

# IL ROUTE 60/83



Community Advisory Group  
Meeting No. 5

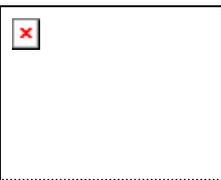
April 28, 2010



Illinois Department of Transportation

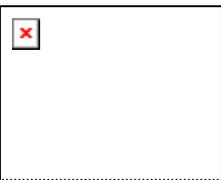
# Outline Agenda

- Introduction to Participants
- Recap of Community Advisory Group Meeting No. 4 - April 21, 2010
- Segment No. 1 – IL Route 176 to South of Midlothian Road; April 21, 2010
- Segment No. 2 – South of Midlothian Road to South of the EJ&E Railroad; April 28, 2010



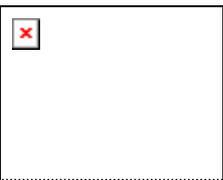
# Outline Agenda

- IDOT Design Criteria
- Cross Sectional Design Elements
- Intersection Improvements
  - Diamond Lake Road
  - IL Route 60 (Townline Road) / Willow Springs Road
- Railroad Crossing Grade Separation Options
- Alternatives Developed to Date



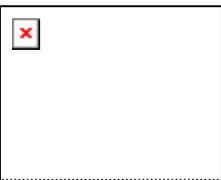
# Who Are We?

## Introductions



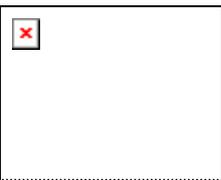
# Recap of Previous Community Advisory Group Meetings

- Four previous CAG meetings
  - July 13, 2009
  - July 28, 2009
  - August 11, 2009
  - April 21, 2010
- Traffic Operations and Safety
- Drainage
- Alternative Modes of Transportation
- Potential RR Grade Separation



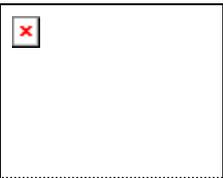
# IDOT Roadway Design Criteria

- Suburban Strategic Regional Arterial (SRA)
  - Design Speed - 45 mph
  - Lane Widths – 12'
  - Raised Curb Median Width – 18'
  - Flush Median Width – 12'
  - Outside Concrete Curb and Gutter
  - Sidewalk Width – 5' with 2' Minimum Behind Back of Curb



# Cross Sectional Elements

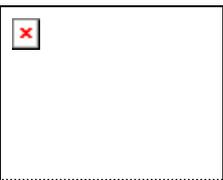
- Continuous Sidewalk on One Side of the Roadway
- Continuous 10' Flat Area Provided on Each Side of the Roadway – Allows for Future Sidewalk or Multi-Use Path



# Meeting Purpose

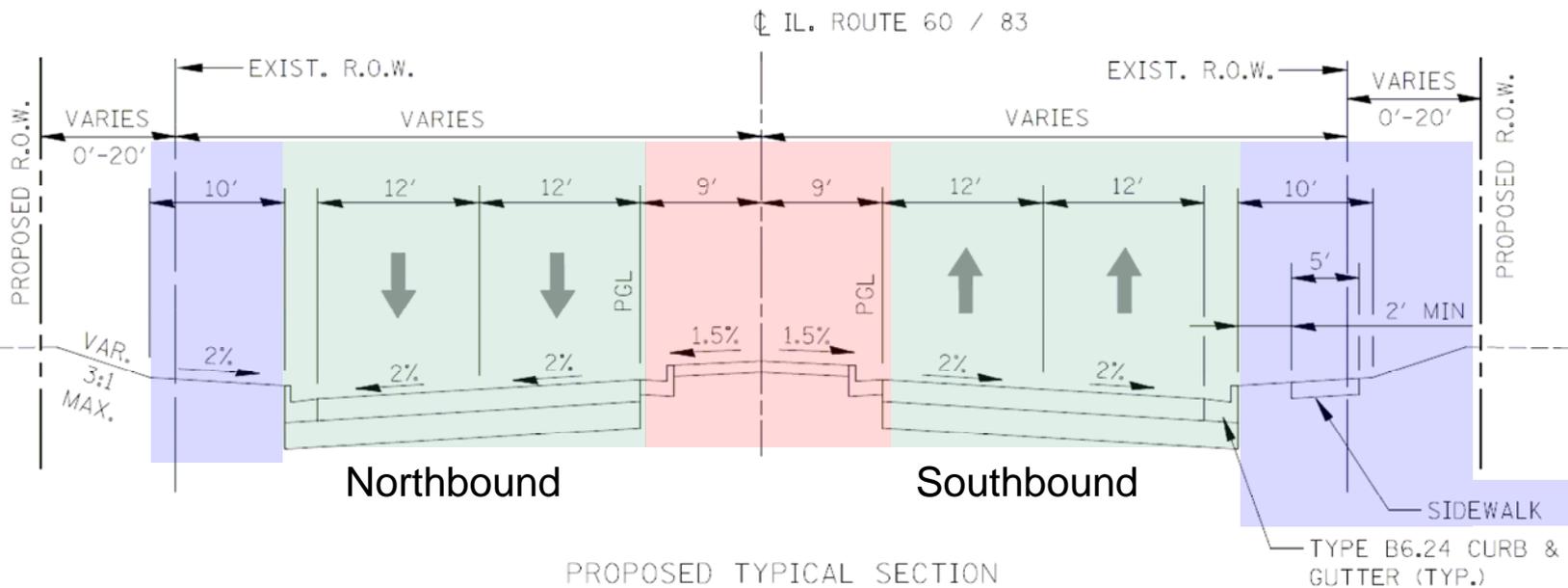
To Present and Obtain Group Feedback on Design Alternatives Developed to Date

- ***Alternative 1***
  - 18' Raised Curb Median
  - 2-12' Lanes in Each Direction
- ***Alternative 2***
  - 12' Flush Median
  - 2-12' Lanes in Each Direction
- ***Alternative 3 - Hybrid of Alt. 1 and Alt. 2***
  - 18' Raised Curb Median
  - 2-12' Lanes in Each Direction
  - Flush Median / Two-Way Left Turn Lane between Circle Drive and Railroad Crossing



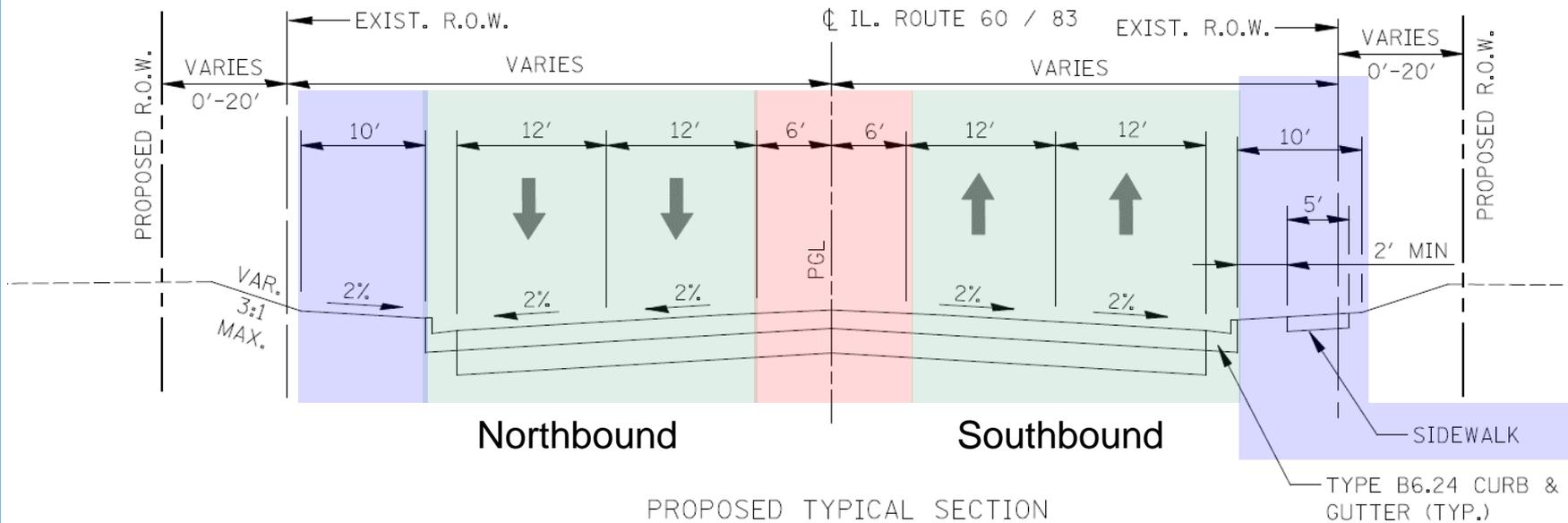
# Alternative 1 Typical Cross Section

- **18' Raised Curb Median**
- **2–12' Lanes in Each Direction with Curb and Gutter**
- **5' Wide Sidewalk and 10' Flat Area**



# Alternative 2 Typical Cross Section

- **12' Flush Median**
- **2–12' Lanes in Each Direction with Curb and Gutter**
- **5' Wide Sidewalk and 10' Flat Area**



PROPOSED TYPICAL SECTION

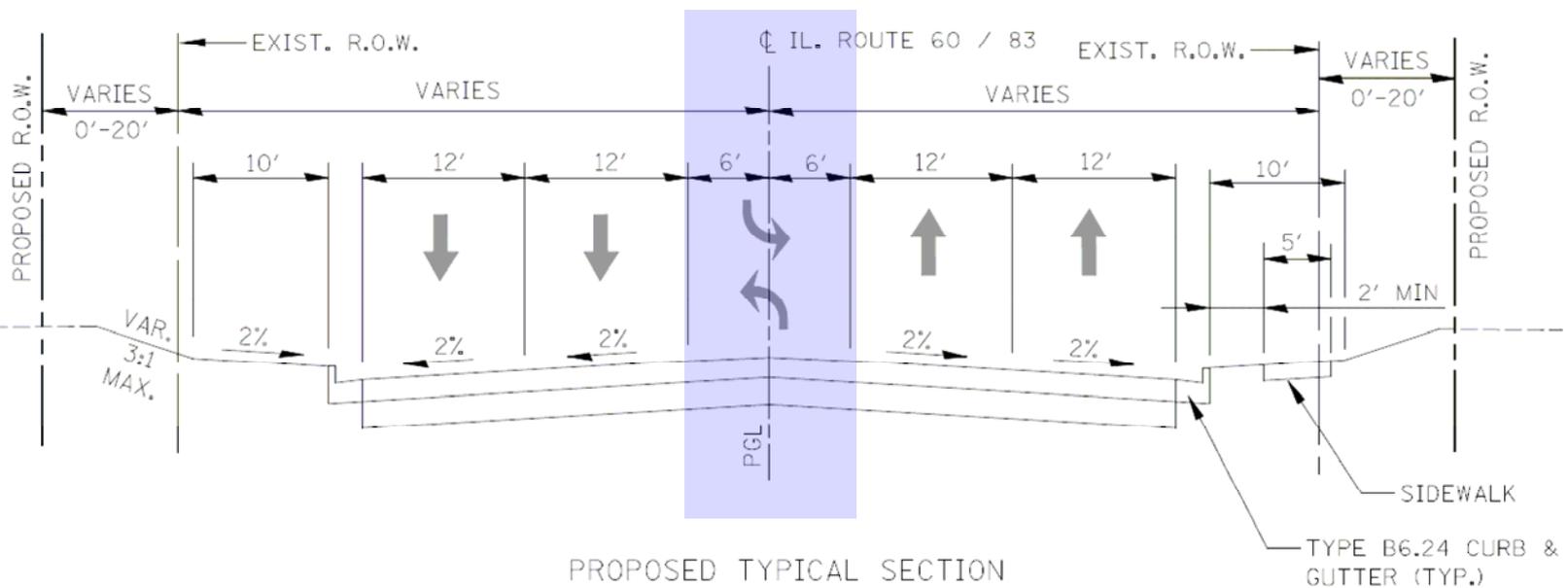
FLUSH (PAINTED) MEDIAN

*PRELIMINARY*



# Alternative 3 Typical Cross Section

- Hybrid of Alternatives 1 and 2
- Typical Section between Circle Drive and RR Crossing
- Two Way Left Turn Lane



PROPOSED TYPICAL SECTION

TWO-WAY LEFT TURN LANE

PRELIMINARY



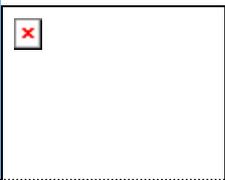
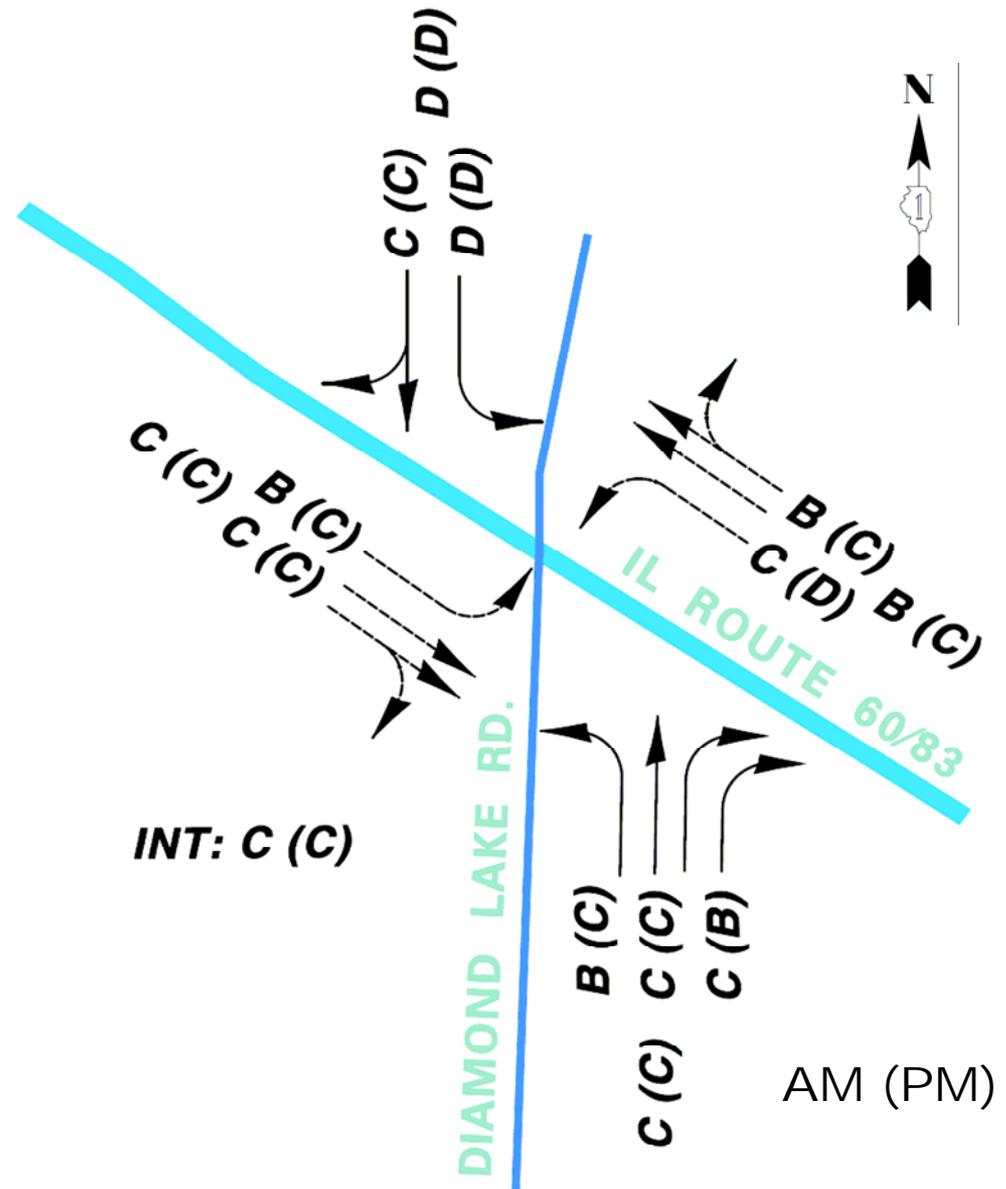
# Intersection Improvements Diamond Lake Rd

- **IL Route 60/83**

- Longer Turn Bays
- Upgrade Traffic Signals

- **Diamond Lake Road**

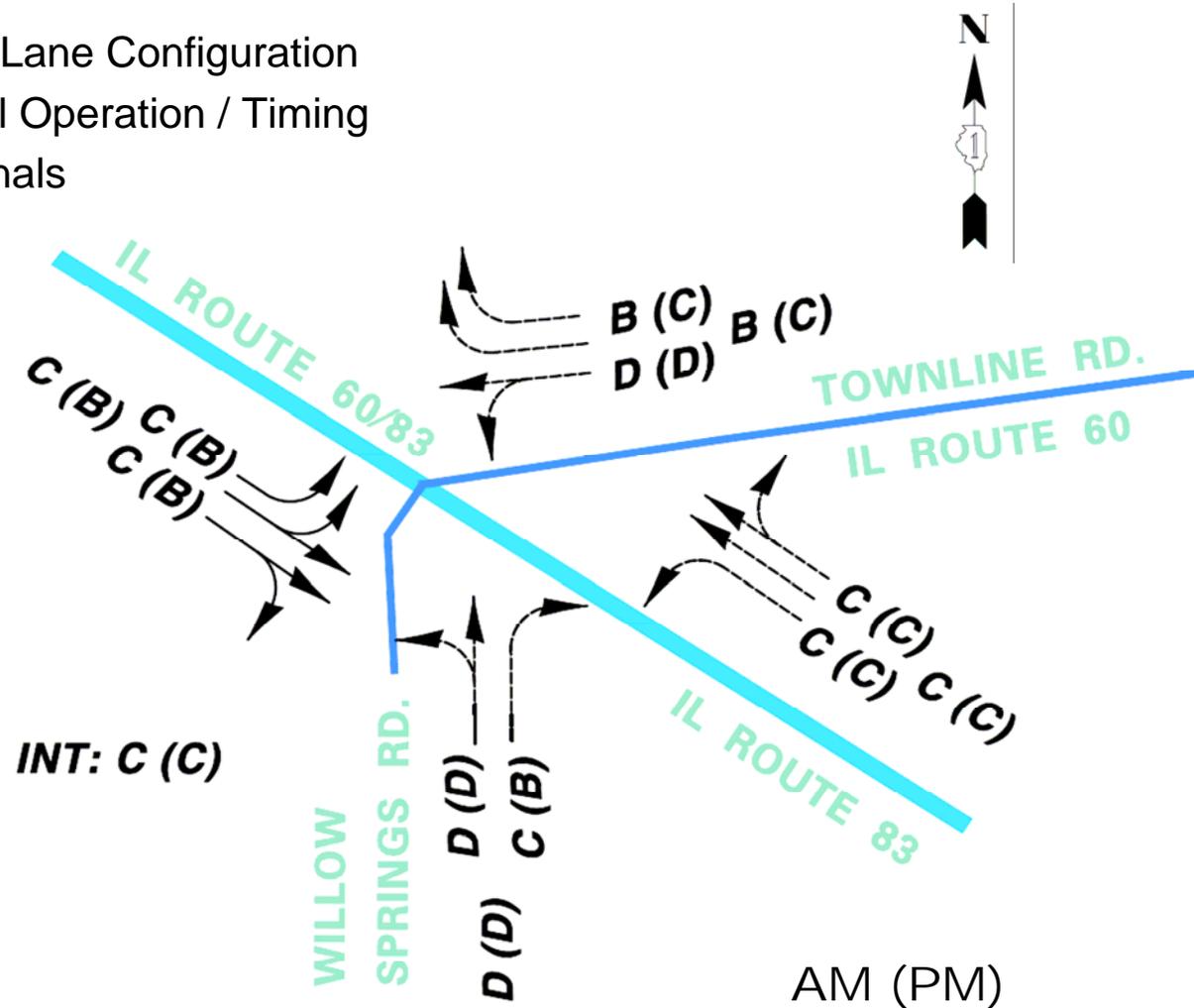
- Left Turn Lanes
- Left Turn Signals
- NB Dual-Right Turn Lanes



# Intersection Improvements Townline Rd

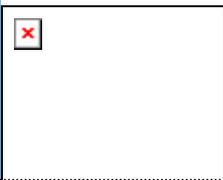
- **IL Route 60/83**

- Modify Southbound Lane Configuration
- Modify Traffic Signal Operation / Timing
- Upgrade Traffic Signals



# IDOT and Railroad Clearance Requirements

- IDOT Roadway under a Railroad:  
Required Vertical Clearance is **14'-9"**
- IDOT Roadway over a Railroad:  
Required Vertical Clearance is **23'-0"**



# Next Steps

- Give Consideration to Findings from Meeting No. 4 and Meeting No. 5
- Refine Alternatives Accordingly
- Present Refined Alternatives to Community Advisory Group – Summer 2010 Tentative
- Obtain Consensus on Preferred Alternative
- Present Preferred Alternative to Public – Fall 2010 Tentative
- Project Website: [www.ilrte6083study.com](http://www.ilrte6083study.com)

