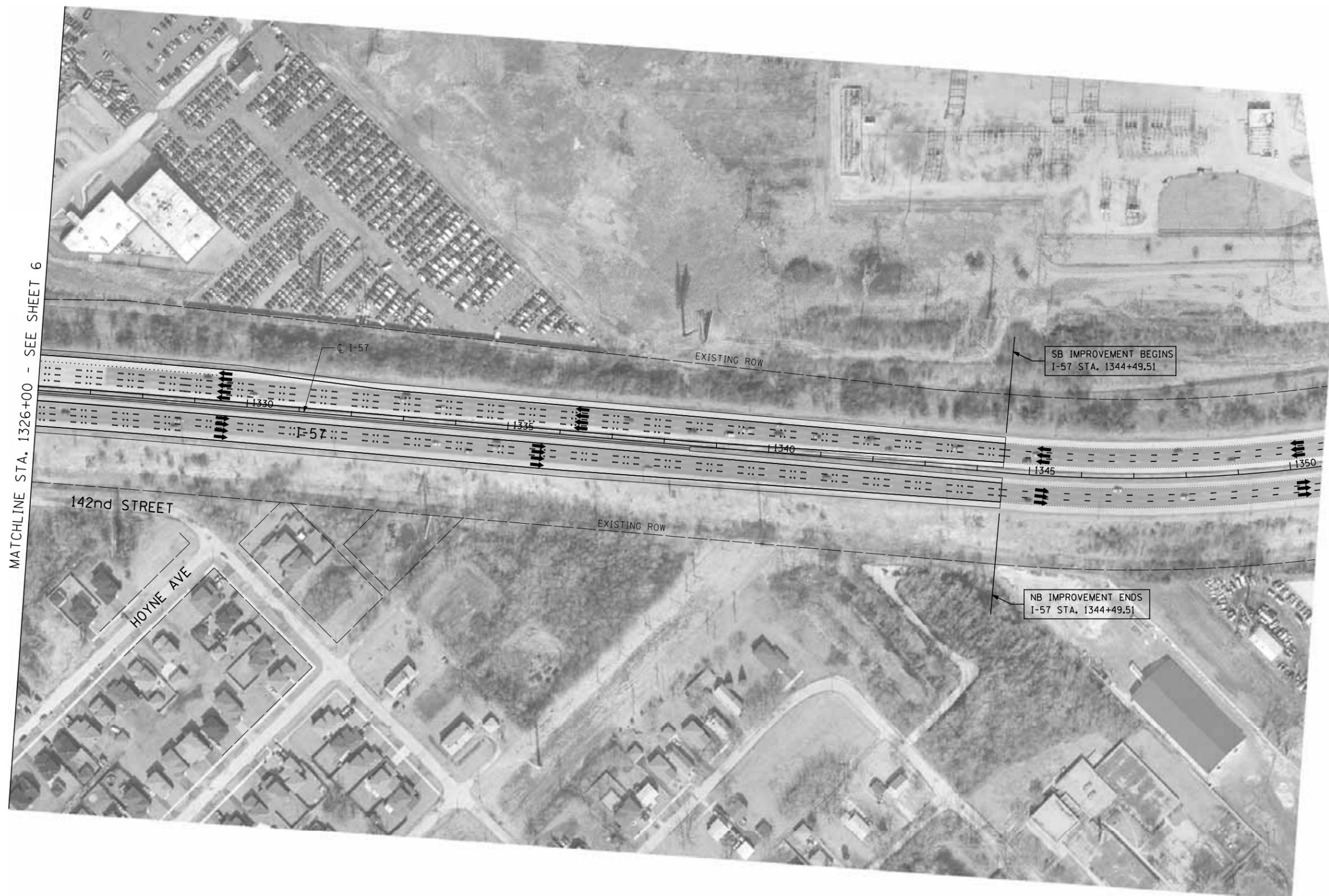


TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE - 12/31/2008	REVISED -

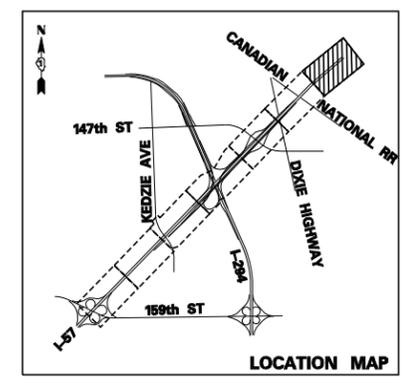
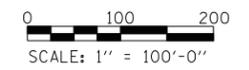
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT				D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION				
SCALE: 1" = 100'	SHEET NO. 6 OF 20 SHEETS	STA. 13010+00 TO STA. 1326+00		

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	6
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



- LEGEND:**
- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE

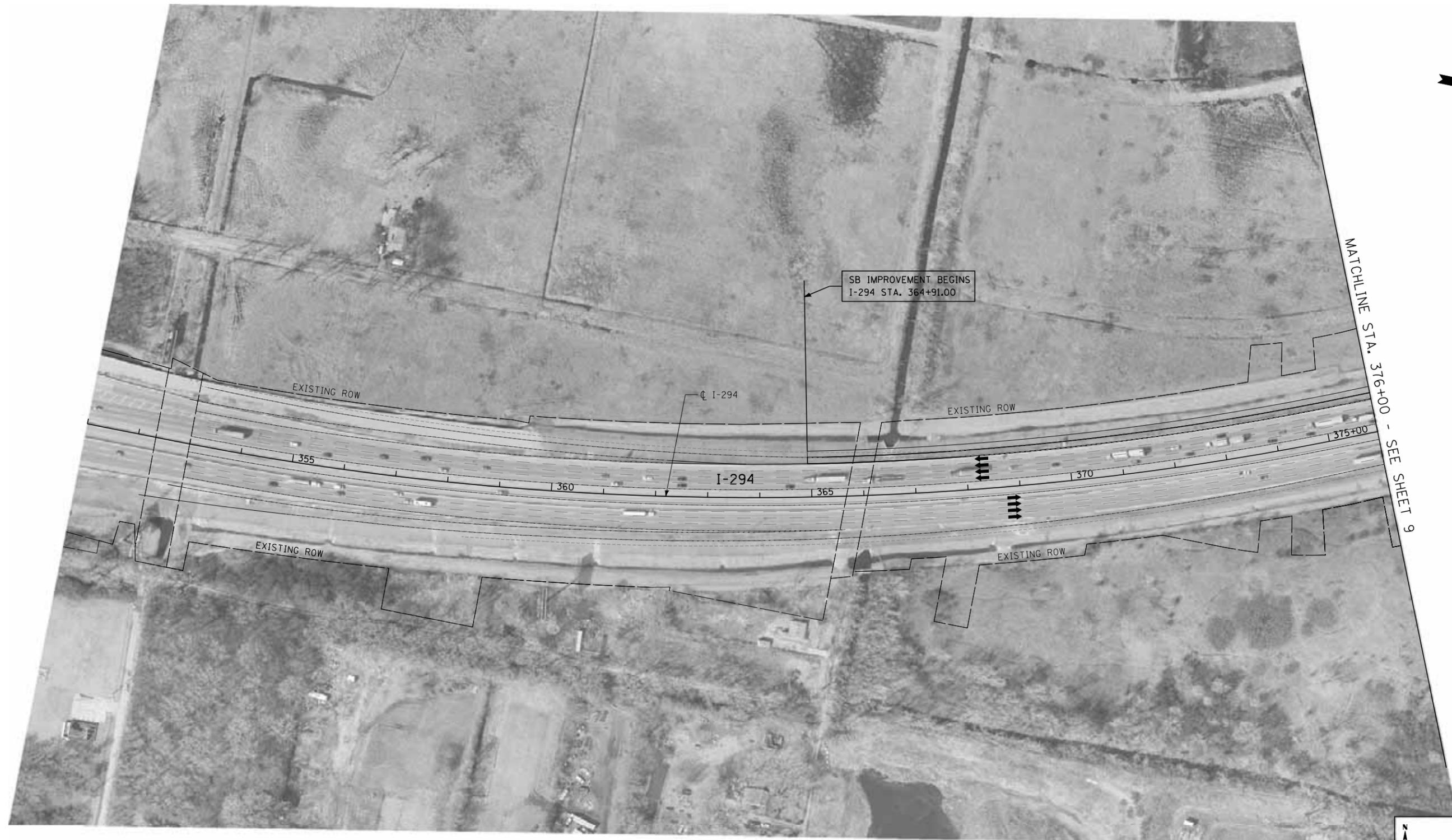


TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE - 12/31/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

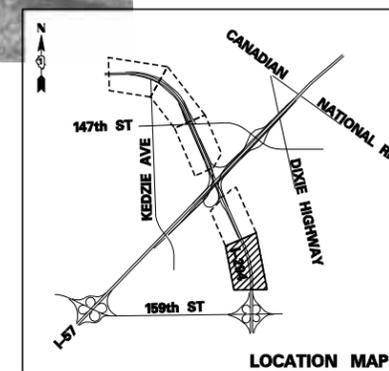
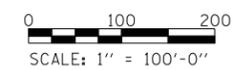
I-294 AT I-57 INTERCHANGE PROJECT			D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION			
SCALE: 1" = 100'	SHEET NO. 7 OF 20 SHEETS	STA. 1326+00 TO STA. 1350+00	

F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 7
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



LEGEND:

-  XXX = PROPERTY NUMBER
YYY = STREET NUMBER
-  PROPERTY ACQUISITION AREA
-  IMPACTED STRUCTURE



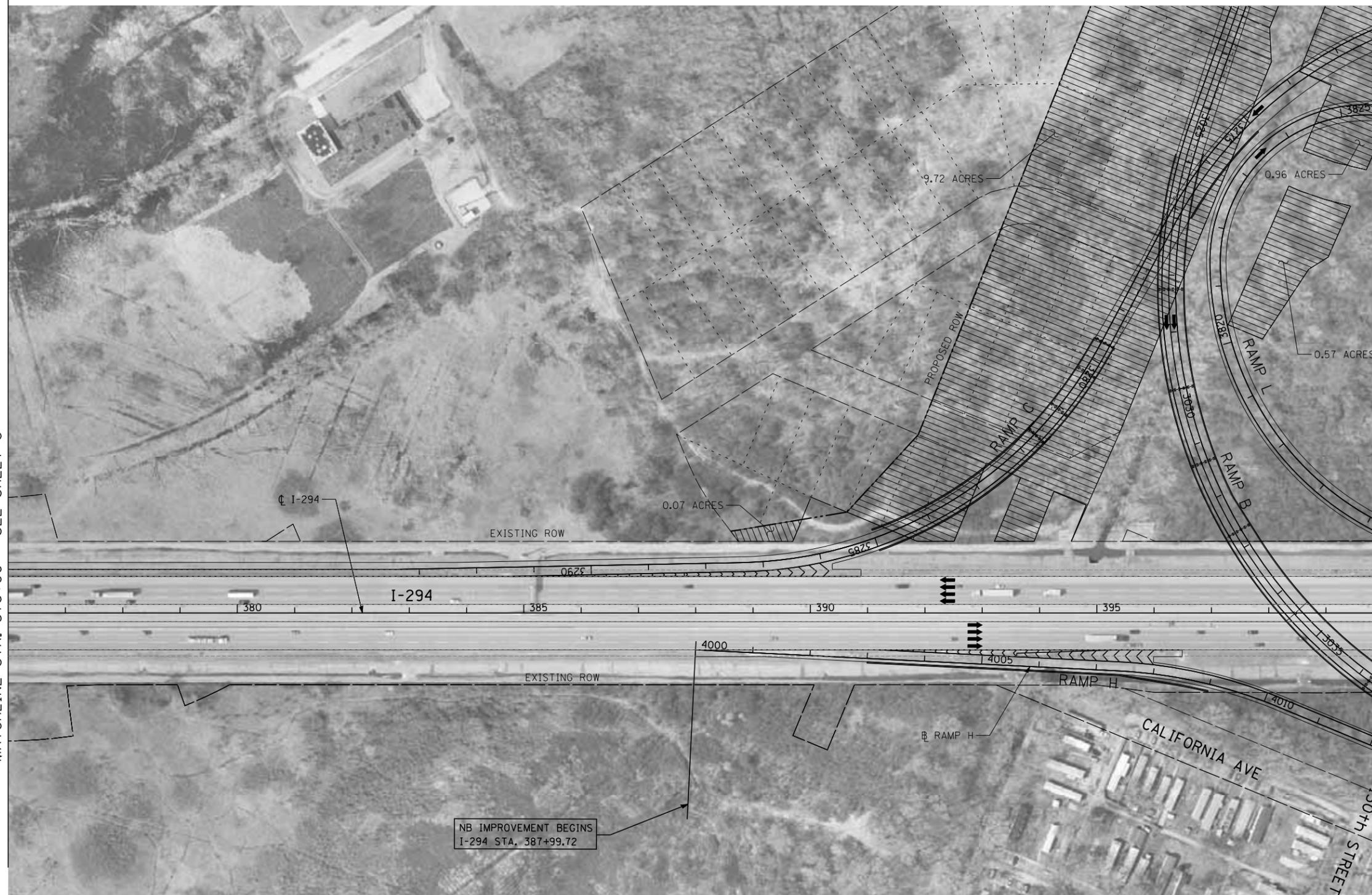
TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
	DATE = 12/31/2008	REVISIONS -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT				D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION				
SCALE: 1" = 100'	SHEET NO. 8 OF 20 SHEETS	STA. 351+00	TO STA. 376+00	

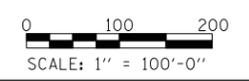
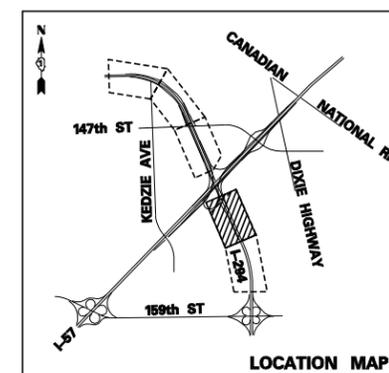
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	8
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MATCHLINE STA. 376+00 - SEE SHEET 8



MATCHLINE STA. 400+00 - SEE SHEET 4

- LEGEND:**
- XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE



TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
	DATE = 12/31/2008	REVISED -	REVISED -

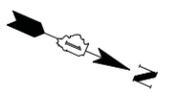
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 100'	SHEET NO. 9 OF 20 SHEETS	STA. 376+00 TO STA. 400+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	9
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MATCHLINE STA. 413+00 - SEE SHEET 4

MATCHLINE STA. 438+00 - SEE SHEET 11

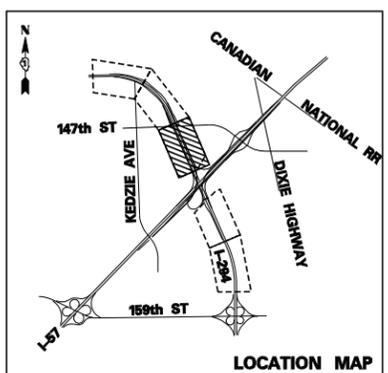
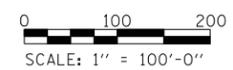


LEGEND:

- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
- PROPERTY ACQUISITION AREA
- IMPACTED STRUCTURE

NOTES:

SEE SHEETS 14 AND 15 FOR PROPERTIES ALONG 147TH ST.



TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-294 AT I-57 INTERCHANGE PROJECT D-5 POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION	F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 10	
	PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -			CONTRACT NO.					
	PLOT DATE = 12/30/2009	DATE = 12/31/2008	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



LEGEND:

XXX = PROPERTY NUMBER
 YYY = STREET NUMBER

IMPACTED STRUCTURE

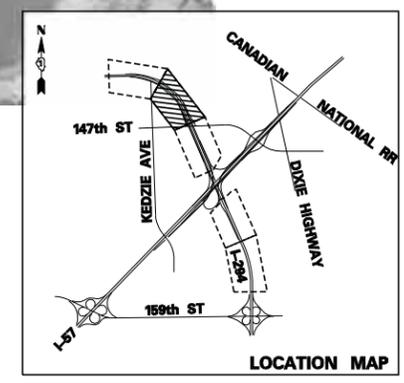
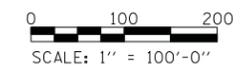
NOTES:

SEE SHEET 14 FOR PROPERTIES ALONG 147TH ST.

MATCHLINE STA. 438+00 - SEE SHEET 10

MATCHLINE STA. 463+00 - SEE SHEET 12

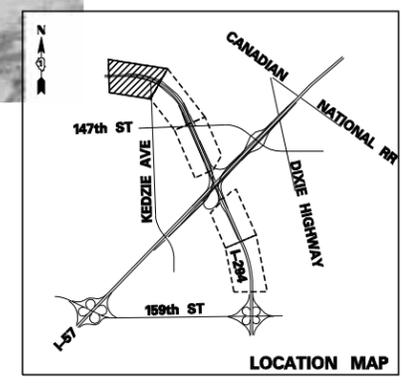
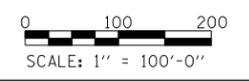
RAMP N STA. 4521+73.65 =
 RAMP B STA. 3094+49.05, 12.00' RT



TYLIN INTERNATIONAL USER NAME = *USERS* PLOT SCALE = *SCALE* PLOT DATE = 12/30/2009	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-294 AT I-57 INTERCHANGE PROJECT		D-5 F.A. RTE. 57 SECTION (46-1)HBK-1 COUNTY COOK TOTAL SHEETS 20 SHEET NO. 11 CONTRACT NO. FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
	DRAWN -	REVISED -		POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
	CHECKED -	REVISED -		SCALE: 1" = 100'	SHEET NO. 11 OF 20 SHEETS	
DATE - 12/31/2008	REVISED -					



- LEGEND:**
- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE



TYLIN INTERNATIONAL	USER NAME = \$USERS\$	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE - 12/31/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 100'	SHEET NO. 12 OF 20 SHEETS	STA. 463+00 TO STA. 488+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	12
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

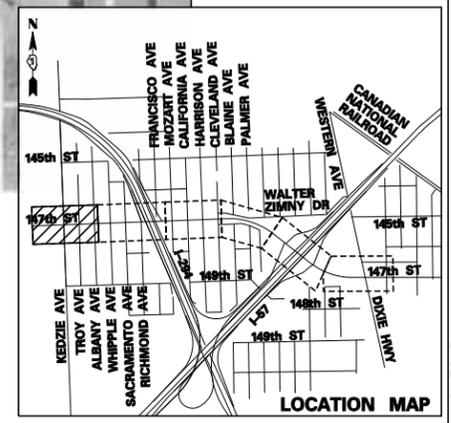
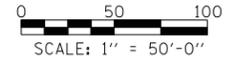


MATCHLINE STA. 1014+00 - SEE SHEET 14



LEGEND:

- | | |
|-----|-----------------------|
| XXX | XXX = PROPERTY NUMBER |
| YYY | |
- | | |
|--|---------------------------|
| | PROPERTY ACQUISITION AREA |
|--|---------------------------|
- | | |
|--|--------------------|
| | IMPACTED STRUCTURE |
|--|--------------------|



TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE = 12/31/2008	REVISED -

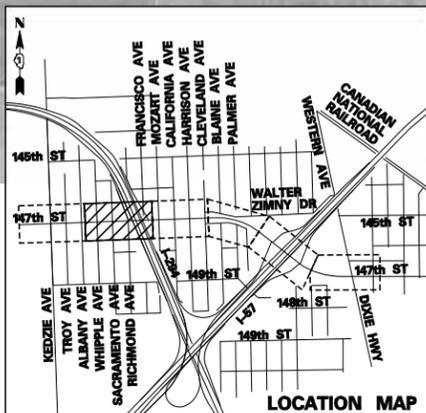
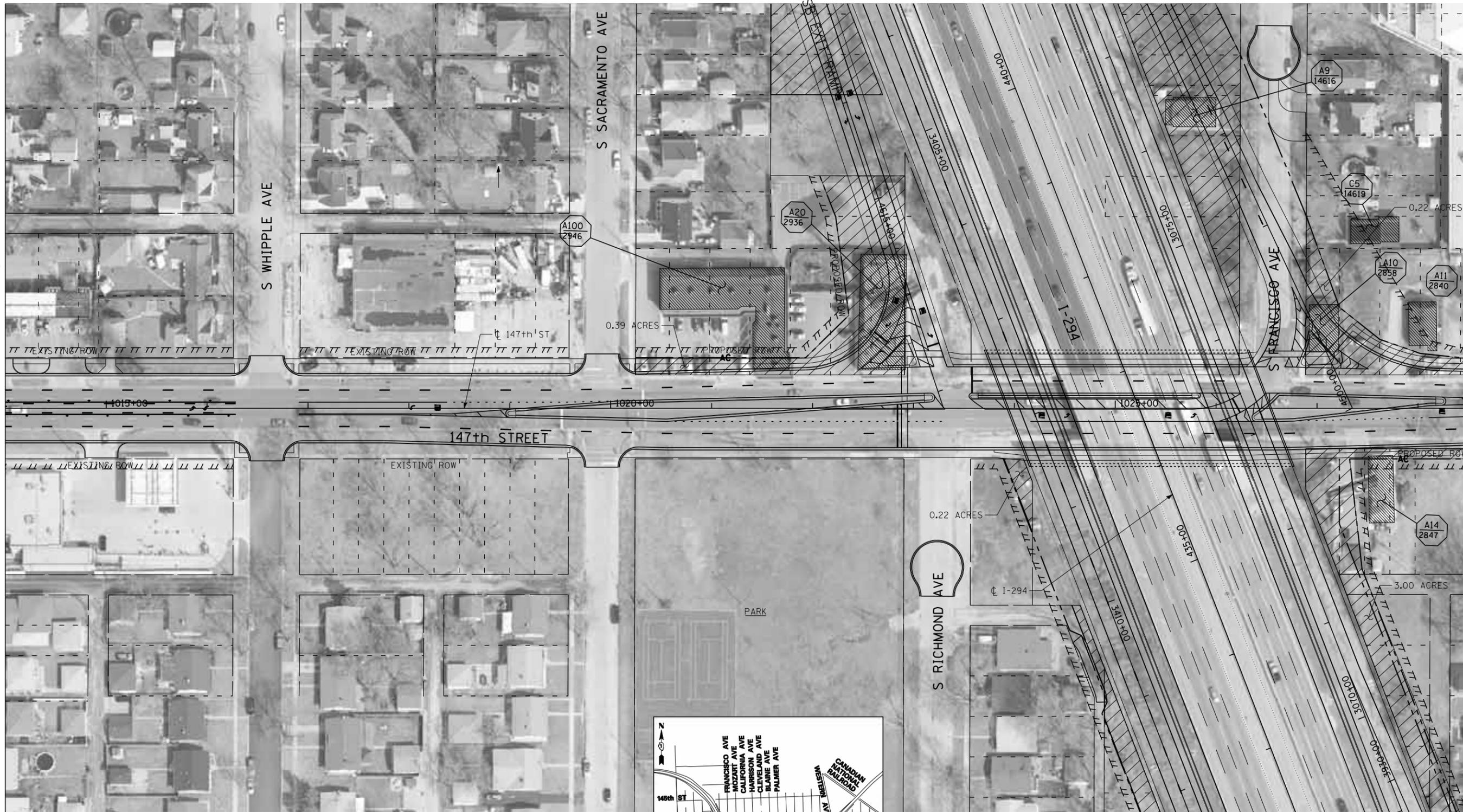
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 50'	SHEET NO. 13 OF 20 SHEETS	STA. 1000+00 TO STA. 1014+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	13
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

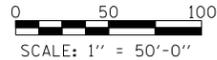
MATCHLINE STA. 1014+00 - SEE SHEET 13

MATCHLINE STA. 1028+50 - SEE SHEET 15



- LEGEND:**
- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE

SEE SHEETS 10 AND 11 FOR PROPERTIES ALONG I-294.



TYLIN INTERNATIONAL	USER NAME = *USERS*	DESIGNED -	REVISED -
	PLOT SCALE = *SCALE*	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
	DATE = 12/31/2008	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 50'	SHEET NO. 14 OF 20 SHEETS	STA. 1014+00 TO STA. 1028+50

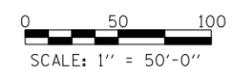
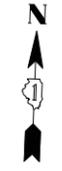
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	14
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



MATCHLINE STA. 1028+50 - SEE SHEET 14

MATCHLINE STA. 1040+50 - SEE SHEET 16

- LEGEND:**
- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE



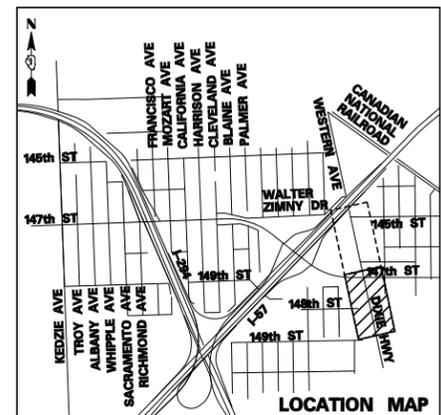
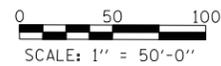
TYLIN INTERNATIONAL	USER NAME = \$USER\$	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-294 AT I-57 INTERCHANGE PROJECT D-5	F.A. RTE. 57	SECTION (46-1)HBK-1	COUNTY COOK	TOTAL SHEETS 20	SHEET NO. 15	
	PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -			SCALE: 1" = 50' SHEET NO. 15 OF 20 SHEETS STA. 1028+50 TO STA. 1040+50					
	PLOT DATE = 12/30/2009	DATE = 12/31/2008	REVISED -			CONTRACT NO.					
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT											



MATCHLINE STA. 6010+00 - SEE SHEET 18

LEGEND:

- XXX = PROPERTY NUMBER
 YYY = STREET NUMBER
- PROPERTY ACQUISITION AREA
- IMPACTED STRUCTURE



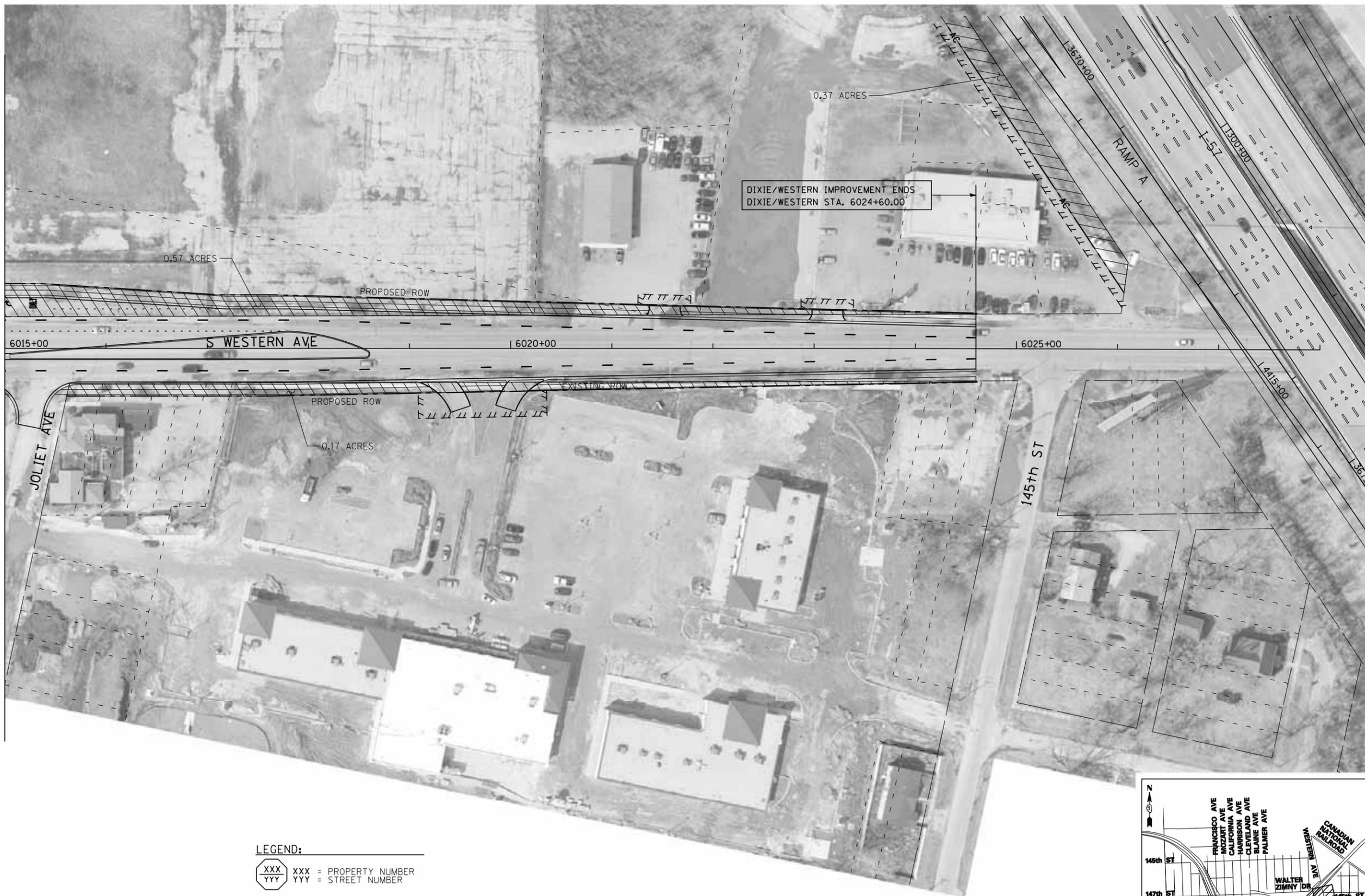
TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE - 12/31/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

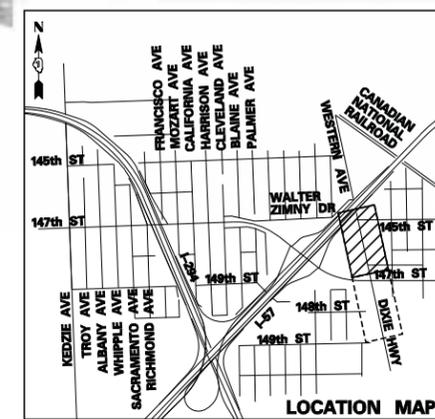
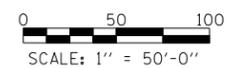
I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 50'	SHEET NO. 19 OF 20 SHEETS	STA. 5998+00 TO STA. 6010+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	19
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

MATCHLINE STA. 6015+00 - SEE SHEET 19



- LEGEND:**
- XXX
YYY XXX = PROPERTY NUMBER
YYY = STREET NUMBER
 - PROPERTY ACQUISITION AREA
 - IMPACTED STRUCTURE



TYLIN INTERNATIONAL	USER NAME = \$USERS	DESIGNED -	REVISED -
	PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
	PLOT DATE = 12/30/2009	CHECKED -	REVISED -
		DATE - 12/31/2008	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-294 AT I-57 INTERCHANGE PROJECT		D-5
POTENTIALLY IMPACTED PROPERTIES/PROP. ROW ACQUISITION		
SCALE: 1" = 50'	SHEET NO. 20 OF 20 SHEETS	STA. 6015+00 TO STA. 6028+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(46-1)HBK-1	COOK	20	20
CONTRACT NO.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		