

IL ROUTE 60/83



Community Advisory Group Meeting
November 17, 2016



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NOTES

Outline Agenda

- Introduction of Participants
- Summary of Previous Stakeholder Involvement and Community Advisory Group Meetings
- Recommended Roadway Geometry
- Recommended Railroad Grade Separation Alternate
- Environmental Impacts and Mitigation Measures
- Water Quality and Best Management Practices (BMPs)
- Schedule



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NOTES

Who Are We?

Introductions



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NOTES

Previous Community Advisory Group Meetings

- **Seven Previous Meetings – Items Discussed**
 - Stakeholder Involvement Plan
 - Community Context Audit
 - Problem Statement
 - Existing Traffic and Drainage
 - Applicable Design Criteria
 - Development of Alternatives
 - Complete Streets Policy
 - Grade Separation Options

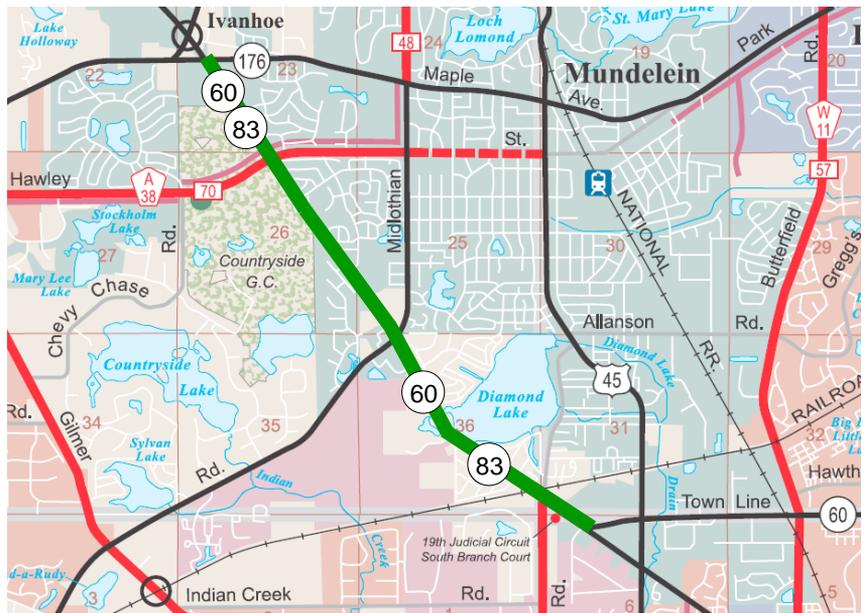


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NOTES

Project Location

- Central Lake County
- Villages of Mundelein and Long Grove
- Townships
 - Freemont
 - Libertyville
 - Vernon
 - Ela
- Between IL Route 176 (Maple Ave) and IL Route 60 (Townline Rd)



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NOTES

Recommended Alternative

- **Roadway (Recommended)**

- Two 11' Lanes in Each Direction with Curb and Gutter
- 18' Raised Curb Median from IL 176 to Circle Drive and Maple Avenue to Diamond Lake Road
- Two-Way Left Turn Lane from Circle Drive to Maple Avenue
- 6 Signalized Intersection Improvements

- **Railroad (Recommended)**

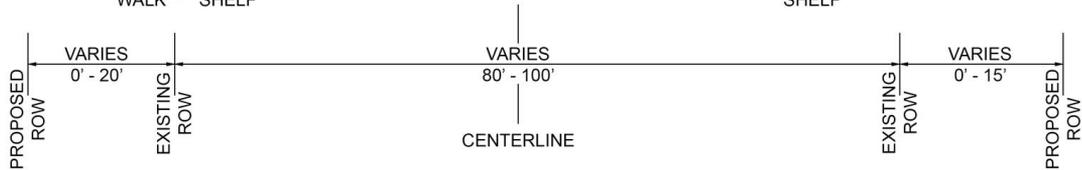
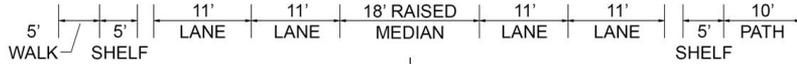
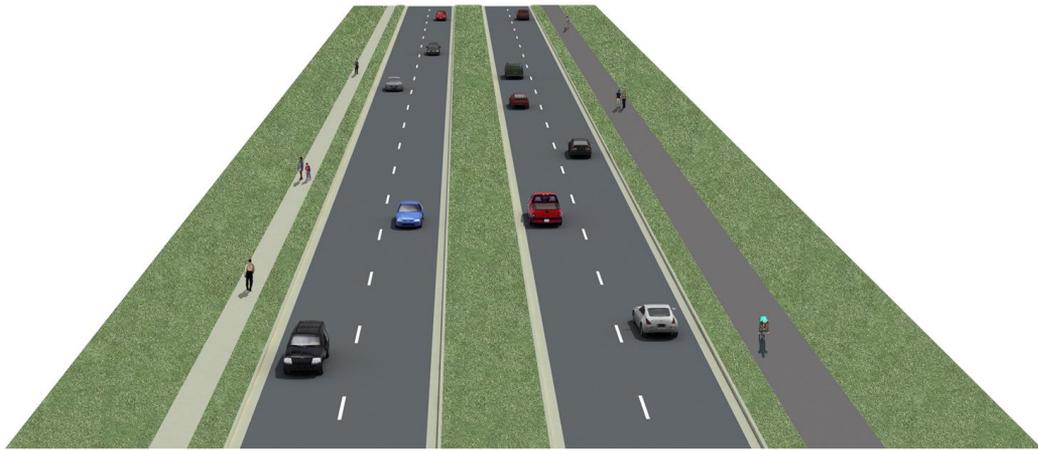
- Raise IL 60/83 over the RR (Roadway Overpass)



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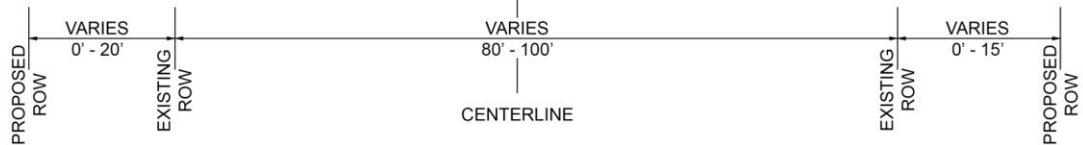
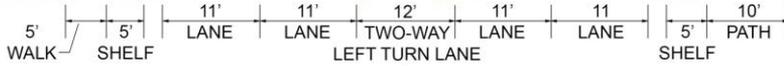
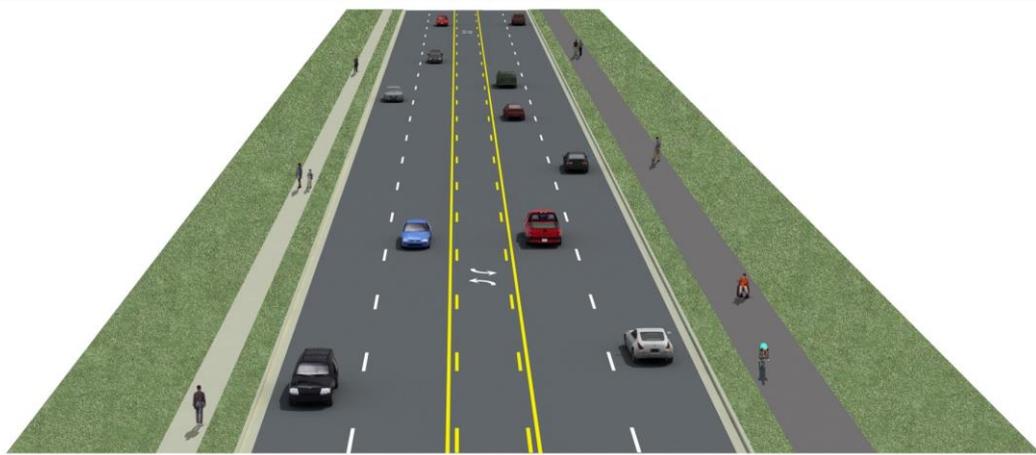
Typical Cross Section – IL Route 176 to Circle Drive Maple Avenue to Diamond Lake Rd



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NOTES

Typical Cross Section – Circle Drive to Maple Avenue



NOTES

Railroad Grade Separation

- IL Route 60/83 Grade Separation Would:
 - Reduce Excessive Traffic Delays and Queues
 - Significant Projected Increase in Train Traffic
 - Lengthy Freight Trains Block Crossing Repeatedly
 - Increase Safety for All Users
 - Conflicts between Trains and Roadway Users are Eliminated
 - Support Emergency Vehicle Response
 - Reduces Fire / Police / Ambulance Response Times



IL Route
60 83



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NOTES

Railroad Grade Crossing Study

- Summary of Alternates Previously Considered
 - RR to Pass Over IL 60/83
 - Lower RR to Pass Under IL 60/83
 - Partially Raise RR and Lower IL 60/83
 - Partially Lower RR and Raise IL 60/83
 - RR Grade Remains and Lower IL 60/83 (Underpass)
 - **RR Grade Remains and Raise IL 60/83 (Overpass)**
 - Maintain At-Grade RR Crossing



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NOTES

Recommended Railroad Crossing Alternate

	AT-GRADE CROSSING	IL 60/83 HIGHWAY OVERPASS	IL 60/83 HIGHWAY UNDERPASS
Daily Train/Car Risk Exposure Volume (ADT x Trains/Day)	27,000 x 540,000	0	0
Railroad Blockage Vehicle Delay	- All Vehicular Delay due to Railroad Operations Is Still Present	- All Vehicular Delay due to Rail Traffic Eliminated at Crossing	- All Vehicular Delay due to Rail Traffic Eliminated at Crossing
Property Access Impacts Right-Of-Way / Easements	- Access to Adjacent Streets and Commercial Entrances Similar to current access - Requires Complete Take of One Property	- Maple Ave Access Modified (via Frontage Rd) Multiple Access Driveways are Impacted with Possible Full Acquisition	- Requires Permanent Closure of Maple Avenue - Multiple Access Driveways are Impacted with Possible Full Acquisition
Emergency Services	- Least Favorable Option as Blockages and Vehicular Delays Still Exist - No Suitable Alternate Route Exists	- Elimination of Blockages and Vehicular Delays - Greatly Improves Emergency Response Times	- Elimination of Blockages and Vehicular Delays - Greatly Improves Emergency Response Times
IL 60/83 HIGHWAY OVERPASS			
Intersection & RR Crossing	- Road Intersection Still Anticipated due to High Vehicle Volume	- Minor Impacts to Diamond Lake Road Intersection - Blockage Issue at Intersection is Eliminated	- Significant Impacts to Diamond Lake Road Intersection - Blockage Issue at Intersection is Eliminated
Railroad Operations	- Construction Impacts Minimized - Required Active Warning Devices in Proposed Condition	- Temporary Active Warning Devices Required During Construction - No Warning Devices Required in Proposed Condition	- Requires Very Complex Construction Staging - High Cost - Would Require Temporary Closures - Would Require Significant Temporary Improvements during Construction
Long-Term Maintenance/Structures	- Proposed Closed Storm Sewer System - No Pump Station or Bridge Structure Required - Maintenance of At-Grade Railroad Crossing, Gates and Warning Devices Required	- Overhead Structure Requires Maintenance - Bridge - Approx. 1,200 feet of Retaining Walls	- Proposed Closed Storm Water System - Pump Station - High Cost - High Maintenance Responsibility - Approx. 2,500 feet of Retaining Walls - Railroad Bridge Structure to Maintain
Construction Duration	- Three Main Stages, - Temp crossing signals and gates required - Duration of Construction 24 months	- Two Pre-Stages - Three Main Stages - Temp crossing signals and gates required - Diamond Lake Road Temporary Closures / Detour - Duration of Construction 36 months	- Three Main Stages - Main Stages 1 & 2 Divided into Nine Sub-Stages Each - Temp crossing signals and gates required, - Diamond Lake Road Temporary Closures - Duration of Construction 48 months
Constructability	- Standard Construction Methods	- Standard Construction Methods	- Complex Construction Methods
Utilities	- Potential Utility Impacts Resulting From Roadway Improvements	- Overhead Utility Relocation Required	- Underpass Lighting Required - Potential Utility Impacts
Construction Cost	\$	\$\$\$	\$\$\$\$\$

COLOR KEY: SMALL IMPACT / MAGNITUDE MODERATE IMPACT / MAGNITUDE LARGE IMPACT / MAGNITUDE

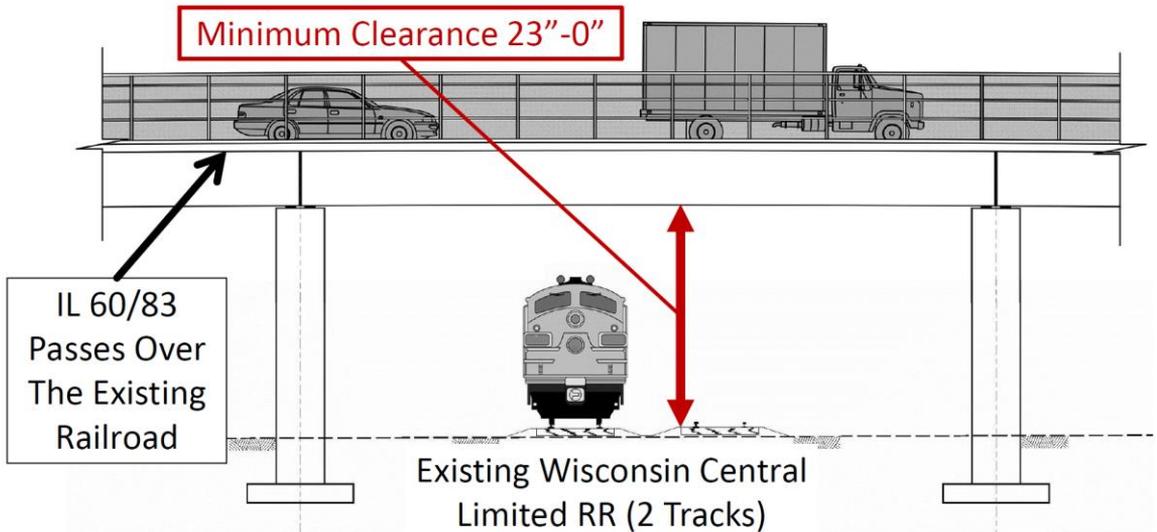


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Recommended Railroad Crossing Alternate

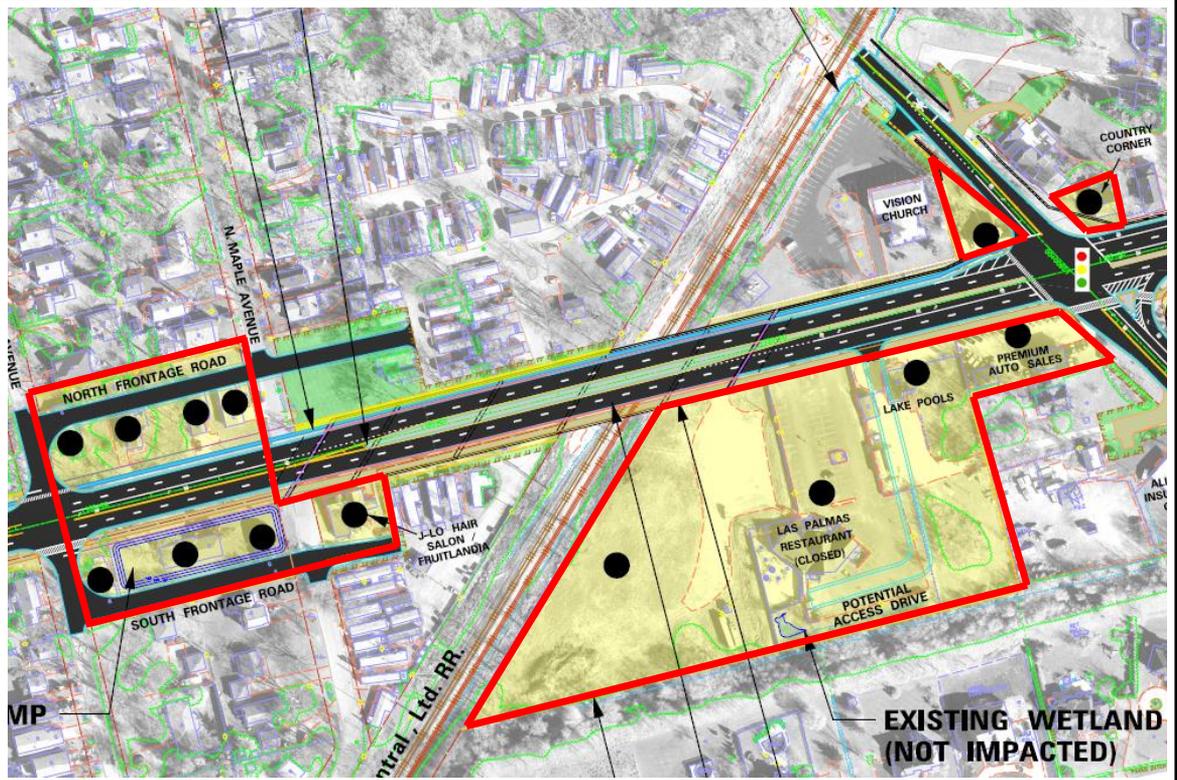
IL 60/83 Over the Railroad (Overpass)



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Property Displacements



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NOTES

Land Acquisition Types

- **Fee Simple**
 - Acquisition of all rights and interest
- **Permanent Easement**
 - Ownership is retained by property owner
 - IDOT is allowed use of property to construct and maintain facilities
- **Temporary Easement**
 - Ownership is retained by property owner
 - IDOT is allowed to construct minor improvements



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Land Acquisition Procedures

- **Determine Ownership**
 - Prepare Property Description / Plat of Survey
- **Independent Appraisal**
- **Negotiation**
- **Condemnation**
- **Relocation Assistance When Building Acquired**
 - Advisory/Referral Services
 - Replacement Housing Payments
 - Reimburse Moving Expenses



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Wetlands

- **5** Wetlands / Waters of the US Impacted
- Total Impacted Area - **0.2 acres**
- Wetland mitigation likely to occur at a **1.5:1** ratio



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Public Lands – Section 4(f) Impacts

Public Land	Land Acquisition (acres)	Temporary Easement (acres)	Permanent Easement (acres)
MUNDELEIN PARK AND RECREATION DISTRICT			
John Wiech Park	0.04	0.03	None
Diamond Lake Sports Complex	0.32	0.01	None
Orchard View Park	0.09	0.09	0.04
LAKE COUNTY FOREST PRESERVE DISTRICT			
Country Side Golf Course	0.89	0.15	None



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NOTES

Noise Analysis – Project Type

- **Type I Project – IL 60/83**
 - New Roadway Construction
 - New Travel Lanes
 - Substantial Roadway Alteration
- **Type II Project**
 - Applies to Existing Roadways
 - Retrofitting
 - IDOT Has No Type II Program



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NOTES

Noise Analysis Process

- **1) Identify Noise Receptor Locations**
- **2) Determine Traffic Noise Level**
 - Modeling
 - Validated by Field Monitoring
- **3) Traffic Noise Impact Identification**
- **4) Traffic Noise Abatement Analysis**



IL Route
60 83



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Noise Receptor Locations

A receptor is an outdoor area of frequent human use along the roadway.



NOTES

FHWA Noise Abatement Criteria

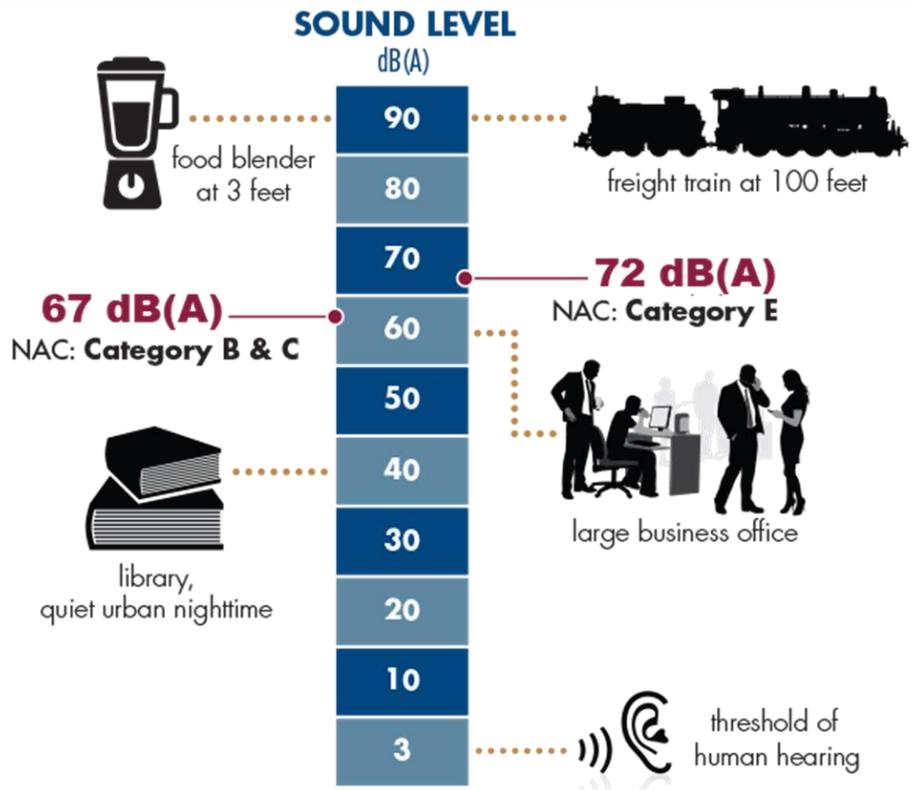
Activity Category	Noise dB(A)	Description of Activity Category
A	57	Serene Lands in which serenity and quiet are of extraordinary significance; rarely applies (Tomb of the Unknown Soldier)
B	67	Residential
C	67	Hospitals, Schools, Places of Worship, Parks, Forest Preserves
D	52 interior	Hospitals, Libraries, Places of Worship, Schools, Institutions
E	72	Hotels, Motels, Offices, Restaurants
F	None	Agriculture, Airports, Industrial, Retail, Utilities
G	None	Undeveloped Lands



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NOTES

Common Noise Levels



NOTES

Interior vs. Exterior Noise

- **IDOT** and **FHWA** stipulate that outdoor areas of frequent human use be given primary consideration.
- Interior noise for private residences not studied (Cat B).
- Interior noise levels are evaluated only if no exterior use areas are identified for those lands in Category D.



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Traffic Noise Level Determination

- Noise calculated at the **Worst-Case** receptor locations
- Predicted Traffic Noise Levels are estimated by using FHWA Traffic Noise Model (TNM)
- Scenarios: Existing, Future-No Build, Future-Build
- Existing noise levels validated with field monitoring

TABLE 5 – PREDICTED TRAFFIC NOISE LEVELS

Receptor	Activity Category	FHWA NAC $L_{eq,h}$ dB(A)	2008 Predicted Existing $L_{eq,h}$ dB(A)	2040 No-Build $L_{eq,h}$ dB(A)	2040 Proposed (Without Abatement) $L_{eq,h}$ dB(A)	Increase from Existing to Proposed $L_{eq,h}$ dB(A)	Impacted Receptor
R1	C	67	63	64	62	-1	NO*
R2	B	67	69	70	71	2	YES
R3	C	67	65	66	68	3	YES
R4	C	67	65	67	70	5	YES
R5	B	67	65	66	67	2	YES
R6	B	67	64	65	68	4	YES
R7	B	67	65	66	67	2	YES
R8	C	67	65	66	70	5	YES
R9	B	67	57	58	60	3	NO



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Feasibility & Reasonableness Policy

- **Feasibility**

- Abatement must achieve at least 5 dB(A) traffic noise reduction
- Abatement must be feasible to construct

- **Reasonableness**

- Generally, noise abatement cost must be **< \$24,000*** per benefitted receptor
- Must achieve at least an 8 dB(A) noise reduction at a benefitted receptor

*Adjustment factors can increase the allowable cost per benefitted receptor



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NOTES

IL 60/83 Potential Noise Walls

- **239** Sensitive Receptors Identified and Studied
- **17** Noise Abatement Walls Studied
- **5** Noise Abatement Walls are Feasible & Reasonable
- Wall Heights **10 – 12.5 Feet**
- **1 ¼ Miles** of Potential New Noise Walls
- **105** Benefitted Receptors
- Recommended Walls **AFTER** the Viewpoint Solicitation



NOTES

Viewpoints Solicitation

- **Benefited Receptors Rental Properties**
 - One Vote For Tenant
 - One Vote For Owner (per unit)
- **Receptors that share property line with IL 60/83**
 - Receive Two (2) Votes
- **Benefitted Receptors** will be contacted up to 2 times to maximize response rate
- Response goal per barrier is **33%**
- Abatement wall is likely to be implemented **if majority vote is in favor**



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NOTES

Viewpoints Example Letter & Form

FILE COPY

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Division of Highways/Region One / District One
 201 West Center Court/Schaumburg, Illinois 60196-1096

Project and Environmental Studies
 I-55 at Weber Road
 Weber Road from 135th Street/Romeo Road to 119th Street/Rodeo Drive
 Will County

November 8, 2013

Re: Viewpoint Solicitation – First Notice
 Noise Barrier Implementation

-fullname-
 -Address1-
 -Address2- -zip-

Dear Property Owner or Resident:

The Illinois Department of Transportation (Department) in cooperation with Will County Department of Highways (County) are currently engaged in preliminary engineering and environmental studies (Phase I) for Weber Road from 135th Street/Romeo Road to 119th Street/Rodeo Drive including the Weber Road interchange at I-55. The proposed improvements include reconstruction of the existing diamond interchange of I-55 at Weber Road to a diverging diamond interchange and widening of Weber Road from four lanes to six lanes. The I-55 at Weber Road improvements are included in the Department's FY 2014-2019 Proposed Multi-Modal Transportation Improvement Program contingent upon the sale of approximately 200 acres of unused property currently owned by the Illinois Department of Corrections as stipulated in Public Act 99-0019, and contingent upon local financial participation for improvements to adjacent highway facilities under local jurisdiction.

As part of the Phase I Study, traffic noise was evaluated for the proposed roadway improvements. The traffic noise analysis indicated that noise levels in your area warrant the consideration of noise abatement. Based on the noise abatement analysis, a noise wall approximately 10 feet high is warranted along the west side of Weber Road from approximately 300 feet north of Rodeo Drive to just north of Countryside Drive. See the enclosed figure for the location of the proposed noise wall. The proposed wall in your area is labeled as "B1B".

The Department is requesting your viewpoint regarding your desire for the noise wall proposed near your location. This letter has been provided to all property owners and tenants who would "benefit" from a noise barrier.

Viewpoint Form

I-55 at Weber Road
 Weber Road from 135th Street/Romeo Road to 119th Street/Rodeo Drive
 Will County
 Wall – B1B

Please provide your response by December 9, 2013.

Am I in favor of a noise barrier:

Yes
 No

Name: _____

Signature: _____

Owner: _____ OR Tenant: _____

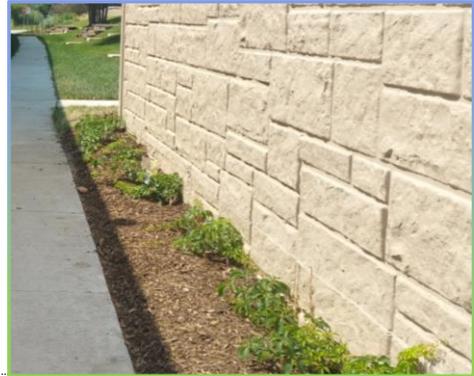
Address: _____

Date: _____

Comments: _____

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IDOT Current Typical Example Walls



IL Route 60 83



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NOTES

Advantages of BMPs

- Improves Overall Water Quality
- Minimizes Soil Erosion
- Controls Stormwater Runoff – Captures Soil Sediment and Roadway Pollutants



PIPE INLET PROTECTION



VEGETATED SWALE



BIOSWALE



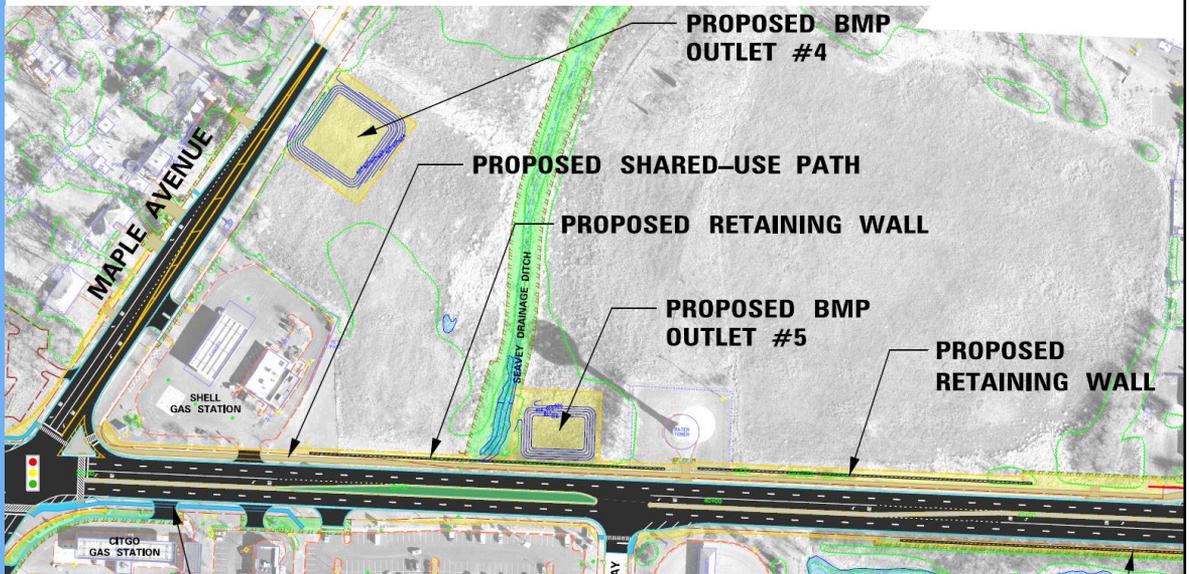
PIPE OUTLET PROTECTION



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Locations of BMPs



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NOTES

Schedule / Funding

Schedule

- Community Advisory Group Meeting No. 7 – 6/18/13
- Public Meeting No. 2 – 10/22/13
- Community Advisory Group Meeting No. 8 – 11/17/16
- Public Hearing – Spring 2017 (Target)
- Design Approval – June 2017 (Target)

Funding Status

- Phase II and Phase III are not currently included in the Department's FY 2017-2022 Proposed Highway Improvement Program
 - Phase II - Contract Plan Preparation & Land Acquisition
 - Phase III - Construction



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NOTES

Next Steps

- Give Consideration to Feedback from this Meeting
- Refine Design, as Applicable
- Present Recommended Alternate to the Public Spring 2017 (Target)
- Project Website: www.ilrte6083study.com



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NOTES

Project Workshop Working Session

Review Recommended Improvements with Meeting Participants



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Questions

- Questions and Comments
- Group Discussion to Follow



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