

FINAL REPORT

STRATEGIC **R**EGIONAL ARTERIAL

IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVE
U.S. Route 34 to Illinois Route 62

MARCH, 1997

By:



For:



**Operation
Greenlight**

FOREWORD

This Strategic Regional Arterial (SRA) report for Illinois Route 25 Corridor has been prepared for the Illinois Department of Transportation and the Chicago Area Transportation Study by Dames & Moore/MCE.

Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue (Illinois Route 25 Corridor) is a Strategic Regional Arterial from U.S. Route 34 to Illinois Route 62. As a SRA route, Illinois Route 25 Corridor is intended to function as part of a regional arterial system. This report is one element of a long range plan for all routes in the SRA network. Together, the route studies constitute a comprehensive, coordinated plan for the entire SRA network.

Included in this report are a description of the SRA study objectives and process, a detailed exposition and analysis of the existing route conditions, recommendations for ultimate improvements, and documentation of the public involvement process including citizen comments.

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EXECUTIVE SUMMARY

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



STRATEGIC
REGIONAL
ARTERIAL
PLANNING STUDY

EXECUTIVE SUMMARY

The Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue (Illinois Route 25 Corridor) has been divided into six sections for a detailed analysis. Recommendations for long range improvements (Year 2010) have been developed for each route section. A summary of the major recommendations is presented below.

Section 1 - Farnsworth Avenue - U.S. Route 34 to New York Street

- Develop two 12-foot lanes in either direction, and a barrier/flush median with an adjacent curb and gutter. Additional right-of-way will be acquired along Farnsworth Avenue.
- Intersection of Farnsworth Avenue & U.S. Route 34 needs to be signalized and expanded. The proposed lane configuration for the north-south legs will consist of a left turn lane, two through lanes with a shared right turn lane. The east-west legs will consist of dual left turn lanes, two through lanes and a right turn lane on U.S. Route 34.
- Evaluate the need for a signalized intersection at Montgomery Road, Simms Street and Fifth Avenue as development warrants.
- Expand the intersection of Farnsworth Avenue and New York Street. The proposed lane configuration for north and south legs will consist of a left turn lane, three through lanes with shared right turn lane. The east and west legs will consist of a left turn lane, two through lanes with a shared right turn lane.

Section 2 - Farnsworth Avenue - New York Street to Illinois Route 56

- Develop two 12-foot lanes in each direction, with a barrier/flush median and adjacent curb & gutter. Additional right-of-way will be required.
- Evaluate the need for a signalized intersection at Dearborn Avenue, Mountain Street and Bilter Road as development warrants.
- Expand the intersection of Farnsworth Avenue and Illinois Route 56. The lane configurations for north-south legs will consist of dual left turn lanes, three through lanes and a right turn lane. The east-west legs will consist of dual left turn lanes, two through lanes and a right turn lane.

Section 3 - Kirk Road - Illinois Route 56 to Illinois Route 64

- Develop three 12-foot lanes in either direction, with a 18-foot barrier median and adjacent curb & gutter.
- Expand the intersection of Kirk Road and Fabayan Parkway. The lane configurations for north-south legs will consist of dual left turn lanes, three through lanes and a right turn lane. The east-west legs will consist of dual left turn lanes, two through lanes and a right turn lane.
- Expand the intersection of Kirk Road and Illinois Route 38. The lane configuration will consist of dual left turn lanes, three through lanes and a right turn lane on all four legs of the intersection.
- Expand the intersection of Kirk Road and Illinois Route 64. The lane configuration will consist of dual left turn lanes, three through lanes and a right turn lane on all four legs of the intersection.

Section 4 - Kirk Road - Illinois Route 64 to Illinois Route 25

- Develop two 12-foot lanes in either direction, with a 18-foot barrier median, and combination adjacent curb and gutter.
- Evaluate the need for a signalized intersection at Dunham Road and Army Trail Road as development warrants.
- Expand the intersection of Dunham Road and Army Trail Road. The proposed lane configuration will consist of a left turn lane, two through lanes with a shared right turn lane on all four legs of the intersection.
- Stearns Road, Illinois Route 25 and Dunham Road will be realigned to form a major signalized intersection. The lane configuration for north leg consists of a left turn lane and two through lanes and a right turn lane. The south leg consist of dual left turn lanes, two through lanes with a shared right turn lane. The east-west leg consists of a left turn lane, two through lanes and a right turn lane.

EXECUTIVE SUMMARY cont.

Section 5 - Illinois Route 25/St. Charles Street/Villa Street/Center Street/Dundee Avenue - Dunham Road to Congdon Avenue

- Develop two lanes in each direction with barrier/flush median and curb and gutter.
- The intersections of St. Charles with Bent Street and Dwight Street have been identified as candidates for future traffic signals. Signals are proposed to be removed at May Street and at North Street. Geometrics and signage redesign is proposed in the vicinity of the St. Charles/Villa Street/National Street intersection.

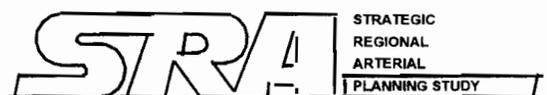
Section 6 - Illinois Route 25 - Congdon Avenue to Illinois Route 62

- Develop three 12-foot lanes in each direction, with an 18-foot barrier median and adjacent curb & gutter, north of I-90 (northwest tollway).
- Expand the intersection of Illinois Route 25 and Illinois Route 72. The lane configuration for north-south legs will consist of dual left turn lanes, three through lanes and a right turn lane. The east-west legs consist of a left turn lane, two through lanes and a right turn lane.
- Expand the intersection of Illinois Route 25 and Illinois Route 68. The lane configuration for north leg will consist of a left turn lane, three through lanes with a shared right turn lane. The south leg will consist of a left turn lane, three through lanes and a right turn lane. The east-west legs will consist of a left turn lane, two through lanes and a right turn lane.
- Expand the intersection of Illinois Route 25 and Illinois Route 62. The lane configuration will consist of dual left turn lanes, three through lanes and a right turn lane on all four legs of the intersection.

INTRODUCTION

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



The SRA System

The 2010 Transportation System Development Plan (TSD) adopted by the Chicago Area Transportation Study (CATS) and the Northeastern Illinois Planning Commission (NIPC) recognizes that it is not possible to accommodate all long distance high volume traffic on the primary expressway system. The arterial roadway system will have to carry some of this traffic. A designated system of Strategic Regional Arterials (SRAs) is proposed in the 2010 TSD plan to address this need most effectively from both a traffic and funding perspective. As shown on Figure I-1, the SRA system is a 1,340-mile network of existing roadways in the northeastern Illinois region. These roadways create a network of 66 corridors intended to serve as a second tier to the expressway system.

As part of a comprehensive approach, the SRA system is intended to:

- Supplement the Primary Expressway System.
- Enhance Public Transportation.
- Accommodate Commercial Vehicle Traffic.
- Increase Personal Mobility and Reduce Congestion.

The configuration of Strategic Regional Arterials will vary depending on the attributes of the area in which they are located. The abilities to preserve right-of-way for expansion and to control and restrict access are important considerations. Although desired typical cross-sections have been developed, there is no single design that will be appropriate for all designated roads. In all cases the compatibility of the roadway design with the needs of public transit will be considered. The desired configuration for each arterial roadway will be determined by a separate detailed study that will invite participation by the counties and municipalities through which it passes.

The system was formulated by first developing a set of candidate roads based on existing road characteristics, previous studies, and input from transportation agency representatives. A desirable spacing between SRAs was determined by the projected 2010 level of travel demand in the area.

This report presents the Illinois Route 25/Dunham/Kirk/Farnsworth corridor, which is illustrated on Figure MAP1-SEC. Majority of this corridor is located in Kane County and is under the jurisdiction of Illinois Department of Transportation, Kane County and other local agencies.

The Strategic Regional Arterial study is a conceptual study prior to Phase I study. The recommendations from this study can be used to acquire right-of-way for the roadway improvements and to prioritize projects by the Illinois Department of Transportation. This pre-Phase I study will be followed by a Phase I study which will address details such as mitigation of existing landscaping, site rehabilitation after construction, type of curb and gutter, maintenance of traffic during

construction, disposition of existing overhead utilities, lighting, potential noise problems, drainage and other issues of importance to local communities.

Functional Classification

The Illinois Route 25/Dunham/Kirk/Farnsworth SRA corridor is classified as a suburban route for the entire length. The desirable cross-section is three continuous lanes in each direction, separated by a raised median for access control.

Planning Framework

Long-range planning for the Illinois Route 25/Dunham/Kirk/Farnsworth corridor takes into account many factors. These factors include regional transportation plans, established SRA design concepts, route type, adjacent land use and future development plans, public transit needs, and community concerns. The planning framework for this SRA study is briefly discussed below.

SRA Design Concept

A report on design concepts for the SRA system, prepared by Harland Bartholomew & Associates, Inc. was endorsed by the CATS Policy Committee. The Design Concept Report provides desirable cross-sections for each type of SRA route. Included are the number and widths of lanes, required right-of-way and median requirements. According to the Design Concept Report, a suburban SRA requires 120 to 150 feet of right-of-way. These concepts have been used as a guide in developing the improvement plan for the Illinois Route 25/Dunham/Kirk/Farnsworth corridor that is described in this report. The Illinois Route 25/Dunham/Kirk/Farnsworth corridor has existing right-of-way varying between 66 and 190 feet.

The 2010 Transportation Network

I-88 (East-West Tollway) has a full interchange with Farnsworth Avenue near the southern end of this corridor. I-90 (Northwest Tollway) has a partial interchange with Illinois Route 25 at the northern end of this corridor. The main purpose of the Illinois Route 25/Dunham/Kirk/Farnsworth corridor, in conjunction with the other SRA routes in the area, is to supplement the expressway system and provide a major through traffic route for north-south travel.

The Illinois Route 25/Dunham/Kirk/Farnsworth corridor is intersected by eight SRA routes. At the south end of the route Farnsworth Avenue intersects U.S. Route 34 and Illinois Route 56. Kirk Road

intersects Fabyan Parkway, Illinois Route 38 & Illinois Route 64. Illinois Route 25 intersects Illinois Route 72, Illinois Route 68 and Illinois Route 62, which are major east-west routes at the north end of the corridor.

2010 Traffic Models

Chicago Area Transportation Study (CATS) provided a raw travel demand model output for the years 1990 and 2010. The model which was run for this study assumed full build-out of all proposed SRA routes to SRA design standards. The 2010 transportation network assumptions are, however, consistent with the CATS' 2010 TSD Plan Update in all other respects. The data were modified by Dames & Moore, in consultation with CATS, to produce the 2010 forecasts shown in this report.

Planned Roadway Improvements

Planning information was obtained from IDOT, CATS, Kane County, and the surrounding local communities and incorporated in the development of this report. The long range Transportation Plan for Kane County indicates several improvements for Kirk Road from Illinois Route 56 to Illinois Route 64. The City of Aurora has plans to improve Farnsworth Avenue.

City and Village Comprehensive Plans

Villages and cities along Illinois Route 25/Dunham/Kirk/Farnsworth corridor provided comprehensive plans detailing information on local transportation plans, zoning maps, and community objectives. These have been considered in developing design concepts for this corridor.

Planned Transit Improvements

The Future Agenda for Suburban Transportation, published jointly by Metra and Pace, was reviewed for any plans that could impact the SRA concepts for the corridor. The Illinois Route 25/Dunham/Kirk/Farnsworth Corridor has many existing transit facilities along its length.

Future Land Use and Development

The Illinois Route 25/Dunham/Kirk/Farnsworth corridor traverses rapidly developing regions. Major residential developments are expected along Farnsworth Avenue, Kirk Road and Illinois Route 25 south of Elgin.

Organization of the Report

This report presents a summary of the SRA planning study for the Illinois Route 25/Dunham/Kirk/Farnsworth corridor. It is organized as follows:

- **Environmental Conditions and Land Use**
 - This chapter discusses environmental and land use conditions. It includes a description of wetlands, historical sites, and hazardous waste sites, as well as other environmental features located within the corridor. Land use, zoning and future developments are also addressed.
- **Existing Roadway Conditions**
 - This chapter presents the existing physical characteristics, traffic operation, safety, and public transportation along the corridor.
- **Corridor Planning Overview**
 - This chapter further discusses the SRA planning objectives for the corridor. The 2010 corridor design characteristics and traffic conditions are described. Future land use and community concerns are reviewed.
- **Recommended Improvements**
 - This chapter presents the recommended SRA corridor plan, including proposed cross-sections, intersection diagrams, right-of-way requirements, access management, and public transit. Cost projections for right-of-way and construction are also included.
- **Public Involvement**
 - This section documents the public involvement process undertaken for the SRA study, which included Individual Community Interviews, Advisory Panel Meetings, Newsletters, and a Public Hearing. These opportunities for participation allowed the general public and elected officials to voice opinions concerning the SRA study.

INTRODUCTION

The Corridor Study Area

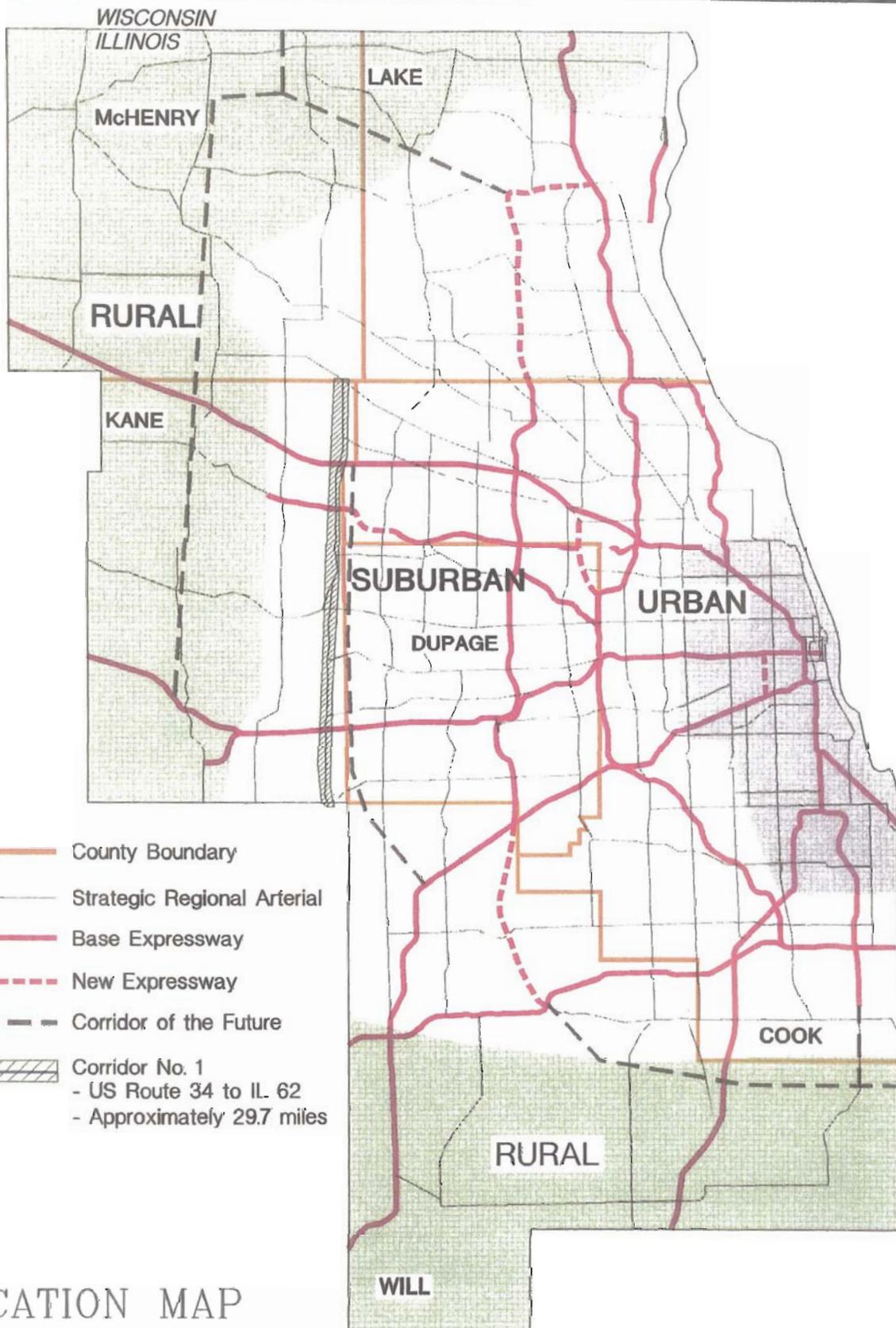
The Illinois Route 25/Dunham/Kirk/Farnsworth corridor is approximately 31 miles in length. The corridor begins on the south at the intersection of Farnsworth Avenue and U.S. Route 34 (Ogden Avenue) in Kendall County and terminates at Illinois Route 62 (Algonquin Road) in Kane County. The corridor vicinity is shown on Figure I-1.

Proceeding north into Kane County, Farnsworth Avenue is discontinuous between Montgomery Road and Binder Street in the City of Aurora. This area is currently vacant, with planned future residential development. Farnsworth Avenue continues as a two-lane street through the residential neighborhood to New York Street.

North of New York Street, Farnsworth Avenue continues as a 4-lane roadway with barrier/flush median to Illinois Route 56. From New York Street, the land use varies from residential, commercial to light industrial as this route proceeds north to Illinois Route 56. Farnsworth Avenue has a full interchange with I-88 (east-west tollway) within the section. North of Illinois Route 56 Farnsworth Avenue becomes Kirk Road. The majority of land is undeveloped along Kirk Road on the east side between Illinois Route 56 and Illinois Route 64 except for the Fermi National Accelerator Laboratory.

Land use is moderate to scattered residential along Dunham Road through the Village of St. Charles and the Village of Wayne. At this point, an alternate route is proposed for the SRA designation that passes through the commercial area of Elgin rather than the residential area along Illinois Route 25. The alternate, which uses St. Charles Street, Villa Street, and Dundee Avenue, has a four-lane cross-section while Illinois Route 25 has only two lanes. North of the City of Elgin, Dundee Avenue continues as Illinois Route 25. Illinois Route 25 has a full interchange with I-90 (Northwest Tollway) north of Elgin.

Illinois Route 25 passes through the Villages of East Dundee, Carpentersville and Barrington Hills with mixed commercial and residential land use. At Illinois Route 72 (Higgins Road), the corridor becomes primarily a commercial corridor through the Villages of East Dundee and Carpentersville. There are residential areas on the east and west, with some major special uses including cemetery and golf course. Illinois Route 25 ends at Illinois Route 62, which is another SRA route.



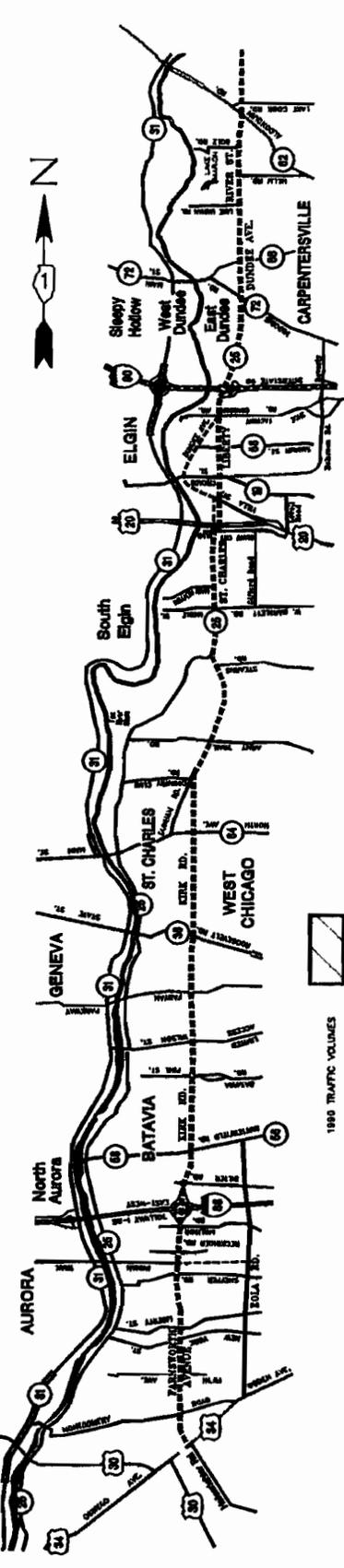
LOCATION MAP

-IL 25/Dunham Rd/Kirk Rd/Farnsworth Ave.

FIGURE i-1

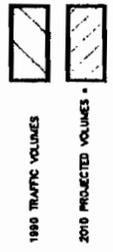
SECTION I	SECTION II	SECTION III	SECTION IV	SECTION V	SECTION VI
2.38 MILES	4.05 MILES	7.37 MILES	4.39 MILES	6.04 MILES	5.47 MILES
66' 80'-120'	66'-80' 80'-120'	130'-210' 130'-210'	100'-110' 100'-110'	86'-30' 86'-100'	100' 120'
1 1	2 2	2 2	1 1	1 1	2 2
1 1	2 2	3 3	2 2	2 2	3 3

EXISTING R.O.W.
PROPOSED R.O.W.
EXISTING LANE CONFIGURATION
PROPOSED LANE CONFIGURATION

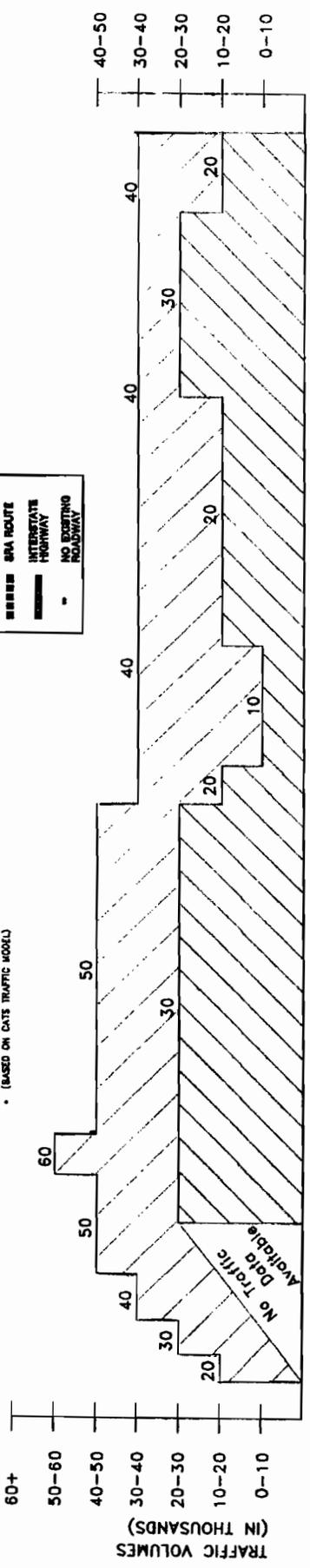


LEGEND

- CROSSING
- SRM ROUTE
- SRM ROUTE
- INTERSTATE
- HIGHWAY
- NO EXISTING ROADWAY



* (BASED ON OATS TRAFFIC MODEL)



IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE

ENVIRONMENTAL CONDITIONS AND LAND USE

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



STRATEGIC
REGIONAL
ARTERIAL

PLANNING STUDY

ENVIRONMENTAL CONDITIONS AND LAND USE

Introduction

As a part of the planning process, the SRA study includes a general assessment of environmental features within the corridor. Environmental issues which are of potential concern for transportation projects include nearly the entire spectrum. The SRA planning process does not define specific mitigation measures. The results of the general assessment, however, will be the basis for future assessments and mitigation. A more detailed analysis of these environmental concerns will take place as individual segments proceed to more advanced design stages.

This SRA corridor combines portions of Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue. The majority of this SRA corridor is located within Kane County, Illinois. The corridor has been divided into six segments for study purposes. Environmental and land use information relative to each section is discussed in this chapter. Major features of the corridor are summarized in tables at the end of the chapter. Tables I-1, I-2, and I-3 list sites identified as having underground storage tanks (USTs), leaking underground storage tanks (LUSTs), and cerclis sites respectively. Historic sites & buildings are listed in Table I-4.

In Kane County, there are twenty Federally listed threatened and endangered species and species of special concern. Site-specific surveys for threatened and endangered species, and delineation of potentially affected, wetlands, may be necessary in future detailed studies for improvement of this corridor.

Section 1 -US Route 34 to New York Street

Exhibit A1-01 to Exhibit A1-03

Section 1 of Farnsworth Avenue begins at US Route 34 (Ogden Avenue) in northern Kendall County, and continues north to New York Street in Kane County. This section passes through unincorporated Kendall and Kane Counties and the City of Aurora.

Environmental Conditions

The main environmental constraint in this section is the crossing of Waubensee Creek, which is surrounded by a large floodplain area. A small wetland is located just north of Allan School. Sites which contain under ground storage tank in this section include VM Muffler (gas station) located on the southwest corner of New York Street and Farnsworth Avenue.

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

Land Use

The land use in this section is predominantly single family residential, although there are some commercial and agricultural uses. A bowling alley and roller skating rink are located on the southwest corner of Montgomery Road and Farnsworth Avenue. Allan Elementary School is located between Simms Street and 7th Avenue on the east side of the road. A playground area is located around this school which may be considered Section 4(f) land. A church is located on the northwest corner of 5th Avenue and Farnsworth Avenue. Farnsworth playground and a portion of Oakhurst Forest Preserve are located between Grand Boulevard and Wermes Avenue.

Section 2 - New York Street to Illinois Route 56

Exhibit A1-03 to Exhibit A1-07

Section 2 of Farnsworth Avenue extends from New York Street to Illinois Route 56. This section passes through the City of Aurora and unincorporated Kane County. The corridor intersects one other SRA route, Illinois Route 56.

Environmental Conditions

Indian Creek and it's adjacent floodplain cross Farnsworth Avenue north of the Burlington Northern Railroad. Indian Creek also crosses Farnsworth Avenue south of Reckinger Street. Another channelized ditch which runs east-west is located north of Marshall Boulevard. Wetlands and deep ditches containing vegetation such as cattails are located north of the I-88 Interchange. Wetlands are also adjacent to the road near Illinois Route 56 east of Kirk Road.

Sites which may contain underground storage tanks (UST's) include a former Marathon gas station (currently being remediated) located on the southwest corner of Front Street and Farnsworth Avenue, a Speedway gas station located on the southwest corner of Sheffer Road and Farnsworth Avenue, a Clark gas station located on the southwest corner of Indian Trail and Farnsworth Avenue, a Shell gas station located on the northeast corner of Molitor and Farnsworth Avenue, a Phillips 66 gas station located on the southeast corner of Reckinger Street and Farnsworth Avenue, Fox Valley Ford Dealer, located on the northeast corner of Reckinger Street and Farnsworth Avenue, a Lincoln Mercury Dealership located on the southwest corner of Bilter Road and Farnsworth Avenue, and an Amoco gas station located on the southwest corner of Illinois Route 56 and Farnsworth Avenue.

Sites which are designated Leaking Underground Storage Tank (LUST) sites include Brickets Car Wash located on the southwest corner of Indian Trail and Farnsworth Avenue, Marathon gas station located on the southeast corner of Molitor and Farnsworth Avenue, Aurora Paperboard Division, of Davey Co. located at 705 N. Farnsworth Avenue, Aurora East School District #131 located at 700 S. Farnsworth Road, and Lerene Inc. located at 420 Farnsworth Avenue.

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

Land Use

The land use in this section is predominantly single family residences with some commercial and industrial sections. The right-of-way is constrained in this section. There are virtually no parkway trees. The Illinois Prairie Path and the Commonwealth Edison right-of-way containing overhead electrical lines are located just south of Marshall. This land is considered Section 4(f) land.

Section 3 - Illinois Route 56 to Illinois Route 64

Exhibit A1-07 to Exhibit A1-14

Section 3 of the corridor is called Kirk Road and extends from Illinois Route 56 to Illinois Route 64. This section passes through unincorporated Kane County, Batavia, Fermi National Laboratory, Geneva, and St. Charles. This section intersects three SRA routes: Fabyan Parkway, Illinois Route 38 (Roosevelt Road), and Illinois Route 64.

Environmental Conditions

The major environmental concern for this section is Fermi Lab (owned by the U.S. Department of Energy) located on the east side of Kirk Road from Illinois Route 56 to Wilson Street. Various wetlands are situated within the Fermi Lab Complex. Wetlands are located on the northwest corner of the Prairie Path and Kirk Road, across from Geise Road, South of Pine Street, between Wilson Street and the Burlington Northern Railroad, northeast corner of Hubbard Avenue and Kirk Road, southwest corner of Fabyan Parkway and Kirk Road and southwest and southeast corner of Chicago & Northwestern Railroad and Kirk Road.

Sites which may contain underground storage tanks (UST's) include Motor Equipment & Truck Repair located south of Illinois Route 38 (Roosevelt Road) on the east side, an Amoco gas station located on the northeast of corner Illinois Route 38 and Kirk Road, a Shell gas station located on the northwest corner of Illinois Route 38 and Kirk Road, and Scotts Auto Repair located on the northeast corner of Stern Avenue and Kirk Road. Sites which are designated Leaking Underground Storage Tank (LUST) sites include Fermi Lab, Aldi Inc. located at 1200 N. Kirk Rd., Suncast Corporation located on the northwest corner of Lathem and Kirk Road, and Settlers Hill Landfill located at 1031 Fabyan Parkway. Settlers Hill Golf Course and Landfill, Fermi Accelerator Lab, and Guarding Packaging Corporation located at 475 N. Kirk Road, are all designated hazardous waste sites (CERCLIS).

Land Use

The land use in this section is a combination of industrial and single family residences. Tabernacle of Praise Church is located on the southwest corner of Cherry Lane and Kirk Road. Kane County

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

Stadium is located on the southwest corner of the Chicago and Northwestern Railroad and Kirk Road. Roberta Campbell Cultural Center is located northwest of the stadium and includes a conference center and art gallery. The DuPage County Airport is located east of the proposed improvement in this section and needs to be considered when studying this roadway. Kindercare Day Care is located on the southwest corner of Hill Road and Kirk Road. A prairie path crosses Kirk Road just north of Illinois Route 56 and another crosses between Harrison Road and Geneva Drive.

Section 4 - Illinois Route 64 to Illinois Route 25

Exhibit A1-14 to Exhibit A1-18

Section 4 of Kirk Road extends through Dunham Road from Illinois Route 64 (North Avenue) to Illinois Route 25. This section passes through the Villages of St. Charles, Wayne, Bartlett, and unincorporated Kane County. This section intersects Stearns Road, a SRA route.

Environmental Conditions

This section of Kirk Road is crossed by three streams with associated flood plains and wetlands. Norton Creek is south of Army Trail Road. Brewster Creek crosses north of CNWRR and an unnamed stream crosses just south of IL RTE 25. A prairie path used mainly for a bike path crosses South of the CC&PRR..

Land Use

The land uses in this section are predominantly low density single family residences. St. John Newman Church is located on northwest corner of Illinois Route 64 and Kirk Road. The Central DuPage Hospital Clinic is currently being built on the northwest corner of Foxfield Road and Kirk Road. St. Charles High School is located north of Foxfield Road on the west side of Kirk Road. The Royal Fox Golf Course is located on Royal Fox Drive on both sides of the route. An underpass for golfers is located just north of Royal Fox Drive. The Oak Lawn Farm Historic District encompasses the intersection of Army Trail Road, and the Wayne Village Historic District is situated along Army Trail Road east of Dunham Road.

Section 5 - Illinois 25/Dunham Road to Congdon Avenue

Exhibit A1-18 to Exhibit A1-23

Section 5 of this corridor follows Illinois Route 25 from Dunham Road to Bluff City Boulevard, where an alternate route begins on St. Charles Street, Villa Street, Center Street and Dundee Avenue, merging back to Illinois Route 25 (Exhibit Alt-Elgin) at Congdon Avenue. This section passes through Bartlett, Elgin and unincorporated Kane County.

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

Environmental Conditions

The environmental concerns in this section include wetlands and flood plains located adjacent to the road. Wetlands are located south of Leeward Corporation and adjacent to Poplar Creek. An Illinois Department of conservation forest preserve is adjacent to Route 25 north of Dunham Road.

Sites which may contain Underground Storage Tanks (UST's) include Grant Truck & Trailer Repair located between Dunham Road and Middle Road on the west side, Graf and Sons Auto Repair located south of Poplar Creek on the east side, Auto Specialist located on the southeast corner of St. Charles Street and Bluff City Boulevard, and an Amoco gas station located on the northwest corner of St. Charles and Bluff City Boulevard. A site which is designated Leaking Underground Storage Tank (LUST) is Brady Ready Mix located south of the Chicago & Northwestern Railroad on the east side. Illinois Tool Works, adjacent to this property, is a CERCLIS hazardous waste site. In Elgin, sites which may contain Underground Storage Tanks (UST's) include a service station located on the southwest corner of Fulton and Villa Avenue, an All Tube and Lube located on the northwest corner of North Street and Dundee Avenue, Car-X located on the southeast corner of North Street and Dundee Avenue, Union 76 gas station located on the southwest corner of Franklin and Dundee Avenue, an old gas station located on the northeast corner of Park and Dundee Avenue, a Yamaha shop located across from Park Street on the west side, an Amoco gas station located on the southeast corner of Ann and Dundee Avenue.

Land Use

The land use south of Elgin is predominantly undeveloped and industrial. In the City of Elgin, the route is bounded by commercial and single family residential development. A nondenominational Church is located on the northwest corner of Kenyon Road and Illinois Route 25. The Calvary Baptist church is located on the northwest corner of May Street and Illinois Route 25.

The Elgin Historic District, roughly bounded by Villa, Center, Park, N. Liberty, and South Channing Streets, is located in this section and is listed on the National Register of Historic Places.

Elgin Fire Barn Museum #5 is located at the intersection of Arlington Street and St. Charles Street. A dedication to Peter Vonnostrand is also located at this intersection, which may be considered Section 4(f) land. Davidson Park is located on the southeast corner of Villa and Prairie Street. A park is located on the northwest corner of Kimball and Dundee Avenue.

Emmanuel Baptist Church is located on the southwest corner of Bent Street and St. Charles Street at 500 St. Charles Street. The First Congregational Church, dated 1889, is located on the northeast

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

corner of Chicago and Center Street. The United Methodist Church is located on the southeast corner Highland Street and Center Street. The First United Church is located on the northwest corner of Highland Street and Center Street. The Church of the Redeemer is located on the southwest corner of Highland Street and Center Street. This church appears to be plaqued. St. Paul Church is located on the northwest corner of Division and Center Street. Iglesias de Dior Pentecostal El Camino Church are located at 650 Center Street.

The house at 363 St. Charles has a plaque showing local historic significance. Gar Memorial Hall, dated 1896, is located on the northeast corner of Fulton and Villa Avenue. The Knights of Columbus is located on the southwest corner of Chicago Street and Center Street. The Wait-Allenson Funeral Home is located on the southeast corner of Division and Center Street. The Schmidt Madison Funeral Home is located on the southeast corner of Park and Dundee Avenue. A Historical Museum is located at 360 Park Street. Larsen Middle School is located at 665 Dundee Avenue. Elgin Academy is located on Kimball Street east of Dundee Avenue. Firehouse #3 is located at the intersection of Dundee and St. Charles Road.

Section 6 - Congdon Avenue to Illinois Route 62 (Algonquin Road)

Exhibit A1-23 to Exhibit A1-29

Section 6 merges back to Illinois Route 25 at Congdon Avenue and extends north to Illinois Route 62 (Algonquin Road). This section passes through Elgin, East Dundee, Carpentersville, Barrington, and unincorporated Kane County.

Environmental Conditions

An environmental concern in this section is Trout Park located on the southwest corner of the I-90 Tollway and Illinois Route 25. Trout Park is owned and maintained by the City of Elgin Park District and is considered Section 4(f) land. According to the Elgin Comprehensive Plan, the ecologically significant areas are those sites where flora and fauna approximate natural conditions prior to settlement in the Elgin area some 150 years ago. These areas are: Bluff Spring Fen, Chicago Junior School area, Poplar Creek Savanna, and Trout Park natural area.

Sites which may contain Underground Storage Tanks (UST's) include an old gas station located on the northeast corner of Congdon & Illinois Route 25, an Amoco gas station located on the northeast corner of River Bluff & Illinois Route 25; Midas Muffler and Brake located on the northeast corner of Elma and Illinois Route 25, a car dealer located on the southeast corner of Parkway and Illinois Route 25, Fleetwing Motors located on the northwest corner of Oakhill & Illinois Route 25, Shell gas station located on the northeast corner of Parkway & Illinois Route 25, an old gas station located between Parkway & Stewart Avenue on east side, Springhill Ford located between I-90 Tollway and Illinois Route 72 on the east side, Mustang Restoration located between I-90 Tollway and Illinois

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

Route 72 on the west side, Collingbourn Country Motors located between I-90 Tollway and Illinois Route 72 on the west side, Old Volkswagen Dealership located between I-90 Tollway and Illinois Route 72 on the west side, American Auto Center located between I-90 Tollway and Illinois Route 72 on the west side, Saturn of Dundee and Mobil gas station located across from Park Street, Amoco gas station located on the northwest corner of Illinois Route 68 & Illinois Route 25, Goodyear located at 80 N. Kennedy Drive, Shell gas station located on the southeast corner of Hazard Road & Illinois Route 25, an old gas station located on the northwest corner of Hazard Road & Illinois Route 25, Shell gas station located on the southeast corner King Road & Illinois Route 25, an old gas station located north of King Road on the west side, International Automotive Specialists located south of Illinois Route 62 on the west side, Algonquin Auto House located south of Illinois Route 62 on the west side, an old gas station located on the southwest corner of Illinois Route 62 & Illinois Route 25, an old gas station located on the southeast corner Illinois Route 62 & Illinois Route 25, and a Phillips 66 gas station located on the northwest corner of Illinois Route 62 & Illinois Route 25. Sites which are designated Leaking Underground Storage Tank (LUST) sites include Checker Gas Station located on the northwest corner River Bluff Road & Illinois Route 25, Oldsmobile Torco Cadillac Dealer located on the northeast corner of Albert Road & Illinois Route 25, Al Piemonte Chevrolet Dealer located between I-90 Tollway and Illinois Route 72 on the east side, Amoco gas station located on the southwest corner of Illinois Route 72 & Illinois Route 25, Meyer Material located at 800 Bolz Road, Night Shift Transmission located on the southeast corner of Bolz Road & Illinois Route 25, and Max McGraw Wildlife Foundation located at 15 N. 392 Illinois Route 25.

Land Use

The land use in this section is a combination of single family residences and commercial. A Head start School is located on the northwest corner Congdon Avenue and Illinois Route 25. Coleman School is located between Ford Street and Oakhill Road on the west side of the street. Chicago Junior School is located on the northwest corner of I-90 Tollway and Illinois Route 25. Lakewood School is located behind Trinity United Methodist Church. Meadowdale School is located between Hazard Road and Robin Road on the east side of the road. St. Monica's School and Church are located just north of Robin Lane on the west side of Illinois Route 25.

Church of the Brethren General Office is located on the northeast corner of Stewart Avenue and Illinois Route 25. Trinity United Methodist Church is located north of Illinois Route 68 on the west side of Illinois Route 25. St. Stephen Lutheran Church is located on the northwest corner of Golfview Drive and Illinois Route 25. St. Nirankari Mission of Universal Brotherhood is located just north of St. Stephen. Dundee Masonic Temple is located between Golfview Drive and Hazard Road on the east side of the road. Calvary Church of God is located on the southeast corner Cherokee Road and Illinois Route 25. The Max McGraw Game Farm Wildlife Preserve is located

ENVIRONMENTAL CONDITIONS & LAND USE - cont'd

north of I-90 Tollway on both sides of the road. Santa's Village, an amusement park, is located on the southwest corner of Illinois Route 72 and Illinois Route 25. The Community Funeral Home is located just north of Park Avenue on the east side of the road. The Dundee Township Park District is located on the northwest corner of Illinois Route 68 and Illinois Route 25 and also includes Day care facilities. Meadowdale Shopping Center is located between Meadowdale Drive and Lake Marian Road on the west side of Illinois Route 25. A meadowdale water tower is located within this shopping center.

The Milk Pail is a shopping area located north of I-90 Tollway on the west side of the road. Some of the buildings within the Milk Pail complex have plaques showing local historical significance. The S.S. Dickenson Building has local historical significance and is located east of Illinois Route 25 on the south side of Illinois Route 62.

**Table I-1
UST SITES
Illinois Route 25/Dunham Rd/Kirk Rd/Farnsworth Ave**

Name	Location	Label No. Exhibit No.
D & M Muffler	SW corner of Farnsworth Avenue & New York Street	U-1 Exhibit A1-03
Old Marathon Gas Station	SW corner of Farnsworth Avenue & Front Street	U-2 Exhibit A1-03
Speedway Gas Station	SW corner Sheffer and Farnsworth Ave	U-3 Exhibit A1-04
Clark Station Gas Station	1180 Farnsworth Ave SW corner Indian Trail and Farnsworth	U-4 Exhibit A1-05
Phillips 66 Gas Station	SE corner Reckinger and Farnsworth Ave	U-5 Exhibit A1-05
Fox Valley Ford Dealer	NE corner Reckinger and Farnsworth Ave	U-6 Exhibit A1-05
Shell Gas Station	NE corner Farnsworth Ave and Molitor Road	U-7 Exhibit A1-05
Lincoln Mercury Dealer	SW corner Bilter Road and Farnsworth Rd.	U-8 Exhibit A1-06
Amoco Gas Station	SW corner IL Route 56 & Kirk Rd.	U-9 Exhibit A1-07
Motor Equipment & Truck Repair	NE corner Kirk Road and Chicago & N.W. RR	U-10 Exhibit A1-11
Shell Gas Station	NW corner IL Route 38 and Kirk Rd	U-11 Exhibit A1-12
Amoco Gas Station	NE corner IL Route 38 and Kirk Rd.	U-12 Exhibit A1-12
Scotts Auto Repair	NE corner Stern Ave and Kirk Rd.	U-13 Exhibit A1-13
Grant Truck & Trailer Repair	Between Bartlett & Dunham Road on West Side	U-14 Exhibit A1-18

**Table I-1
UST SITES
Illinois Route 25/Dunham Rd/Kirk Rd/Farnsworth Ave.**

Name	Location	Label No. Exhibit No.
Graf & Sons Auto Repair	Between Hammond Ave and C & NW RR East Side	U-15 Exhibit A1-20
Carmellos Auto Repair	NE corner Bluff City and St. Charles St.	U-16 Exhibit A1-21
Auto Specialist	SE corner Bluff City and St. Charles St.	U-17 Exhibit A1-21
Amoco Gas Station	NW corner Bluff City and IL 25	U-18 Exhibit A1-21
Old Gas Station Servicio Sanchez	SE corner Slade Avenue and IL 25	U-19 Exhibit A1-23
Meineke Muffler	Between Cooper and Congdon Ave., W. Side of Dundee Avenue	U-20 Exhibit A1-23
Alexies Auto Repair	West of IL 25, North side of Page Ave.	U-21 Exhibit A1-23
Midwest Transmission	Across the street from Alexis Auto Repair	U-22 Exhibit A1-23
Auto Repair	SE corner Dundee Ave. & Page	U-23 Exhibit A1-23
Auto Repair	NE corner of Dundee and Page Avenue	U-23 Exhibit A1-23
Old Gas Station (900)	Just south of Page Ave., W Side of IL 25	U-24 Exhibit A1-23
Old Gas Station (1103)	NE corner Congdon/IL 25	U-25 Exhibit A1- 23
Emils Garage	SW corner Congdon/IL 25	U-26 Exhibit A1- 23

Table I-1
UST SITES
Illinois Route 25/Dunham Rd/Kirk Rd/Farnsworth Ave.

Name	Location	Label No. Exhibit No.
Amoco Gas Station	Across from River Bluff Avenue	U-27 Exhibit A1- 23
Midas Muffler	NE corner IL 25 and Elma Street	U-28 Exhibit A1-23
Car Dealer	SE corner IL 25 and Parkway Street	U-32 Exhibit A1-23
Springhill Ford	NE Corner Albert Road and IL 25	U-33 Exhibit A1- 25
Collingbourn Country Motors	Between I-90 and IL 72, W. Side	U-34 Exhibit A1- 25
Old Volkswagen Dealership	Between I-90 and IL 72, W. Side	U-35 Exhibit A1- 25
Mustang Restoration	Between I-90 and IL 72, W. Side	U-36 Exhibit A1- 25
American Auto Center	SW corner Richardson/IL 25	U-37 Exhibit A1- 25
Saturn of Dundee Dealer	Between I-90 and IL 72, E. Side	U-38 Exhibit A1- 25
Mobil Gas Station	Across from Park Street	U-39 Exhibit A1- 25
Amoco Gas Station	NW corner IL 68/IL 25	U-40 Exhibit A1- 25
Shell Gas Station	SE Corner Lake Marian/ IL 25	U-41 Exhibit A1- 27
Old Gas Station	NE corner Lake Marian/IL 25	U-42 Exhibit A1- 27
Goodyear (80 N. Kennedy Drive)	Between Lake Marian Road and Berkley Street, W. Side	U-43 Exhibit A1- 27

**Table I-1
UST SITES
Illinois Route 25/Dunham Rd/Kirk Rd/Farnsworth Ave.**

Name	Location	Incident No. IEPA No.
Old Gas Station (H & L Liquor)	NW Corner King Road and IL 25	U-44 Exhibit A1- 28
Sell Gas Station	SW Corner King/IL 25	U-45 Exhibit A1- 28
International Automotive Specialists	South of Algonquin Auto House	U-46 Exhibit A1-29
Algonquin Ato House	South of IL 62, W. Side	U-47 Exhibit A1- 29
Old Gas Station	SE Corner Algonquin/IL 25	U-48 Exhibit A1- 29
Old Gas Station	SW Corner Algonquin/IL 25	U-49 Exhibit A1- 29
Phillips 66 Gas Station	NW Corner Algonquin/IL 25	U-50 Exhibit A1- 29
Service Station	SW Corner of Fulton/Villa	U-51 Exhibit A1-22A
All Tune and Lube	NW Corner of North St/Dundee	U-52 Exhibit A1-22A
CAR-X Muffler and Repair Shop	SE Corner of Franklin/Dundee	U-53 Exhibit A1-22A
Union 76 Gas Station	SW Corner of Franklin/Dundee	U-54 Exhibit A1-22A
Old Gas Station	NE Corner of Park/Dundee	U-55 Exhibit A1-22A

**Table I-2
LUST SITES
Illinois Route 25/Dunham Rd/Kirk Rd/Farnsworth Ave.**

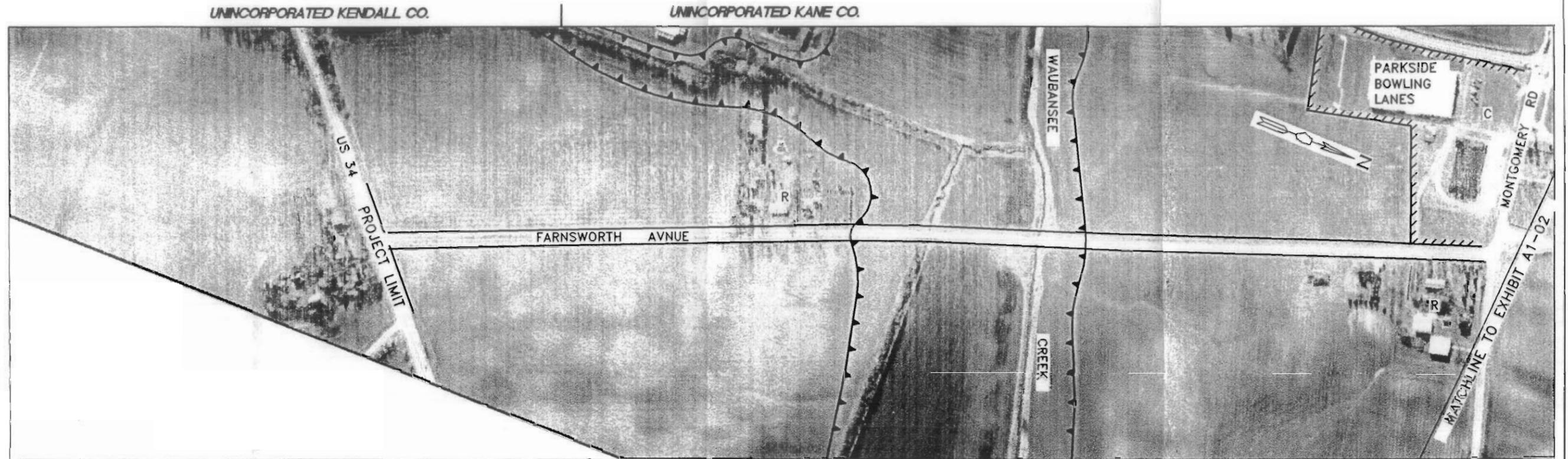
Name	Location	Incident No. IEPA No.
Brickets Car Wash	SW corner Indian Trail/ Farnsworth	L-1,902387 0894075162
Marathon Gas Station	SE corner Molitor/IL 25	L-2,902387 0894075162
Fermi National Accelerator Lab	Between Prairie Path and Wilson Rd., E. Side	L-3,890803 0890105010
Aldi Inc.	1200 N. Kirk Rd.	L-4922556 0890105034
Suncast Corp.	SW corner Hubbard/ Kirk Rd.	L-5,932658 0890105023
Settlers Hill Landfill	1031 Fabyan Pkwy.	L-6,930585 0890100009
Brady Ready Mix	South of the Chicago & NW RR, E. Side	L-7,911444 0894385163
Checker Gas Station	NW corner River Bluff and Dundee Ave.	L-8,860218 0894385070
Oldsmobile Torco Cadillac Dealer	NE corner Albert Road and IL 25	L-9,920655 0890250003
Amoco Gas Station	SW corner IL 72 and IL 25	L-1, 921071 0898065010
Al Piemonte Chevrolet Dealer	Between I-90 Tollway & IL 72, E. Side	L-10,920126 0898065009
Nightshift Transmission	SE corner Bolz Road/ IL 25	L-12,932242 0890205046
Meyer Material	800 Bolz Rd.	L-13,950764 0890205036

**Table I-3
CERCLIS SITES
Illinois Route 25/Dunham/Kirk/Farnsworth Ave.**

Settlers Hill Golf Course and Landfill	1031 Fabyan Parkway	C-2 Exhibit A1-11
Fermi Accelerator Lab	Between Prairie Path and Wilson Road, East Side	C-1 Exhibit A1-09
Illinois Tool Works	SE Corner of IL 25 and the Chicago NW Railroad	C-3 Exhibit A1-20

Table I-4
Historic Buildings and Sites
Illinois Route 25/Dunham Rd/Kirk Rd/Farnsworth Ave

Exhibit Symbol	Name	Location
H-1	Wayne Historic District	NW corner of Kirk Road & Army Trail Rd.
H-2	Dunham Castle	SE corner of Kirk Road & Army Trail Rd.
H-3	Elgin Historic Fire Station	SE corner of St. Charles Street & Bent St.
H-4	Elgin Historic District	SE corner St. Charles Street & Villa St.
H-5	Elgin Historic Structure	East of Villa Street south of Gifford St.
H-6	Gar Memorial Hall	NE Corner of Fulton and Villa Street
H-7	Historical Museum	360 Park Street
H-8	American Legion Hall	SW corner of Slade Street & IL 25
H-9	Milk-Pail Local Historical Plaque	North of Max-McGraw Farm
H-10	S.S. Dickenson Building	SE corner of IL 25 & IL 62



AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

- Waubensee Creek floodplain crossed on structures.

DESCRIPTION OF LAND USE CONDITIONS:

UNINCORPORATED KANE CO.

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

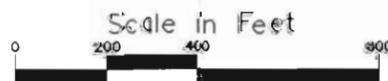
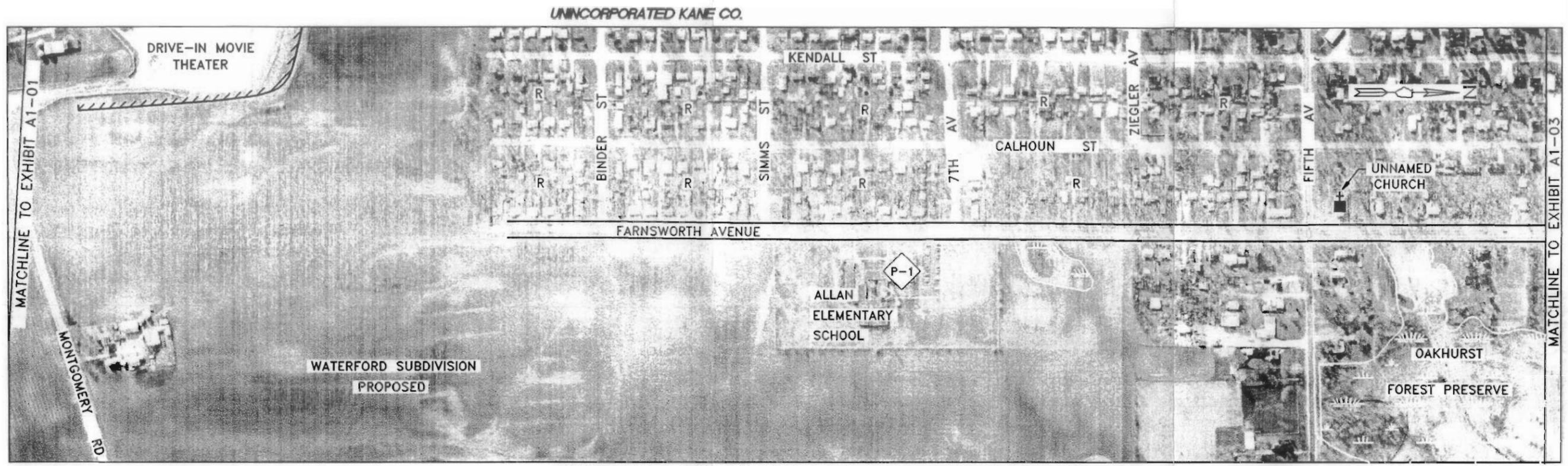


EXHIBIT A1-01



UNINCORPORATED KANE CO.

UNINCORPORATED KANE CO.

AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

- Oakhurst Forest Preserve northeast corner of Farnsworth Avenue and Fifth Avenue.

DESCRIPTION OF LAND USE CONDITIONS:

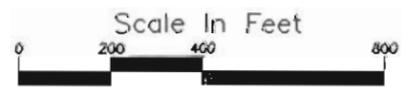
= Allan Elementary School

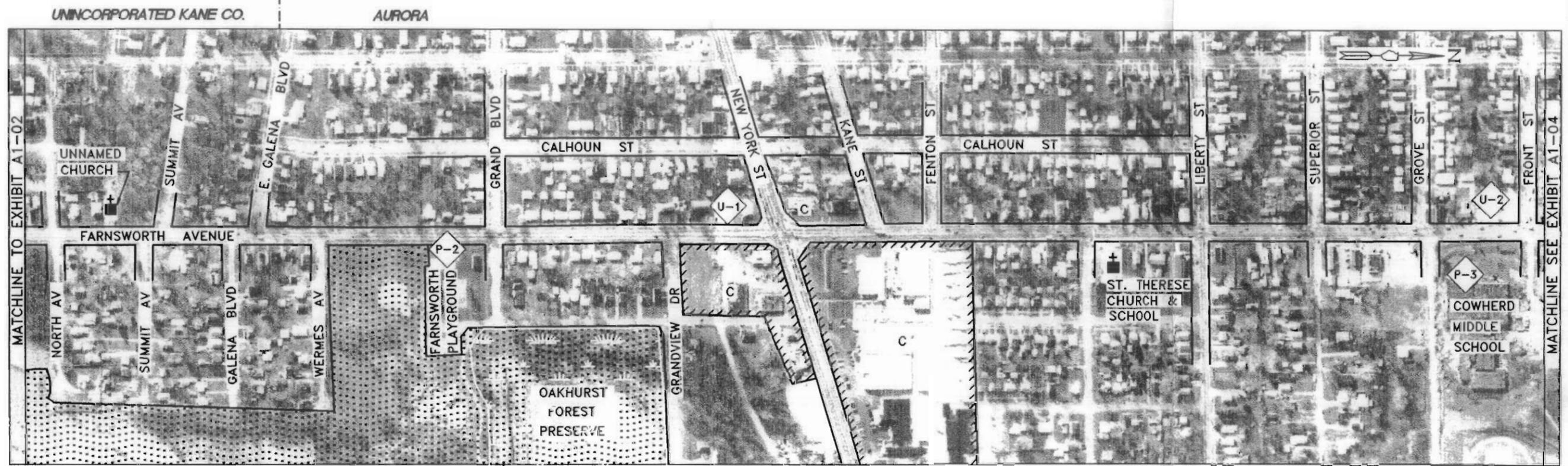
LEGEND			
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	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



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MATCHLINE TO EXHIBIT A1-02

MATCHLINE SEE EXHIBIT A1-04

UNINCORPORATED KANE CO.

AURORA

UNINCORPORATED KANE CO.

AURORA

AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

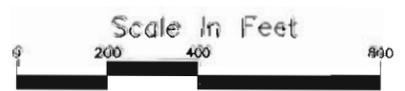
-  = Gas station (D & M Muffler)
-  = Marathon 76 Gas station

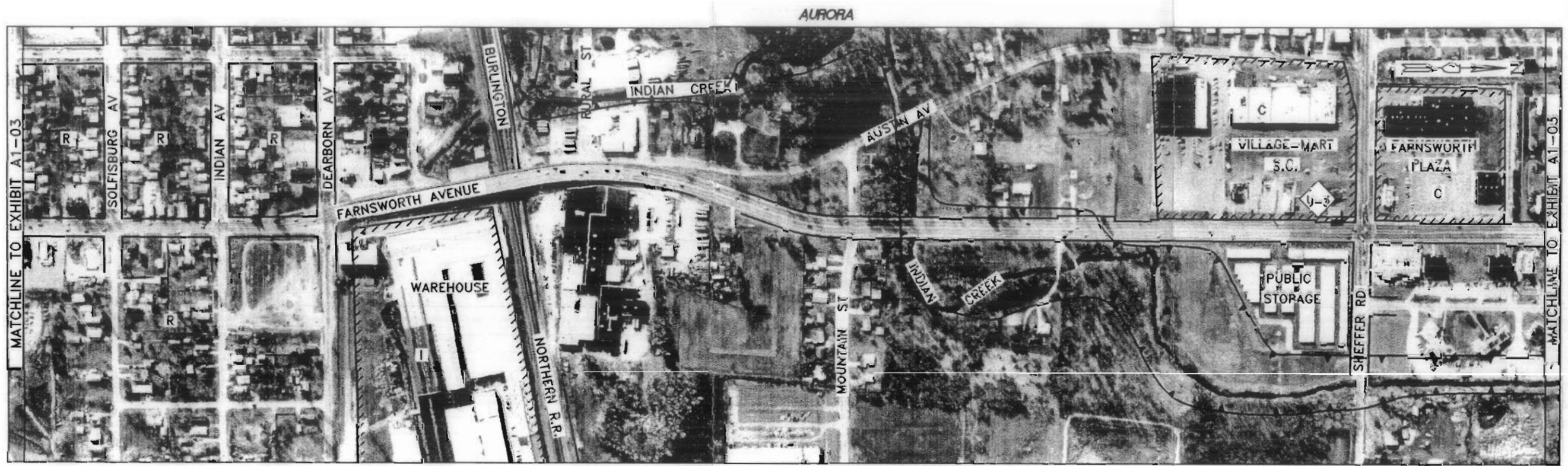
DESCRIPTION OF LAND USE CONDITIONS:

-  = Farnsworth Playground
-  = Cowherd Middle School

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		
	= RESIDENTIAL		= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE





AURORA

AURORA

AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

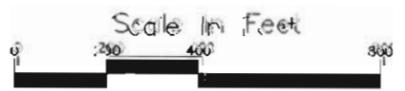
 = Speedway Gas Station

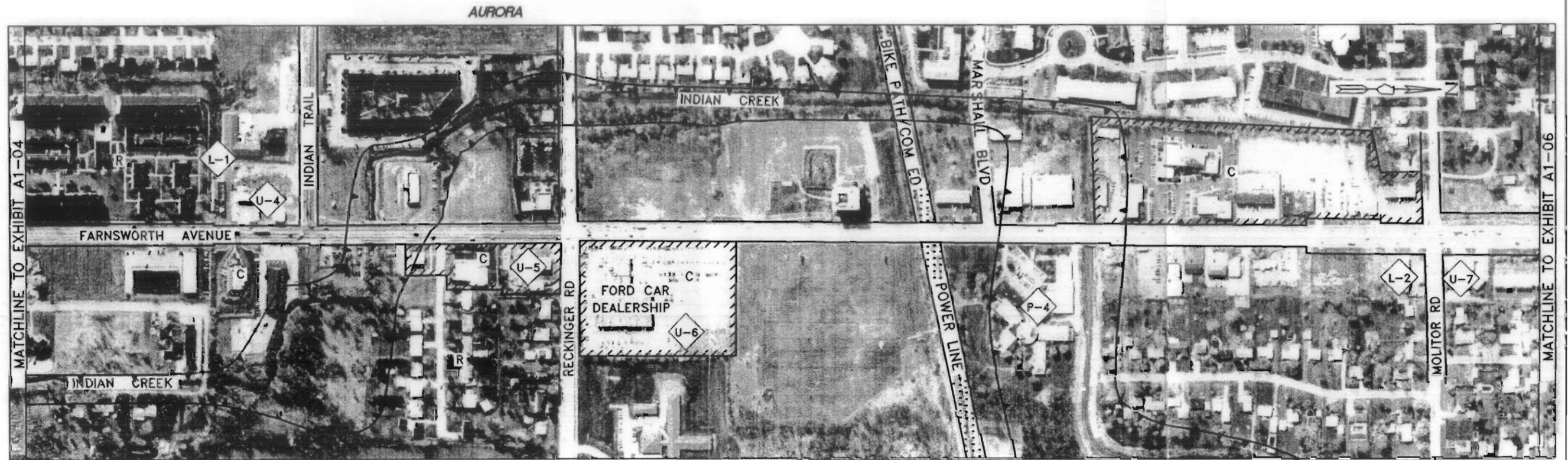
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	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLUS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



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AURORA

AURORA

AERIAL PHOTO DATE: 5-01-92 UNINCORPORATED KANE CO.

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

- = Brickets Car Wash
- = Marathon Gas Station
- = Clark Gas Station
- = Phillips 66 Gas Station
- = Fox Valley Ford Dealer
- = Shell Gas Station

- = Fornsworth Manor (nursing home)

LEGEND

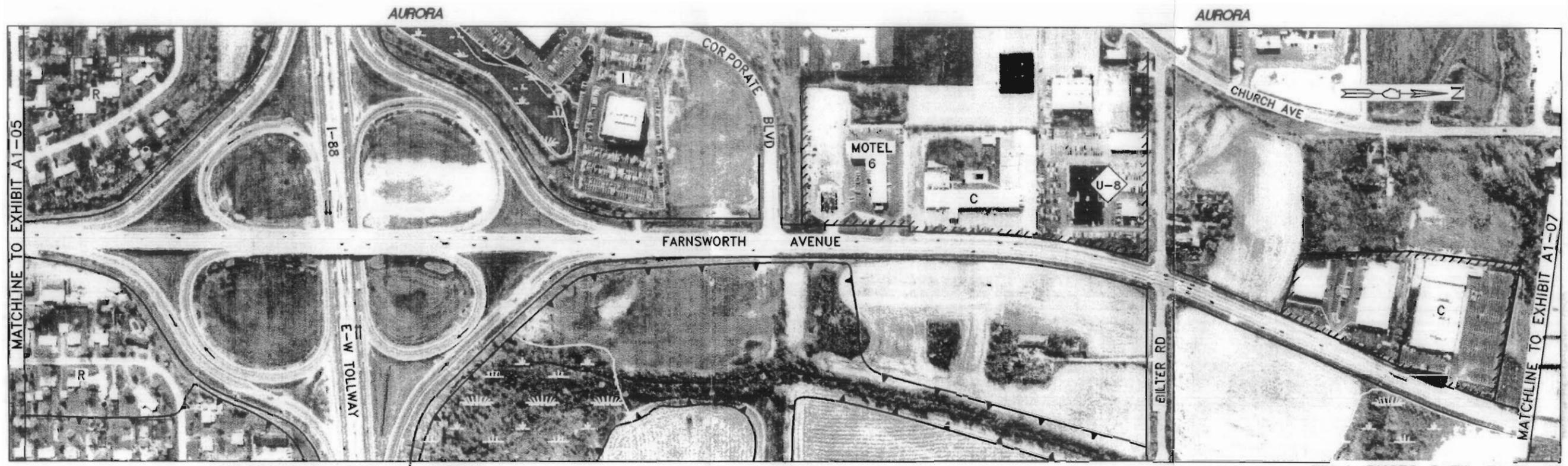
= WETLANDS	= L.U.S.T. SITE
= 100 YEAR FLOODPLAIN	= U.S.T. SITE
= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES	= CERCLIS OR HAZARDOUS MATERIAL SITE
= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE	= HISTORIC SITE
= CEMETERY	= PUBLIC FACILITY
= RELIGIOUS INSTITUTION	= CITY/VILLAGE BOUNDARY
= RESIDENTIAL	

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the Illinois Department of Transportation





UNINCORPORATED KANE CO. | AURORA

AURORA

AERIAL PHOTO DATE: 5-01-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

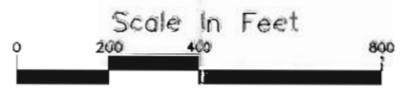
U-8 = Lincoln / Mercury Dealership

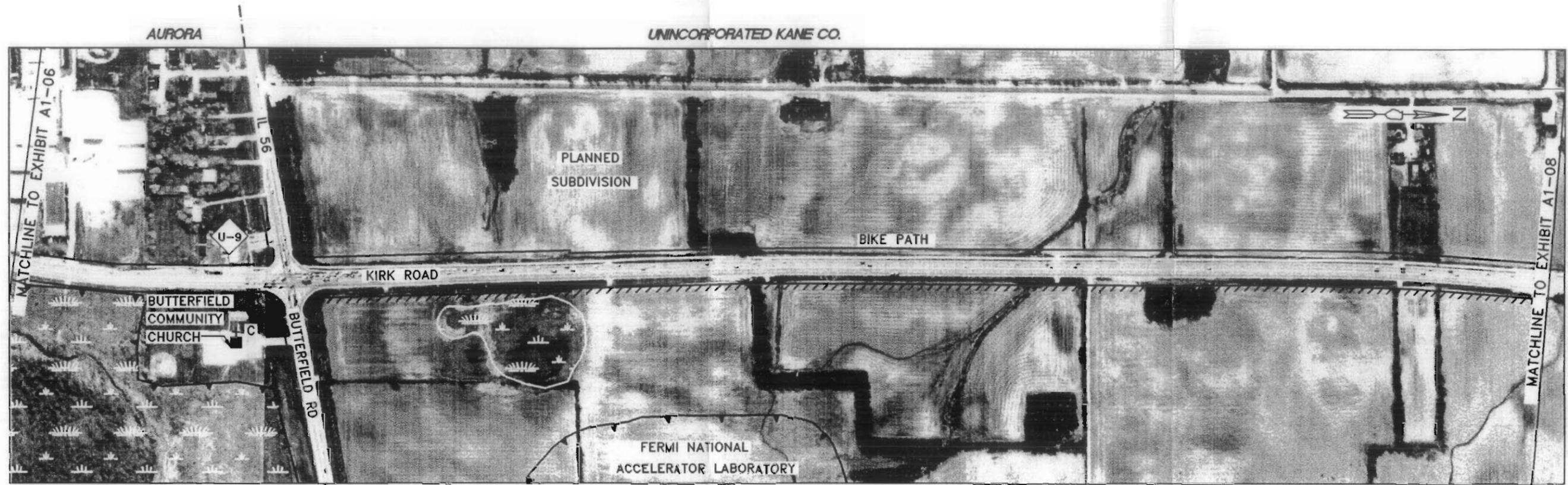
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



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AERIAL PHOTO DATE: 5-01-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

U-9 = Amoco Gas Station

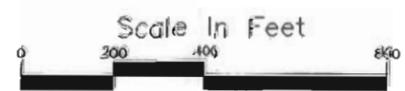
DESCRIPTION OF LAND USE CONDITIONS:

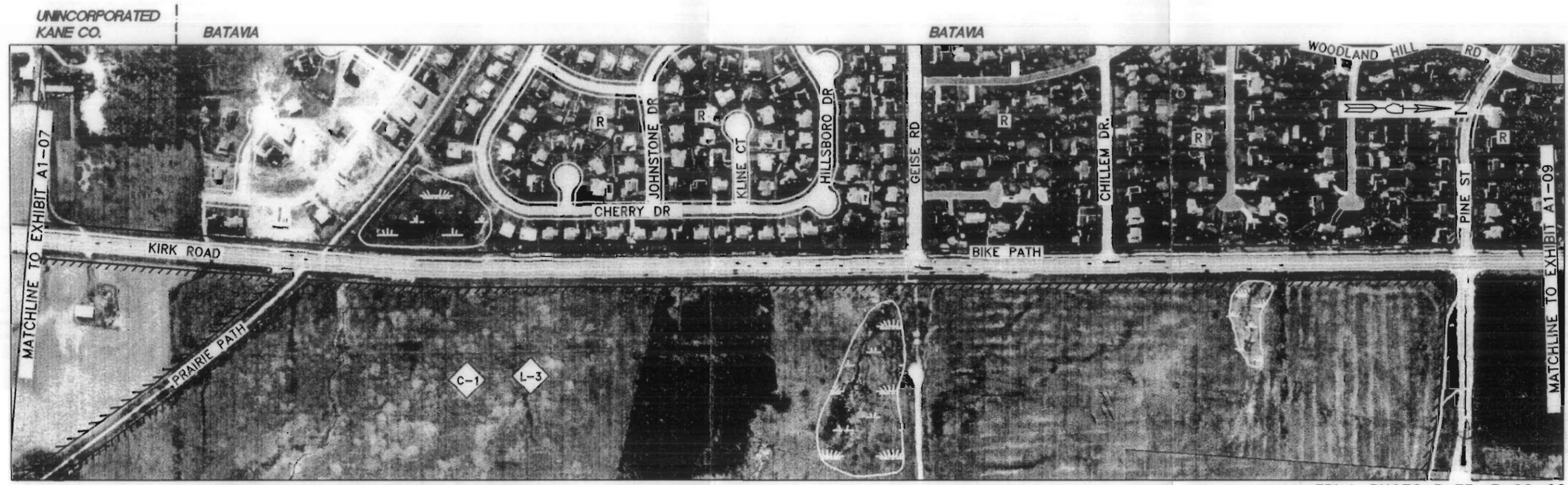
- Subdivision being constructed on west side of Kirk Road.

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





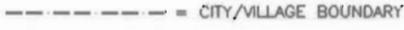
FERMI NATIONAL ACCELERATOR LABORATORY

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

-  = Fermi National Accelerator Laboratory
-  = Fermi National Accelerator Laboratory

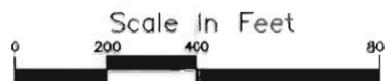
DESCRIPTION OF LAND USE CONDITIONS:

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLUS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

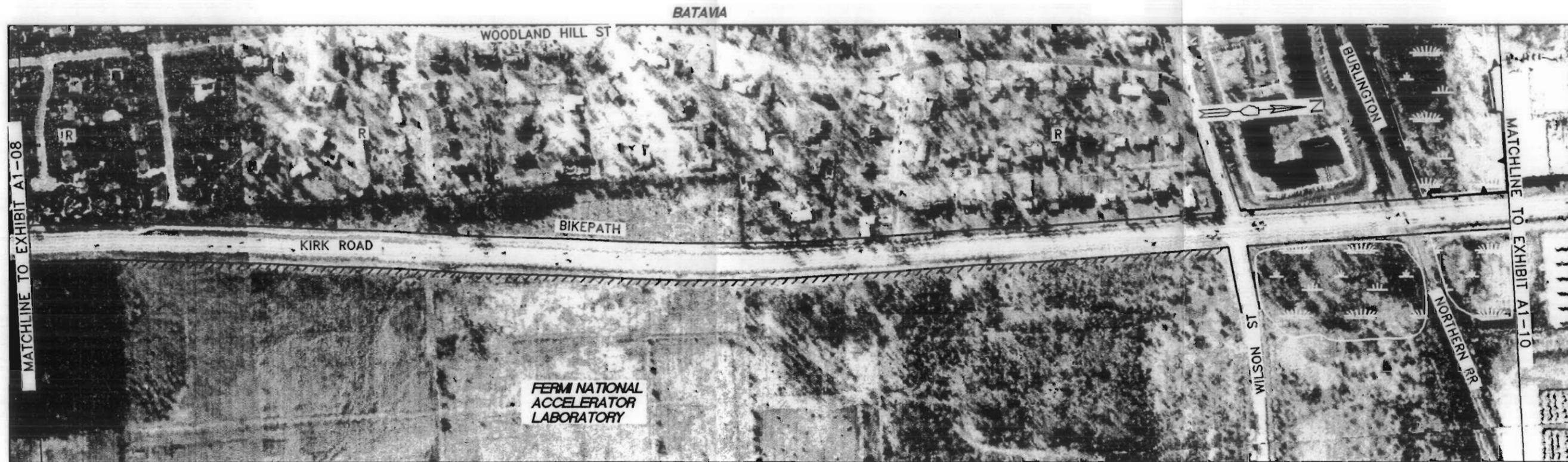
IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

 Illinois Department of Transportation



SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY



FERMI NATIONAL ACCELERATOR LABORATORY

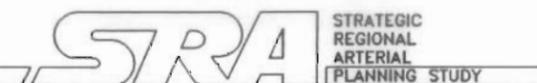
AERIAL PHOTO DATE: 3-02-92

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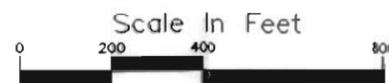
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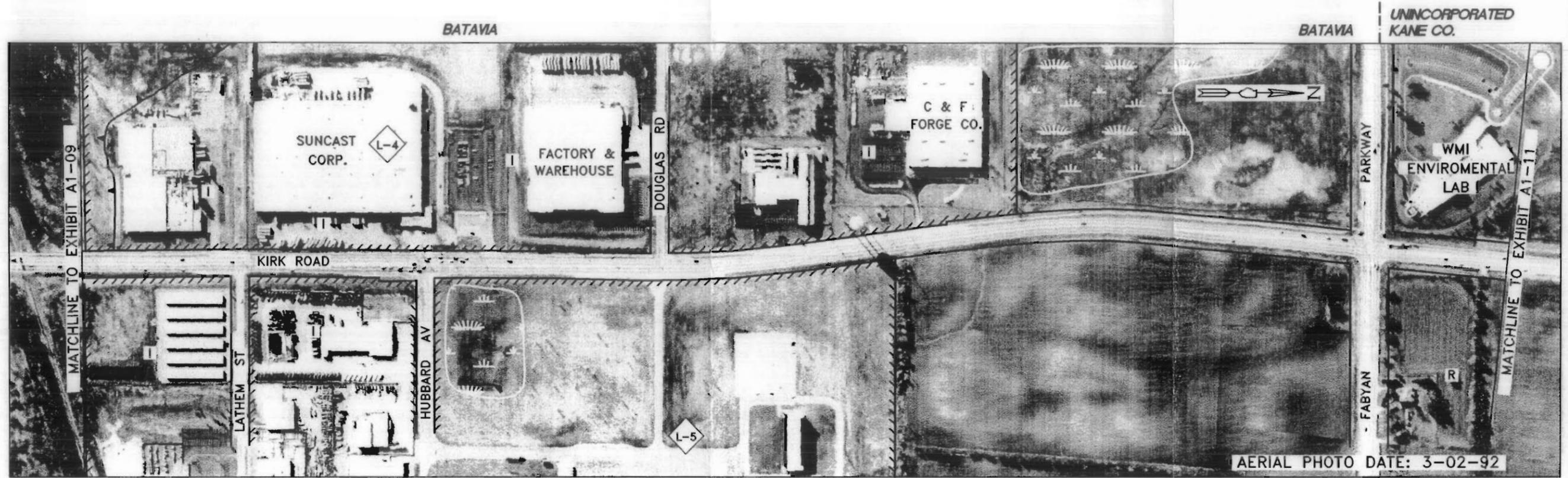
LEGEND			
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	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

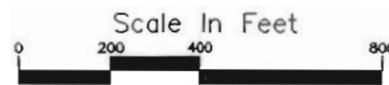
-  = Suncast Corp.
-  = Aldi Inc. (1200 N. Kirk Road).

DESCRIPTION OF LAND USE CONDITIONS:

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





UNINCORPORATED KANE CO.

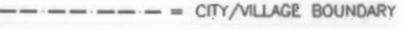
AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

-  = Settlers Hill Landfill
-  = Settlers Hill Landfill
-  = Motor Equipment and Truck Repair.

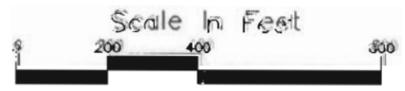
-  = Kane CO. Stadium

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the Illinois Department of Transportation





UNINCORPORATED KANE CO.

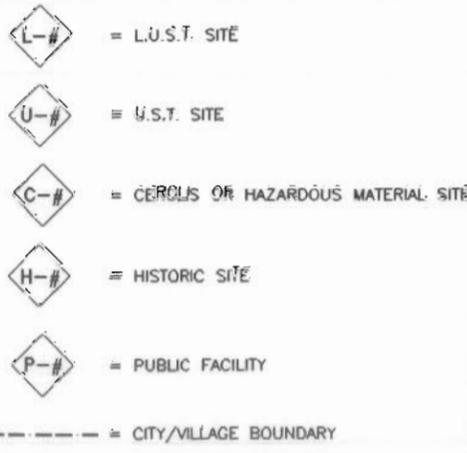
AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

-  = Shell Gas Station
-  = Amoco Gas Station

DESCRIPTION OF LAND USE CONDITIONS:

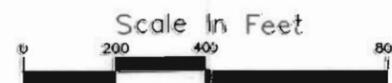
-  = Kindercare Day Care

LEGEND	
	= WETLANDS
	= 100 YEAR FLOODPLAIN
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE
	= CEMETERY
	= RELIGIOUS INSTITUTION
	= RESIDENTIAL
	= L.U.S.T. SITE = U.S.T. SITE = CERCLIS OR HAZARDOUS MATERIAL SITE = HISTORIC SITE = PUBLIC FACILITY = CITY/VILLAGE BOUNDARY

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





UNINCORPORATED KANE CORP.

ST. CHARLES

AERIAL DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

U-13 = Scotts Auto Repair

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

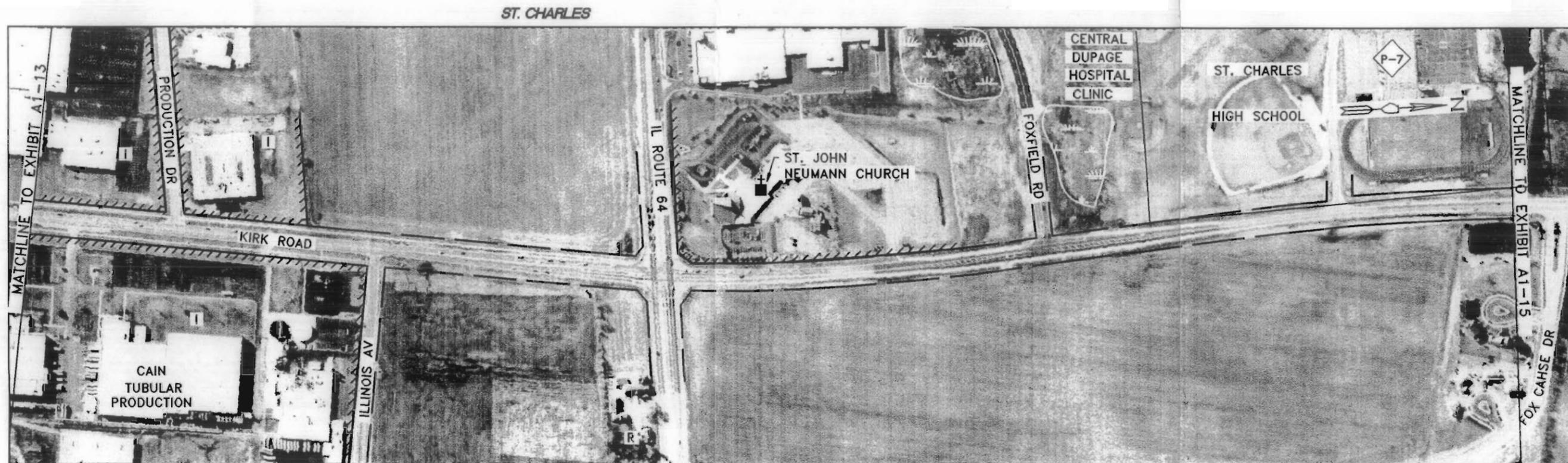
IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation





AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

 = St. Charles High School

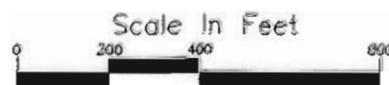
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

 Illinois Department of Transportation





AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

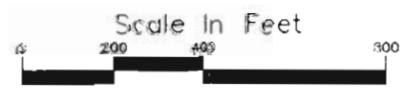
DESCRIPTION OF LAND USE CONDITIONS:

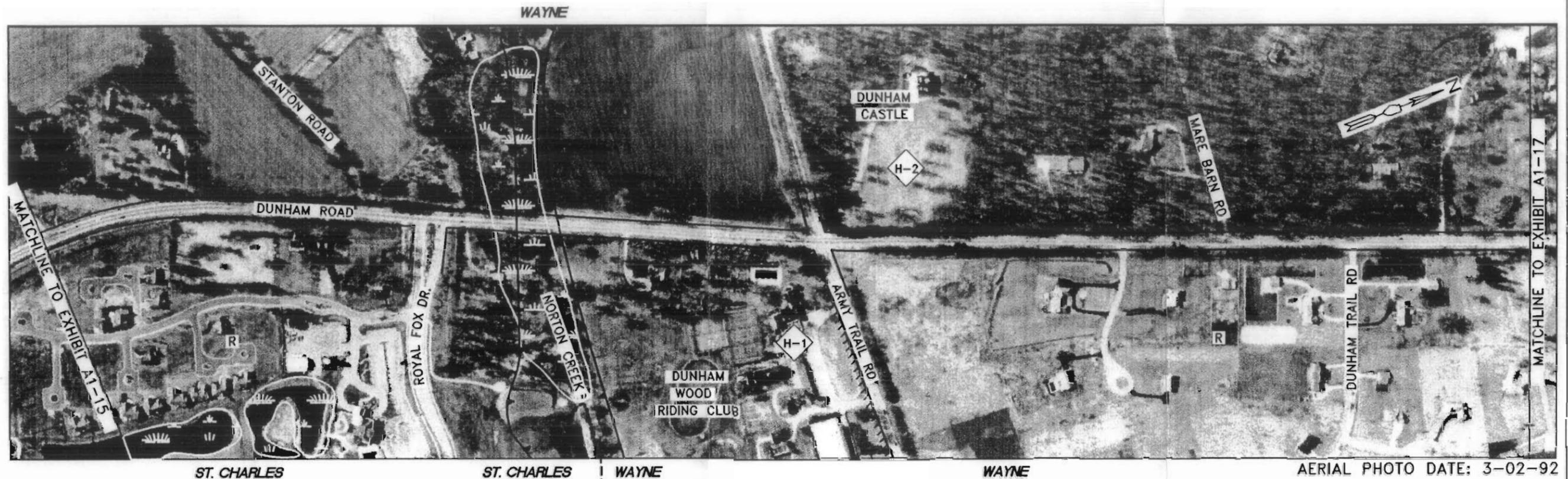
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the Illinois Department of Transportation

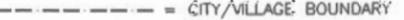




DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

-  = Wayne Historic District
-  = Dunham Castle

DESCRIPTION OF LAND USE CONDITIONS:

LEGEND			
	= WETLANDS		= L-U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL. 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

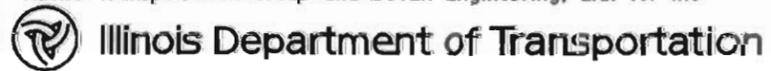
DESCRIPTION OF LAND USE CONDITIONS:

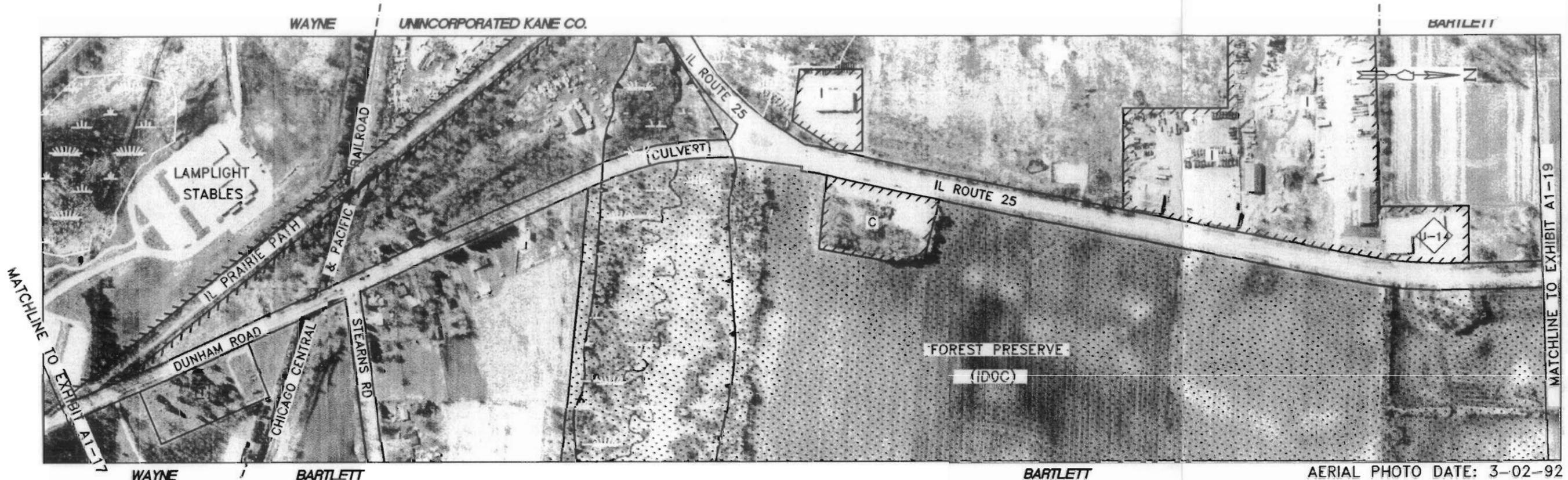
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CEROUS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and IBOYER Engineering, Ltd. for the





DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

U-14 = Grant Truck & Trailer Repair.

DESCRIPTION OF LAND USE CONDITIONS:

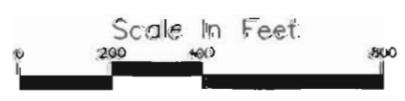
[T] = Little Woods Cemetery

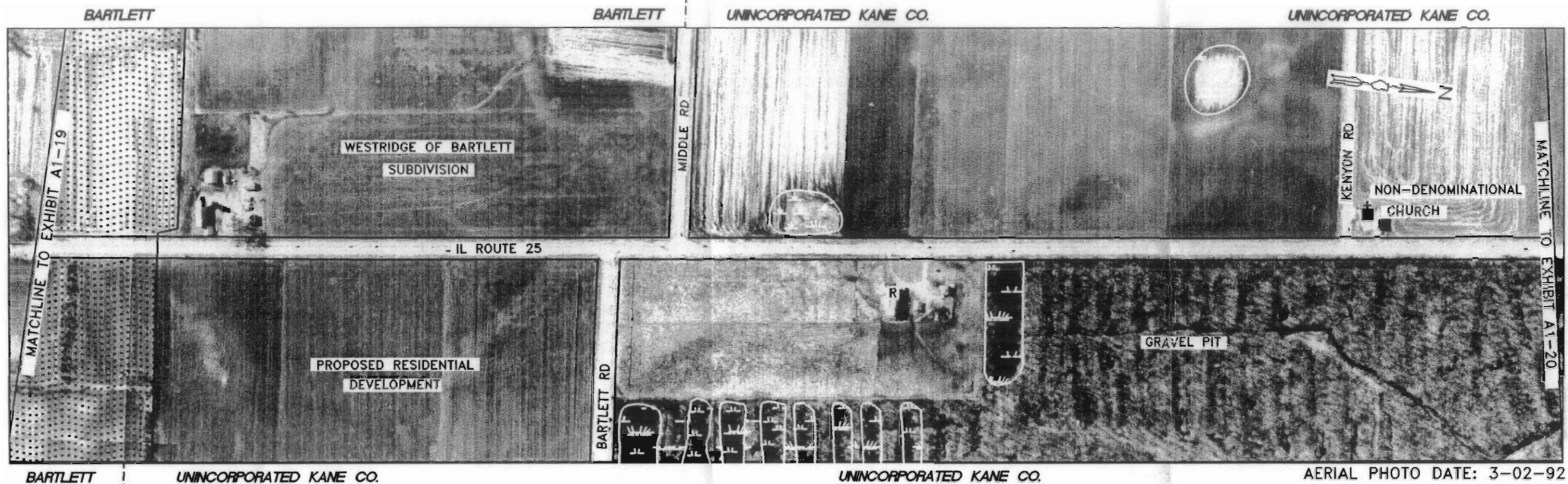
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

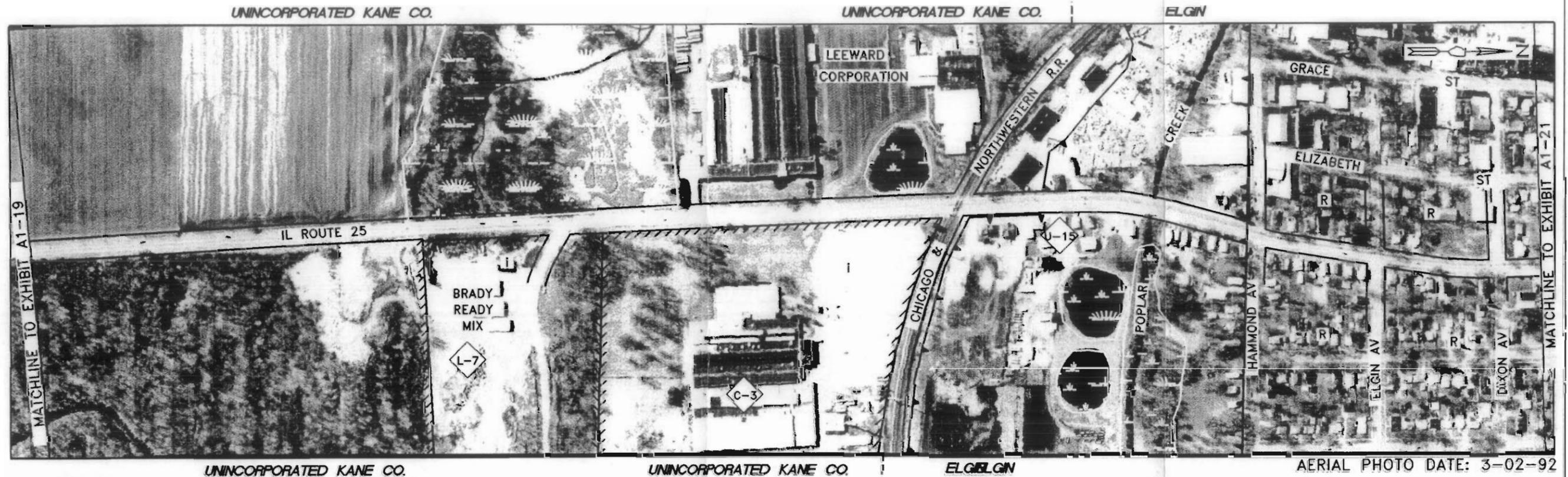
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERGLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL. 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

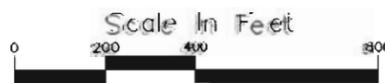
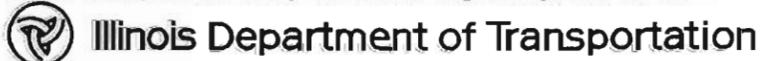
DESCRIPTION OF LAND USE CONDITIONS:

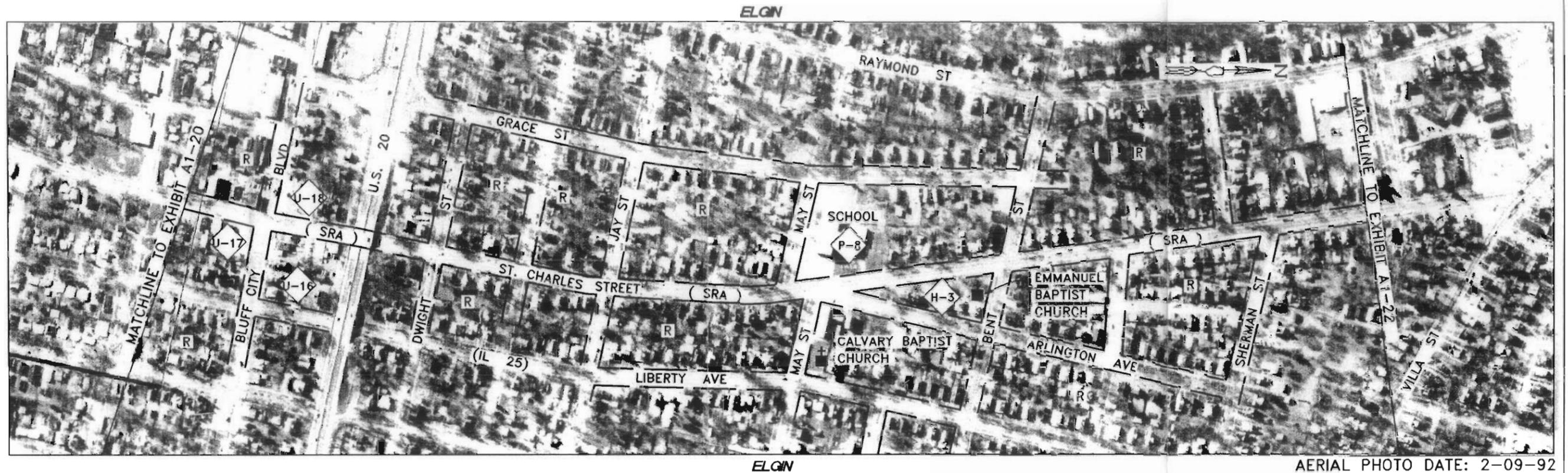
-  = Illinois Tool Works
-  = Brady Ready Mix
-  = Graf and Sons Auto Repair

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

- U-16 = Carmellos Auto Repair
- U-17 = Auto Specialist
- U-18 = Amoco Gas Station

DESCRIPTION OF LAND USE CONDITIONS:

- P-8 = Garfield School
- H-3 = Elgin Fire Barn #5
- Emmanuel Baptist Church is located on the south east corner of Bent Street and St. Charles Street.

LEGEND

	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

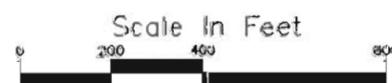


EXHIBIT A1-21

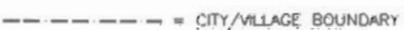


AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

-  = Elgin Historic District
-  = Historic Structure

LEGEND			
	= WETLANDS		= I.U.S.T. SITE
	= 100-YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLUS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

 Illinois Department of Transportation

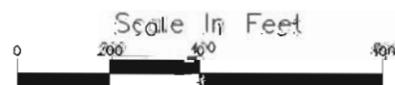
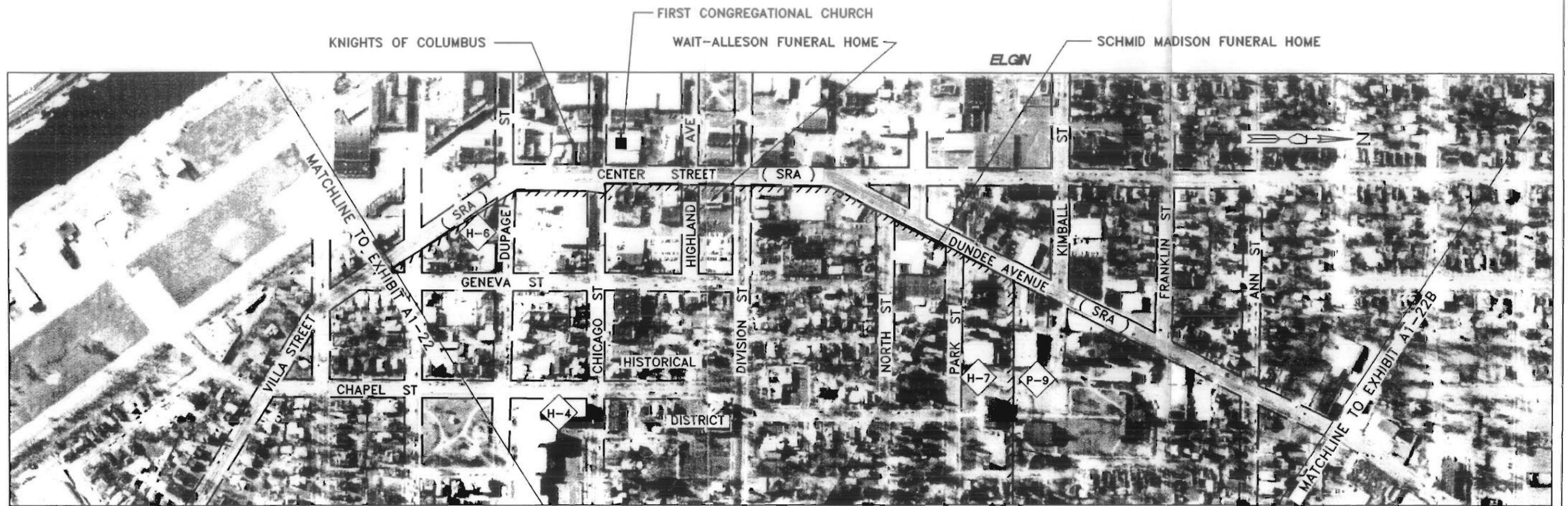


EXHIBIT A1-22



ELGN

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

-  = Elgin Historic District adjacent to Villa Street, Center Street and Dundee Street.
-  = Historic Structure
-  = Historic Structure
-  = Elgin Academy

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

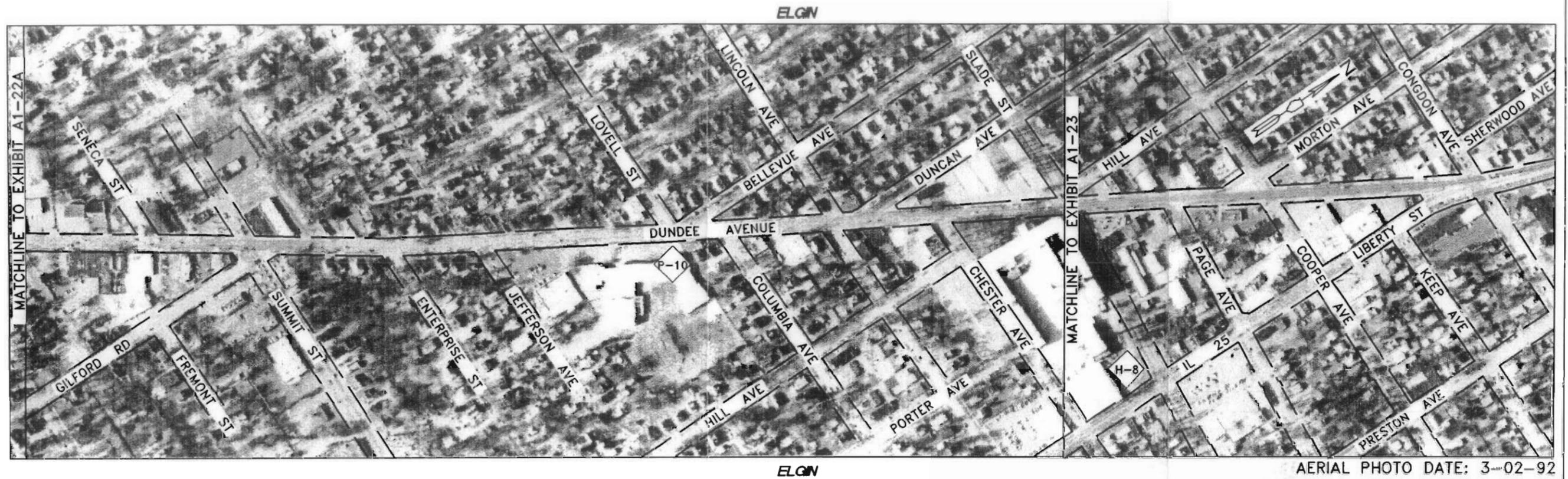
IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXHIBIT A1-22A



DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

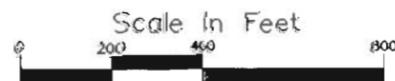
-  = Larsen Middle School
-  = American Legion Hall

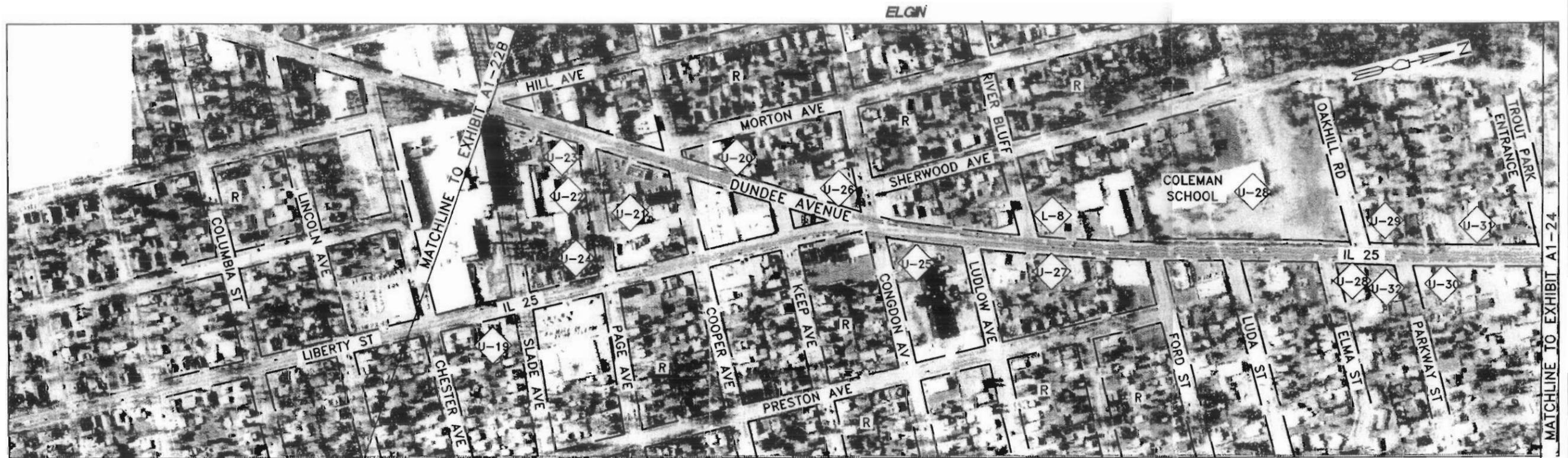
LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL, OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





ELGIN

AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

- | | |
|---|----------------------------------|
| U-19 = Old Gas Station (Servicio Sanchez) | U-27 = Amoco Gas Station |
| U-20 = Meineke Muffler | U-28 = Midas Muffler and Brake |
| U-21 = Alexis Auto Repair | U-29 = Fleeting Motors |
| U-22 = Midwest Transmission | U-30 = Shell Gas Station |
| U-23 = Auto Repair | U-31 = Old Gas Station |
| U-24 = Old Gas Station (900) | U-32 = Car Dealer |
| U-25 = Old Gas Station (1103) | L-8 = Checker Gas Station (1156) |
| U-26 = Emil's Garage | |

DESCRIPTION OF LAND USE CONDITIONS:

- U-28 = Coleman School

LEGEND

	= WETLANDS	L-#	= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN	U-#	= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES	C-#	= CERCLUS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE	H-#	= HISTORIC SITE
	= CEMETERY	P-#	= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		
R	= RESIDENTIAL		
			- - - - - CITY/VILLAGE BOUNDARY

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

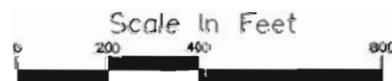
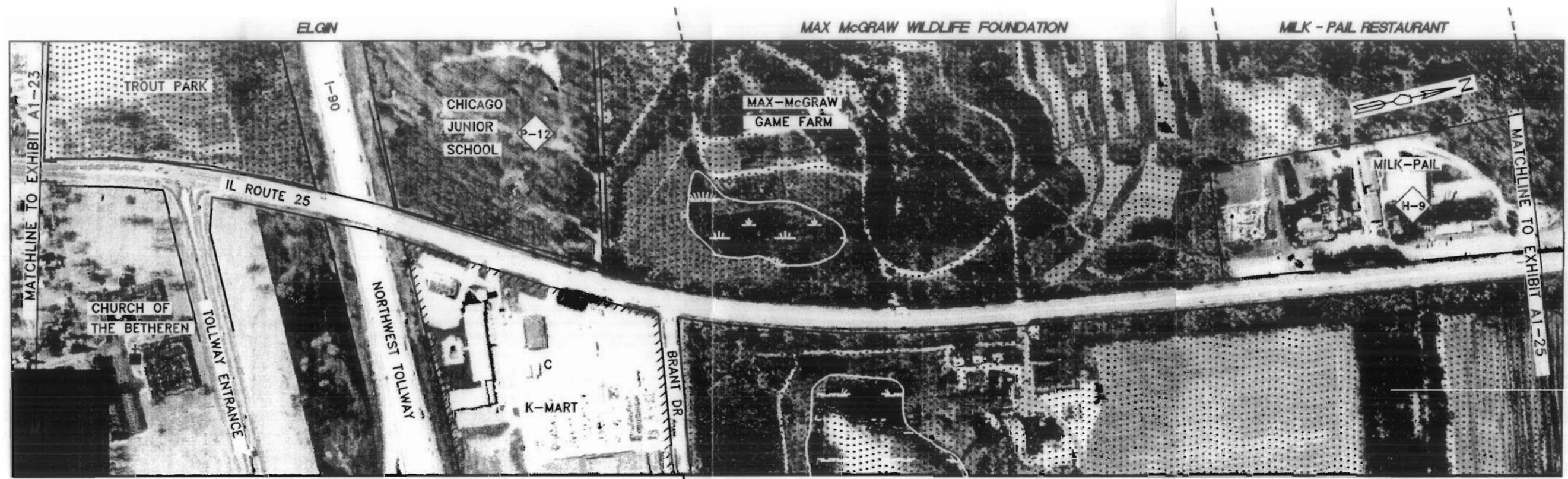


EXHIBIT A1-23



ELGIN

MAX McGRAW WILDLIFE FOUNDATION

MILK - PAIL RESTAURANT

ELGIN

MAX McGRAW WILDLIFE FOUNDATION

AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

- Wetlands and forest.

DESCRIPTION OF LAND USE CONDITIONS:

- = Local Historical plaque Milk Pail Restaurant.
- = Chicago Junior School.

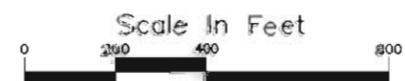
LEGEND

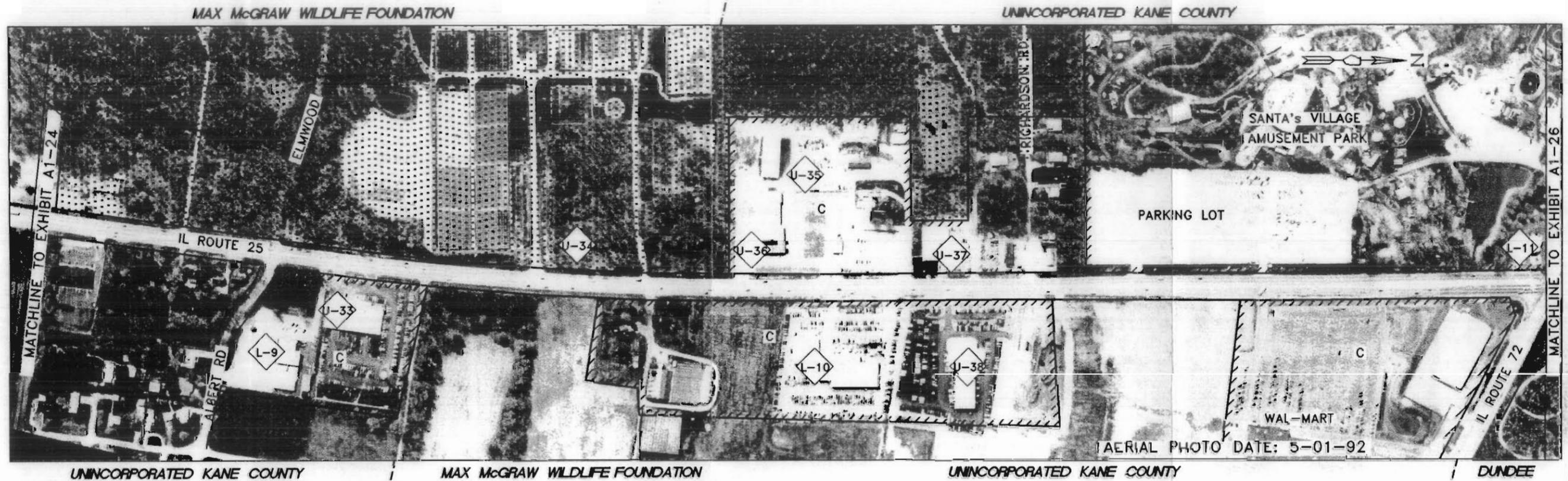
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

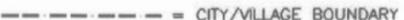




DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

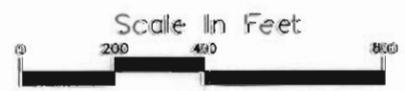
-  = Oldsmobile Torco Cadillac Dealer
-  = Al Piemonte Chevrolet Dealer
-  = Amoco Gas Station
-  = Springhill Ford
-  = Collingbourn County Motor
-  = Old Volkswagon Dealership
-  = Mustang Restoration
-  = American Auto Center
-  = Saturn of Dundee

DESCRIPTION OF LAND USE CONDITIONS:

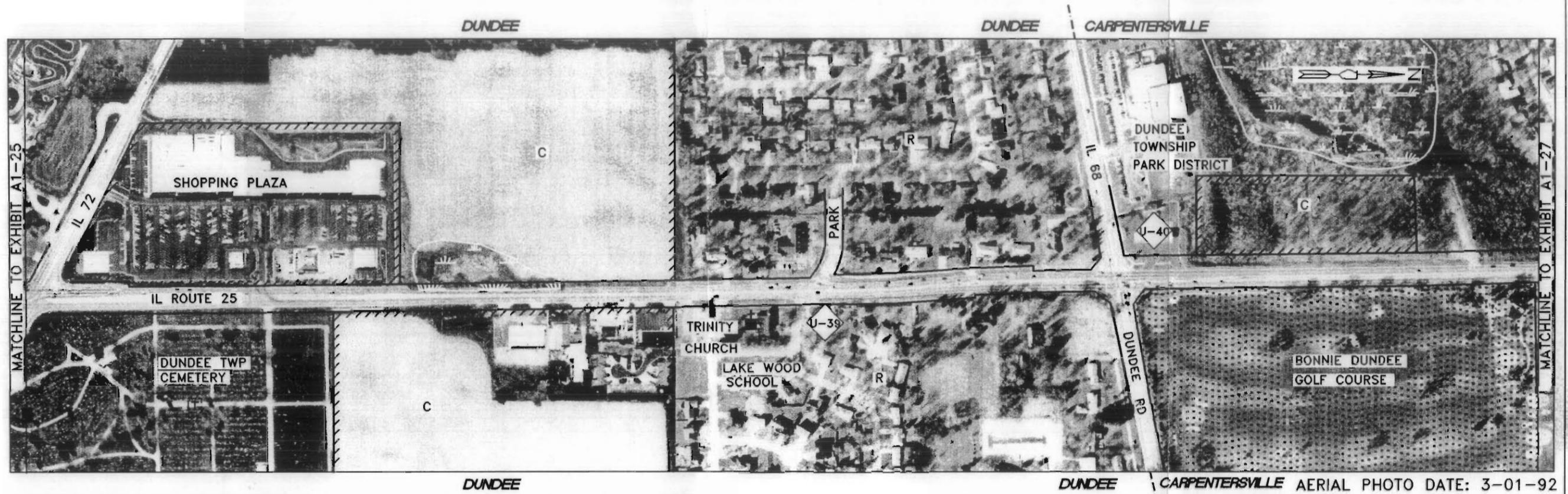
LEGEND			
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	= 100' YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLUS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the Illinois Department of Transportation



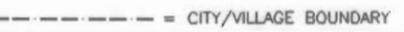
SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY



DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

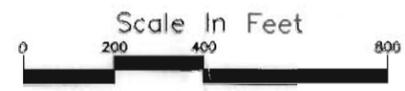
-  = Mobil Gas Station
-  = Amoco Gas Station

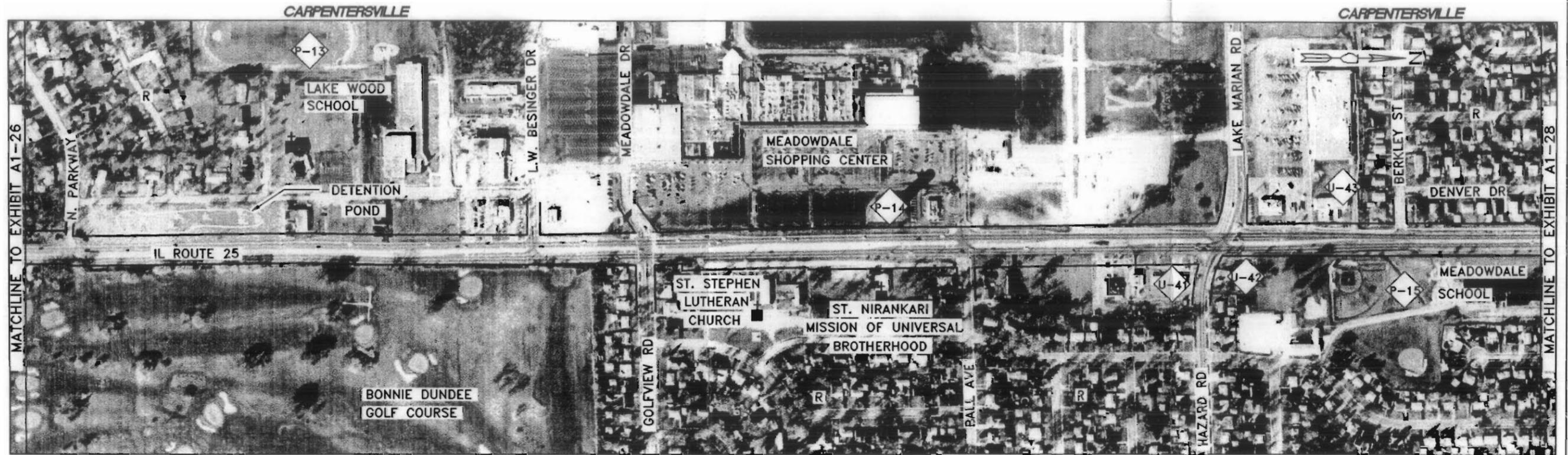
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	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the
 Illinois Department of Transportation





AERIAL PHOTO DATE: 3-01-92

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

- U-41 = Shell Gas Station
- U-42 = Old Gas Station
- U-43 = Goodyear Tire Store

DESCRIPTION OF LAND USE CONDITIONS:

- P-13 = Lakewood School
- P-14 = Meadowdale Water Tower
- P-15 = Meadowdale School

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
	= PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE		= HISTORIC SITE
	= CEMETERY		= PUBLIC FACILITY
	= RELIGIOUS INSTITUTION		= CITY/VILLAGE BOUNDARY
	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation



SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

CARPENTERSVILLE



MATCHLINE TO EXHIBIT A1-27

MATCHLINE TO EXHIBIT A1-29

CARPENTERSVILLE

AERIAL PHOTO DATE: 3-01-92

BARRINGTON HILLS

DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

-  = Shell Gas Station
-  = Old Gas Station
-  = Nightshift Transmission

DESCRIPTION OF LAND USE CONDITIONS:

-  = St Monica's School and Church.

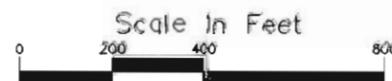
LEGEND

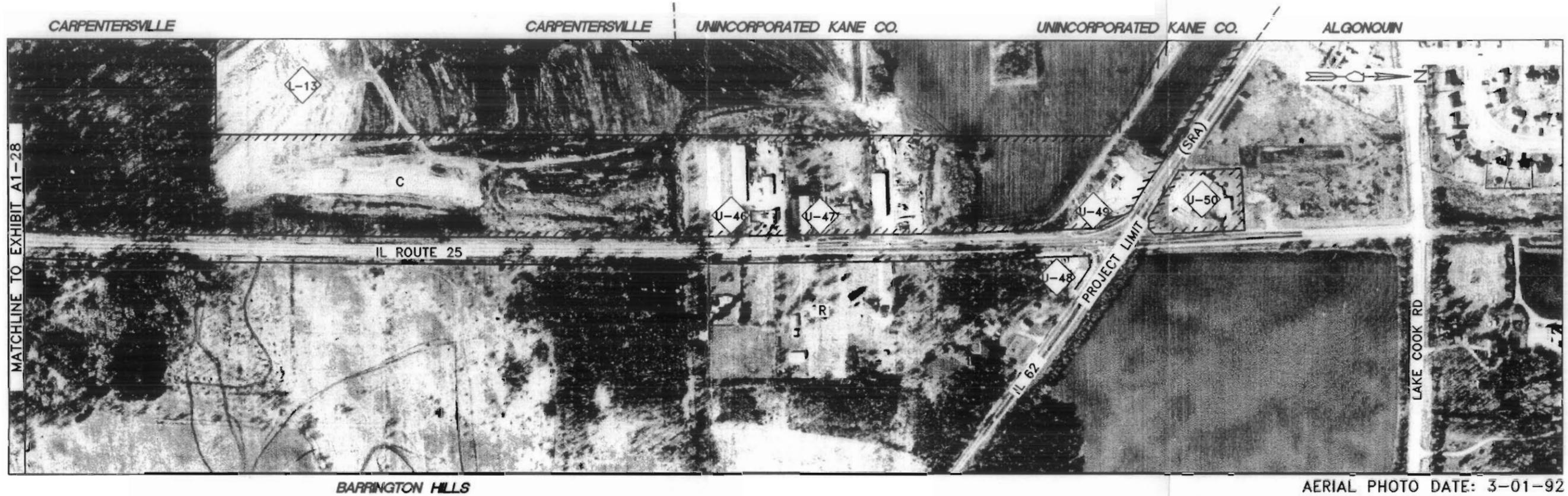
- | | |
|---|--|
|  = WETLANDS |  = L.U.S.T. SITE |
|  = 100 YEAR FLOODPLAIN |  = U.S.T. SITE |
|  = OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES |  = CERCLUS OR HAZARDOUS MATERIAL SITE |
|  = PARKS, FOREST PRESERVES, OR PUBLIC OPEN SPACE |  = HISTORIC SITE |
|  = CEMETERY |  = PUBLIC FACILITY |
|  = RELIGIOUS INSTITUTION |  = CITY/VILLAGE BOUNDARY |
|  = RESIDENTIAL | |

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF ENVIRONMENTAL CONDITIONS:

DESCRIPTION OF LAND USE CONDITIONS:

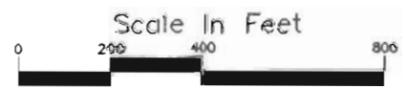
-  = Meyer Material
-  = International Automotive Specialist
-  = Algonquin Auto House
-  = Old Gas Station
-  = Old Gas Station
-  = Phillips 66 Gas Station

LEGEND			
	= WETLANDS		= L.U.S.T. SITE
	= 100 YEAR FLOODPLAIN		= U.S.T. SITE
	= OFFICE, INDUSTRIAL OR COMMERCIAL PROPERTIES		= CERCLIS OR HAZARDOUS MATERIAL SITE
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	= RESIDENTIAL		

IL 25/DUNHAM RD./KIRK RD./FARNSWORTH AVE. - ENVIRONMENTAL CONDITIONS AND LAND USE



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 Illinois Department of Transportation



EXISTING ROADWAY CONDITIONS

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



STRATEGIC
REGIONAL
ARTERIAL
PLANNING STUDY

Introduction

As part of the planning process, the SRA study includes a detailed evaluation of the existing roadway conditions. In this chapter, physical characteristics of each segment of the route are discussed including cross-sections, roadway structures, and other geometric concerns. In addition, aspects of traffic flow and operations such as average daily traffic, accident rates, and parking are examined. Finally, public transit issues including bus and rail service operating along and intersecting the corridor are evaluated.

This information is summarized in several tables at the end of this chapter. Table II-1 lists structures along the route. Tables II-2 and II-3 provide accident rates at intersections and on route segments, respectively. Table II-4 provides sources for the data discussed here.

Section 1 - Farnsworth Avenue - US Route 34 to New York Street (2.38 miles)

Exhibit B1-01 to Exhibit B1-03

Section 1 of Farnsworth Avenue begins at US Route 34 (Ogden Avenue) in northern Kendall County, and continues north to New York Street in Kane County. This section passes through unincorporated Kendall County and the City of Aurora.

Physical Characteristics

This section consists of two 10-foot lanes with adjacent 6-foot aggregate shoulders from US Route 34 to Montgomery Road, at this point Farnsworth Avenue ends. Farnsworth Avenue begins again just south of Binder Street and continues north to New York Street. In this area Farnsworth Avenue consists of two 12-foot lanes with 6-foot shoulder on both sides. The existing right-of-way for this section is 66 feet.

There are two structures in this section of Farnsworth Avenue. These are the structures over the south and north branch of Waubonsie Creek situated between US Route 34 and Montgomery Road. Structure number 046-3138 over an unnamed creek and Structure number 046-3137 over the Wabanssee Creek has two lanes in each direction.

Traffic Control, Operations, and Safety

The existing Average Daily Traffic (ADT) for this section is not available from the Kane County Traffic Map. The speed limit for this section is 30 mph. There are all way stop signs at US Route 34, Montgomery Road, Fifth Avenue and Grand Boulevard.

Intersection of Farnsworth Avenue and New York Street is a major intersection within this section. The lane configuration for the east & west legs consists of a left turn, two through lanes with a

EXISTING ROADWAY CONDITIONS - cont'd

shared right turn lane. The south leg consists of a shared through right lane and a left turn lane. The north leg consists of a left turn lane, a through lane and a right turn lane.

Public Transportation

The Pace # 527 (Moecherville) operates between Grand Boulevard and Binder Street along Farnsworth Avenue and Pace #530 (Fox Valley Center) intersects Farnsworth Avenue at New York Street.

Section 2 - New York Street to Illinois Route 56 (4.05 miles)

Exhibit B1-03 to Exhibit B1-07

Section 2 of Farnsworth Avenue extends from New York Avenue to Illinois Route 56. This section passes through the City of Aurora and unincorporated Kane County. The Farnsworth Avenue intersects two other SRA routes, US Route 34 and Illinois Route 56 in this section.

Physical Characteristics

This section of Farnsworth Avenue consists of four 10 to 12-foot lanes with varying median and adjacent curb and gutter. Painted median exists from New York Street to Dearborn Avenue. From Dearborn Avenue to Mountain Street a 4-foot raised median exists. There is a 14-foot flush median between Mountain Street and Molitor Road, and a 4-foot painted/raised median exists from Molitor Road to Illinois Route 56. Burlington Northern railroad runs under Farnsworth Avenue north of Dearborn Avenue.

The existing R.O.W. from New York Street to Dearborn Street varies between 63-feet and 66-feet, and varies from 66-feet to 100-feet from Dearborn Street to Illinois Route 56, with the exception at East-West Tollway Overpass.

There are five structures in this section of Farnsworth Avenue. Structure number 045-6052 over Burlington Northern Railroad has a clear width of 48-feet. Structure number 045-6012, 045-6011 and 045-6010 are located over the Indian Creek at various locations along Farnsworth Avenue. Structure number 045-9944 carries Farnsworth Avenue over I-88 (East-West Tollway) and has a clear width of 84-feet.

Traffic Control, Operations, and Safety

The ADT for this section of Farnsworth Avenue from New York Street to I-88 (East-West Tollway) is not available. According to the 1992 Traffic Map for Kane County, the ADT for Farnsworth Avenue between I-88 and Illinois Route 56 is 23,000 vpd (vehicles per day). Traffic signals are

EXISTING ROADWAY CONDITIONS - cont'd

located at New York Street, Liberty Street, Front Street, Sheffer Road, Indian Trail, Reckinger Street, and Molitor Road. There is a four-way stop sign at Bilter Road and Farnsworth Avenue. The speed limit is 30 to 35 mph from New York Street to the East-West Tollway, and is 40 mph from I-88 to Illinois Route 56. Parking is not permitted along this section.

The intersection of Farnsworth Avenue & Illinois Route 56 is a major intersection within this section. The lane configuration for the east-west legs consists of a left turn lane, two through lanes with a shared right turn lane. The north-south legs consist of a left turn lane, two through lanes and a right turn lane.

Public Transportation

Three Pace Bus routes operate along this section. Pace bus #525 operates from Dearborn Street South to Fenton Street, Pace bus #533 operates from Bilter Road south to Sheffer Road and Pace bus #521 operates from Sheffer Road south to Mountain Street.

Section 3 - Kirk Road - Illinois Route 56 to Illinois Route 64 (10.87 miles)

Exhibit B1-07 to Exhibit B1-14

Section 3 of the Illinois 25 corridor (Kirk Road) extends from Illinois Route 56 to Illinois Route 64. This section passes through unincorporated Kane County, Batavia, Fermi National Laboratory, Geneva, and St. Charles. Kirk Road intersects three SRA routes: Fabyan Parkway, Illinois Route 38 (Roosevelt Road), and Illinois Route 64.

Physical Characteristics

This section of Kirk Road consists of four 12-foot lanes and 10-foot shoulders with painted median and curb and gutter from Illinois Route 56 to Illinois Route 38. The existing right-of-way for this section varies from 100-feet to 155-feet with the exception of the Chicago & Northwest Railroad where the R.O.W. varies from 100-feet to 210-feet. A prairie path at grade crossing is located 1.1 miles north of Illinois Route 56.

The only structure (structure number 045-3096) located within this section of Kirk Road is the bridge over the Chicago & Northwestern railroad located just south of Illinois Route 38 (Roosevelt Road).

Traffic Control, Operations, and Safety

From the 1992 Traffic for Kane County Map, the ADT for this section varies from 20,000 vpd to 25,000 vpd. The existing traffic signals are located at Pine street, Wilson Street, Hubbard Avenue, Fabyan Parkway, Cherry Lane, Illinois Route 38 (Roosevelt Road), Division Street, Tyler Road, DuKane Road, and Illinois Route 64 (North Avenue). Some of the major traffic generators in this

EXISTING ROADWAY CONDITIONS - cont'd

section are Fermi National Laboratory, Kane County Cougars Stadium, Pheasant Run Resort, DuPage County Airport and St. Charles Mall shopping center. There are two at grade railroad crossings in this section of Kirk Road. The first is Burlington Northern railroad located north of Wilson Street. The second is Chicago & Northwestern Railroad located just south of DuKane Street. The speed limit is 40 to 45 mph. Parking is not permitted along this section.

Kirk Road & Fabyan Parkway is a major intersection within this section. The lane configuration for the north leg consists of a left turn lane, three through lanes and a right turn lane. The south leg consists of dual left turn lanes, two through lanes and a right turn lane. The east leg consists of dual left turn lanes, two through lanes with a shared right turn lane. The west leg consists of dual left turn lanes, two through lanes and a right turn lane.

Another major intersection is Kirk Road & Illinois Route 38 (Roosevelt Road). The lane configuration consists of a left turn lane, two through lanes with a shared right turn lane on all four legs of the intersection.

Kirk Road & Illinois Route 64 (North Avenue) is another major intersection in this section. The lane configuration for the north leg consists of a left turn lane, two through lanes with a shared right turn lane. The south leg consists of a left turn lane, two through lanes, and a right turn lane. The east leg consists of dual left turn lanes, two through lanes, and a right turn lane. The west leg consists of a left turn lane, two through lanes, and a right turn lane.

Public Transportation

Two Pace bus routes operate within this section of Kirk Road. Pace bus #801 (Elgin-Geneva) and Pace #802 (Aurora-St. Charles) operates along Kirk Road from Illinois Route 64 to Illinois Route 38.

Section 4 - Illinois Route 64 to Illinois Route 25 (4.39 miles)

Exhibit B1-14 to Exhibit B1-18

Section 4 of Kirk Road extends through Dunham Road from Illinois Route 64 (North Avenue) to Illinois Route 25. This section passes through St. Charles, Wayne, Bartlett, and unincorporated Kane County. This section intersects Stearns Road which is a SRA route.

Physical Characteristics

The existing cross-sections consists of two 11 to 12-foot lanes with painted median and adjacent 6-foot aggregate shoulders. The existing R.O.W. varies from 66-feet to 100-feet.

Structure number 045-3021 is the bridge over the Chicago & Central Pacific railroad is located south of Stearns Road in this section of Dunham Road. A golfer's underpass is located north of the Royal

EXISTING ROADWAY CONDITIONS - cont'd

Fox Drive connecting the two halves of the golf course on either side of the Kirk Road. The Chicago & Northwestern Railroad underpass and Brewster Creek culverts are located adjacent to each other just north of Brewster Creek Circle. A Prairie Path crossing is located south of Little Wood Cemetery.

Traffic Control, Operations, and Safety

From the 1992 Traffic Map for Kane County, the ADT for this section varies from 7,600 vpd to 10,500 vpd. The speed limit is 40 mph. A traffic signal is located at Stearns and Illinois Route 25, and there is a four-way stop sign at Army Trail Road. Parking is not permitted along this section.

Dunham Road and Stearns Road is a major "T" intersection. The north leg consists of a through lane and a left turn lane. The east leg consists of a left turn lane and a right turn lane. The south leg is made up of a through lane and a right turn lane.

Dunham Road and Illinois Route 25 is also a major intersection. Dunham Road merges with Illinois Route 25 at this intersection. The lane configuration consists of a through lane with shared right turn and left turn on all four legs of the intersection.

Public Transportation

There are no public transit routes within this section of the corridor.

Section 5 - Illinois 25/Dunham Road to Congdon Avenue (6.06miles)

Exhibit B1-18 to Exhibit B1-23

Section 5 of Illinois Route 25 extends from Dunham Road to Bluff City Boulevard, north of Bluff City Boulevard the SRA route continues along St. Charles Street, Villa Street, Center Street and Dundee Avenue, and merges back to Illinois Route 25 (Exhibit B1-23) at Page Avenue south of Congdon Avenue. This section passes through Bartlett, Elgin and unincorporated Kane County.

Physical Characteristics

The existing cross-section consists of two 12-foot lanes with painted median and adjacent gravel shoulder from Dunham Road to Poplar Creek. From Poplar Creek this section is characterized by one to two lanes in each direction with a painted/raised median and adjacent curb & gutter. Dundee Avenue is one-way southbound between Page Avenue and Congdon Avenue. The existing right-of-way varies from 66 feet to 90 feet in this section. This section passes through the completely developed residential neighborhoods of the city of Elgin.

EXISTING ROADWAY CONDITIONS - cont'd

There are two structures in this section of. Structure number 045-0500 over Poplar Creek is located north of Chicago & Northwest Railroad. Structure number 045-007 carries U.S. Route 20 over U.S. 20. The vertical clearance meets the SRA standards.

Traffic Control, Operations, and Safety

From the 1992 Traffic Map for Kane County, the ADT for this section is 15,000 vpd from Dunham Road to Hammond Avenue. Traffic signals are located at Bluff City Boulevard, May Street, Arlington Avenue, Villa Street, National Street, Chicago Street, Highland Avenue, Division Street, Park Street, Kimball Street, Summit Street, Slade Avenue, and Congdon Avenue and Illinois Route 25 (Junction). The speed limits in this section are 40-50 mph between Dunham Road and Chicago & Northwestern Railroad, and 30-35 mph within the City of Elgin limits. Parking is permitted along St. Charles Street and Center Street.

Public Transportation

The Pace #541 (Douglas), #542 (Grove), #543(Dundee), #544 (Chicago St.), and #553 (Park/Summit) operates within the downtown Elgin District. The two Metra Stations located near the corridor are National Street Stations located at 291 Hardee Street, and Elgin Station located at 109 W. Chicago Street.

Section 6 - Congdon Avenue to Illinois Route 62 (6.60 miles)

Exhibit B1-23 to Exhibit B1-29

Section 6 extends from Congdon Avenue as Dundee Avenue and continues north as Illinois Route 25 to Illinois Route 62 (Algonquin Road). This section passes through Elgin, East Dundee, Carpentersville, Barrington, and unincorporated Kane County.

Existing Roadway Conditions

The section of Illinois Route 25 from Congdon Avenue to Ford Street consists of a total of a 42-foot pavement (4 lanes) with curb and gutter separated by painted median; there are four 11-foot lanes with curb and gutter separated by a 4-foot mountable median between Ford Street and Brant Drive, four 12-foot lanes with curb and gutter separated by a 4 to 30-foot median between Brant Drive and Illinois Route 62. The existing right-of-way is 66 feet from Congdon Avenue to Trout Park, 70-feet to 110 feet between Trout Park and Illinois Route 72, and 100 to 120 feet between Illinois Route 72 and Illinois Route 62 (Algonquin Road). This section passes through the city of Elgin is considered as an urban section.

Structure number 045-0042, the bridge over I-90 (Northwest Tollway), is located south of Max McGraw Game Farm & Wildlife Preserve.

EXISTING ROADWAY CONDITIONS - cont'd

Traffic Control, Operations, and Safety

From the Chicago Area Transportation Study (CATS) and the 1992 Traffic Map for Kane County, the ADT for this section is 19,000 vpd from Congdon Avenue to the Northwest Tollway (I-90) and varies from 21,000 vpd to 23,000 vpd between I-90 and Illinois Route 62. The speed limit is 30 mph between Congdon Ave. and I-90, and 45 mph between I-90 and Illinois Route 62. Traffic signals are located at Luda Street, Tollway entrance, Brant Drive, Illinois Route 72 and 68, L.W. Besinger Drive, Meadowdale Drive, Lake Marian Road, King Road, Helm Road, and Illinois Route 62. Parking is not permitted along this section.

Public Transportation

The only bus route operating within this section is Pace bus #803 (Carpentersville Local). This bus operates on Illinois Route 25 within the Carpentersville area.

**TABLE II-1
STRUCTURE INVENTORY
IL ROUTE 25/DUNHAM RD/KIRK RD/FARNSWOTH AVENUE
US ROUTE 34 TO ILLINOIS ROUTE 62**

EXHIBIT LABEL	IDOT NUMBER	OVER	UNDER	CLEAR WIDTH	LENGTH	RECOMMENDED REQUIREMENTS
SN # 1	045-3138	Waubonsie Creek		28.0'	55.3'	Reconstruction
SN # 2	045-3138	Waubonsie Creek		28.0'	46.4'	Reconstruction
SN # 3	045-6052	B.N. Railroad		58.9'	356'	N/A
SN # 4	045-6012	Indian Creek		56.0'	28.0'	Modification
SN # 5	045-6011	Indian Creek		56.6'	28.0'	Modification
SN # 6	045-6010	Indian Creek		56.6'	23.0'	Reconstruction
SN # 7	045-9944	I-88 E-W Toll		68.0'	231.0	Modification
SN # 8	045-3096	C & NW RR		55.5'	1,376.3'	Modification
SN # 9	045-3021	C & CP Railroad		34.8'	148.0'	Modification
SN # 10	045-5000	Poplar Creek		44.0'	51.2'	Modification
SN # 11	045-0007		US Rte 20	74.0'	148.0'	N/A
SN # 12	045-0042	I-90 Tollway		57.0'	308.0'	Modification

**TABLE II-2
ACCIDENT RATES AT INTERSECTIONS
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62**

Cross Street	Number of Accidents			Rate
	1990	1991	1992	
U.S. Route 34	Unavailable Data			
Montgomery Road	Unavailable Data			
Fifth Avenue	0	0	1	0.4
New York Street	22	11	9	1.38
Liberty Street	9	14	14	1.57
Sheffer Road	37	16	13	1.52
Indian Trail Road	6	14	11	1.07
Reckinger Road	13	13	5	0.71
Molitor Road	27	13	6	1.39
E-W Tollway, I-88	-	-	-	-
Bilster Road	1	0	1	0.06
Illinois Route 56	34	24	27	2.97
Wilson Street	8	7	10	0.48
Fabyan Parkway	12	6	10	0.65
Illinois Route 38	28	19	15	1.35
Tyler Road	3	3	4	0.42
Illinois Route 64 (North Ave)	33	16	9	1.25
Kirk/Dunham Road	3	3	4	0.35
Army Trail Road	-	-	-	-
Stearns Road	2	6	4	0.42
Illinois Route 25	12	20	22	1.76

**TABLE II-2
ACCIDENT RATES AT INTERSECTIONS
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62**

Cross Street	Number of Accidents			Rate
	1990	1991	1992	
W. Bartlett Road/Middle Street	9	9	15	1.22
Bluff City Boulevard*	22	15	26	3.57
Villa Street/National Drive*	8	9	7	0.88
Kimball Avenue*	16	23	17	2.56
Illinois Route 58 (Summit Street)*	8	9	10	0.7
Congdon Avenue*	11	9	7	0.77
N-W Tollway I-90	11	9	9	2.87
Illinois Route 72	29	25	35	1.29
Illinois Route 68	20	14	21	3.62
Lake Marian Road	27	35	30	0.8
Helm Road	6	7	8	1.84
Illinois Route 62	13	20	12	5.30

* Elgin alternate route on St. Charles Street, Villa Street, and Dundee Avenue

**TABLE II-3
ACCIDENT RATES ON SEGMENTS
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62**

LOCATION	Segments Length Miles	No. Of Accidents			Rate
		1990	1991	1992	
U.S. Route 34 to Montgomery Road	1.11	-	-	-	-
South of Binder Street to Fifth Avenue	0.54	3	2	2	
Fifth Avenue to New York Street	0.70	12	7	3	2.92
New York Street to Liberty Street	0.37	10	6	3	4.78
Liberty Street to Sheffer Road	1.22	60	47	28	6.87
Sheffer Road to Indian Trail Road	0.32	23	19	18	6.96
Indian Trail Road to Reckinger Street	0.17	6	17	15	7.01
Reckinger Street to Molitor Road	0.53	16	11	12	2.30
Molitor Road to Bilter Road	0.94	9	11	8	1.20
Bilter Road to Illinois Route 56	0.50	2	6	3	0.96
Illinois Route 56 to Wilson Street	2.79	40	22	22	1.37
Wilson Street to Fabyan Parkway	1.04	21	26	24	2.49
Fabyan Parkway to Illinois Route 38	1.06	15	15	11	2.30
Illinois Route 38 to Tyler Road	1.50	29	30	30	2.85
Tyler Road to Illinois Route 64 (North Ave)	0.98	31	27	26	6.11
Illinois Route 64 to Kirk/Dunham Road	1.61	5	2	6	0.74
Kirk/Dunham Road to Army Trail Road	0.38	5	4	18	5.27
Army Trail Road to Stearns Road	1.89	4	2	17	0.90
Stearns Road to Illinois Route 25	0.32	7	5	5	2.97
Illinois Rte 25 to West Bartlett Road	0.90	6	9	8	1.44
West Bartlett Road Road to Bluff City Blvd.	1.90	28	26	32	2.90
Corridor Segments	Segment Length Miles	No. of Accidents			Rate
		1990	1991	1992	
Bluff City Boulevard to Villa Street *	0.82	25	23	28	10.28

**TABLE II-3
ACCIDENT RATES ON SEGMENTS
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62**

Villa Street to Chicago Street *	0.62	27	25	16	6.68
Chicago Street to Illinois Rt. 58 (Summit St)*	0.92	18	32	27	3.82
Illinois Rte 58 (Summit St) to Congdon Ave*	0.85	35	18	19	4.27
Congdon Avenue to I-90 N-W Tollway	0.6	28	29	23	8.01
I-90 N-W Tollway to Illinois Route 72	2.02	51	64	56	6.55
Illinois Route 72 to Illinois Route 68	0.76	16	19	12	3.36
Illinois Route 68 to Lake Marian Road	1.14	49	43	54	8.53
Lake Marian Road to Helm Road	0.68	24	24	25	5.05

*Elgin alternate route on St. Charles Street, Villa Street, and Dundee Avenue.

Table II-4
Sources of Data for Traffic and Transportation Characteristics
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE

Item	Data Source
Traffic Volumes <ul style="list-style-type: none"> • Average Daily Traffic • Intersection Turning Movement Counts 	<ul style="list-style-type: none"> - US DOT Office of Planning and Programming, 1989 Traffic Map, Kane County - Illinois Department of Transportation, Office of Planning & Programming, Planning Services Section, Roadway Scope Report
Accidents	<ul style="list-style-type: none"> - Illinois Department of Transportation, Office of Planning & Programming, Planning Services Section, Roadway Scope Report
Transit <ul style="list-style-type: none"> • Routes 	<ul style="list-style-type: none"> - Metra - Pace
Traffic Control <ul style="list-style-type: none"> • Signalized Intersection Locations • Other Traffic Control 	<ul style="list-style-type: none"> - Field Reconnaissance
Cross Section <ul style="list-style-type: none"> • Lane Widths and Arrangements • Shoulder Widths • Type of Section 	<ul style="list-style-type: none"> - As-Built Plans - Illinois Department of Transportation, Office of Planning & Programming, Planning Services Section, Roadway Scope Report - Field Reconnaissance
Right-of-Way	<ul style="list-style-type: none"> - Illinois Department of Transportation, Office of Planning & Programming, Planning Services Section, Roadway Scope Report - As-Built Plans, Sidwell Maps
Curb/Roadside Use <ul style="list-style-type: none"> • Parking • Bus and Loading Zones 	<ul style="list-style-type: none"> - Field Reconnaissance
Structures	<ul style="list-style-type: none"> - Illinois Department of Transportation, Office of Planning & Programming, Planning Services Section, Roadway Scope Report
Other Features	<ul style="list-style-type: none"> - Illinois Department of Transportation, Office of Planning & Programming, Planning Services Section, Roadway Scope Report - Field Reconnaissance

EXISTING LANE CONFIGURATION

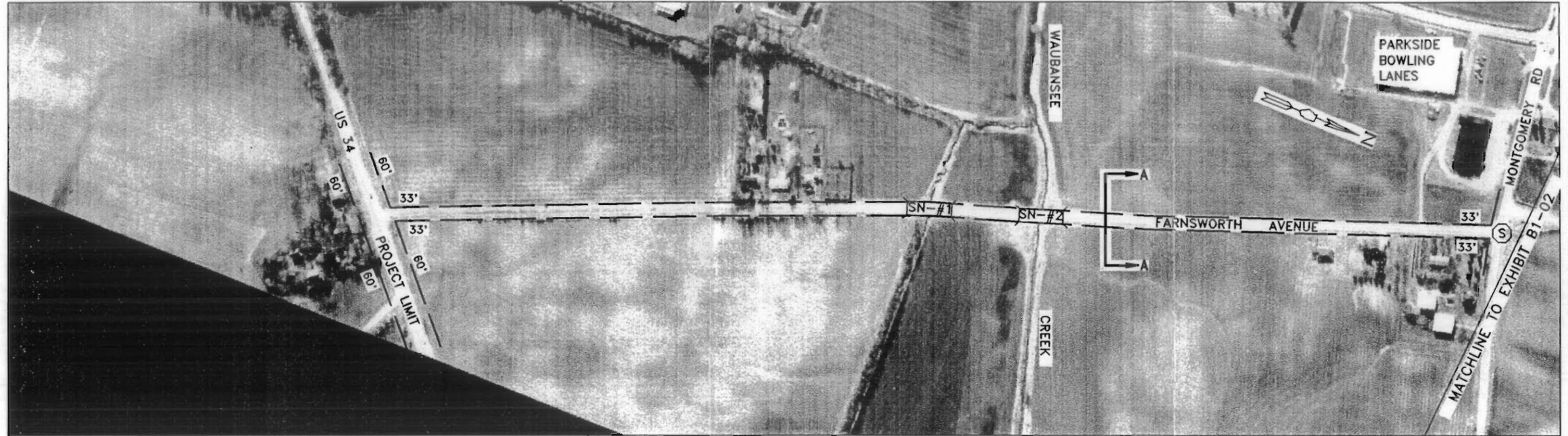
SIGNAL SPACING

EXISTING R.O.W.

1.11 MILES

UNINCORPORATED KENDALL CO.

AURORA



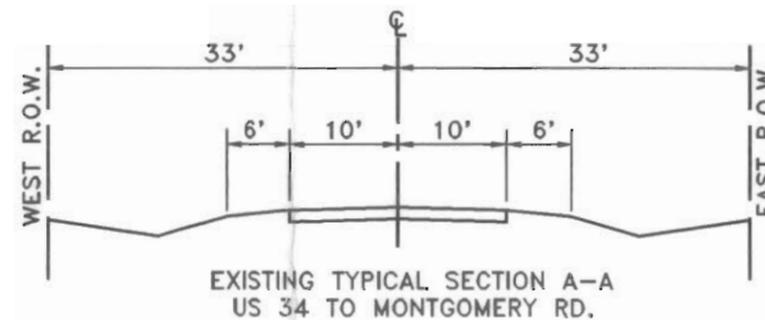
UNINCORPORATED KENDALL CO.

AURORA

AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF EXISTING CONDITIONS:

- * SN-1 = Structure number 046-3138
- * SN-2 = Structure number 046-3137



LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING STRUCTURE NUMBER
	= EXISTING TRAFFIC LANE CONFIGURATION
	= EXISTING PUBLIC TRANSIT LOCATION
	= STOP SIGN
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE In association with METRO Transportation Group and BOYER Engineering, Ltd. for the

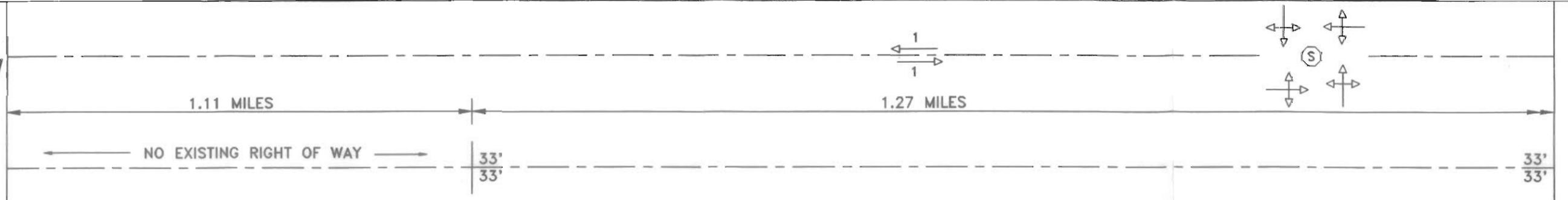


EXHIBIT B1-01

EXISTING LANE CONFIGURATION

SIGNAL SPACING

EXISTING R.O.W.

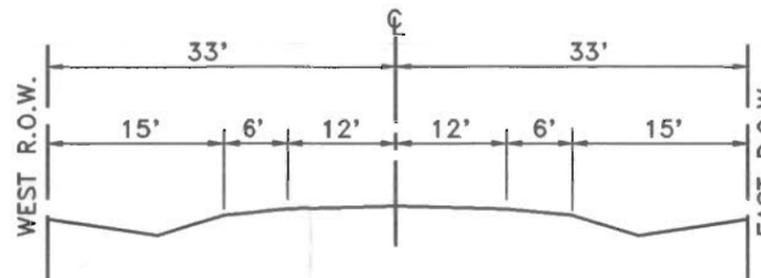


AURORA

AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF EXISTING CONDITIONS:

* City of Aurora plans to extend Farnsworth Avenue from Binder Street to Montgomery Road.

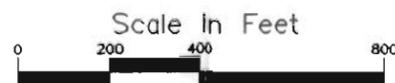


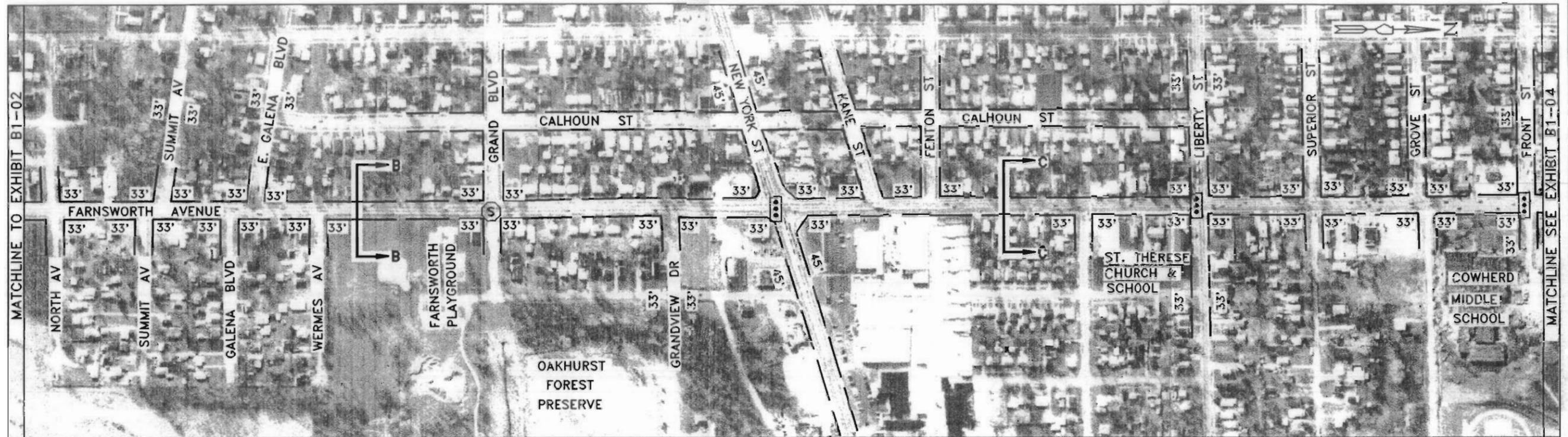
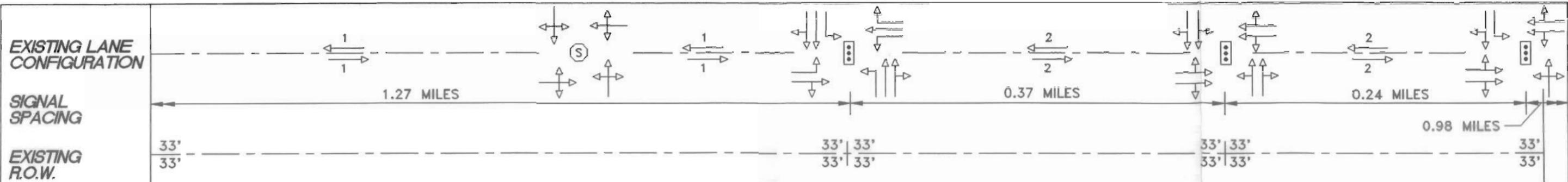
EXISTING TYPICAL SECTION B-B
BINDER ST. TO MATCHLINE B1-03

LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= EXISTING PUBLIC TRANSIT LOCATION
	= STOP SIGN

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

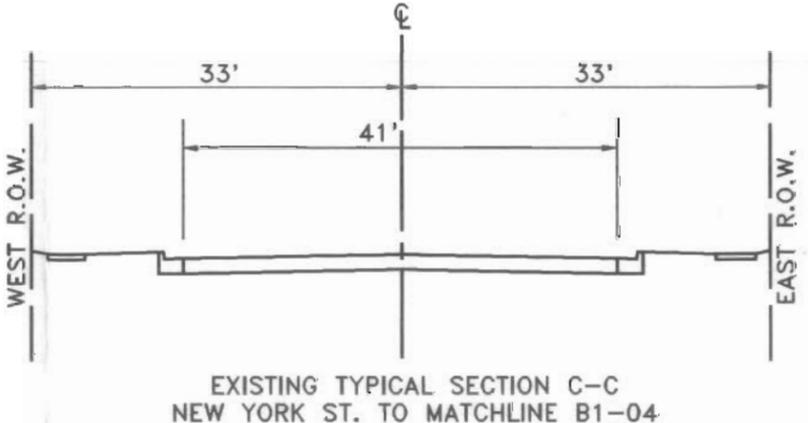
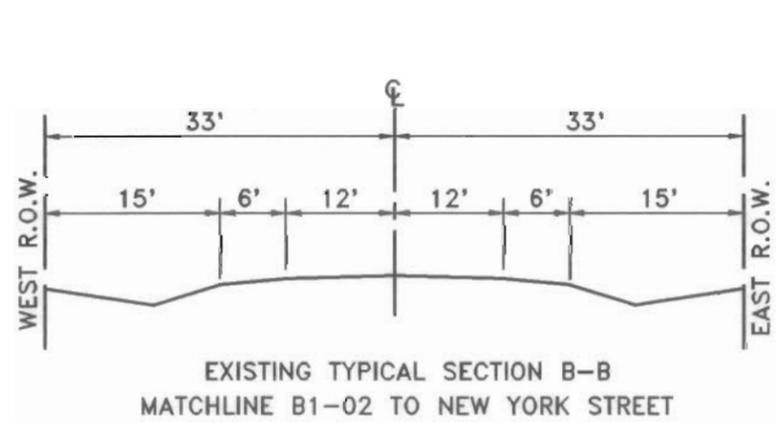




AURORA

AERIAL PHOTO DATE: 3-19-92

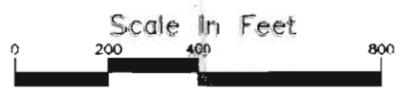
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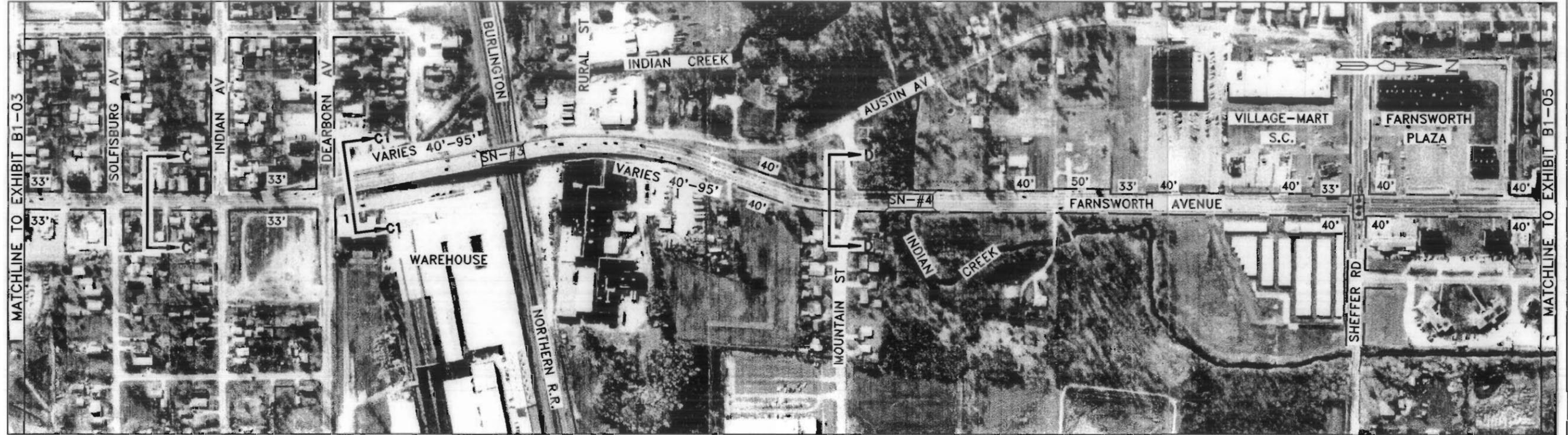
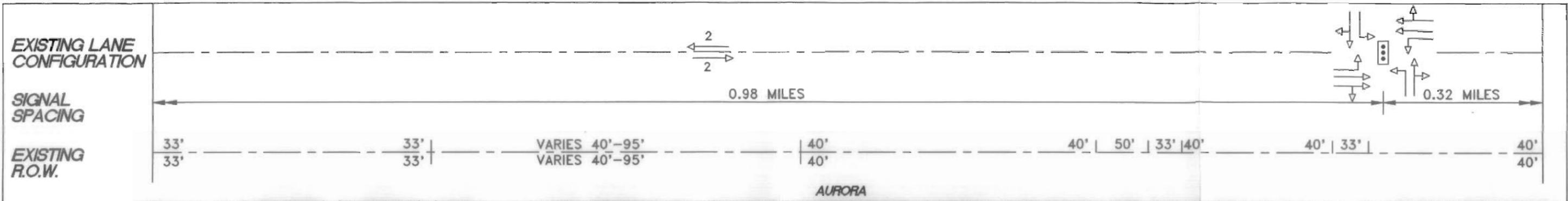


LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= STOP SIGN

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

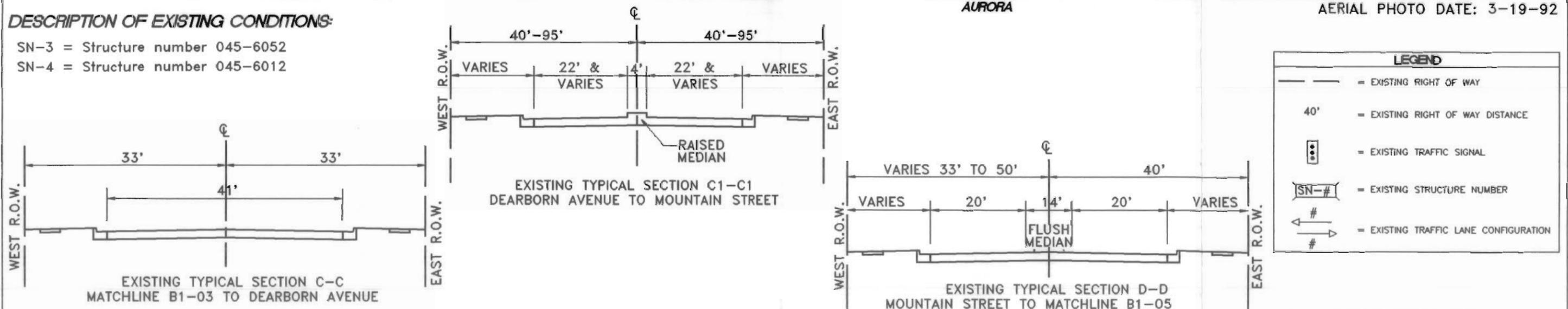
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF EXISTING CONDITIONS:

SN-3 = Structure number 045-6052
 SN-4 = Structure number 045-6012

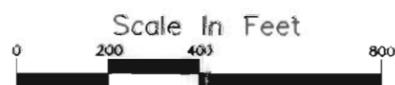


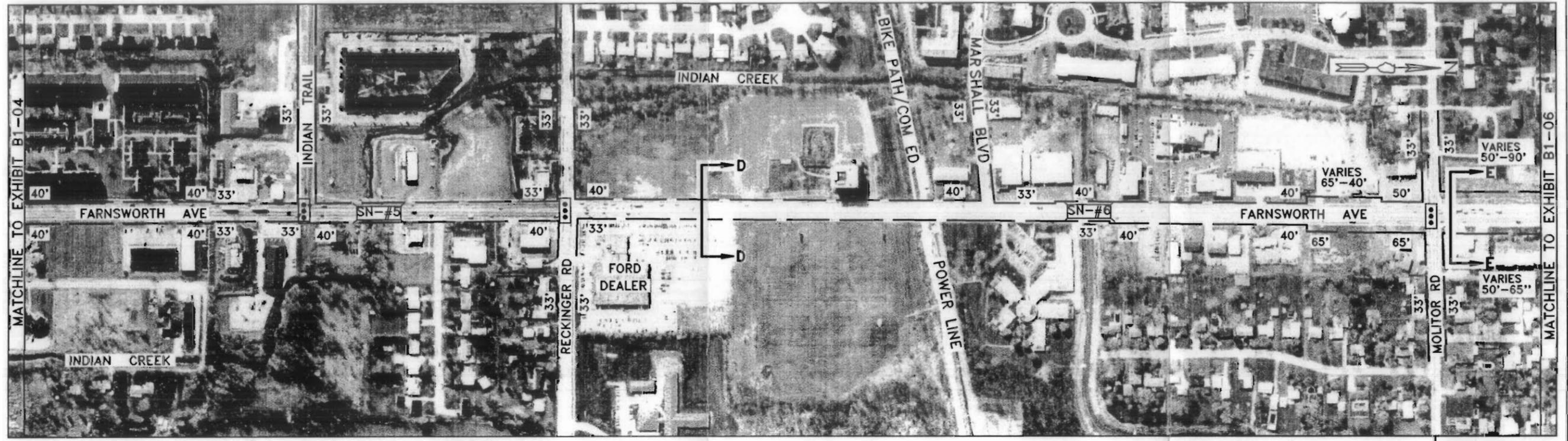
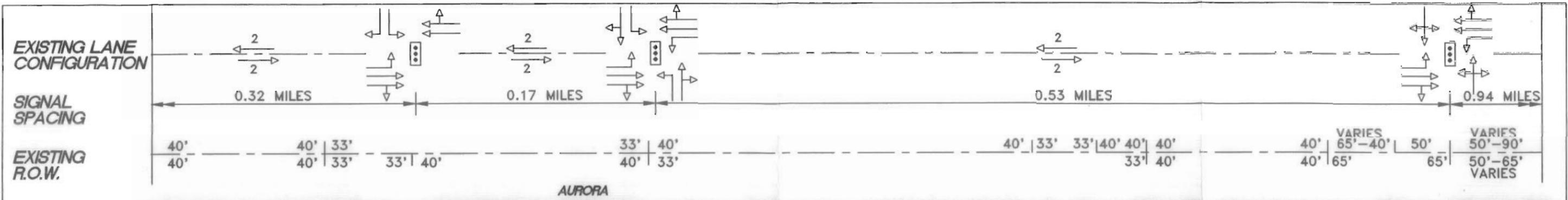
LEGEND	
	= EXISTING RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING STRUCTURE NUMBER
	= EXISTING TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

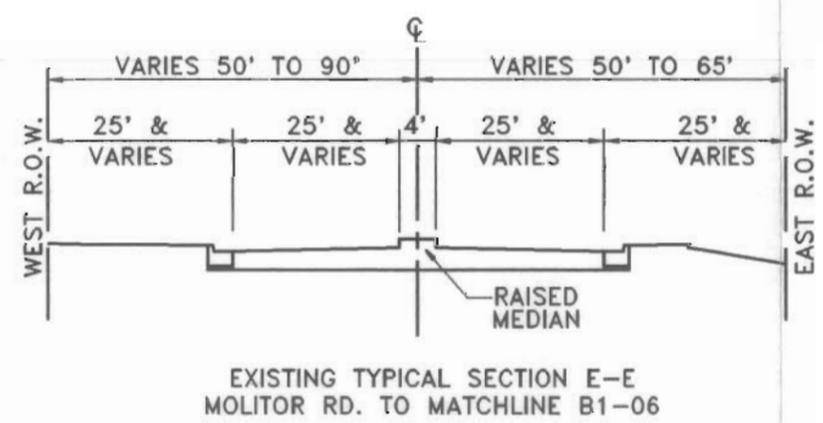
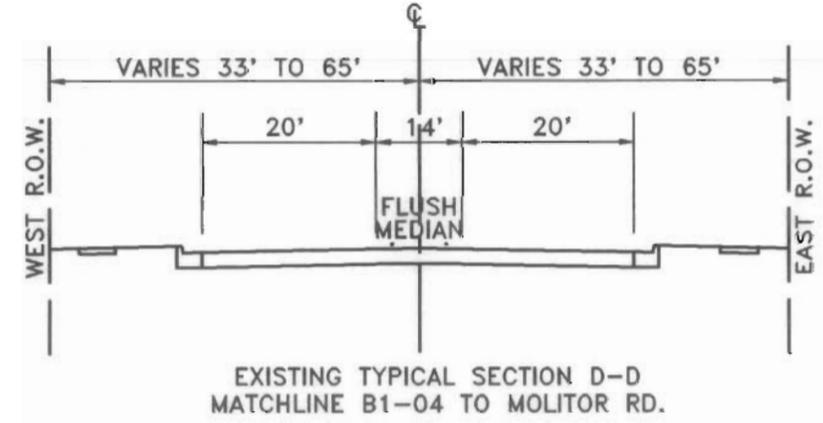




AERIAL PHOTO DATE: 5-01-92 UNINCORPORATED KANE CO.

DESCRIPTION OF EXISTING CONDITIONS:

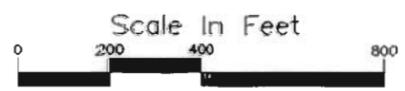
SN-5 = Structure number 045-6011
 SN-6 = Structure number 045-6010

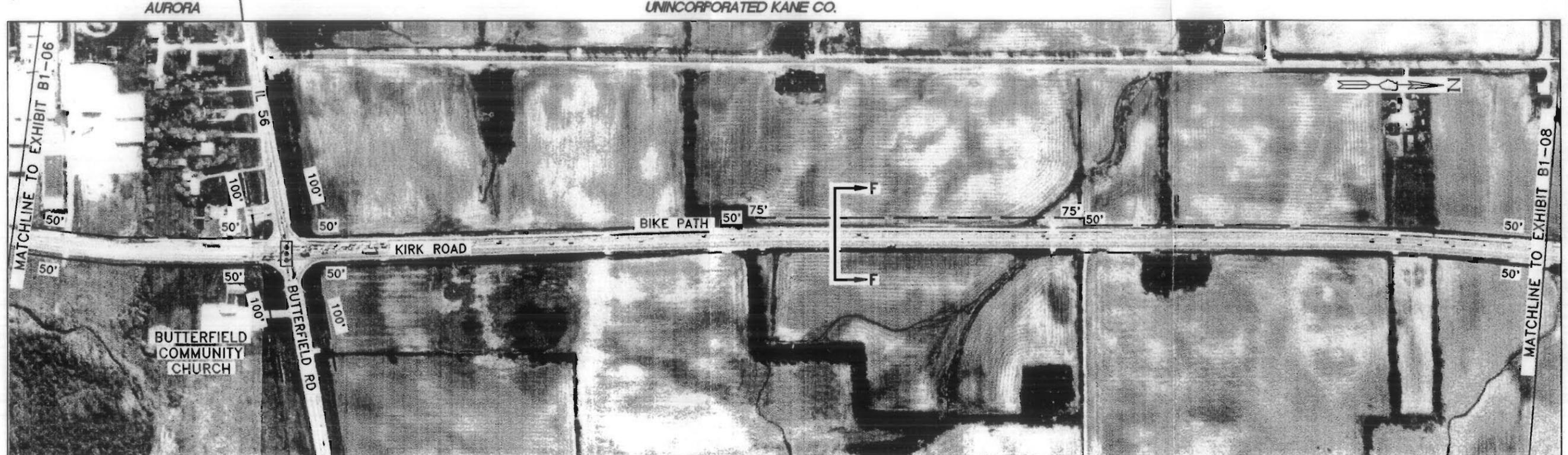
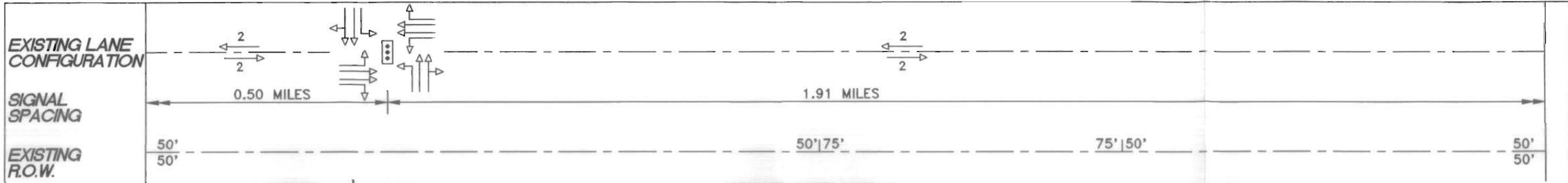


LEGEND	
---	= EXISTING RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
[Signal Symbol]	= EXISTING TRAFFIC SIGNAL
[SN-#]	= EXISTING STRUCTURE NUMBER
[Lane Config Symbol]	= EXISTING TRAFFIC LANE CONFIGURATION
- - - -	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

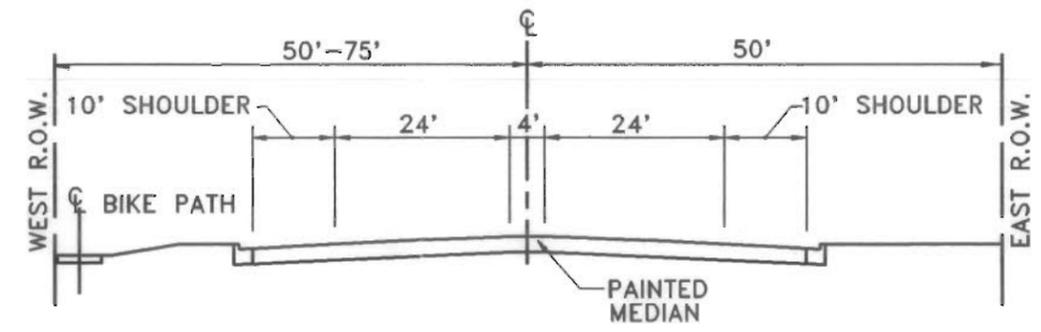




AERIAL PHOTO DATE: 5-01-92

DESCRIPTION OF EXISTING CONDITIONS:

* The intersection of Kirk Road and IL Route 56 is a high accident location.

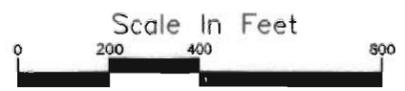


EXISTING TYPICAL SECTION F-F
ILLINOIS ROUTE 56 TO MATCHLINE B1-08

LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

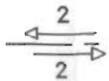
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



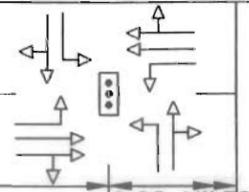
EXISTING LANE CONFIGURATION

SIGNAL SPACING

EXISTING R.O.W.

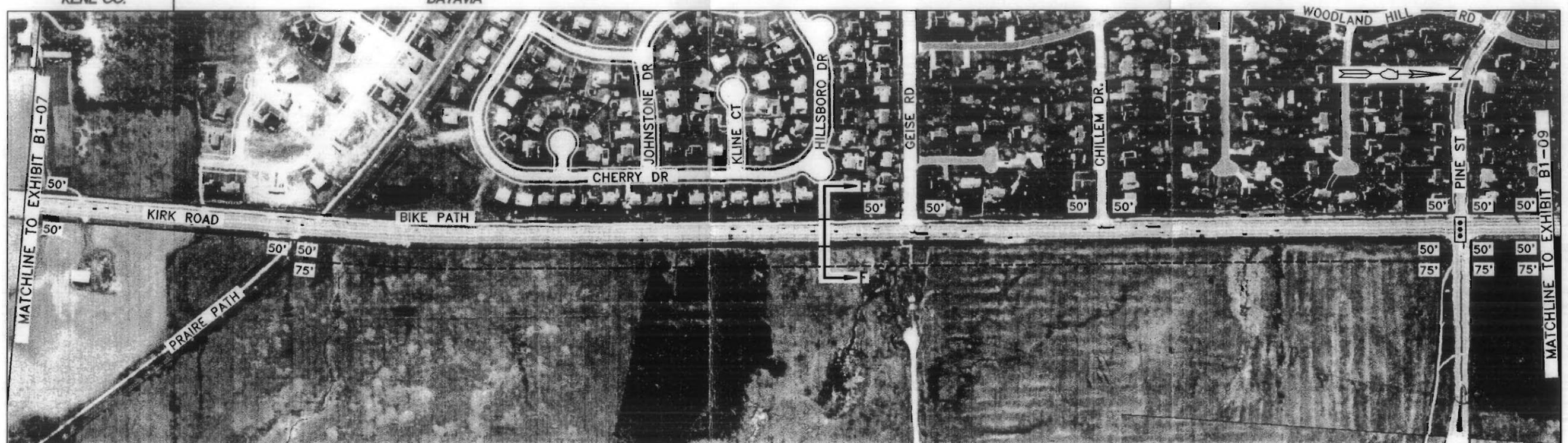


1.91 MILES



0.88 MILES

50' 50' UNINCORPORATED KENE CO. 50' 150' PLUS 75' (COUNTY HIGHWAY EASEMENT) 50' PLUS 75' (COUNTY HIGHWAY EASEMENT) 50'

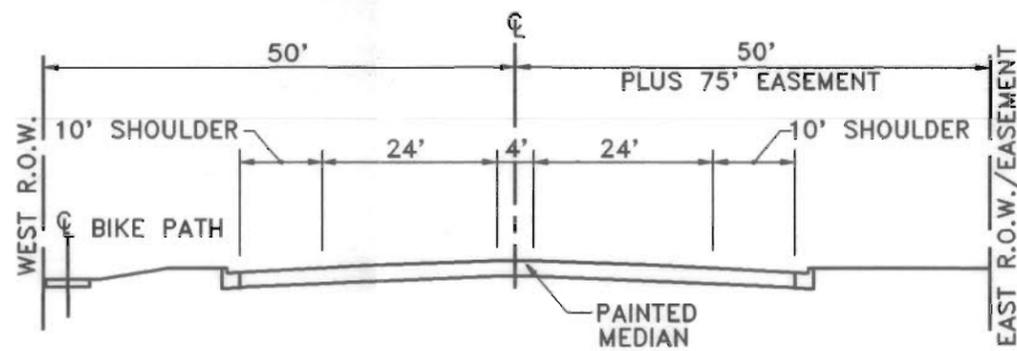


FERMI NATIONAL ACCELERATOR LABORATORY

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

* Illinois Praire Path @ grade crossing is located 1.1 miles North of IL Route 56.

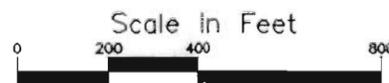


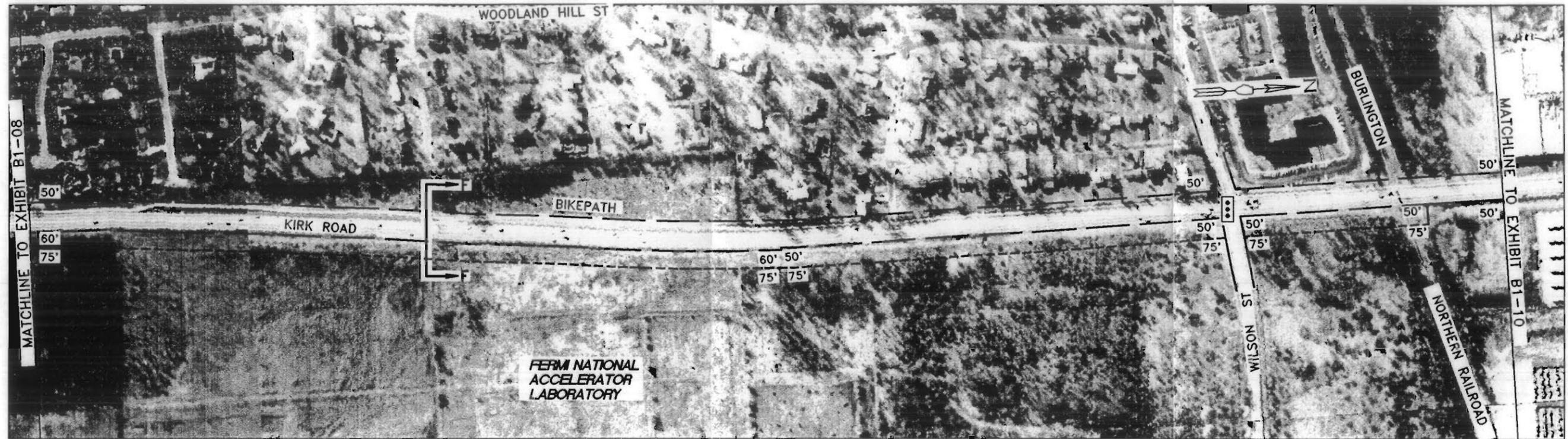
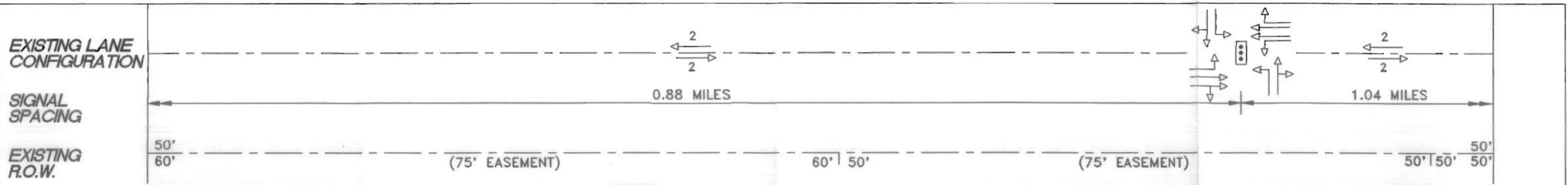
EXISTING TYPICAL SECTION F-F MATCHLINE B1-07 TO MATCHLINE B1-09

LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY / VILLAGE BOUNDARY
75'	= COUNTY HIGHWAY EASEMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

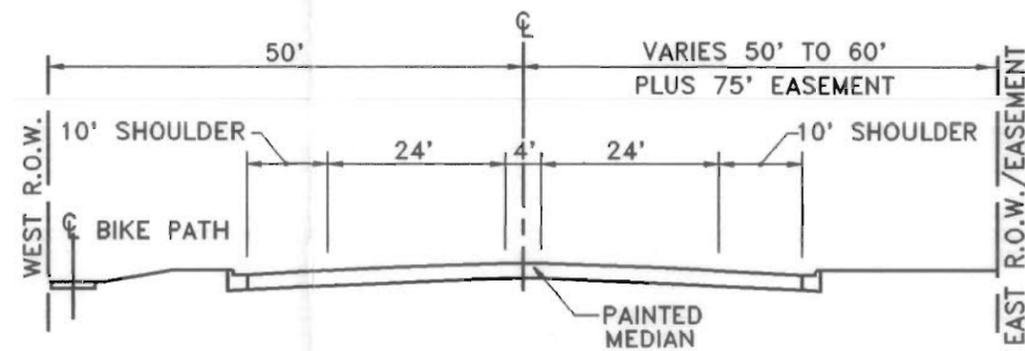




FERM NATIONAL ACCELERATOR LABORATORY

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

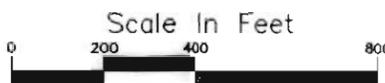


EXISTING TYPICAL SECTION F-F
MATCHLINE B1-08 TO MATCHLINE B1-10

LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
75'	= COUNTY HIGHWAY EASEMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

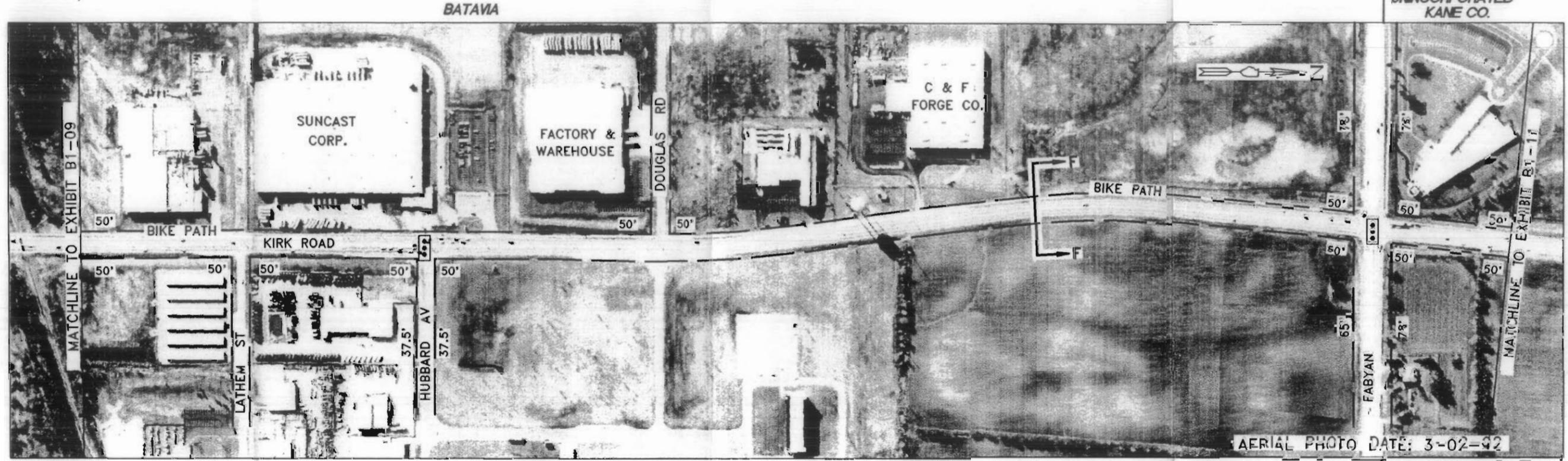
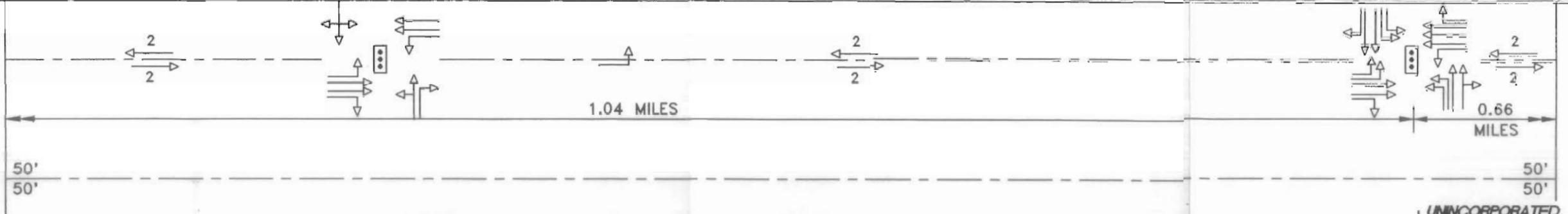
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

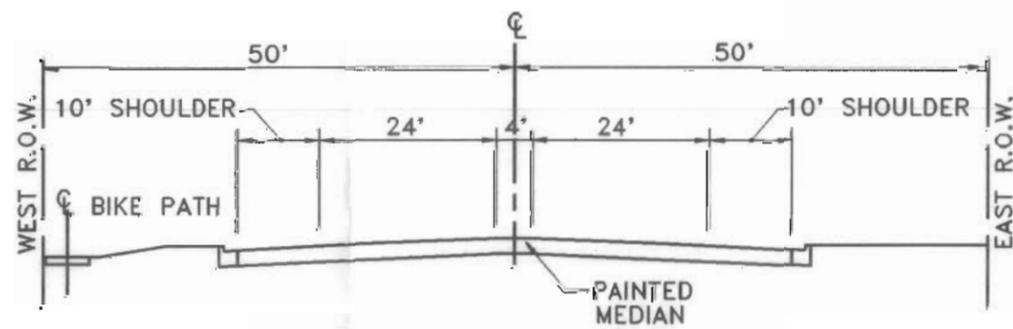
SIGNAL SPACING

EXISTING R.O.W.



DESCRIPTION OF EXISTING CONDITIONS:

- * Existing left turn lane Northbound Kirk Road to Westbound Douglas Road.

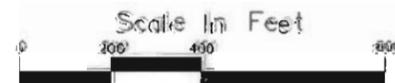


EXISTING TYPICAL SECTION F-F
MATCHLINE B1-09 TO MATCHLINE B1-11

LEGEND	
	= EXISTING RIGHT OF WAY
	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH ROAD - EXISTING CONDITIONS

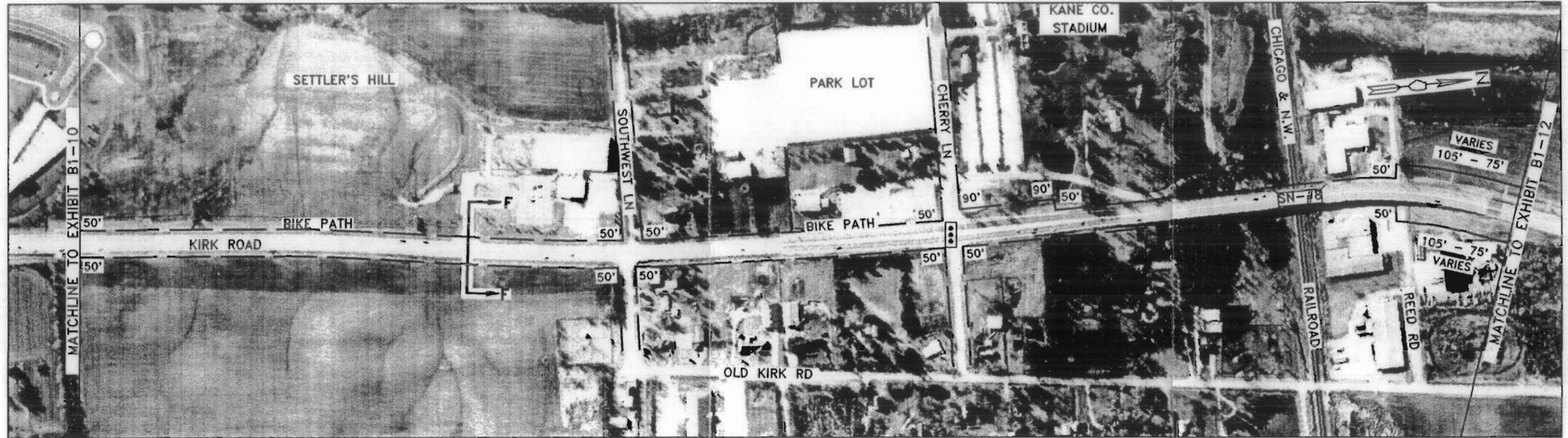
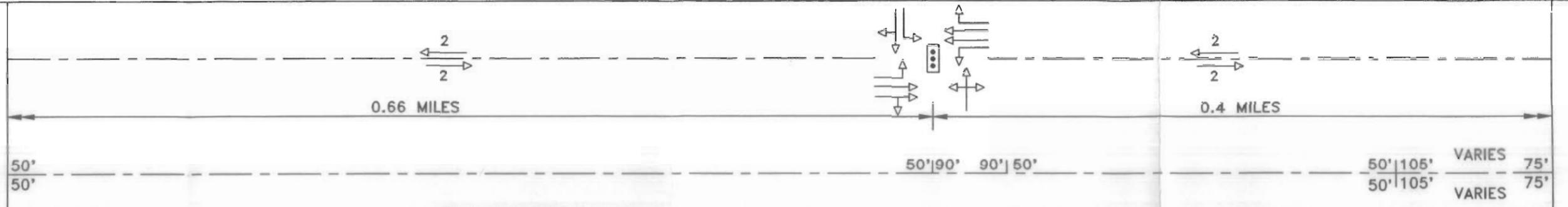
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

SIGNAL SPACING

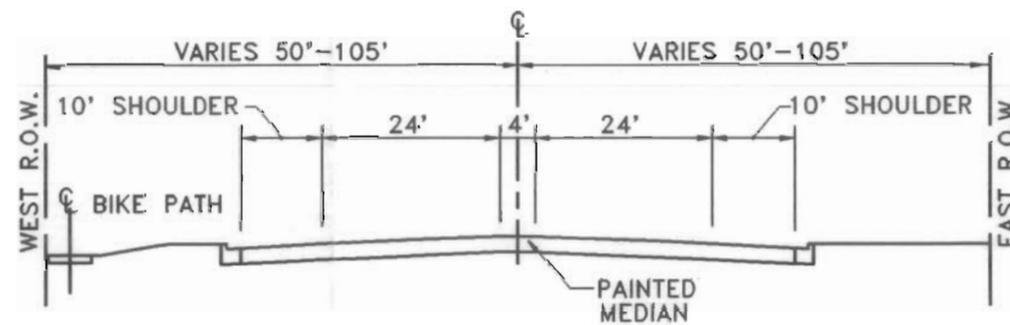
EXISTING R.O.W.



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

SN-8 = Structure number 045-3096



EXISTING TYPICAL SECTION F-F
MATCHLINE B1-10 TO MATCHLINE B1-12

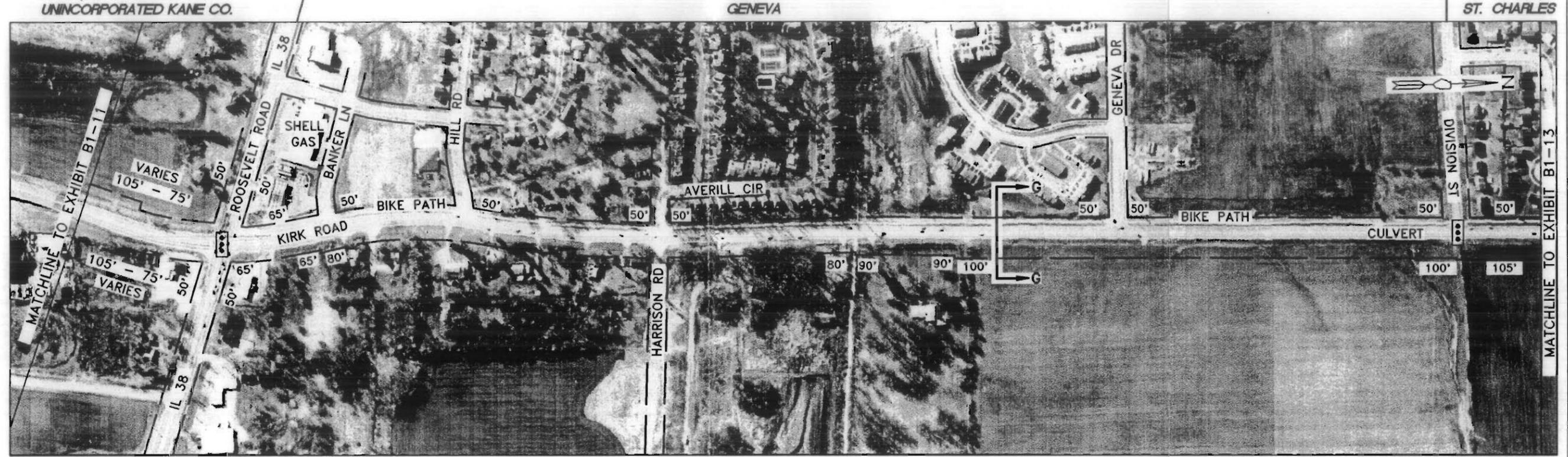
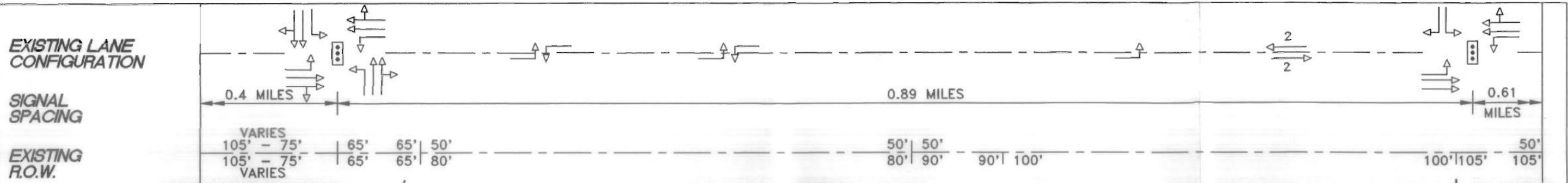
LEGEND	
	= EXISTING RIGHT OF WAY
75'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING STRUCTURE NUMBER
	= EXISTING TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation



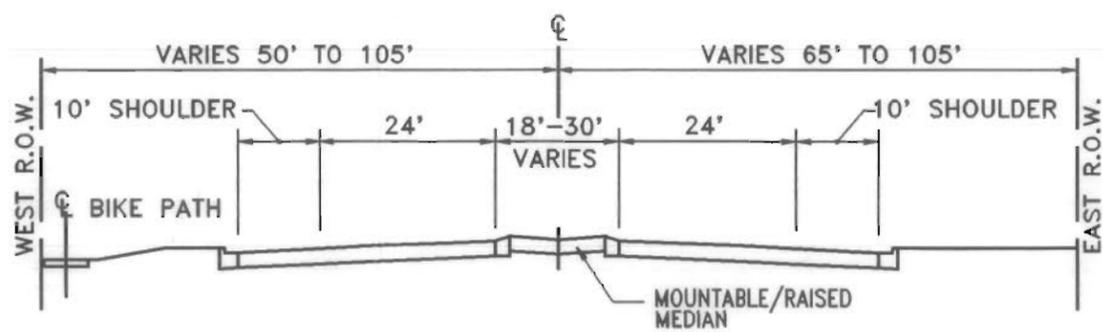


UNINCORPORATED KANE CO.

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

- * The existing R.O.W. within this section is based on phase II plans of Kane County. (County section 90-00192-05-WR)
- * Existing left turn lane along Kirk Road to Hill Road, Averill Circle and Geneva Drive.



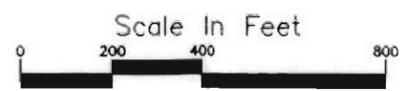
EXISTING TYPICAL SECTION G-G
ROOSEVELT ROAD TO MATCHLINE B1-13

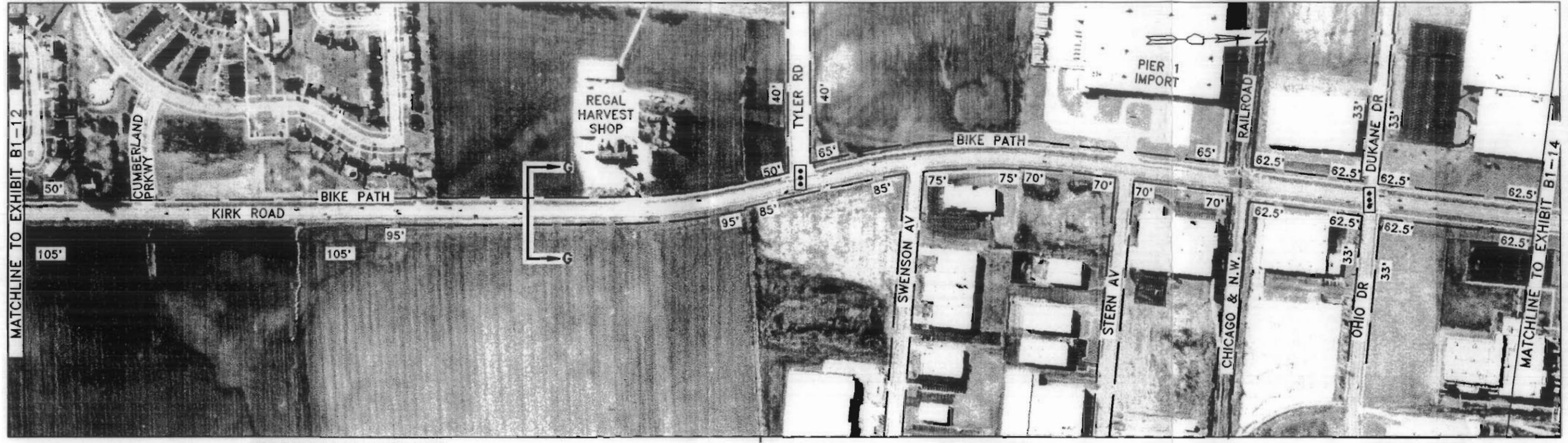
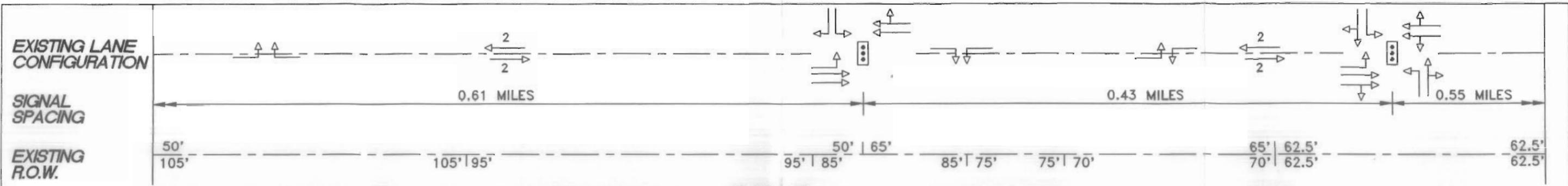
LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





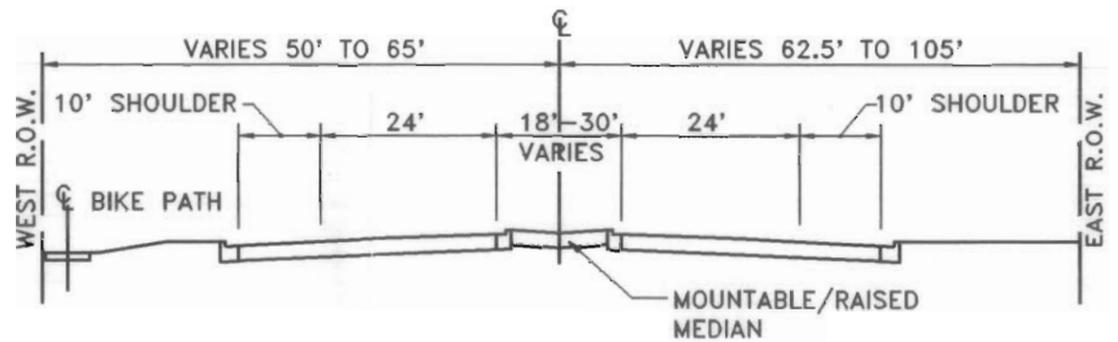
UNINCORPORATED KANE CORP.

ST. CHARLES

AERIAL DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

- * The existing R.O.W. within this section is based on phase II plans of Kane County (County Section 90-00192-05-WR)
- * Existing left turn lane along Kirk Road to Cumberland Parkway, Swenson Avenue and Stern Street.



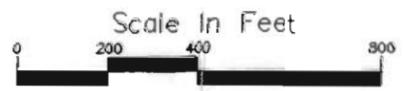
EXISTING TYPICAL SECTION G-G
MATCHLINE B1-12 TO MATCHLINE B1-14

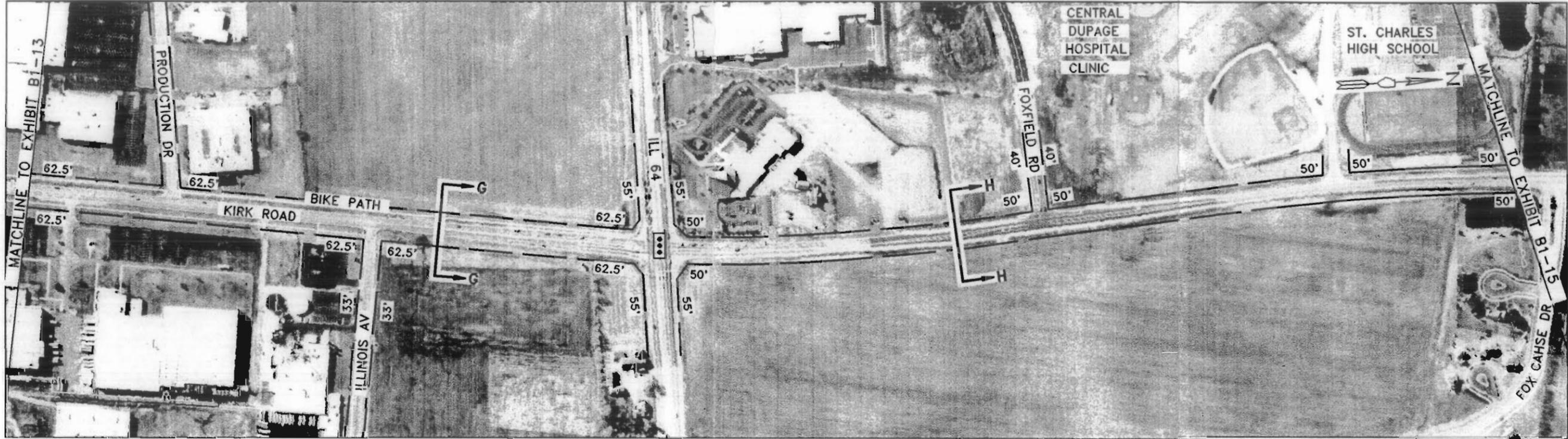
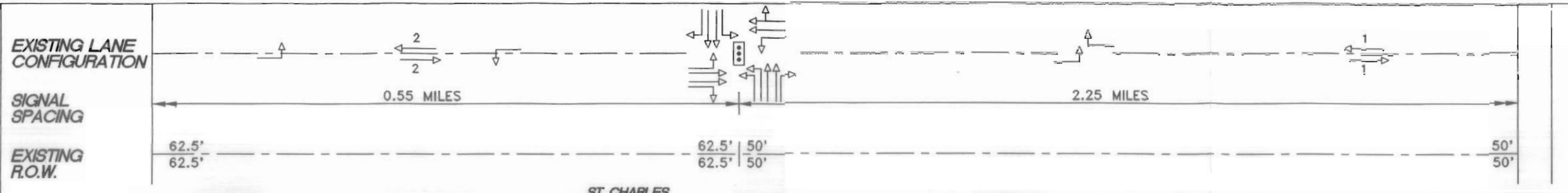
LEGEND	
---	= EXISTING RIGHT OF WAY
95'	= EXISTING RIGHT OF WAY DISTANCE
⊠	= EXISTING TRAFFIC SIGNAL
← # →	= EXISTING TRAFFIC LANE CONFIGURATION
- - -	= CITU/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



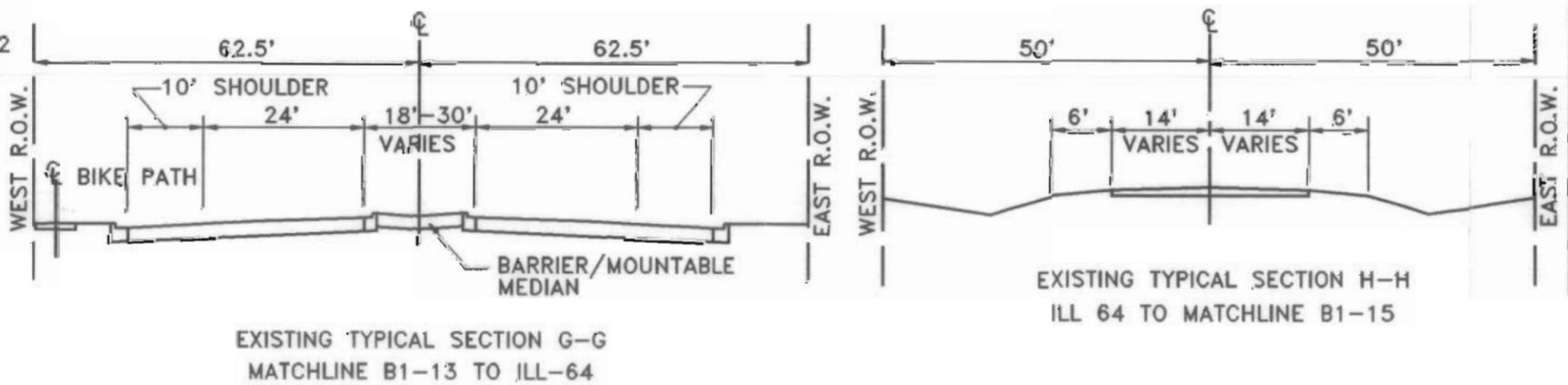
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF EXISTING CONDITIONS:

- * The existing R.O.W. from matchline B1-12 to Illinois Route 64 is based on phase II plans of Kane County.
- * Existing left turn lane along Kirk Road to Production Drive and Illinois Avenue.
- * Existing right/left turn lanes from Kirk Road to Foxfield Road.



LEGEND	
	= EXISTING RIGHT OF WAY
	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



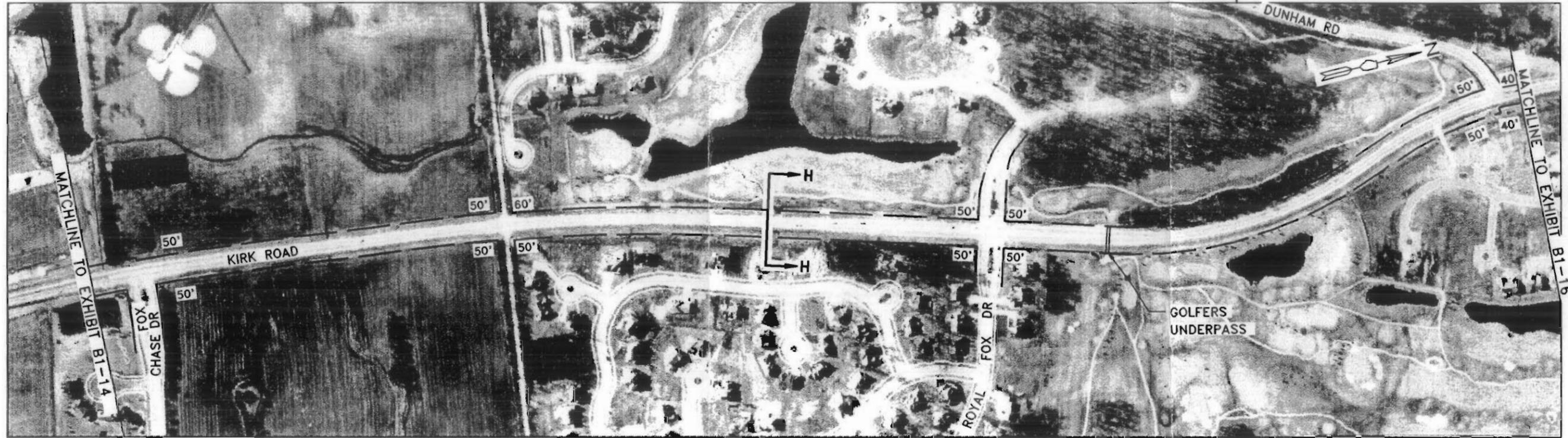
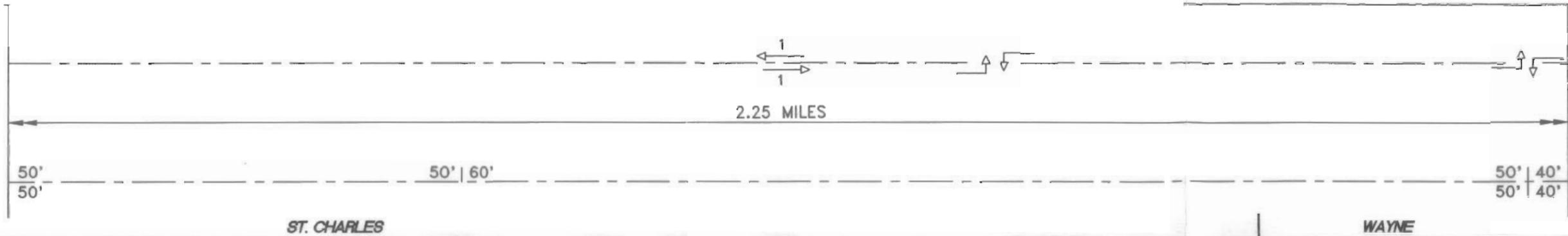
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and EOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

SIGNAL SPACING

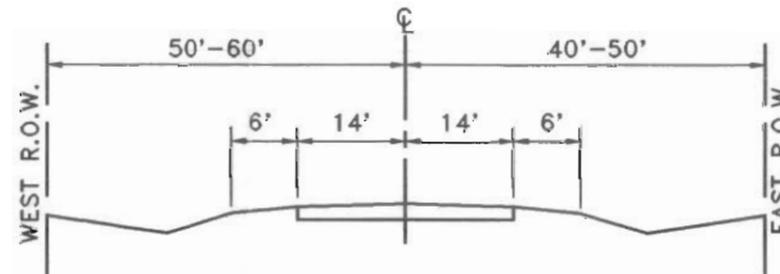
EXISTING R.O.W.



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

- Existing right/left turn lanes along Kirk Road to Royal Fox Drive and Dunham Road.



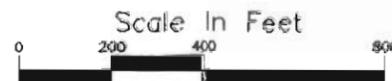
EXISTING TYPICAL SECTION H - H
MATCHLINE B1-14 TO MATCHLINE B1-16

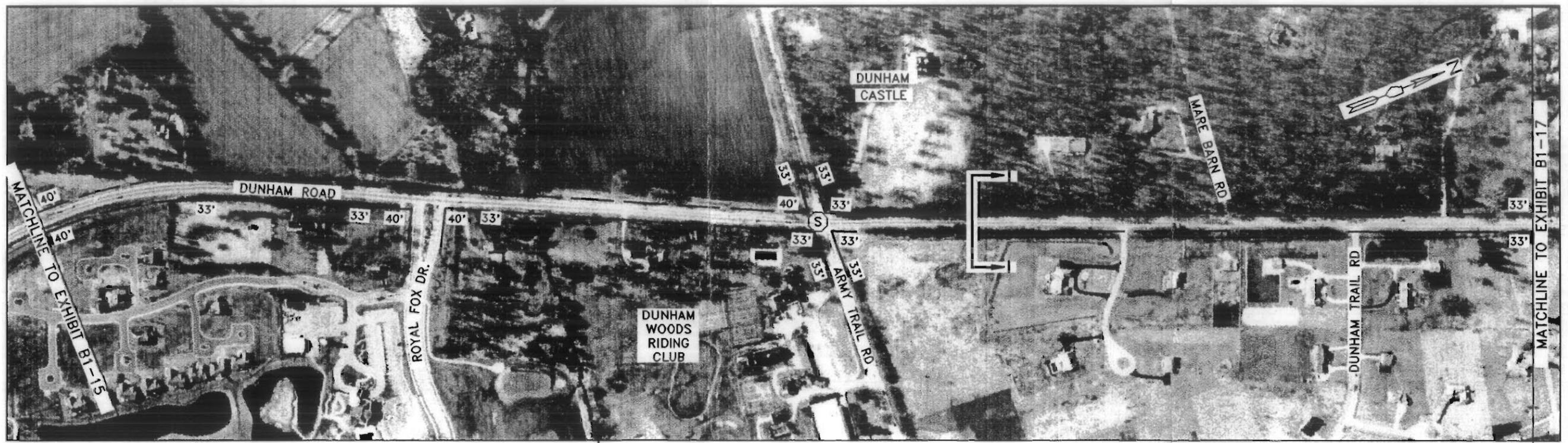
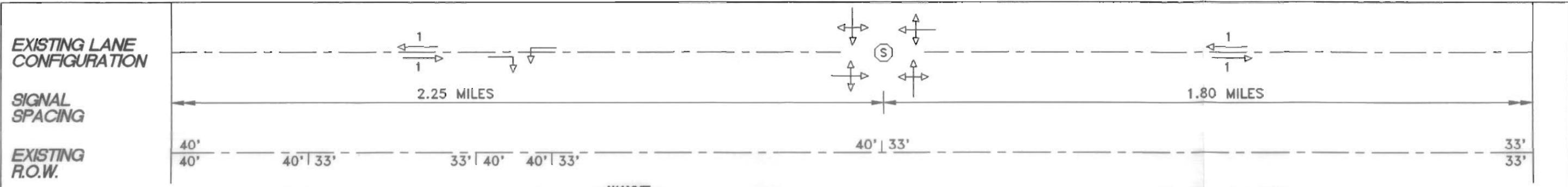
LEGEND	
	= EXISTING RIGHT OF WAY
60'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





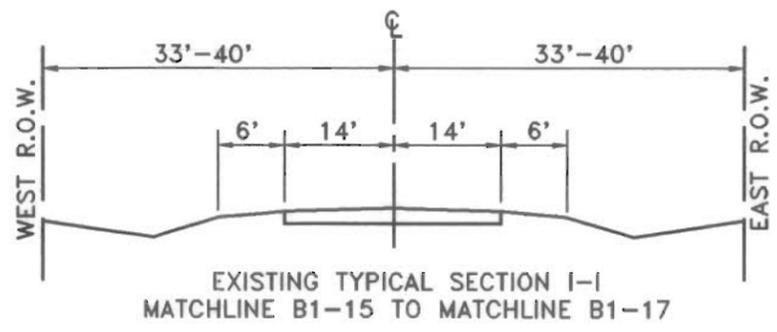
ST. CHARLES

WAYNE

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

- * Existing right/left turn lanes Northbound Dunham Road to Royal Fox Trail.



LEGEND	
	= EXISTING RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
	= STOP SIGN
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



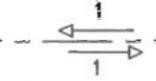
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

SIGNAL SPACING

EXISTING R.O.W.

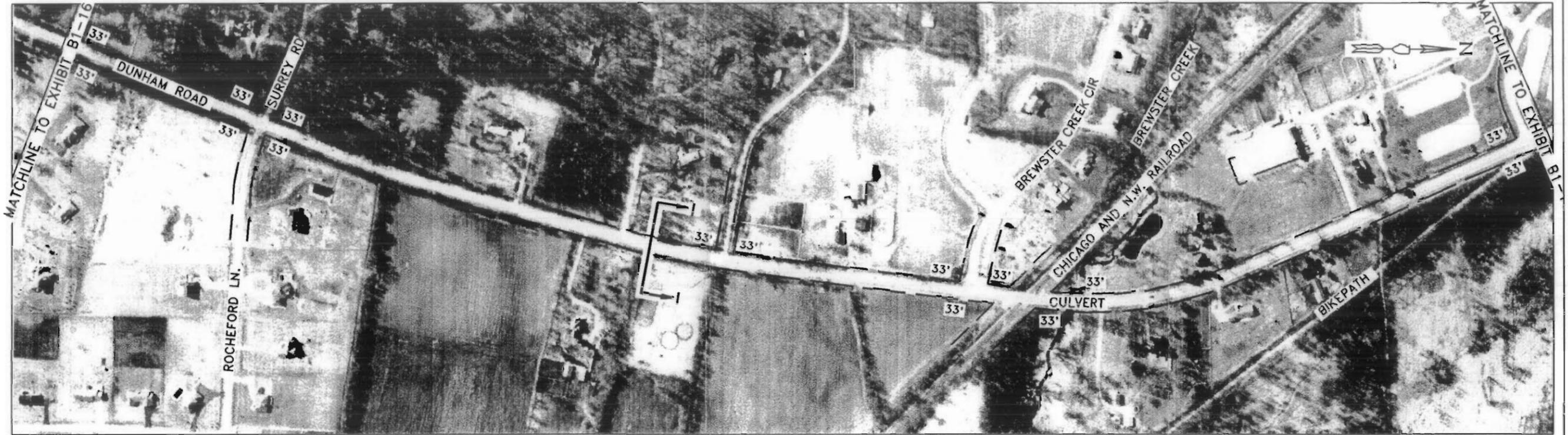


1.80 MILES

33'
33'

33'
33'

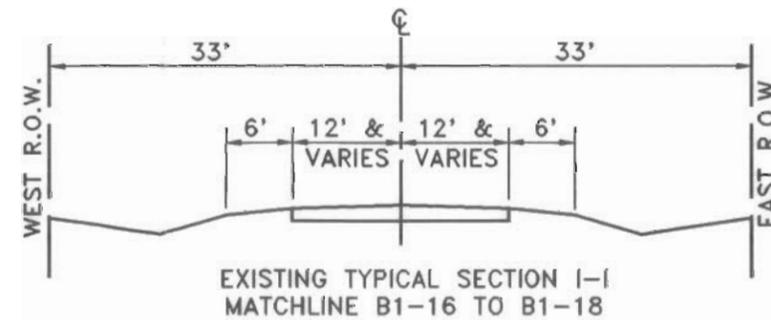
WAYNE



WAYNE

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

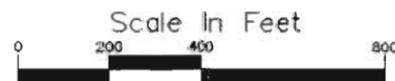


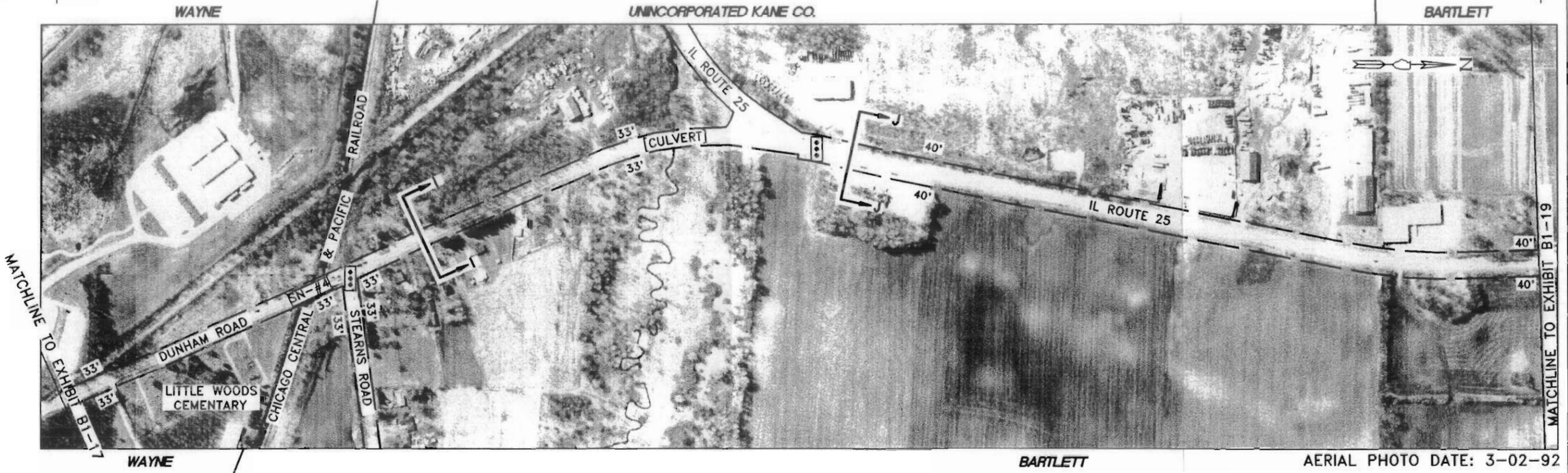
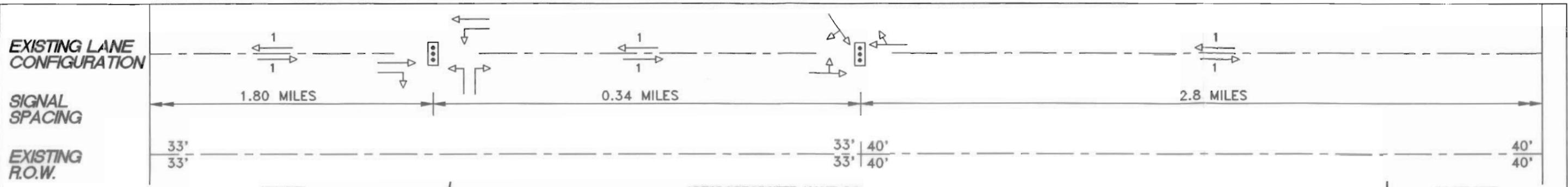
LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

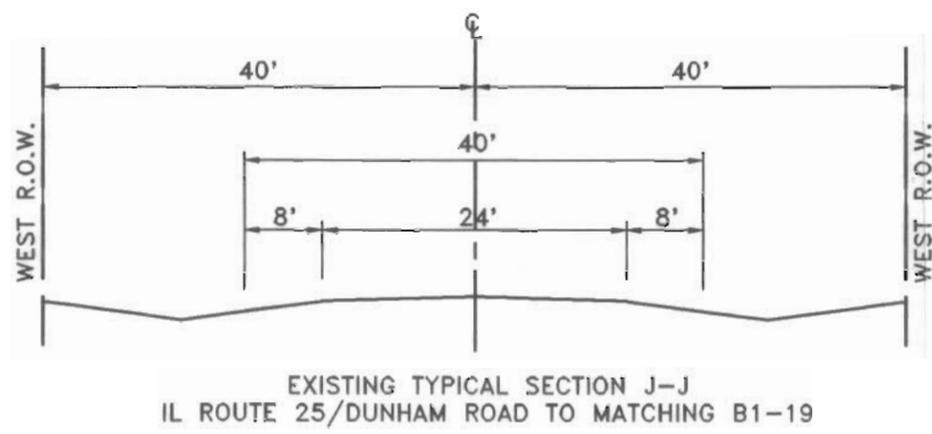
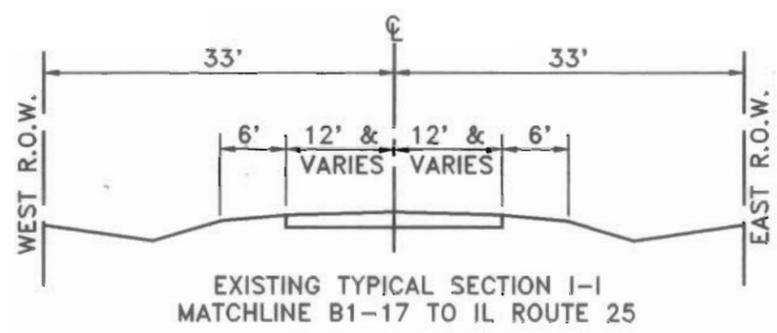


Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF EXISTING CONDITIONS:
 SN - 9 = Structure number 045-3021

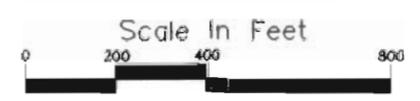


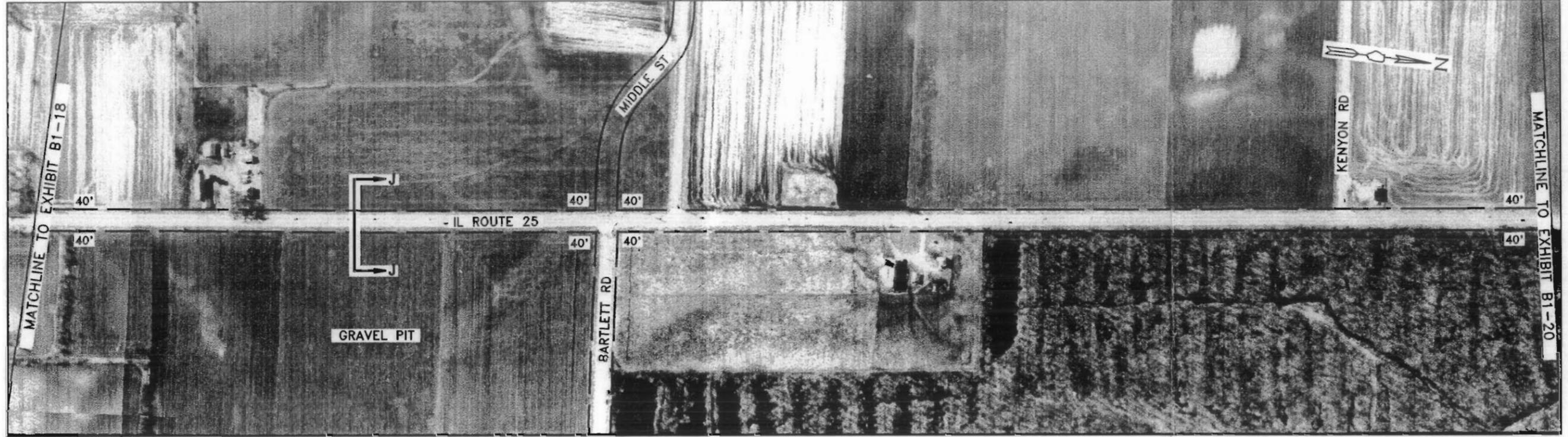
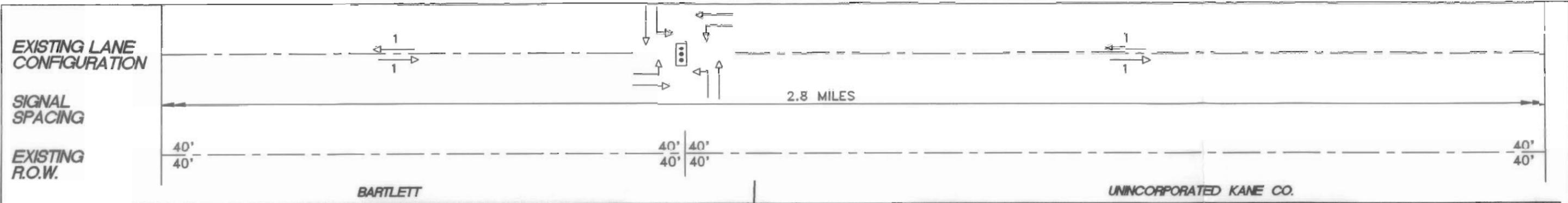
LEGEND	
—	= EXISTING RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
⋮	= EXISTING TRAFFIC SIGNAL
SN-#	= EXISTING STRUCTURE NUMBER
# ← # →	= EXISTING TRAFFIC LANE CONFIGURATION
- - -	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



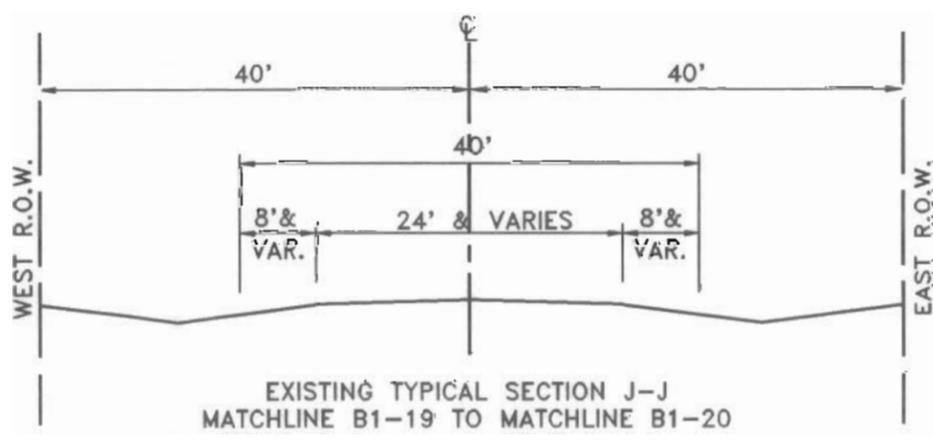
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the
Illinois Department of Transportation





AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF EXISTING CONDITIONS:

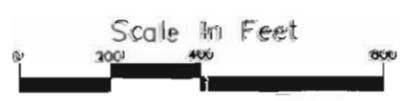


LEGEND	
	= EXISTING RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



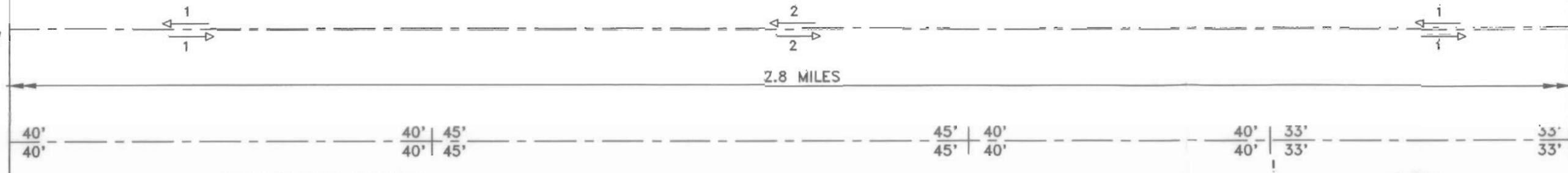
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

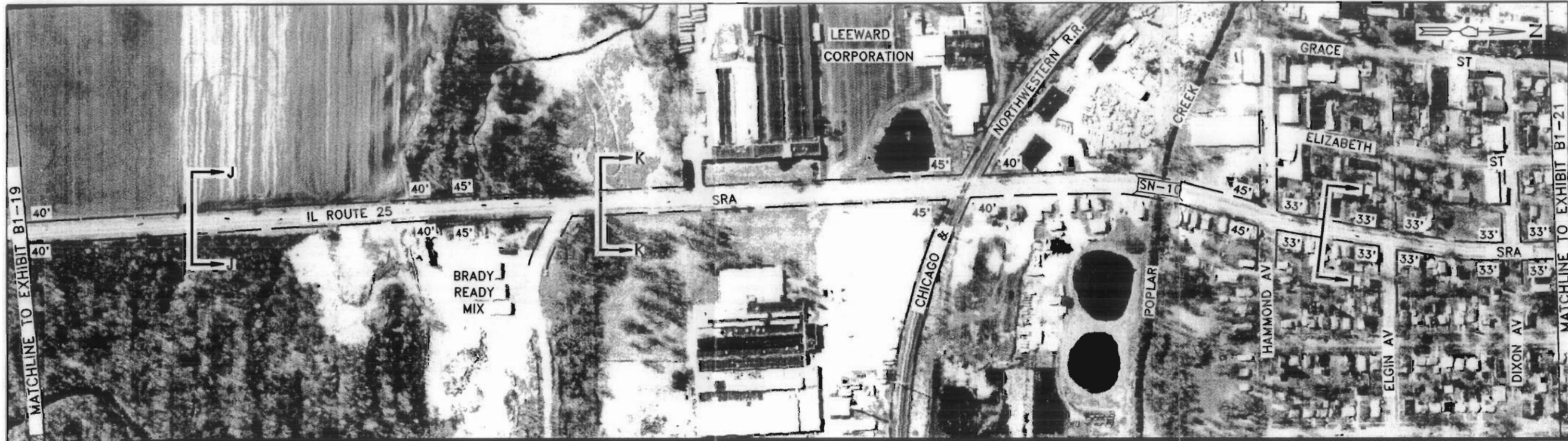
SIGNAL SPACING

EXISTING R.O.W.



UNINCORPORATED KANE CO.

ELGIN

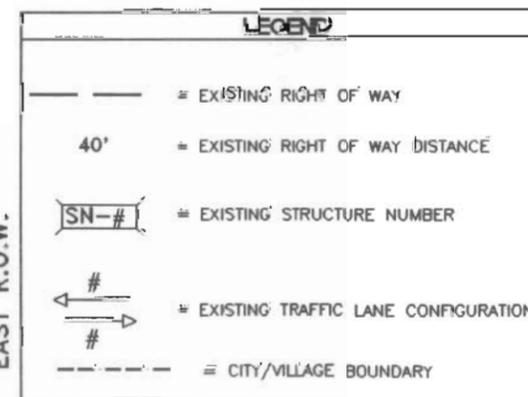
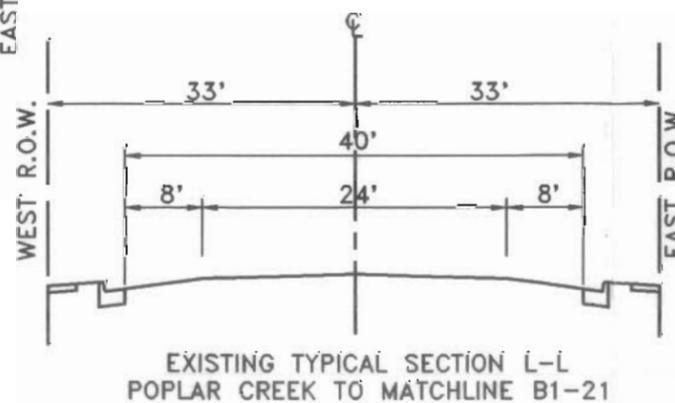
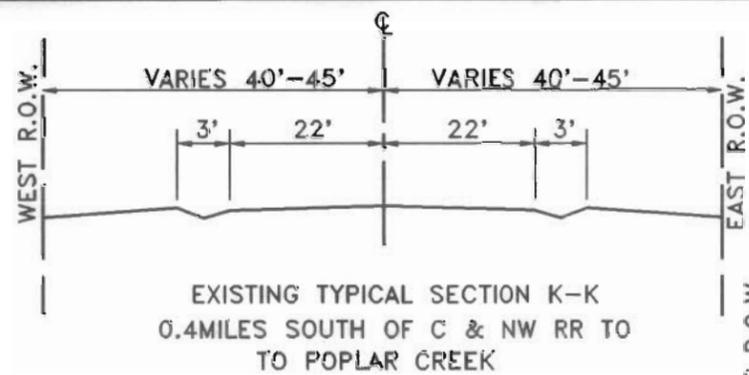
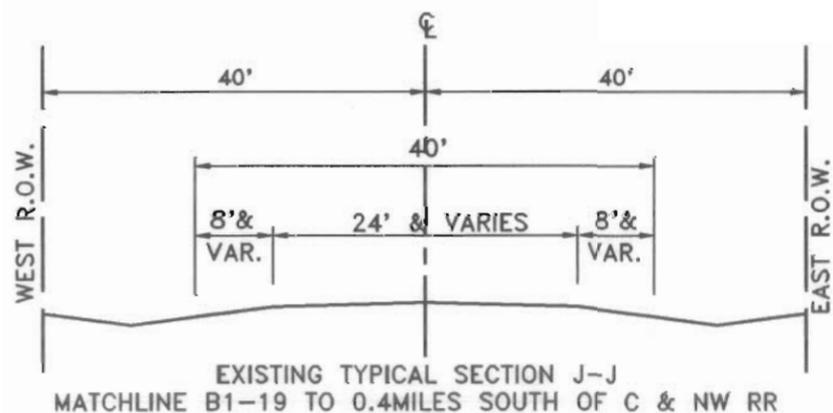


UNINCORPORATED KANE CO.

AERIAL PHOTO DATE: 3-02-92
ELGIN

DESCRIPTION OF EXISTING CONDITIONS:

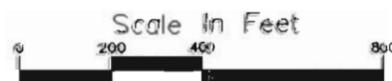
SN - 10 = Structure number 045-500



IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



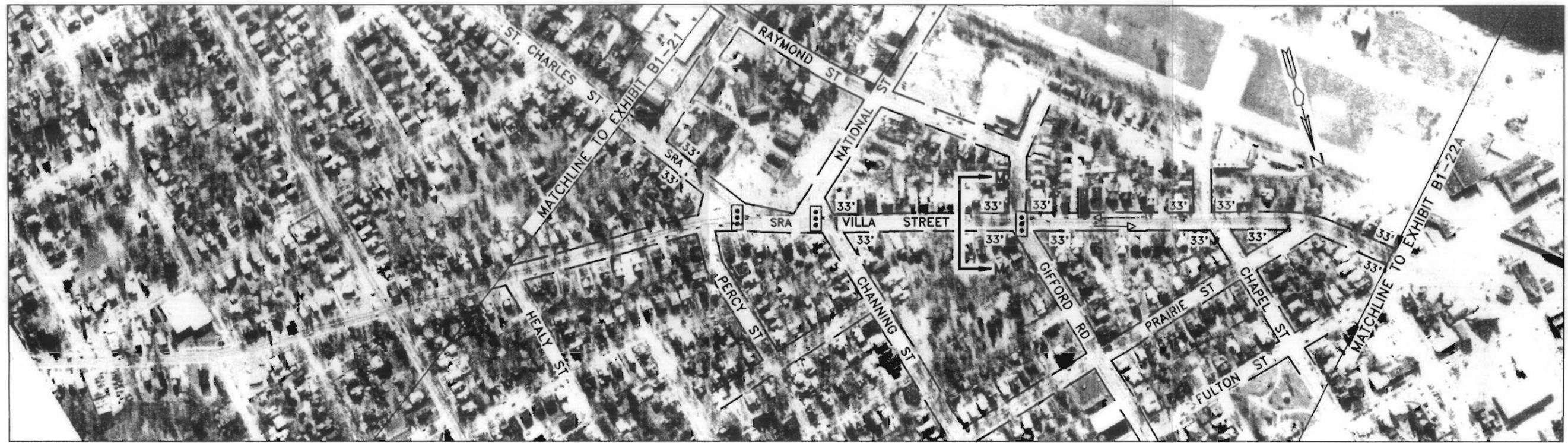
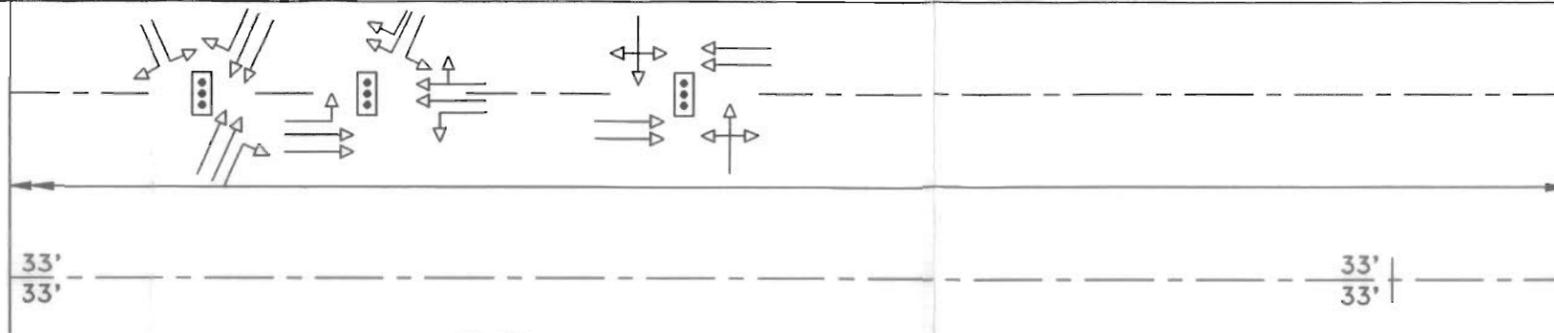
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

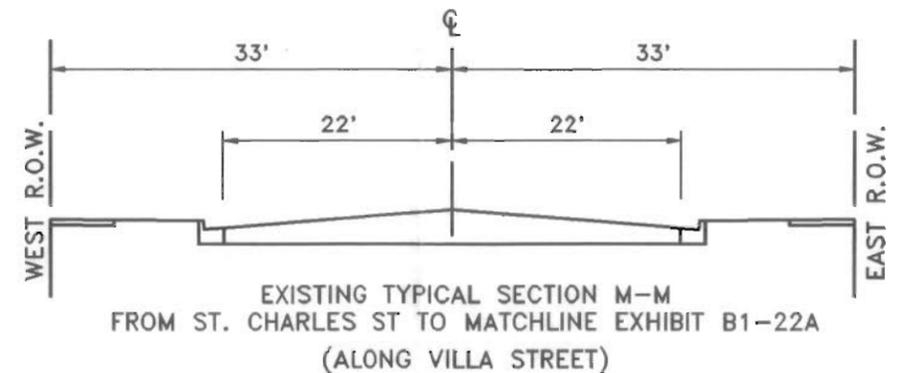
SIGNAL SPACING

EXISTING R.O.W.



AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF EXISTING CONDITIONS:



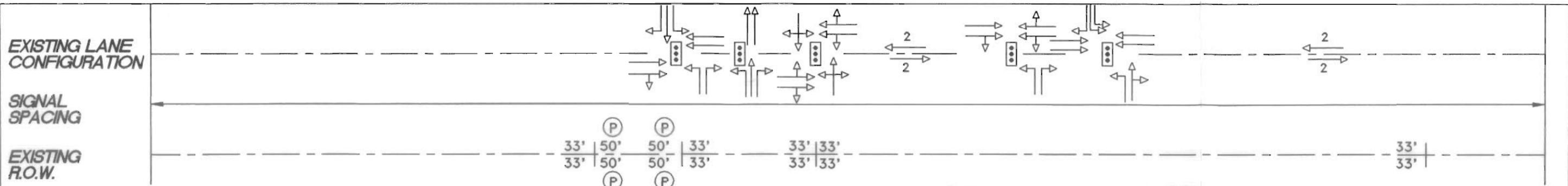
LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION

ST. CHARLES ST. / VILLA ST. - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

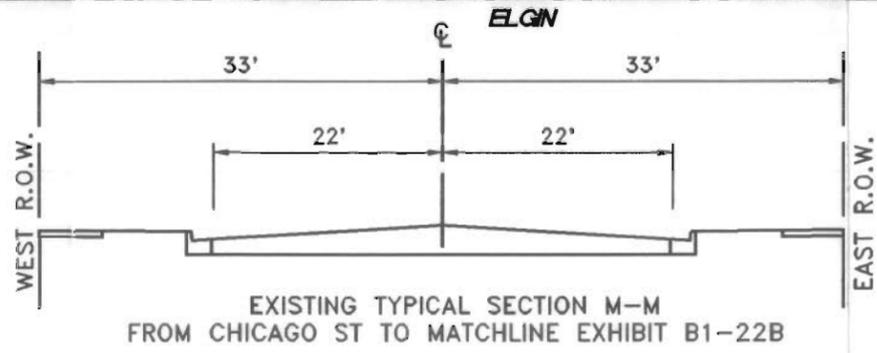
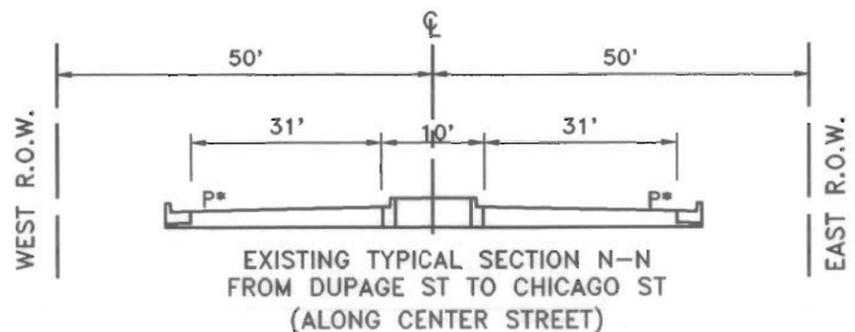




AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF EXISTING CONDITIONS:

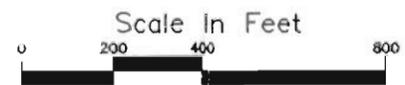
- * The intersection of Kimball Street and Dundee Avenue construction is tentatively scheduled for 1997.
- P* Parking is allowed in the curb lane along Center Street.

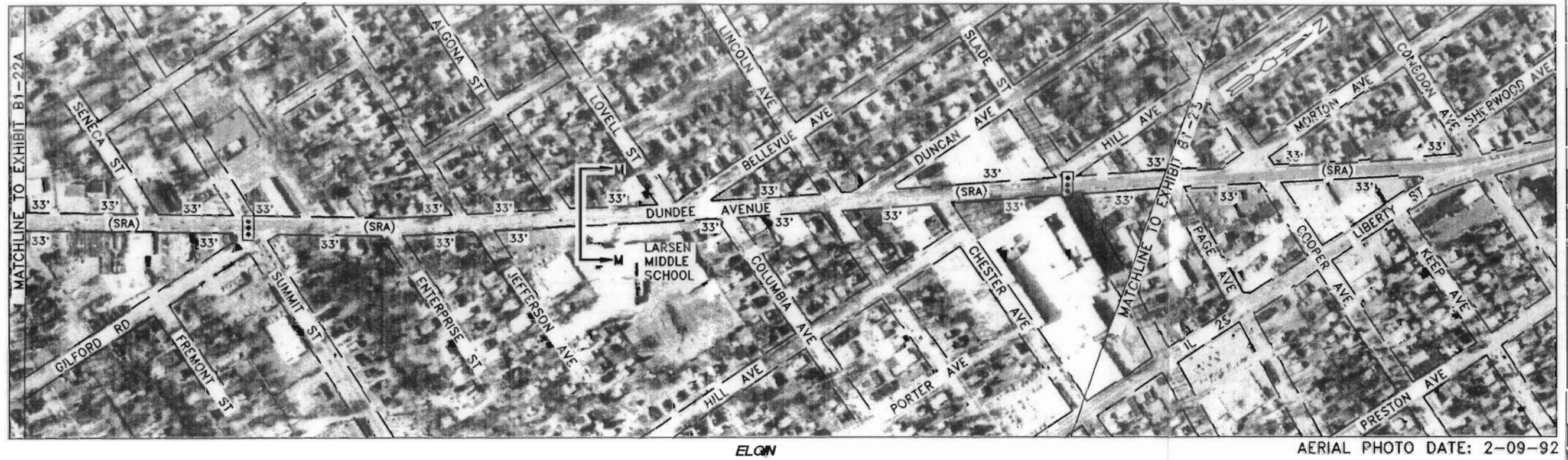
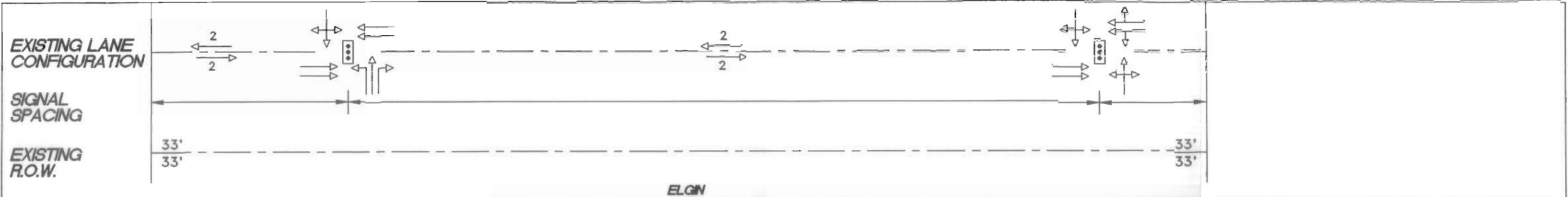


LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= ON STREET PARKING

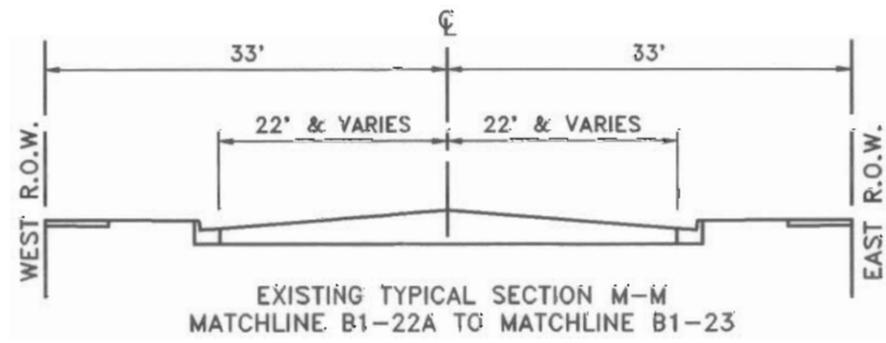
VILLA ST. / CENTER ST. / DUNDEE AVE. - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





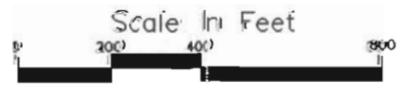
DESCRIPTION OF EXISTING CONDITIONS:



LEGEND	
	= EXISTING RIGHT OF WAY
	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= RIGHT IN / RIGHT OUT

DUNDEE AVENUE / ILLINOIS ROUTE 25 - EXISTING CONDITIONS

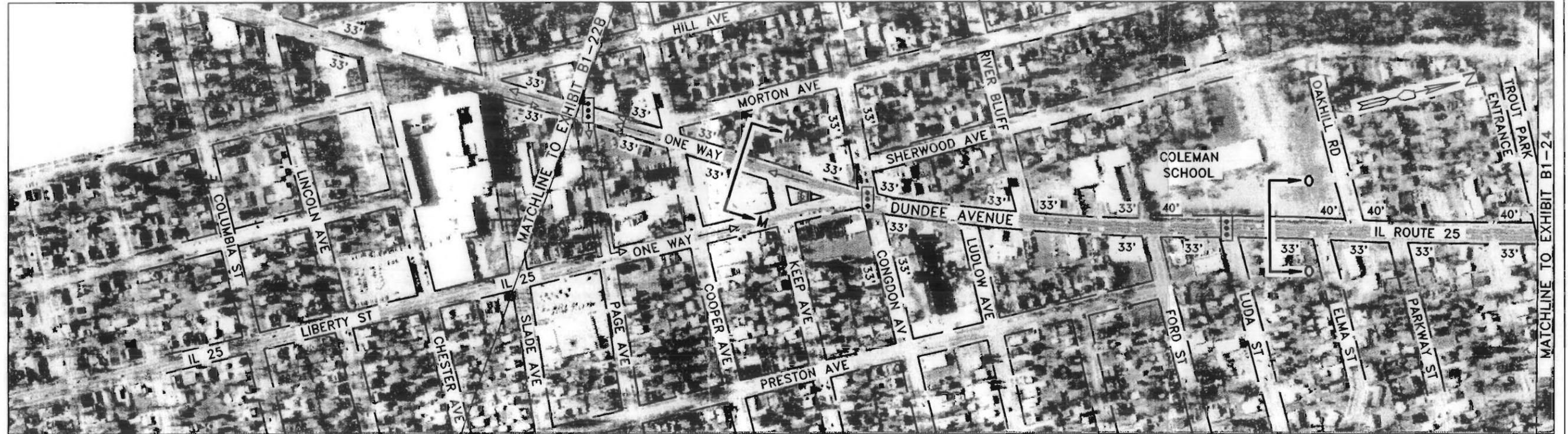
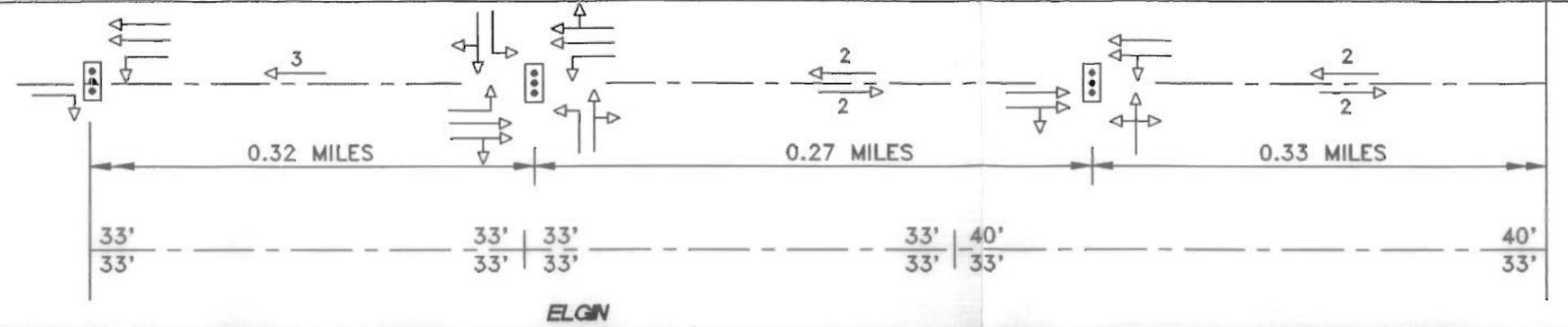
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

SIGNAL SPACING

EXISTING R.O.W.

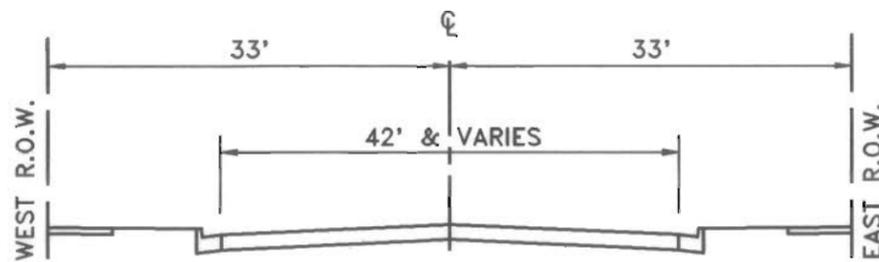


ELGN

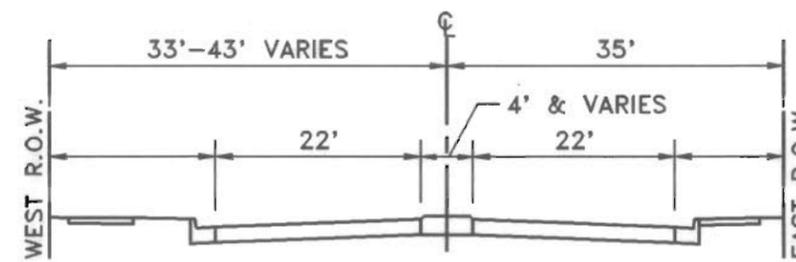
AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF EXISTING CONDITIONS:

* Dundee Avenue is one way southbound from Congdon Avenue to Page Avenue.



EXISTING TYPICAL SECTION M-M
MATCHLINE B1-22B TO CONGDON AVENUE



EXISTING TYPICAL SECTION O-O
CONGDON AVENUE TO MATCHLINE B1-24

LEGEND	
	= EXISTING RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION

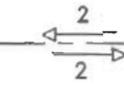
IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION



1.68 MILES

SIGNAL SPACING

EXISTING R.O.W.

50'
50'

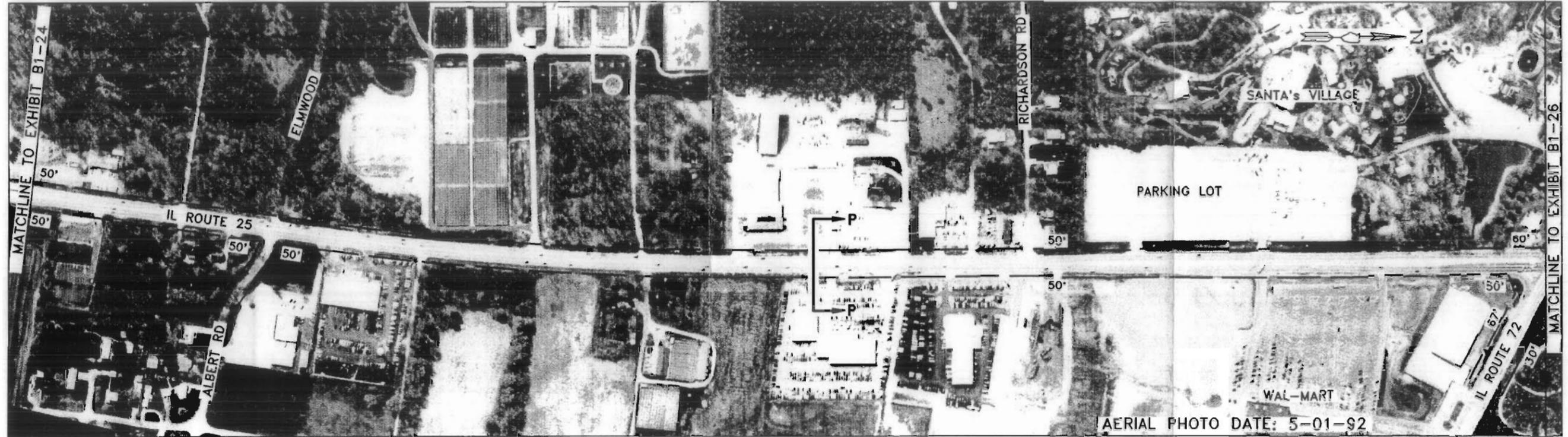
50'
50' | 50'

50'-60' VARIES

60'
50'

MAX MCGRAW WILDLIFE FOUNDATION

UNINCORPORATED KANE COUNTY



AERIAL PHOTO DATE: 5-01-92

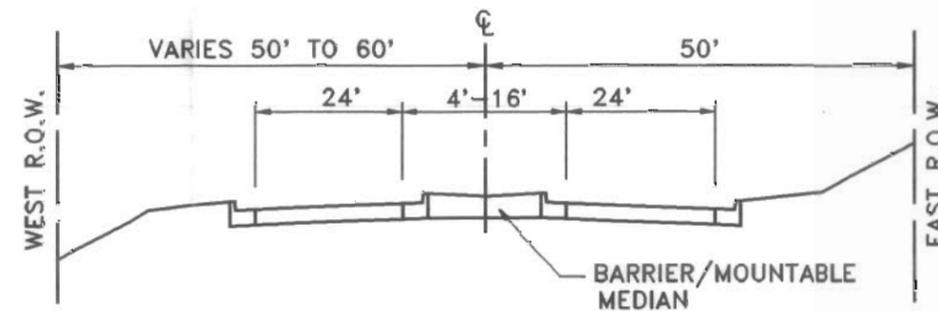
UNINCORPORATED KANE COUNTY

MAX MCGRAW WILDLIFE FOUNDATION

UNINCORPORATED KANE COUNTY

DUNDEE

DESCRIPTION OF EXISTING CONDITIONS:



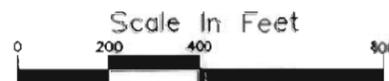
EXISTING TYPICAL SECTION P-P
MATCHLINE B1-24 TO MATCHLINE B1-26

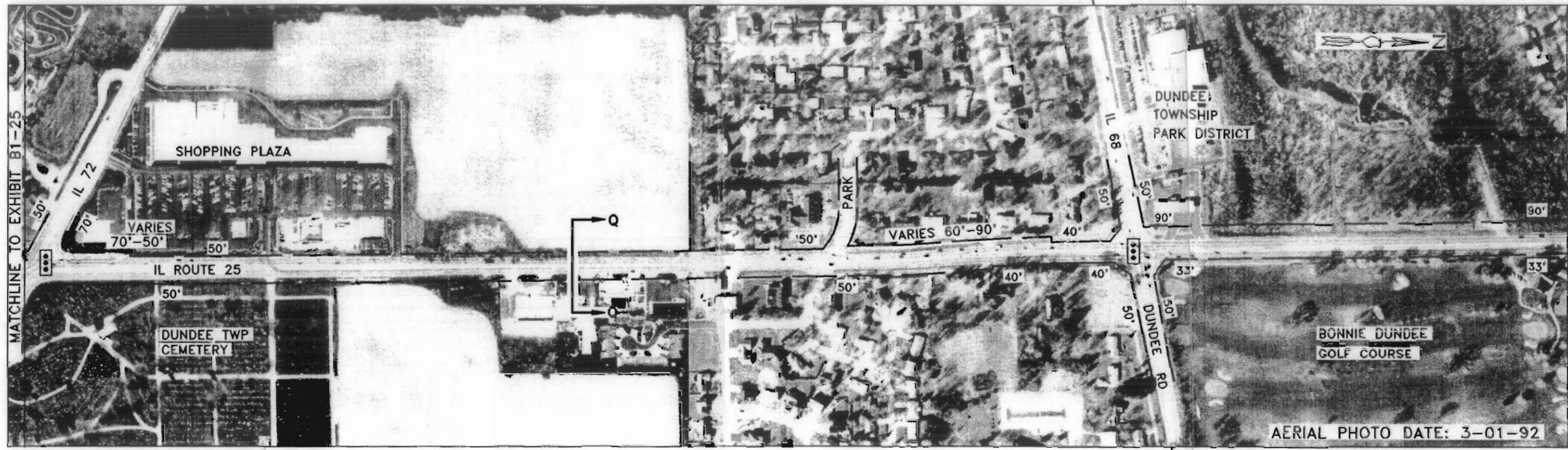
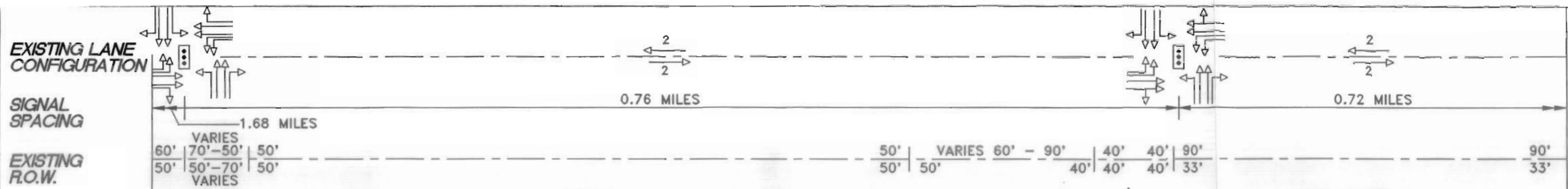
LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY / VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

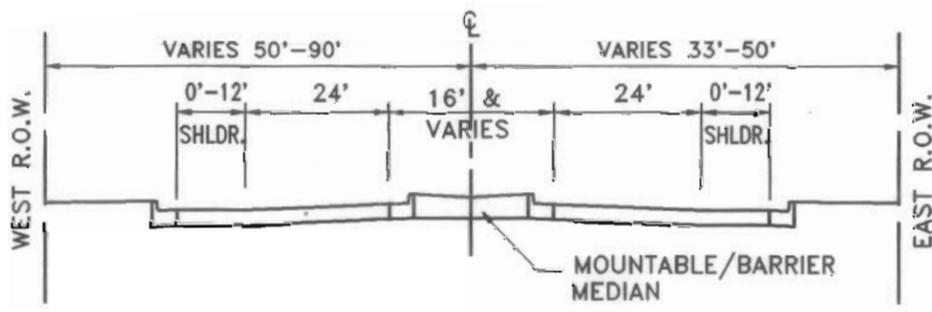


Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF EXISTING CONDITIONS:



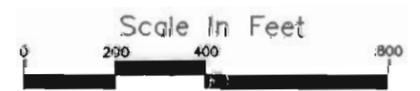
EXISTING TYPICAL SECTION Q-Q
MATCHLINE B1-25 TO MATCHLINE B1-27

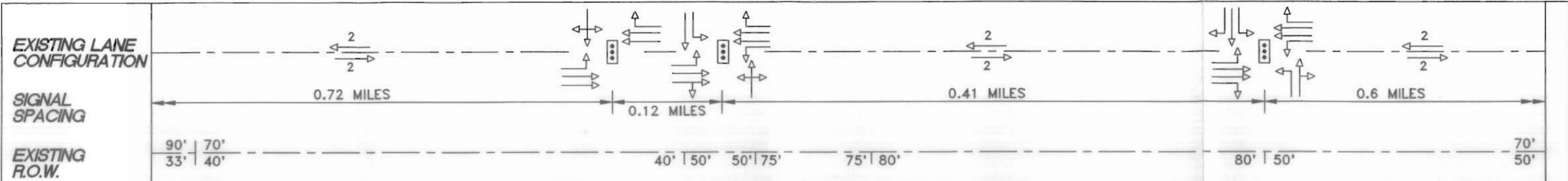
LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= CITY / VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



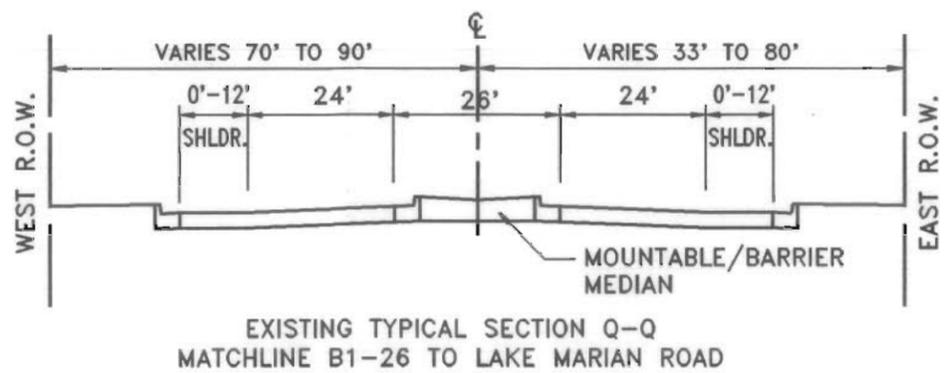
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the
 Illinois Department of Transportation



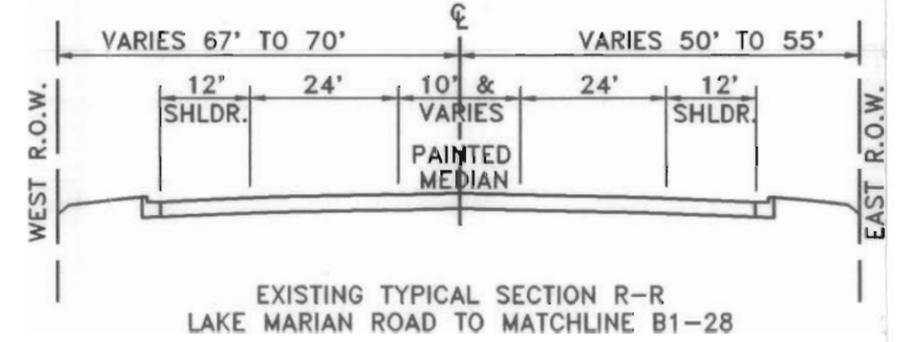


AERIAL PHOTO DATE: 3-01-92

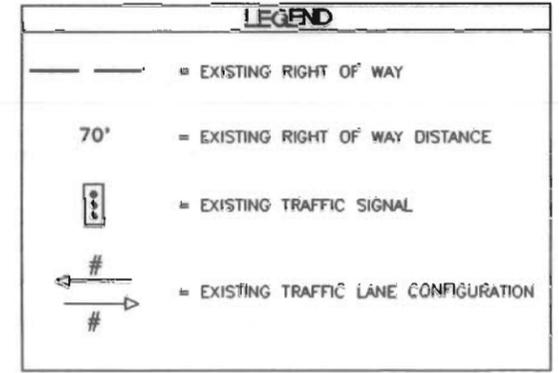
DESCRIPTION OF EXISTING CONDITIONS:



EXISTING TYPICAL SECTION Q-Q
MATCHLINE B1-26 TO LAKE MARIAN ROAD



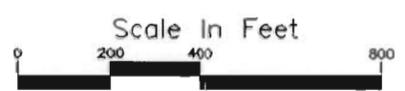
EXISTING TYPICAL SECTION R-R
LAKE MARIAN ROAD TO MATCHLINE B1-28



IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS



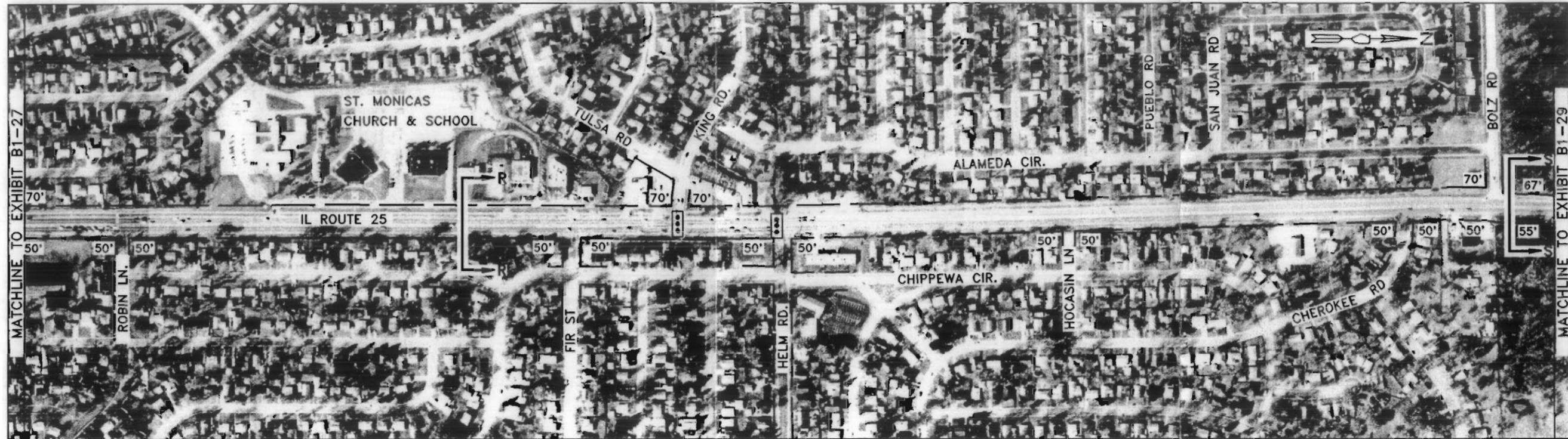
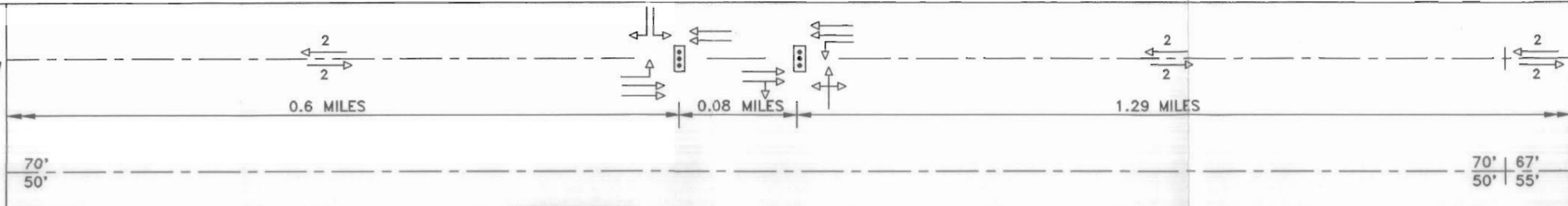
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



EXISTING LANE CONFIGURATION

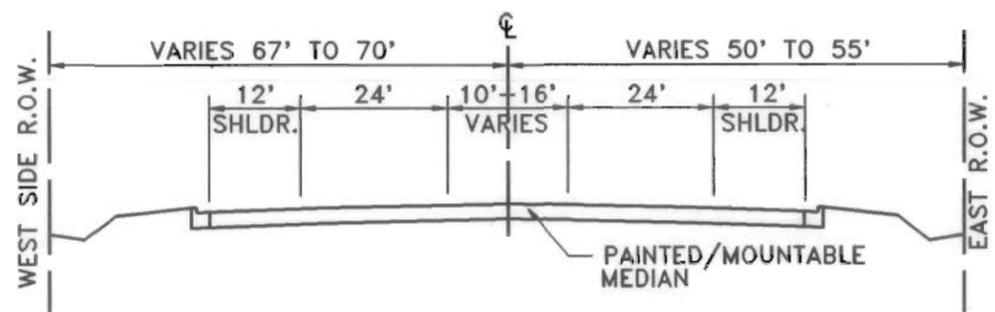
SIGNAL SPACING

EXISTING R.O.W.

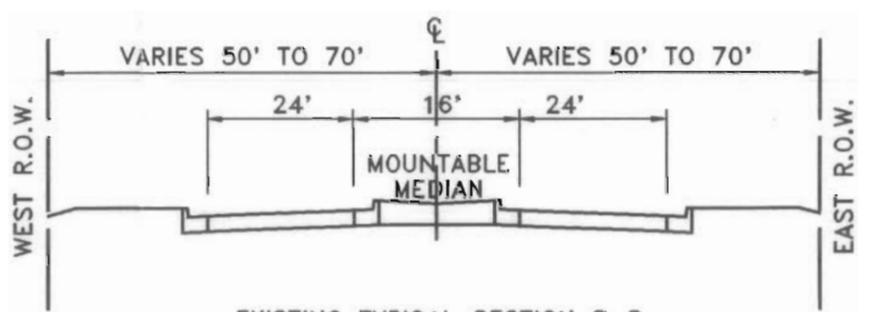


AERIAL PHOTO DATE: 3-01-92 BARRINGTON HILLS

DESCRIPTION OF EXISTING CONDITIONS:



EXISTING TYPICAL SECTION R-R MATCHLINE B1-27 TO BOLZ ROAD

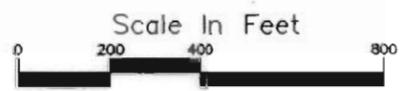


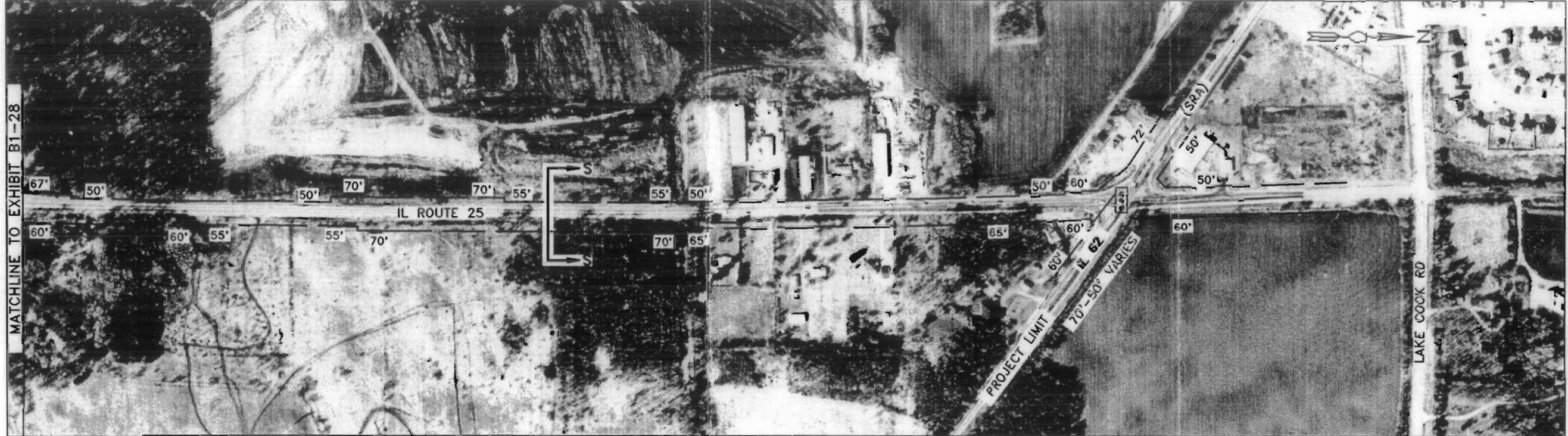
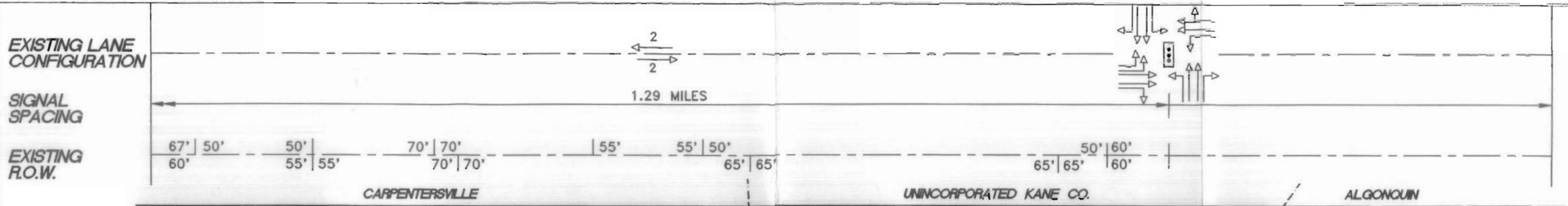
EXISTING TYPICAL SECTION S-S BOLZ ROAD TO MATCHLINE B1-29

LEGEND	
---	= EXISTING RIGHT OF WAY
70'	= EXISTING RIGHT OF WAY DISTANCE
[Signal Symbol]	= EXISTING TRAFFIC SIGNAL
[Lane Symbol]	= EXISTING TRAFFIC LANE CONFIGURATION
- - -	= CITY / VILLAGE BOUNDARY

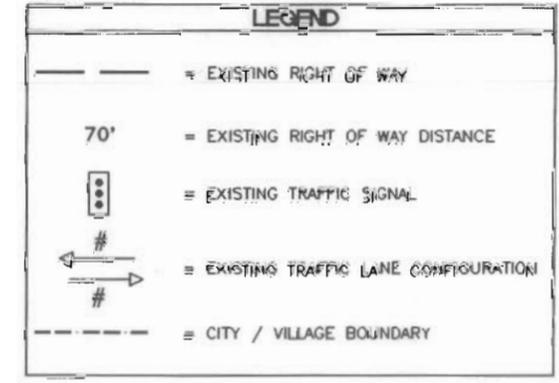
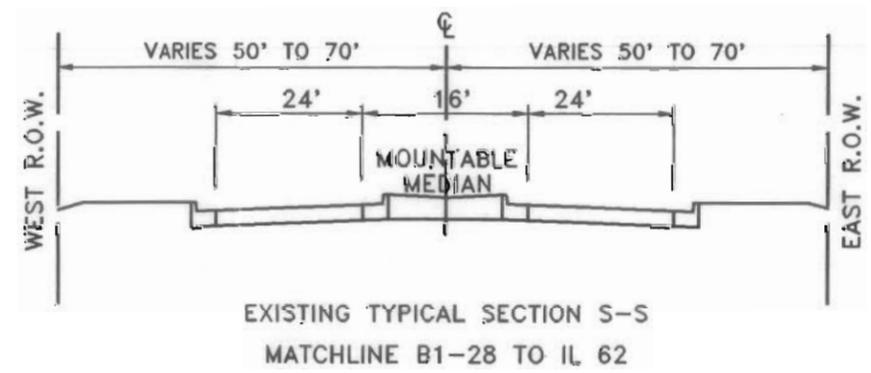
IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





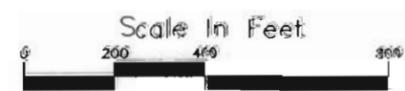
DESCRIPTION OF EXISTING CONDITIONS:



IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - EXISTING CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation



CORRIDOR PLANNING FRAMEWORK

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



STRATEGIC
REGIONAL
ARTERIAL
PLANNING STUDY

Planning Considerations

Long-range planning for the Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue (Illinois Route 25 Corridor) SRA corridor takes into account many factors. These factors include adjacent regional transportation plans, land use, route type, community concerns, public transit, proposed development, and the SRA design concept. The ultimate plan will be an attempt to develop a balance among all of these design considerations to best address the transportation needs of the region.

This chapter outlines the planning considerations that influenced the recommended improvements for the Illinois Route 25 corridor. Several tables at the end of the chapter provide additional information not specifically addressed in the text. Table III-1 lists the 2010 desirable route characteristics for a suburban SRA route, Table III-2 lists the suburban SRA design criteria, Table III-3 provides the existing and projected average daily traffic, Table III-4 lists the future transportation improvement projects and Table III-5 lists the previous and current planning studies along the corridor.

Recommended improvements have been developed based on the following planning considerations:

- Functional classification of the road
- SRA route design considerations and characteristics
- Long-range forecast of highway traffic activity along Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue
- Other planned transportation improvements within, crossing, or near the Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue corridor
- Long-range land use plans for the communities along Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue
- Existing safety and traffic operational problems along Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue
- Existing environmental conditions and constraints
- Community concerns, interests, and attitudes

The concept for Illinois Route 25 corridor was developed after compiling the information mentioned above and includes recommendations for the following design and operation concerns:

- The number of continuous through lanes in each direction along Illinois Route 25 corridor.

- Right-of-way requirements
- Locations of signalized intersections
- Locations and specifications of special intersections
- Access management
- The need for and locations of special or unique highway solutions
- Transit and pedestrian access improvements

Functional Classification

The Illinois Route 25 SRA corridor is classified as a suburban route for the entire length. The desirable cross-section is three continuous lanes in each direction, separated by a raised median for access control, as illustrated on Figure III-1.

Route Design Considerations

The desirable right-of-way for a suburban SRA is 120 to 150-feet of right-of-way. Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue corridor has existing right-of-way varying between 66 and 190-feet. A full listing of desirable suburban SRA characteristics appears in Table III-1. The Suburban SRA Roadway Design Criteria are listed in Table III-2.

The 2010 Transportation Network

I-88 has full interchange with Farnsworth Avenue at the south end and I-90 has full interchange with Illinois Route 25 near the northern end of this corridor. The main purpose of the Illinois Route 25 SRA corridor, in conjunction with the other SRA routes in the area, is to supplement the tollway/expressway system and provide a major through traffic route for north-south travel in eastern Kane County.

The Illinois Route SRA corridor is intersected by eight SRA routes. At the north end of the route it is crossed by Illinois Route 62, Illinois Route 68 and Illinois Route 72. Stearns Road ends at Dunham Road which is an SRA Route. Kirk Road is intersected by SRA routes Illinois Route 64, Illinois Route 38 & Illinois Route 56. Farnsworth Avenue ends at Ogden Avenue which is an SRA route.

2010 Traffic Models

Chicago Area Transportation Study (CATS) provided Dames & Moore/MCE with a raw travel demand model output for the years 1990 and 2010. The model, which was run for this study assumed full build-out of all proposed SRA routes to SRA design standards. The 2010 transportation network assumptions are, however, consistent with CATS 2010 Transportation System Development (TSD)

CORRIDOR PLANNING FRAMEWORK - cont'd

Plan Update in all other respects. The data were modified by Dames & Moore/MCE, in consultation with CATS, to produce the 2010 forecasts shown in this report.

The existing (1990) ADT and the projected (2010) ADT volumes are shown in Table III-3.

Other Corridor Planning Activities

Roadway Improvements

Planning information was obtained from IDOT, CATS, Kane County, and the surrounding communities. The long range Transportation Plan for the City of Aurora and Kane County indicates that Farnsworth Avenue will be developed as a five - lane route between U.S. Route 34 and New York Street. Dunham Road, Illinois 25 and Stearns Road will be realigned in the future. Several major intersections along this corridor are being studied by IDOT, Kane County and local communities to improve through traffic movement.

City and Village Comprehensive Plans

Villages and cities along Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue SRA corridor provided comprehensive plans detailing information on local transportation plans, zoning maps, and community objectives. These plans are listed in Table III-5.

Transit Improvements

The Illinois Route 25 Corridor has limited existing transit, mostly concentrated at the north & south end of this corridor. Pace routes are the exclusive transit routes in this corridor. The Future Agenda for Suburban Transportation, published jointly by Metra and Pace, was reviewed for planning projects that affect the corridor. There are several transit improvements proposed along this corridor.

Future Land Use and Development

The communities along Illinois Route 25 Corridor are among the most rapidly developing in theregion. Major commercial developments are expected at Kirk Road and U.S. Route 25. There are several residential developments along entire length of this corridor. The mature commercial region at the north end will experience sporadic growth.

CORRIDOR PLANNING FRAMEWORK - cont'd

Planning Framework and Recommendations

The planning framework factors described above were used to determine the best possible alternates for the Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue corridor. Applying the information obtained from the communities, counties, and other agencies to the planning framework criteria led to the recommended improvements discussed in the next chapter. The topics discussed in the next chapter include cross-sections and geometrics, operations, access management, public transit, and short term alternates. These topics are briefly defined below.

Cross Section and Geometrics

These considerations include the number and width of through lanes, type and width of median, shoulder descriptions, intersection configurations, and intersection signalization. In addition, topics such as structure modifications and additional structures are examined.

Operations

Operational concerns pertain to projected traffic volume, proposed speed limit, and predicted capacity and level of service. This analysis also examines accident rates and contains general solutions for areas indicated as high accident locations.

Access Management

Since vehicles entering and leaving the SRA route will have a large impact on the flow of traffic, access management plays an important role in future improvements. This analysis evaluates methods to coordinate access for vehicles entering and leaving the corridor. On-street parking concerns are also addressed.

Public Transit

Bus and rail service enhancements as well as pedestrian and bicycle accessibility are considered an important objective of the SRA system. Recommendations for such improvements are included in the long range design concepts.

Table III-1
2010 Desirable Route Characteristics
Rural Strategic Regional Arterial

Right-of-Way Width	188' - 212'
Level of Service (Peak Hour)/Design Speed	C / 60 mph
Number of Through Lanes	2 in each direction; 12' width; with provision for future expansion to 6 total lanes
Median Width	50' - 74'
Right Turns	Turn lanes at major intersections
Left Turns	Turn lanes at all intersections
Shoulders	10' right paved; 6' left paved
Curbs	No
Parking	Not recommended
Cross Street Intersections	Permitted. Stop sign control for cross street. Crossovers permitted at ½ mile spacing.
Curb Cut Access	Right-in / Right-out design
Transit	Bus pull-off and shelter. Express bus service and signal pre-emption potential.
Number of Traffic Signals Per Mile	2, signals spaced ½ mile apart until frontage roads are constructed.
Signalization	Fully-actuated
Freight: Radii	WB 60; Standard
Vertical Clearances	New structures: 16'-3" Existing structures: 14'-6"
Loading	Off-street loading

* Adapted from SRA Design Concept Report, HB & A, Inc.

**Table III-2
Rural SRA Roadway Design Criteria**

Horizontal Alignment	
Minimum Design Speed	60 mph
Minimum Stopping Sight Distance	525'
Minimum Radius Horizontal Curve	1350'
Maximum Degree of Curvature	4° 15'
Maximum Superelevation	6%
Minimum Length of Superelevation	
- Four Lane With Probability of Six Lanes	234'
- Six Lane Section	258'
Horizontal Clearance	Compatible with Design Speed
Vertical Alignment	
Maximum Grades	5%
Length Crest Vertical Curve	Compatible with Design Speed
Length Sag Vertical Curve	Compatible with Design Speed
Vertical Clearance (Minimum New Construction)	16'-3"
Vertical Clearance (Minimum Reconstruction)	14'-6"

* Adapted from SRA Design Concept Report, HB & A, Inc.

**TABLE III-3
EXISTING AND PROJECTED AVERAGE DAILY TRAFFIC
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62**

LOCATION	1990 ADT	2010 ADT
U.S. Route 34 to Montgomery Road	250	18,000
Montgomery Road to Fifth Avenue	-	31,000
Fifth Avenue to New York Street	-	38,000
New York Street to Liberty Street	-	41,000
Liberty Street to Sheffer Road	-	45,000
Sheffer Road to Indian Trail Road	-	47,000
Indian Trail Road to E-W Tollway I-88	-	61-59,000
E-W Tollway I-88 to Bilter Road	23,000	53,000
Bilter Road to Illinois Route 56	23,000	44,000
Illinois Route 56 to Wilson Street	24,000	39,000
Wilson Street to Fabyan Parkway	22,900	44,000
Fabyan Parkway to Illinois Route 38	20,100	37,300
Illinois Route 38 to Illinois Route 64 (North Ave)	22,700	34,000
Illinois Route 64 to Army Trail Road	7,600	30,000
Army Trail Road to Stearns Road	7,300	28,000
Stearns Road to Illinois Route 25	10,900	25,000
Illinois Route 25 to W. Bartlett Rd/Middle Rd	14,000	28,000
W. Bartlett Rd/Middle Rd to Bluff City Blvd	14,900	25,000
Bluff City Boulevard to Villa Street *	8,000	30,000
Villa Street to Chicago Street *	15,000	30,000
Chicago Street to Illinois Route 58 (Summit St)*	20,000	42,000
Illinois Route 58 (Summit St) to Congdon Ave*	18,100	37,000
Congdon Avenue to I-90 N-W Tollway	18,900	36,000

**TABLE III-3
EXISTING AND PROJECTED AVERAGE DAILY TRAFFIC
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62**

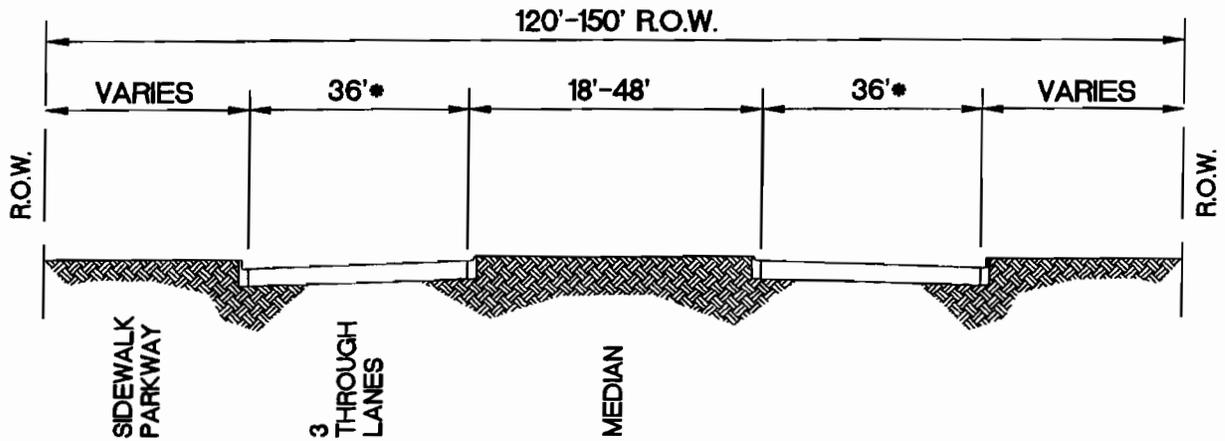
LOCATION	1990 ADT	2010 ADT
I-90 N-W Tollway to Illinois Route 72	22,200	37,000
Illinois Route 72 to Illinois Route 68	22,700	47,000
Illinois Route 68 to Lake Marian Road	21,100	35,000
Lake Marian Road to Helm Road	13,800	42,000
Helm Road to Illinois Route 62	13,800	30,000

*Elgin alternate route on St. Charles Street, Villa Street, and Dundee Avenue

Table III-4
Summary of Previous and Current Planning Studies
Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue

Study, Plan, or Report	Source	Status as of 1994
Transportation Planning Studies • CATS 2010 Transportation System Development Plan	CATS	Official
Land Use and Comprehensive Plans • Zoning Map (1990) • Zoning Map (1993) • Zoning Map (1993) and Future Land Use Plan (1983) • Zoning Map (1993) and Comprehensive Land Use Plan (1989) • Zoning Map (1993) and Comprehensive Land Use Plan (1984)	Lombard Warrenville Downers Grove Wheaton Aurora	Official Official Official Official Official/Interim
Other Plans and Studies • Comprehensive Transportation Study • Arterial Corridor Development Plan (1985) • Ten Year Comprehensive Road Improvement Plan (1990) • Technical Site Information (1993) • Aurora Comprehensive Plan: Butterfield Project (1991) • Major Development Map (1993) • Illustrative Master Plan Cantera (1991)	Lombard Warrenville DuPage County DOT Fermilab Aurora Aurora Warrenville	— — — — — — —

SUBURBAN SRA ROUTES



STANDARD SUBURBAN SRA CROSS SECTION

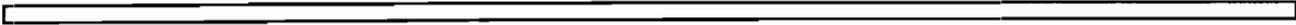
- AN ADDITIONAL 1' COULD BE ADDED TO ACCOMMODATE BICYCLE DEMAND WHERE R.O.W. IS NOT CONSTRAINED OR WHERE PARKWAY WIDTH CAN BE REDUCED.

FIGURE III-1

DESIRABLE SUBURBAN CROSS SECTION

SRA

STRATEGIC
REGIONAL
ARTERIAL
PLANNING STUDY



RECOMMENDED IMPROVEMENTS

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



STRATEGIC
REGIONAL
ARTERIAL
PLANNING STUDY



RECOMMENDED IMPROVEMENTS

This chapter describes recommended improvements in design, operation, access management, and public transit for each study section of the Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue (Illinois Route 25) corridor. Several tables at the end of the chapter provide additional information not specifically addressed in the text. Right-of-way requirements are summarized in Table IV-1. Table IV-2 provides estimates of construction cost for the various improvements. Intersection and arterial levels of service are shown in Tables IV-3 and IV-4, respectively.

Section 1 - Farnsworth Avenue -U.S. Route 34 to New York Street

Exhibit C1-01 to Exhibit C1-03

Section 1 of Farnsworth Avenue begins at U.S. Route 34 (Ogden Avenue) in Kendall County, and continues north to New York Street in Kane County. This section passes through unincorporated Kendall and Kane Counties and the City of Aurora. Jurisdiction over this route segment is the City of Aurora.

Cross-Section and Geometrics

The proposed cross-section for this section is modified from the standard suburban SRA cross-section. Although the Design Concept Report shows a desirable six-lane cross-section for a suburban SRA route, it is not feasible to provide the six-lane cross-section because of the existing land use. The proposed four-lane cross-section will provide two 12-foot lanes in each direction, with a 18-foot barrier median and adjacent barrier curb south of Fifth Avenue and 12-foot flush median from Fifth Avenue to New York Street. Farnsworth Avenue is discontinuous between Montgomery Road and Binder Street. A new alignment will be required as shown in Exhibit C1-02 to provide a continuous roadway. Farnsworth Avenue is recommended to be realigned with Hefenrechter Road north of U.S. Route 34 (see Exhibit C1-01). The proposed cross section can be accommodated within 80 to 100 feet of right-of-way, requiring zero to 50 feet of additional right-of-way (see Exhibits C3-01 through C3-03). Table IV-1 provides an estimate of the total right-of-way required and the associated cost. To accommodate the proposed roadway cross-section it will be necessary to reconstruct structures number 045-3138 and 045-3137 over the Waubonsie Creek.

The intersection of realigned Farnsworth Avenue and U.S. Route 34 is major intersection of two SRA routes. The proposed lane configuration for north-south legs would consist of a left turn lane and two through lanes, with a shared right turn lane. The east-west legs would consist of dual left turn lanes, two through lanes, and a right turn lane on U.S. Route 34. Capacity analysis shows that a level of service "D+" can be achieved.

RECOMMENDED IMPROVEMENTS - cont'd

The intersection of Farnsworth Avenue and New York Street is another major intersection. The proposed lane configuration for north and south legs will consist of a left turn lane, three through lanes, with a shared right turn lane. The east and west legs will consist of a left turn lane, a through lane and a through lane with shared right turn lane. Capacity analysis shows that a level of service "E" can be achieved.

Operations

The 2010 projected Average Daily Traffic (ADT) in this section varies from 18,000 vehicles per day (vpd) to 38,000 vpd. The proposed speed limits for this section ranges from 35 mph to 40 mph. A capacity analysis was performed for this section by applying the projected ADT to the recommended model. The overall arterial level of service by segment from U.S. Route 34 to Binder Street is a "B," and from Binder Street to New York Street is a "C."

The intersection of realigned Farnsworth Avenue and U.S. Route 34 is a major intersection of two SRA routes and warrants signalization in the 2010 time frame. The intersections of Farnsworth Avenue with Montgomery Road, Simm Street and Fifth Avenue have been identified as potential locations for future signalizations. According to the SRA Design Concept Report, signalized intersections should be spaced a minimum of ¼ mile apart.

Access Management

Areas where access consolidation may be appropriate are identified in this report. Local agencies will be responsible for taking the lead role in implementing zoning and access policies which are consistent with the SRA planning report. Access consolidation should be applied, where possible, with median breaks allowing left turn access. In this section of Farnsworth Avenue access to future developments and individual residences should be limited to "Right Turn Only" movements. Access management needs to be enforced between Fifth Avenue and New York Street. Several cross street accesses to Farnsworth Avenue have been evaluated. The cross street access to Farnsworth Avenue should be limited to through streets. No parking is allowed in this section.

Public Transit

One Pace Bus route operates along Farnsworth Avenue and one route intersects Farnsworth Avenue in this section. Bus turnouts are recommended to be provided at the bus stops.

RECOMMENDED IMPROVEMENTS - cont'd

Section 2 - Farnsworth Avenue - New York Street to Illinois Route 56

Exhibit C1-03 to Exhibit C1-07

Section 2 of Farnsworth Avenue extends from New York Street to Illinois Route 56. This section passes through the City of Aurora and unincorporated Kane County. It intersects Interstate-88 (East-West Tollway) and two major arterials, New York Street and Illinois Route 56. Jurisdiction over this section belongs to the City of Aurora.

Cross-Section and Geometrics

The proposed cross-section for this section of the route is modified from the standard suburban SRA cross-section and consists of four to six lanes. The specific lane and median widths are as follows:

New York Street to Mountain street:

Two 12-foot lanes in each direction with a 12-foot flush median and adjacent curb and gutter, except over Burlington Northern Railroad, a four foot raised median is recommended.

Mountain street to South of Molitor Road:

Two 12-foot lanes in each direction with 14-foot flush median and an adjacent curb and gutter.

South of Molitor Road to Illinois Route 56:

Three 12-foot lanes in each direction with 18-foot barrier median and adjacent curb and gutter.

The proposed cross-section can be accommodated within 80 to 120 feet of right-of-way, requiring up to 34-feet of additional right-of-way (see Exhibits C3-03 through C3-07). To accommodate the proposed roadway cross-section it will be necessary to modify structures number 045-6012, 045-6110 and 045-6010 the bridges over the Indian Creek. The bridge over Burlington Northern (structure number 045-6052) and the bridge over I-88 (East-West Tollway, structure number 045-9944) does not require any modifications to accommodate the proposed roadway cross-section.

The intersection of Illinois Route 56 and Farnsworth Avenue is major intersection of two SRA routes. The lane configurations for north-south legs will consist of dual left turn lanes, three through lanes and a right turn lane. The east-west legs will consist of dual left turn lanes, two through lanes, and a right turn lane. Capacity analysis shows that a level of service "C+" can be achieved with the proposed lane configuration.

RECOMMENDED IMPROVEMENTS - cont'd

Operations

The 2010 projected ADT for Section 2 varies from 41,000 vpd to 61,000 vpd. The proposed speed limit for this section is 35 mph to 45 mph. A capacity analysis was performed for this section by applying the projected ADT to the recommended model. The overall arterial level of service from New York Street to Front Road is a "C," and from Front Road to Illinois Route 56 is a "B."

The intersections of Farnsworth Avenue and Dearborn Avenue, Mountain Street and Bilter Road have been identified as potential locations for future signalization. All signalized intersections will maintain a minimum of a quarter mile spacing.

Access Management

Areas where access consolidation may be appropriate are identified in this report. Local agencies will be responsible for taking the lead role in implementing zoning and access policies which are consistent with the SRA planning report. In this section of Farnsworth Avenue access to future developments and individual residences should be limited to "Right Turn Only" movements. Access to Farnsworth Avenue should be limited to right-in/right-out at Kane Street and Fenton Street. A pedestrian activated signal is recommended for the bike path crossing at Marshall Boulevard.

Public Transit

Pace currently operates three bus routes along this section. The proposed transit recommendations include future bus turnouts at existing bus stop locations.

Section 3 - Kirk Road - Illinois Route 56 to Illinois Route 64

Exhibit C1-7 to Exhibit C1-14

Section 3 of Illinois Route 25 Corridor is Kirk Road extending from Illinois Route 56 (Butterfield Road) to Illinois Route 64 (North Avenue), both of which are SRA routes. This section passes through unincorporated Kane County, the Cities of Batavia, Geneva, and St. Charles, and Fermi National Laboratory. This section intersects two other SRA Routes, Fabyan Parkway and Illinois Route 38 (Roosevelt Road). Kane County and the City of St. Charles have jurisdiction over this section of Kirk Road.

RECOMMENDED IMPROVEMENTS - cont'd

Cross-Section and Geometrics

The proposed cross-section for Section 3 is based on the desirable suburban SRA cross-section presented in the Design Concept Report. The proposed cross-section from Illinois Route 56 to Illinois Route 64 provides three 12-foot lanes in each direction with a 18-foot barrier median and adjacent concrete curb and gutter. Up to 50' of additional right-of-way will be required to accommodate the cross-section (see Exhibits C1-07 through C1-14).

Kane County has completed Phase I and II studies for this section of Kirk road and the construction has begun to implement the improvements between Illinois Route 38 and Illinois Route 64. The proposed roadway cross-section is consistent with the County's plans. To accommodate the proposed roadway cross-section it will be necessary to reconstruct structure number 045-3096 over the Chicago & Northwestern Railroad.

The intersection of Kirk Road and Fabyan Parkway is a major intersection of two SRA routes. The lane configurations for north-south legs will consist of dual left turn lanes, three through lanes and a right turn lane. The east-west legs will consist of dual left turn lanes, two through lanes, and a right turn lane. Capacity analysis shows that a level of service "C+" can be achieved with the proposed lane configuration.

The intersection of Kirk Road and Illinois Route 38 (Roosevelt Road) is a major intersection of two SRA routes. The lane configuration will consist of dual left turn lanes, three through lanes and a right turn lane on all four legs of the intersection. Capacity analysis shows that a level of service "C" can be achieved with the proposed lane configuration.

Kirk Road and Illinois Route 64 is another major intersection of two SRA routes. The lane configuration will consist of dual left turn lanes, three through lanes and a right turn lane on all four legs of the intersection. Capacity analysis shows that a level of service "B" can be achieved with the proposed configuration.

Operations

The 2010 projected ADT for Section 3 varies between 34,000 vpd and 41,000 vpd. The proposed speed limit for this section is 45 mph. A capacity analysis was performed for this section by applying the projected ADT to the recommended model. The overall arterial level of service from Illinois Route 56 to Illinois Route 64 is an "A."

RECOMMENDED IMPROVEMENTS - cont'd

Access Management

Areas where access consolidation may be appropriate are identified in this report. Local agencies will be responsible for taking the lead role in implementing zoning and access policies which are consistent with the SRA planning report. In this section of Kirk Road access to future developments and individual residences should be limited to "Right Turn Only" movements. Access to Kirk Road should be limited to right-in/right-out at Geise Road and Chillem Drive. Access consolidation should be applied, where possible, with median breaks allowing left turn access. In this section of the corridor, median breaks allowing full access should be provided at Geise Road, Douglas Road, Southwest lane, Hill Road, Avehrill Circle, Geneva Drive, Cumberland Parkway, Swenson Avenue, Stern Avenue, Production Drive and Illinois Avenue. Access to future developments and individual residences should be limited to "Right Turn Only" movements.

Public Transit

Two Pace Bus routes operate within this section of Kirk Road. The bus turnouts are recommended at the bus stop locations.

Section 4 - Kirk Road/Dunham Road - Illinois Route 64 to Illinois Route 25

Exhibit C1-14 to Exhibit C1-18

Section IV of this corridor extends from Illinois Route 64 to the intersection with Dunham Road, at which point the corridor follows Dunham Road to Illinois Route 25. This section passes through St. Charles, Wayne, Bartlett, and unincorporated Kane County. This section intersects one SRA route, which is Stearns Road. This section is within Kane County's jurisdiction.

Cross-Section and Geometrics

The proposed cross-section for this section of the route is modified from the standard suburban SRA cross-section. Although the Design Concept Report shows a desirable six-lane cross-section for a suburban SRA route, a four-lane cross-section will accommodate the projected 2010 average daily traffic on Dunham Road. The proposed four-lane cross-section will provide 11- 12 foot lanes in each direction. Different median width is recommended based on the proposed right-of-way as follows:

From Illinois Route 64 to Royal Fox Drive through the Village of St. Charles 18-foot landscape barrier median with adjacent barrier curb and gutter. A four foot painted median is recommended along Dunham Road from Royal Fox Drive to Chicago Central & Pacific Railroad through the Village of Wayne. North of Chicago Central & Pacific Railroad an 18-foot median is recommended.

RECOMMENDED IMPROVEMENTS - cont'd

The proposed cross-section will require 20-feet of additional right-of-way (see Exhibits C1-14 through C1-18). The structure number 045-3021 over the Chicago Central & Pacific Railroad will have to be modified to accommodate the proposed cross-section. The Chicago Central Northwestern Railroad intersects Dunham Road at grade. The typical pavement markings at railroad highway grade crossings should be installed.

The intersection of Dunham Road and Army Trail Road is a major intersection. The proposed lane configuration for the north-south legs will consist of a left turn lane, two through lanes with a shared right turn lane. No additional lanes are recommended on the east-west legs. The lane configuration for the east-west legs will consist of a through lane with shared left & right turn lanes.

Realignment of Illinois Route 25, Dunham Road and Stearns Road is in the feasibility study phase to form a major intersection of three SRA routes as shown on the Exhibit C1-18. The proposed lane configuration for the east-west leg will consist of a left turn lane, two through lanes, and a right turn lane. The south leg consists of dual left turn lanes, two through lanes with a shared right turn lane. The north leg will consist of a left turn lane, two through lanes, and a right turn lane.

Operations

According to CATS forecasts, the 2010 projected ADT for Section IV varies from 28,000 vpd to 30,000 vpd. The proposed speed limits for this section ranges from 40 mph to 45 mph. A capacity analysis was performed for this section by applying the projected ADT to the recommended model. The overall arterial level of service from Illinois Route 64 to Dunham Road/ Illinois Route 25 is a "B."

The intersections of Dunham Road with Fox Chase Drive, and Army Trail Road have been identified as potential locations for future signalization. Although these intersections may not warrant signalization in the 2010 time frame, signals may be warranted for the ultimate improvement. According to the SRA Design Concept Report, signalized intersections should be spaced a minimum of ¼ mile apart.

Access Management

Areas where access consolidation may be appropriate are identified in this report. Local agencies will be responsible for taking the lead role in implementing zoning and access policies which are consistent with the SRA planning report. In this section of Kirk Road access to future developments and individual residences should be limited to "Right Turn Only" movements. Access consolidation should be applied, where possible, with median breaks allowing left turn access. In this section of the corridor, median breaks allowing full access should be provided at Foxfield Road, St. Charles School entrance, Private Road, Royal Fox Drive, Royal Fox Trail and Surrey Road.

RECOMMENDED IMPROVEMENTS - cont'd

Public Transit

There is no public transportation in this section.

Section 5 - Illinois Route 25/St. Charles Street/Villa Street/Center Street/Dundee Avenue - Dunham Road to Congdon Avenue

Exhibit C1-18 to Exhibit C1-23 and B1-ALT1 TO B1-ALT4

Section 5 of Illinois Route 25 extends from the Dunham Road/Illinois Route 25 intersection to Bluff City Boulevard, where an alternate route uses St. Charles Street, Villa Street, Central Street, and Dundee Avenue in Elgin, and merges back to Illinois Route 25. This section passes through Bartlett, Elgin, and unincorporated Kane County.

Cross-Section and Geometrics

The proposed cross-section for this section of the route is modified from the standard suburban SRA cross-section. Although the Design Concept Report shows a desirable six-lane cross-section for a suburban SRA route, it is not feasible to provide the six-lane cross-section because of the existing land use. A four lane urban SRA cross-section can be adopted for this section. This section consists of different routes with diversified physical characteristics. The proposed cross-section for various segments is as follows:

Dunham Road to Poplar Creek:

The proposed cross-section consists of two 12-foot lanes in each direction with a 18-foot barrier median and adjacent curb and gutter.

Poplar creek to Villa Street:

The proposed cross-section consists of two 11-foot lanes in each direction with a 11-foot flush median and adjacent curb and gutter.

The proposed cross-section along Villa Street and Center Street consist of two through lanes in each direction with a painted/barrier median and adjacent curb and gutter. No modifications are recommended to the cross-section along Dundee Avenue from Center Street to Congdon Avenue. The proposed cross-section will require 14-feet of additional right-of-way (see Exhibits C1-18 through C1-23 & C1-ALT1 through C1-ALT4). The structure number 045-5000 over Poplar Creek has to be modified to accommodate the proposed roadway cross-section. Removal of on-street parking is recommended between Bluff City Boulevard and Villa Street on St. Charles Street. Parking can be accommodated on residential driveways and cross streets.

RECOMMENDED IMPROVEMENTS - cont'd

The intersection of Dundee Avenue, Illinois Route 25, and Congdon Avenue is a major and complex intersection. It involves the merging of Dundee Avenue to Illinois Route 25 at an obtuse angle. This arrangement essentially functions as a triangular one-way couple with Page Avenue. The north leg on Illinois Route 25 consists of two through lanes to Dundee Avenue, and a left turn lane to Congdon Avenue. The Dundee Avenue leg has three lanes one-way southwest bound between Congdon Avenue and Page Avenue. Page Avenue is one-way eastbound between Dundee Avenue & Illinois Route 25. From Page Avenue traffic can access southbound or northbound Illinois Route 25, and Illinois Route 25 has three lanes one-way northbound between Page Avenue and Congdon Avenue. At Congdon Avenue, there are two through northbound lanes and a right turn lane. The east leg of the intersection on Congdon Avenue consists of dual left turn lanes to Dundee Avenue with a shared right turn lane. The recommended improvements to this intersection include:

The west leg of Congdon Avenue is recommended to be closed at the intersection (Exhibit C1-Alt 3). The recommended improvements also include converting Liberty Street one-way southbound from

Congdon Avenue to Page Avenue. Page Avenue is recommended to be a two-way street between Dundee Avenue and Liberty Street. Dundee Avenue is recommended to accommodate two-way traffic.

Operations

According to CATS forecasts, the 2010 projected ADT for the Section V alternative ranges from 30,000 vpd to 42,000 vpd. The proposed speed limit for this section is 35 mph. A capacity analysis was performed for this section by applying the projected ADT to the recommended model. The overall arterial level of service by segment from Dunham Road/Illinois Route 25 to Bluff City Boulevard is "B" and from Bluff City Boulevard to Congdon Avenue is "C".

The intersections of St. Charles with Bent Street and Dwight Street have been identified as candidates for future traffic signals. Signals are proposed to be removed at May Street and North Street. The intersection of St. Charles and Villa Street is the major intersection. The predominant traffic movement at this intersection is from St. Charles Street to northwest bound Villa Street, as well as the reverse movement. The northwest leg of Villa Street has dual left turn lanes to southeast bound Villa Street, and dual shared right turn lanes to southbound St. Charles Street. Left turns from northwest bound Villa Street to St. Charles Street should be prohibited. The geometrics and proper signage in the vicinity of the St. Charles/Villa Street/National Street intersection will be very important in managing traffic flow along this alternate route.

Access Management

Full access should be provided at all major commercial and industrial entrances. It is proposed to close access to Arlington Avenue and May Street, Prairie Street, Fulton Street, Geneva Street, and the

RECOMMENDED IMPROVEMENTS - cont'd

west side of Congdon Avenue. The following streets should be limited to right-in/right-out: Dwight Street, Jay Street, Sherman Street, Percy Street, DuPage Street, Franklin Street, Gilford Road and Chester Avenue.

Public Transit

Five Pace Bus routes operate within the downtown Elgin area. The recommended improvements include bus turnout bays at the existing bus stops.

Section 6 -Congdon Avenue to Illinois Route 62

Exhibit C1-23 to Exhibit C1-29

Section 6 merges back to Illinois Route 25 at Congdon Avenue and extends north to IllinoisRoute 62 (Algonquin Road). This section passes through Elgin, East Dundee, Carpentersville, Barrington, and unincorporated Kane County.

Cross-Section and Geometrics

The proposed cross-section from Trout Park entrance (south of Northwest Tollway) to Illinois Route 62 provides three 12-foot lanes in each direction with a 18-foot barrier median and adjacent concrete curb and gutter. From Congdon Avenue to Trout Park entrance is two 11-foot lanes in each direction with 11-foot flush median. The proposed cross-section will require 40-feet of additional right-of-way (see Exhibits C1-23 through C1-29). Table IV-1 provides an estimate of the total right-of-way required and the associated cost. The structure number 045-0042 over the Chicago Central & Pacific Railroad will have to be modified to accommodate the proposed roadway cross-section.

The intersection of Illinois Route 25 and Illinois Route 72 is a major intersection of two SRA routes. The lane configuration for north-south legs will consist of dual left turn lanes, three through lanes and a right turn lane. The east-west legs consist of a left turn lane, two through lanes, and a right turn lane. Capacity analysis shows that only a level of service "B" can be achieved with the proposed configuration.

The intersection of Illinois Route 25 and Illinois Route 68 is another major intersection of two SRA routes. The lane configuration for the north leg will consist of a left turn lane, three through lanes, with a shared right turn lane. The south leg will consist of a left turn lane, three through lanes and a right turn lane. The east-west legs will consist of a left turn lane, two through lanes, and a right turn lane. Capacity analysis shows that a level of service "E+" can be achieved.

RECOMMENDED IMPROVEMENTS - cont'd

The intersection of Illinois Route 25 and Illinois Route 62 is the intersection of two SRA routes. The lane configuration will consist of dual left turn lanes, three through lanes, and a right turn lane on all four legs of the intersection. Capacity analysis shows that a level of service of "D+" can be achieved with the proposed configuration.

Operations

The 2010 projected ADT for Section 6 varies from 36,000 vpd to 47,000 vpd. The recommended speed limits for this section ranges from 35 mph to 45 mph. A capacity analysis was performed for this section by applying the projected ADT to the recommended model. The overall arterial level of service from the I-90 Tollway to Illinois Route 62 is a "B."

Access Management

All direct access to Illinois Route 25 should be limited to right-in/right-out. Full access should be provided at the Chicago Junior School entrance, Max-McGraw entrance, Milk Pail entrance, Albert Road, Dundee Township Cemetery, Park Street, North Parkway, Ball Avenue, Berkley Street, Robin Lane, Hocaslan Lane, Cherokee Road, and major commercial entrances.

Public Transit

One Pace Bus route operates within this section of Illinois Route 25. The recommended improvements include bus turnouts at the following locations:

- a. Northbound along Illinois Route 25 at Illinois Route 72 (Wal-Mart)
- b. Southbound along Illinois Route 25 at Santa's Village
- c. Northbound and southbound along Illinois Route 25 at Meawdowdale Shopping Center

A park & ride facility is also recommended at Illinois Route 25 & I-90 (Northwest Tollway).

Table IV-1
Estimated R.O.W. Requirements for
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62

Section	Intersecting Street	Estimated Additional R.O.W. Required (acres)	Cost Estimate (1995 Dollars)
I	U.S. Route 34	0	\$0.00
I	Montgomery Road	0	\$0.00
I	Farnsworth Ave. Requirements	23.5	\$4,230,000.00
Section I Total		23.5	\$4,230,000.00
II	New York Street	0	\$0.00
II	Indian Trail Road	0	\$0.00
II	Farnsworth Ave. Requirements	7.4	\$1,340,000.00
Section II Total		7.4	\$1,340,000.00
IV	Roosevelt Road	1.15	\$207,000.00
IV	Kirk Road Requirements	0.33	\$80,000.00
Section III Total		1.48	\$287,000.00
IV	Illinois Route 64	0.7	\$126,000.00
IV	Army Trail Road	0	\$0.00
IV	Illinois Route 25	xx	xx
IV	Stearns Road	xx	xx
IV	Kirk/Dunham Rd Requirements	5.2	\$1,600,000.00
Section IV Total		5.9	\$1,726,000.00
V	IL 25/ST. Charles Ave/Villa St/Center St/Dundee Ave Requirements	8.7	\$1,566,000.00
Section V Total		8.7	\$1,566,000.00

Table IV-1
Estimated R.O.W. Requirements for
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62

Section	Intersecting Street	Estimated Additional R.O.W. Required (acres)	Cost Estimate (1995 Dollars)
VI	Dundee Ave/IL 25 Requirements**	7.7	\$1,400,000.00
Section VI Total		7.7	\$1,400,000.00
GRAND TOTAL		54.7	\$10,549,000.00

** Stearns Road & Illinois Route 25 alignment need to be finalized to complete the right-of-way requirement for section IV between Illinois Route 64 & Illinois Route 25.

Table IV-2
Estimate of Construction Cost
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62

Recommended Improvement	Estimated Cost (1995 Dollars)
Section I	
Roadway	\$5,140,000.00
Intersection/Interchange Improvement	\$3,200,000.00
Structure Modification/Replacement	\$711,900.00
Right-of-Way	\$4,230,000.00
Transit Improvement	\$1,000,000.00
Total Estimated Cost for Recommended Improvements - Section I	\$10,170,900.00
Section II	
Roadway	\$8,750,000.00
Intersection/Interchange Improvement	\$5,250,000.00
Structure Modification/Replacement	\$1,110,000.00
Right-of-Way	\$287,000.00
Transit Improvement	\$1,000,000.00
Total Estimated Cost for Recommended Improvements - Section II	\$16,310,000.00
Section III	
Roadway	\$35,220,000.00
Intersection/Interchange Improvement	\$6,000,000.00
Structure Modification/Replacement	\$9,540,000.00
Right-of-Way	\$287,000.00
Transit Improvement	\$1,000,000.00
Total Estimated Cost for Recommended Improvements - Section III	\$52,047,000.00

Table IV-2 (Con't)
Estimate of Construction Cost
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE
U.S. ROUTE 34 TO ILLINOIS ROUTE 62

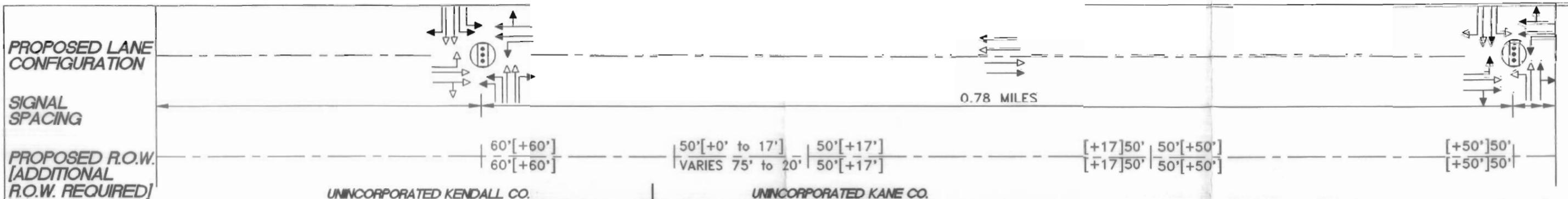
Recommended Improvement	Estimated Cost (1995 Dollars)
Section IV	
Roadway	\$9,500,000.00
Intersection/Interchange Improvement	\$2,900,000.00
Structure Modification/Replacement	\$640,000.00
Right-of-Way	\$1,726,000.00
Transit Improvement	\$500,000.00
Total Estimated Cost for Recommended Improvements - Section IV	\$15,266,000.00
Section V	
Roadway	\$10,040,000.00
Intersection/Interchange Improvement	\$1,450,000.00
Structure Modification/Replacement	\$650,000.00
Right-of-Way	\$1,566,000.00
Transit Improvement	\$500,000.00
Total Estimated Cost for Recommended Improvements - Section IV	\$14,206,000.00
Section VI	
Roadway	\$21,400,000.00
Intersection/Interchange Improvement	\$5,625,000.00
Structure Modification/Replacement	\$4,400,000.00
Right-of-Way	\$1,400,000.00
Transit Improvement	\$330,000.00
Total Estimated Cost for Recommended Improvements - Section V	\$33,155,000.00
Estimated Cost for All Recommended Improvements	\$158,414,800.00

Table IV-3
Intersection Level of Service (2010)
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE

Intersecting Road	N	S	E	W	INT
U.S. Route 34	C	D	C	D+	D+
New York Street	E	E	E	C	E
Illinois Route 56	D	E	A	C	C+
Fabayan Parkway	B	C+	C+	B	C+
Illinois Route 38	C+	C	C	C	C
Illinois Route 64	D	A	B	A	B
Illinois Route 72	B	C+	B	B	B
Illinois Route 68	E+	E	D	E+	E+
Illinois Route 62	D	D	D	C	D+

Table IV-4
Arterial Level of Service (2010)
IL 25/DUNHAM RD/KIRK RD/FARNSWORTH AVENUE

Section	Segment	EB	WB
Section I	U.S. 34 to New York Street	C	C
Section II	New York Street to IL Route 56	C	C
Section III	IL Route 56 to IL Route 64	C	C
Section IV	IL Route 64 to IL Route 25	D	D
Section V	IL Route 64 to IL Route 25	D	D
Section VI	IL Route 64 to IL Route 25	B	B
	Overall	C	C



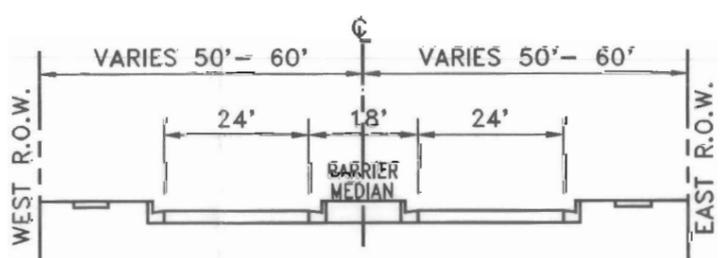
UNINCORPORATED KENDALL CO.

UNINCORPORATED KANE CO.

AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF PROPOSED CONDITIONS:

- SN-1 = STRUCTURE NUMBER 045-3138
- SN-2 = STRUCTURE NUMBER 045-3137
- Modification of these structures will be necessary to accommodate the proposed roadway section.
- * The intersections of US Route 34/Farnsworth Avenue and Montgomery Road/Farnsworth Avenue have been identified as candidate sites for future traffic signals.



PROPOSED TYPICAL SECTION A-A
US ROUTE 34 TO MATCHLINE C1-02

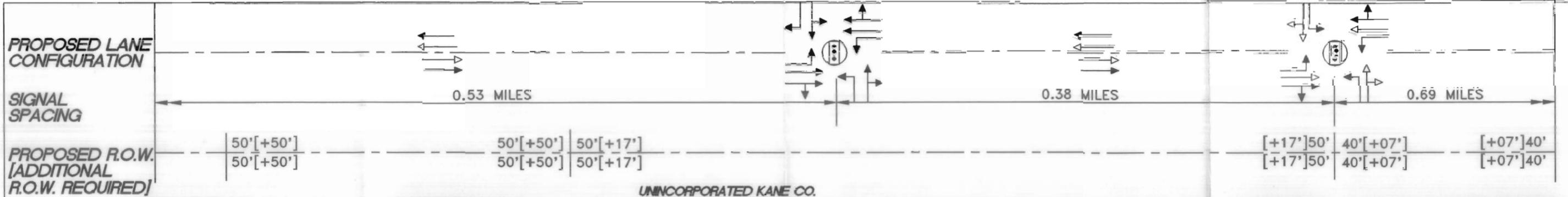
LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
	= EXISTING RIGHT OF WAY DISTANCE
	= PROPOSED RIGHT OF WAY DISTANCE
	= PROPOSED TRAFFIC SIGNAL
	= MODIFY EXISTING STRUCTURE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY
	= ROADWAY REALIGNMENT
	= CUL-DE-SAC

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

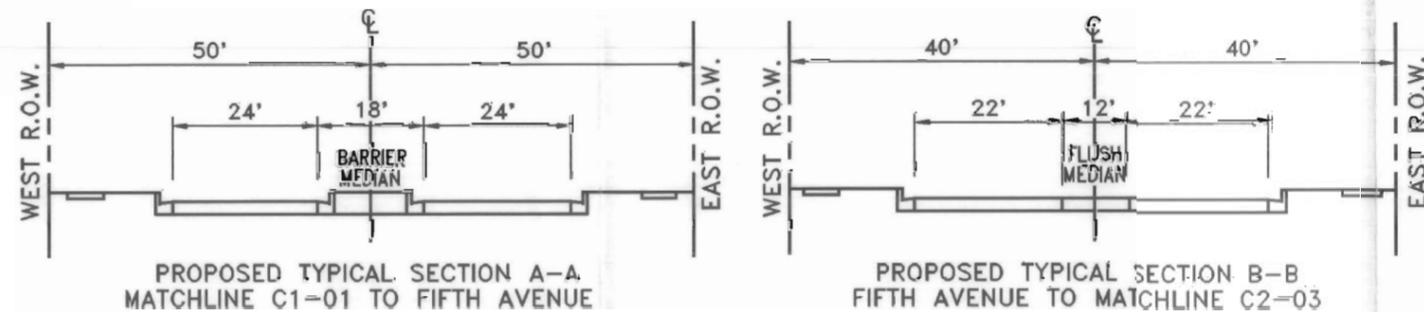


EXHIBIT C1-01



DESCRIPTION OF PROPOSED CONDITIONS:

* The intersection of Farnsworth Avenue and Fifth Avenue has been identified as a candidate for future traffic signalization. The need for signalization should be evaluated as future development warrants.



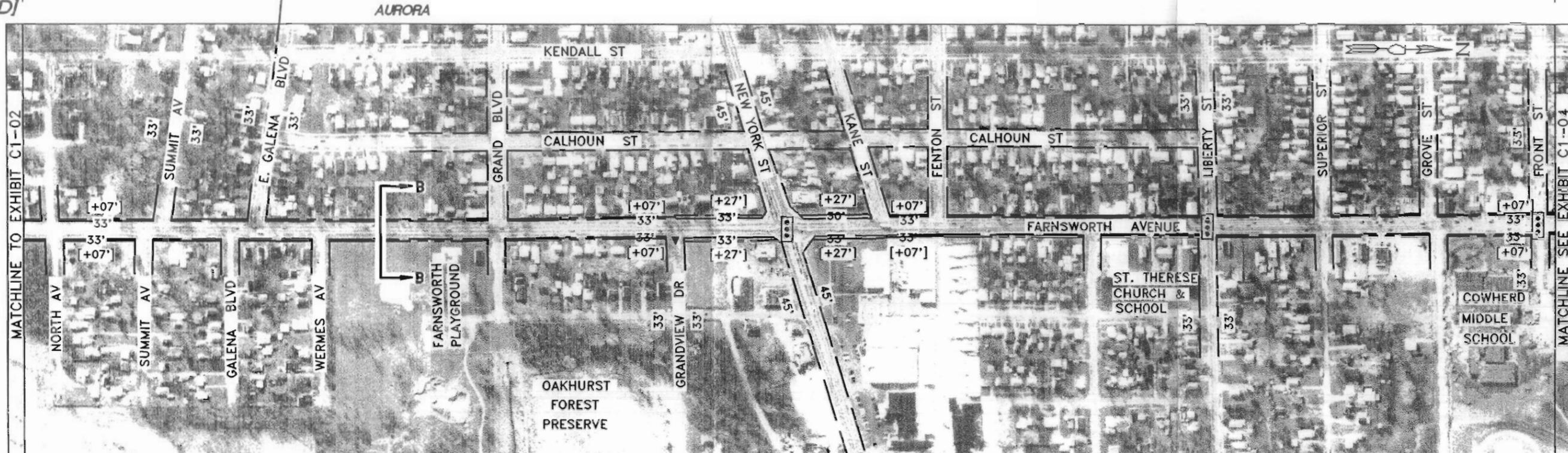
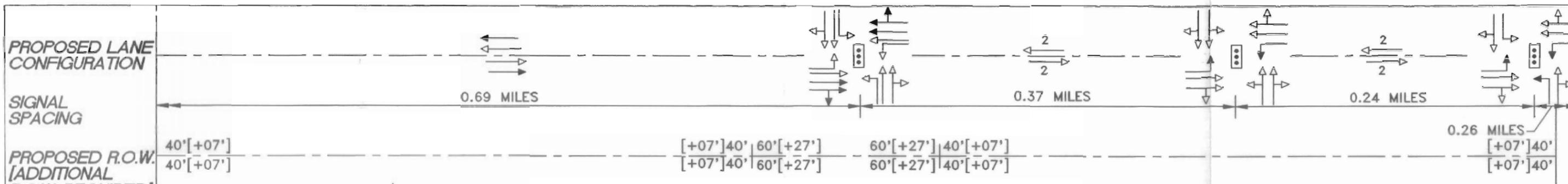
LEGEND

- = EXISTING RIGHT OF WAY
- - - = PROPOSED RIGHT OF WAY
- 33' = EXISTING RIGHT OF WAY DISTANCE
- [+07'] = PROPOSED RIGHT OF WAY DISTANCE
- ⊙ = PROPOSED TRAFFIC SIGNAL
- ⇨ = EXISTING TRAFFIC LANE CONFIGURATION
- = PROPOSED TRAFFIC LANE CONFIGURATION
- ▬▬▬▬ = ROADWAY REALIGNMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

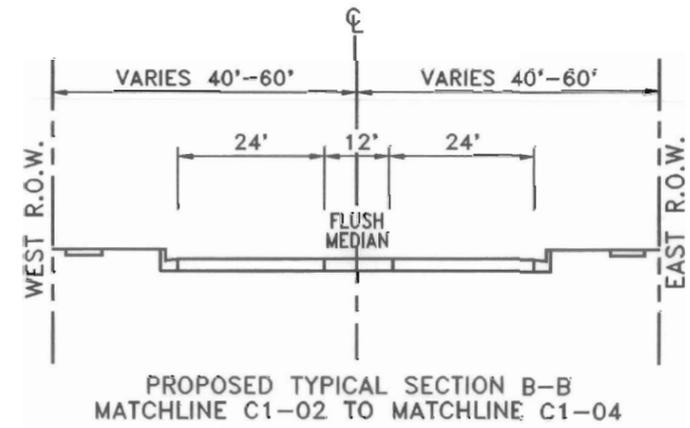




AERIAL PHOTO DATE: 3-19-92

DESCRIPTION OF PROPOSED CONDITIONS:

* Access management needs to be enforced between Fifth Avenue and Liberty Street. Several cross street accesses to Farnsworth Avenue have been evaluated. The cross street access to Farnsworth Avenue should be limited to through streets. This determination will be made after the extension of Farnsworth Avenue to U.S. 34 is in place.

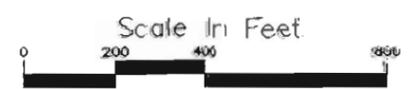


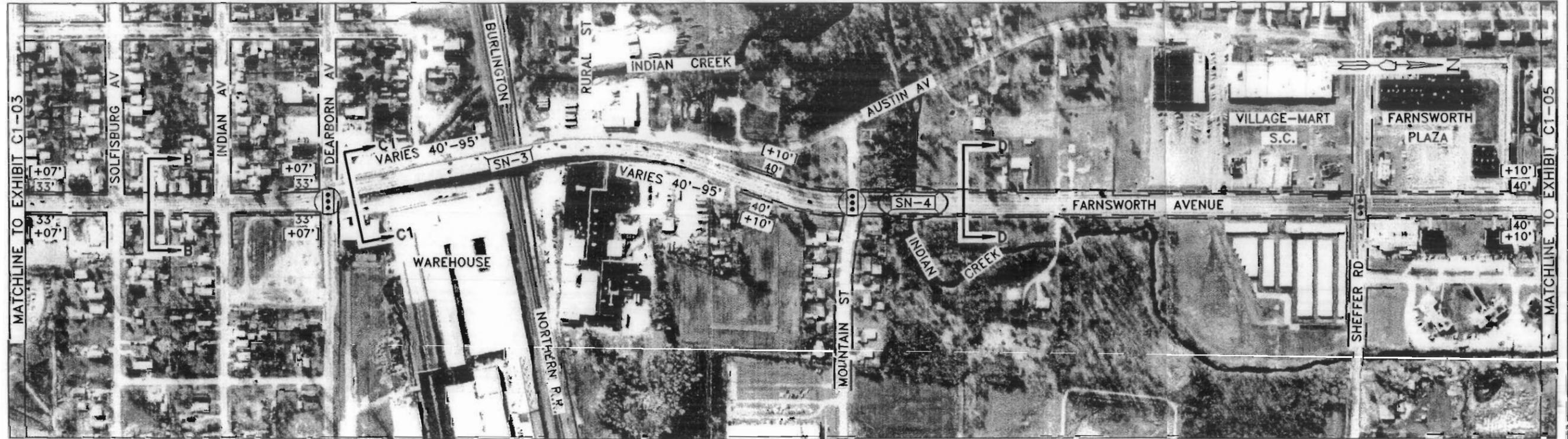
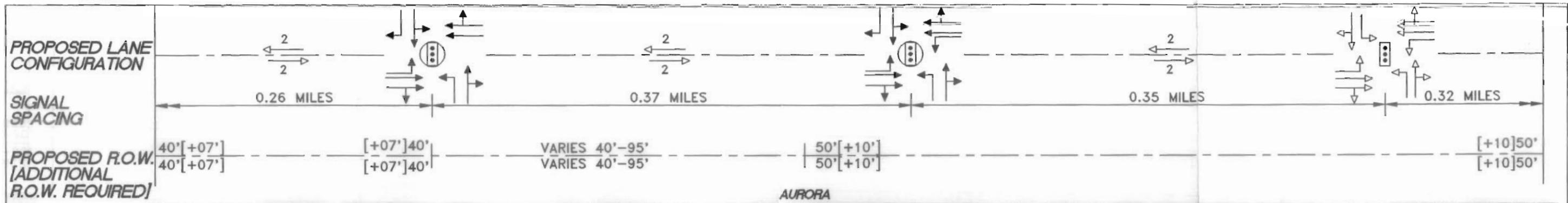
LEGEND

	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
[+07']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY / VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



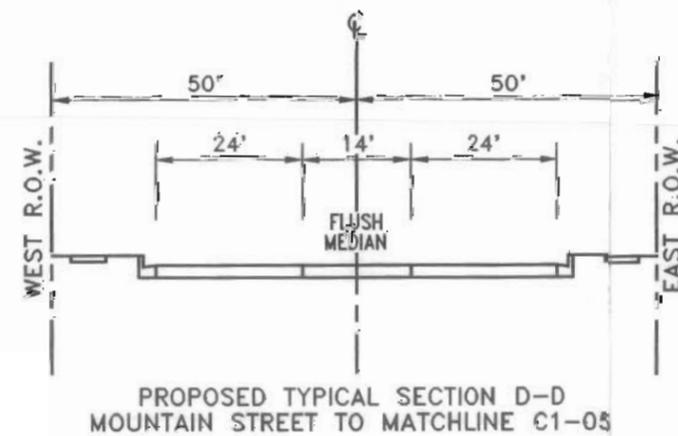
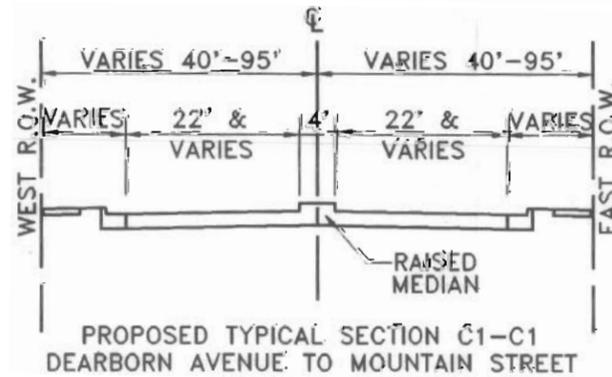
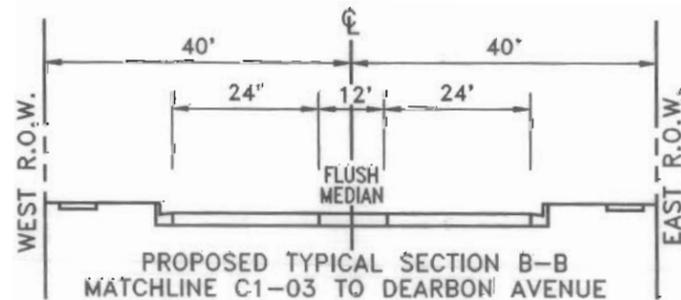


DESCRIPTION OF PROPOSED CONDITIONS:

SN-3 = STRUCTURE NUMBER 045-6052

SN-4 = STRUCTURE NUMBER 045-6012

Modification of this structure will be necessary to accommodate the proposed roadway section.

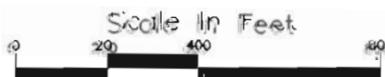


LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= PROPOSED TRAFFIC SIGNAL
	= EXISTING STRUCTURE
	= MODIFY EXISTING STRUCTURE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION

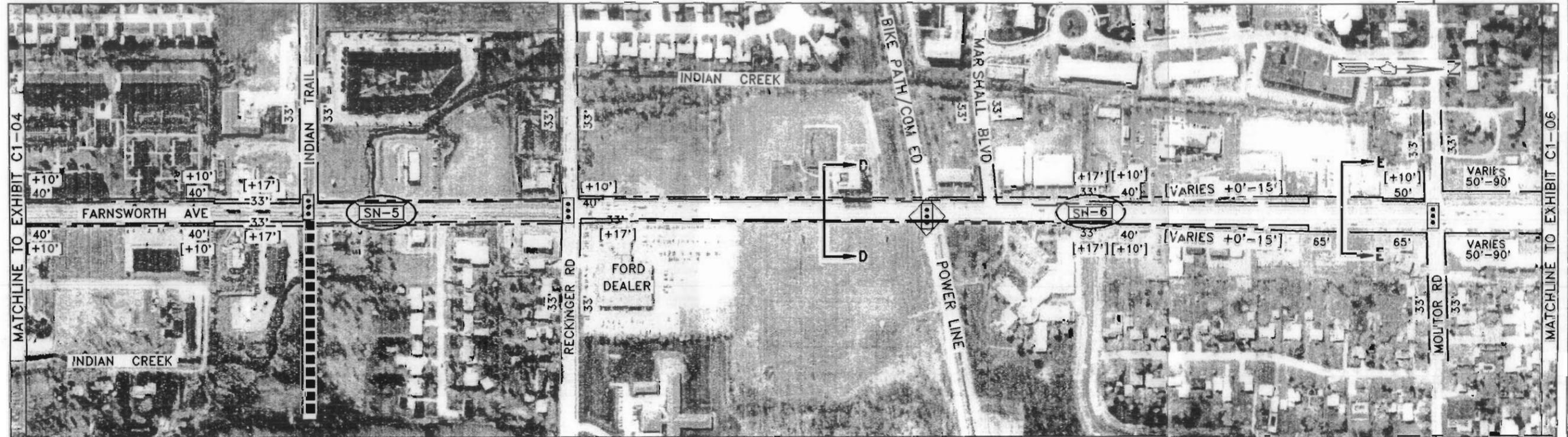
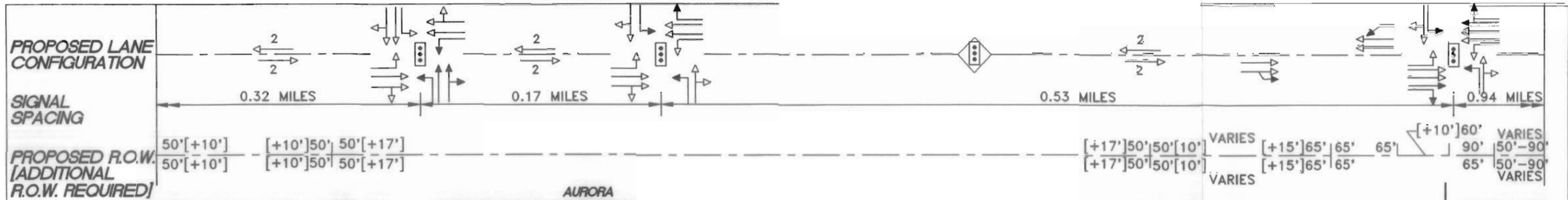
IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DANES & MOORE/WICE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation



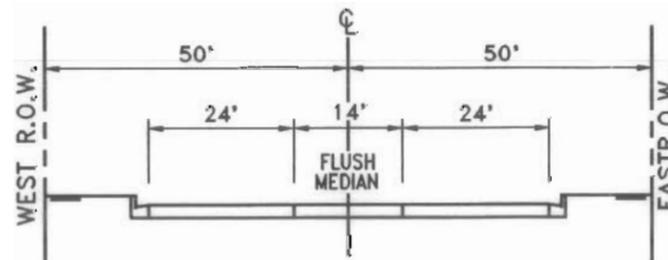
SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY



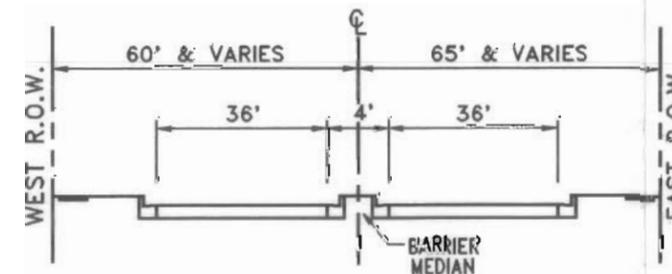
AERIAL PHOTO DATE: 5-01-92
UNINCORPORATED KANE CO.

DESCRIPTION OF PROPOSED CONDITIONS:

- SN-5 = STRUCTURE NUMBER 045-611
- SN-6 = STRUCTURE NUMBER 045-6010
- Modification of this structure will be necessary to accommodate the proposed roadway section.
- * City of Aurora plans to extend Indian Trail east to Eola Road.
- * Pedestrian activated signal is recommended at bike path crossing between Reckinger Road and Molitor Road.



PROPOSED TYPICAL SECTION D-D
MATCHLINE C1-04 TO SOUTH OF MOLITOR ROAD

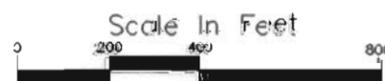


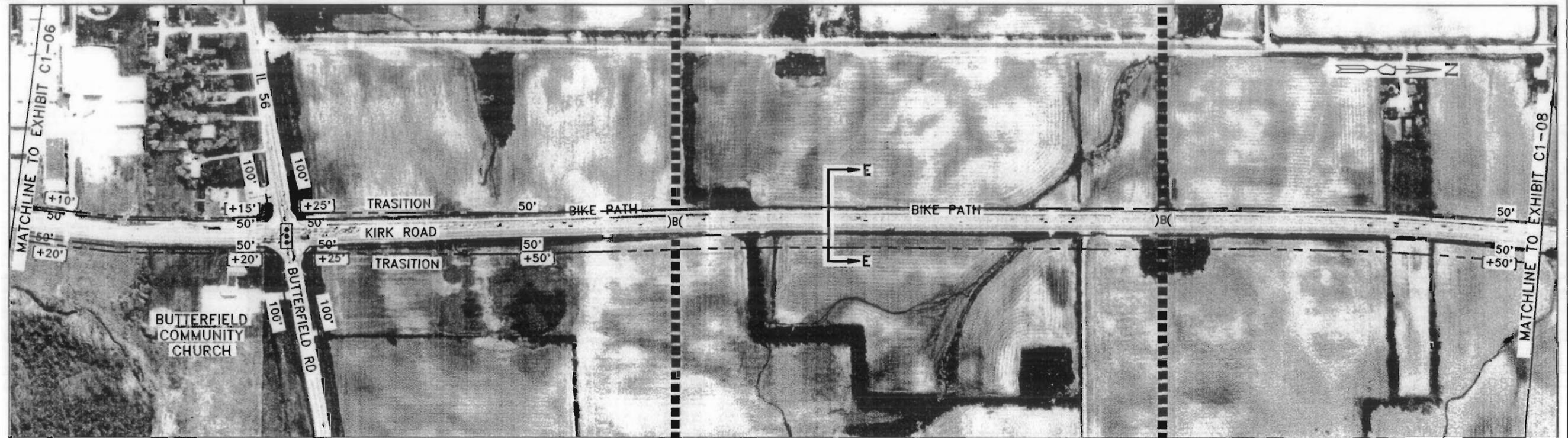
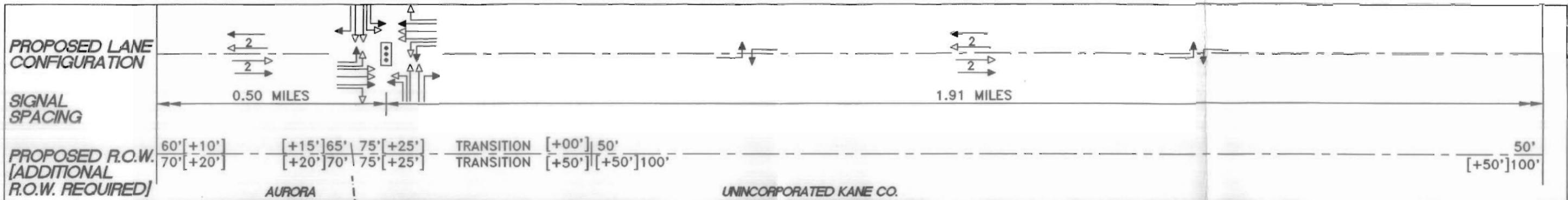
PROPOSED TYPICAL SECTION E-E
SOUTH OF MOLITOR ROAD TO MATCHLINE C1-06

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= PEDESTRIAN ACTIVATED SIGNAL
SN-#	= MODIFY EXISTING STRUCTURE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY
	= ROADWAY REALIGNMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

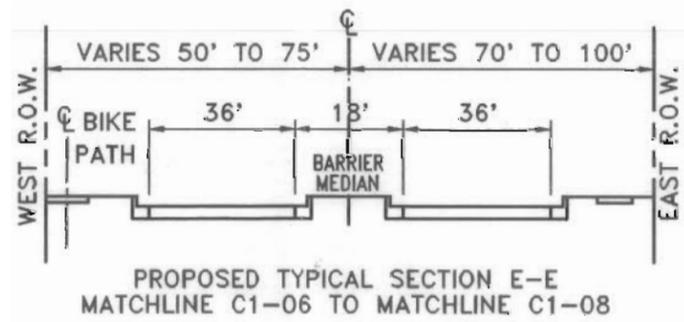
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and ROYER Engineering, Ltd. for the





AERIAL PHOTO DATE: 5-01-92

DESCRIPTION OF PROPOSED CONDITIONS:

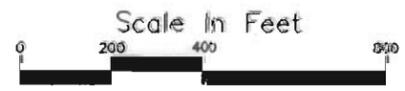


LEGEND

- = EXISTING RIGHT OF WAY
- - - = PROPOSED RIGHT OF WAY
- 75' = EXISTING RIGHT OF WAY DISTANCE
- [+20'] = PROPOSED RIGHT OF WAY DISTANCE
- ⊞ = EXISTING TRAFFIC SIGNAL
-)B(= MEDIAN BREAK
- = EXISTING TRAFFIC LANE CONFIGURATION
- = PROPOSED TRAFFIC LANE CONFIGURATION
- - - = CITY/VILLAGE BOUNDARY
- ▬▬▬▬ = PROPOSED ROADWAY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

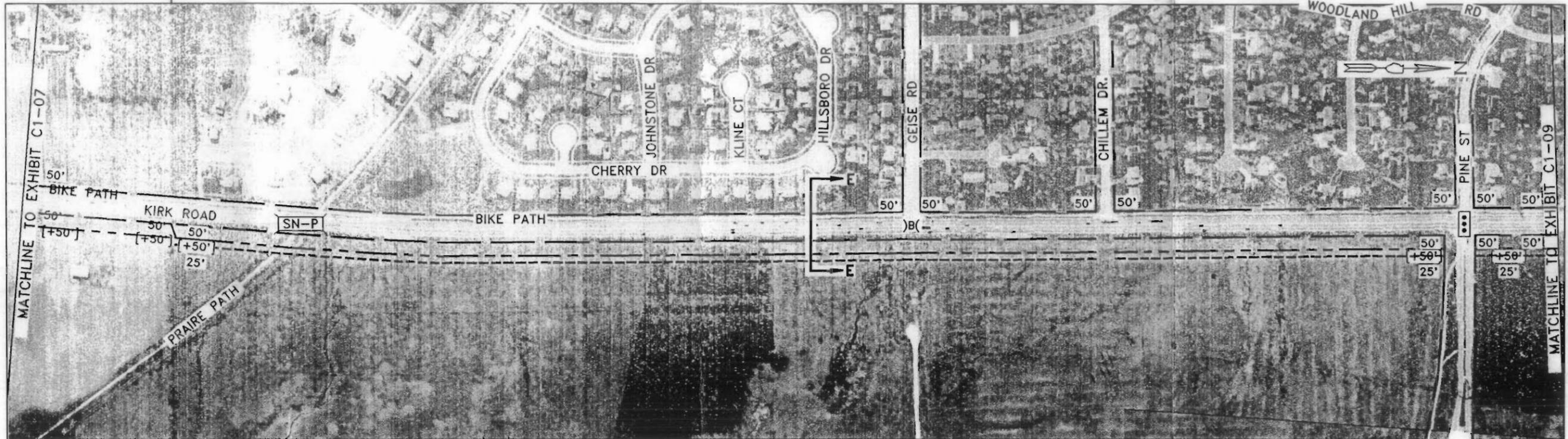
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



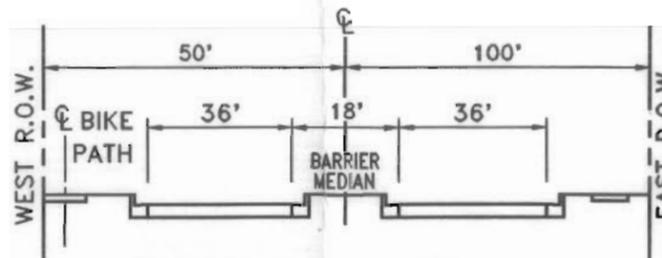
FERMI NATIONAL ACCELERATOR LABORATORY

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

SN-P = Existing overpass for Praire Path over Kirk Road.

* Median break with full access should be provided at Geise Road.

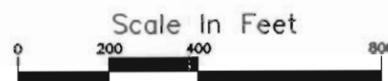


PROPOSED TYPICAL SECTION E-E
MATCHLINE C1-07 TO MATCHLINE C1-09

LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING STRUCTURE
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY
	= EXISTING EASEMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

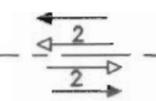
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



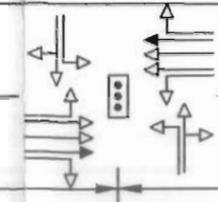
PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



0.88 MILES

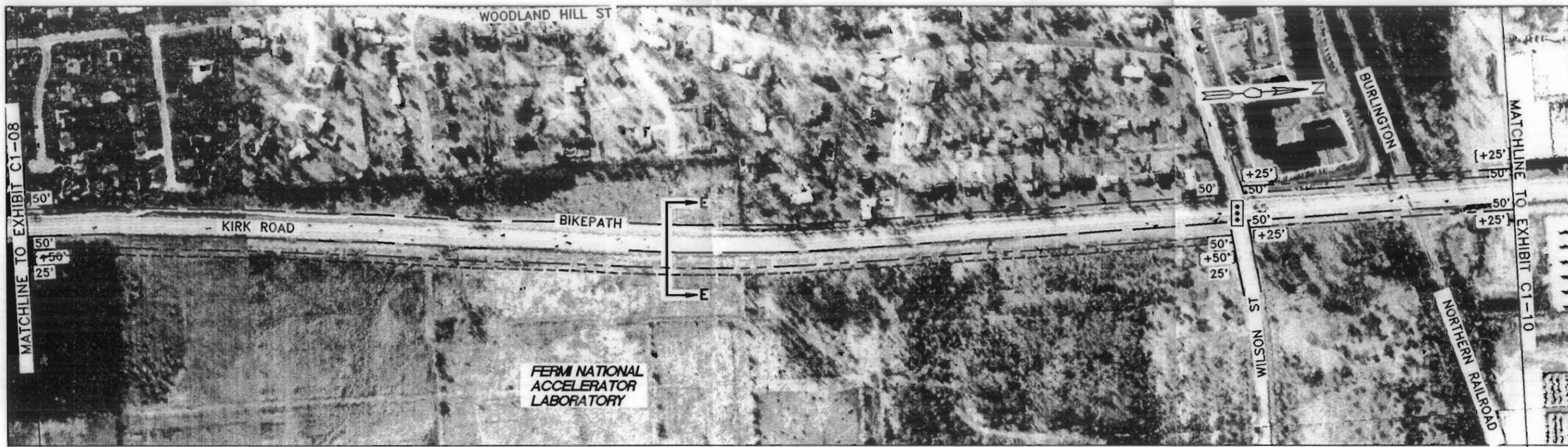


0.43 MILES

50'
100' [+50']

50' 75' [+25']
[+50'] 100' 75' [+25']

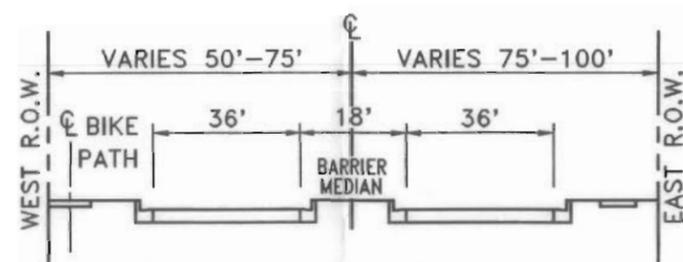
[+25'] 75'
[+25'] 75'



FERMI NATIONAL ACCELERATOR LABORATORY

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:



PROPOSED TYPICAL SECTION E-E
MATCHLINE C1-08 TO MATCHLINE C1-10

LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
[+50']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= EXISTING EASEMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

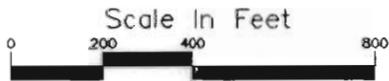
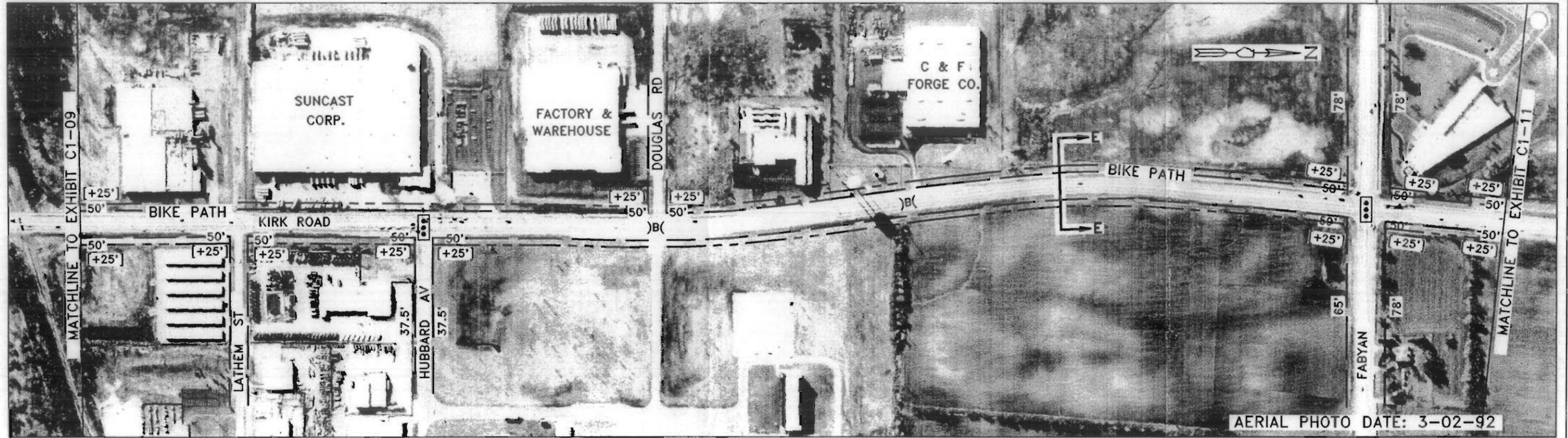
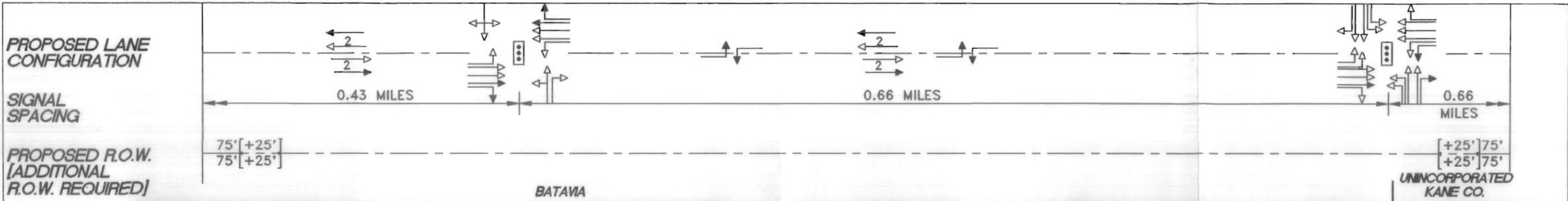
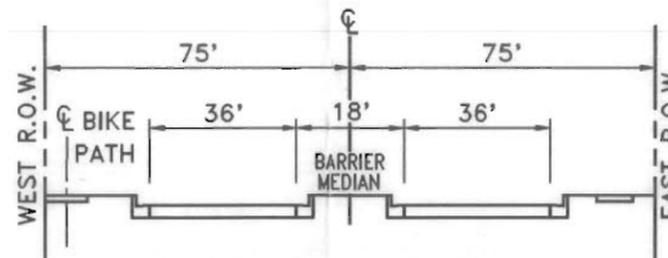


EXHIBIT C1-09



DESCRIPTION OF PROPOSED CONDITIONS:

- * Median break with full access should be provided at Douglas Road, and C & F Forge Co. entrance.

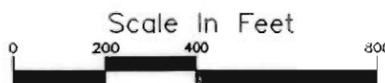


PROPOSED TYPICAL SECTION E-E
MATCHLINE C1-09 TO MATCHLINE C1-11

LEGEND	
	= EXISTING RIGHT OF WAY
65'	= EXISTING RIGHT OF WAY DISTANCE
[+25']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

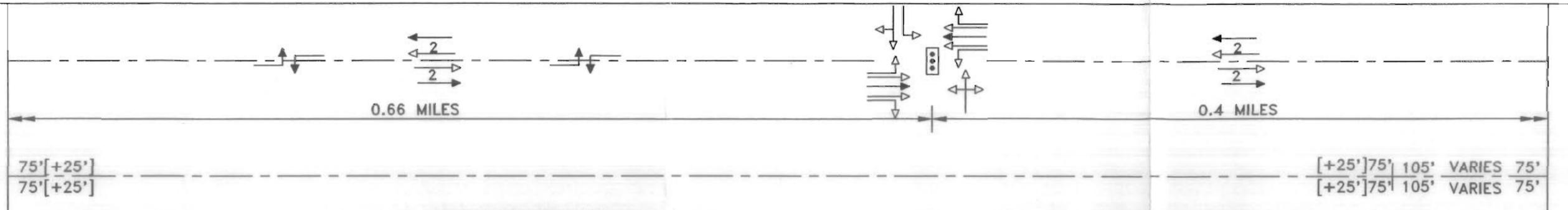
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]

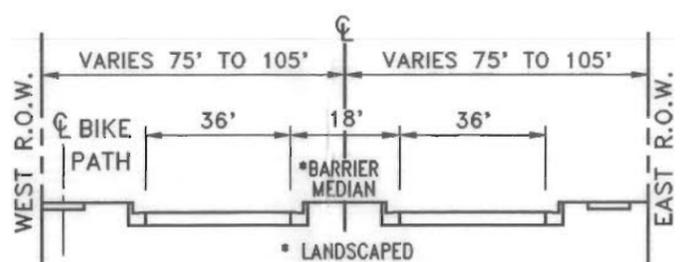


UNINCORPORATED KANE CO.

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

SN-8 = STRUCTURE NUMBER 045-3096
 Modification of this structure will be necessary to accommodate the proposed roadway section.



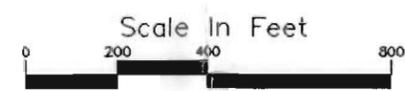
PROPOSED TYPICAL SECTION E-E
 MATCHLINE C1-10 TO MATCHLINE C1-12

LEGEND	
	= EXISTING RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
[+25']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= MODIFY EXISTING STRUCTURE
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

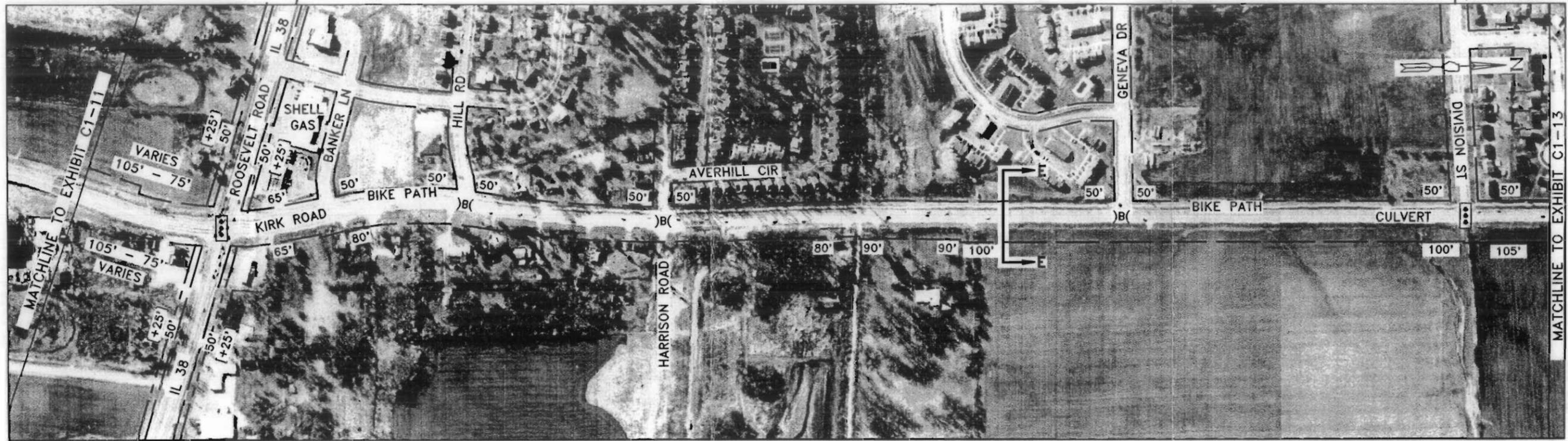
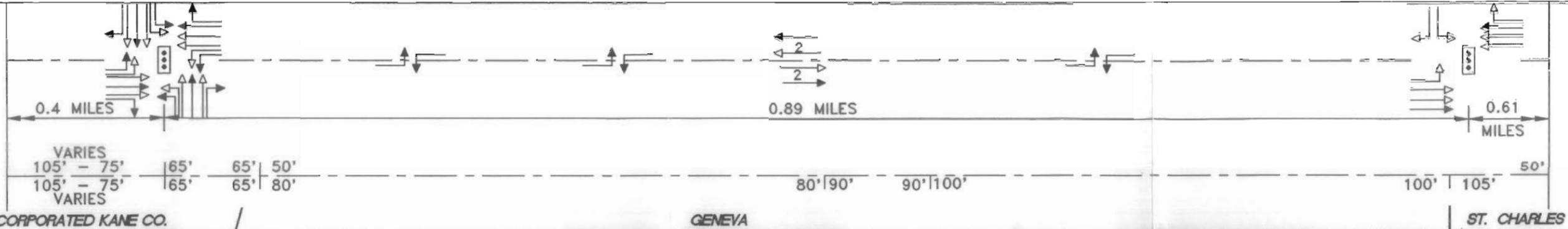
Illinois Department of Transportation



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]

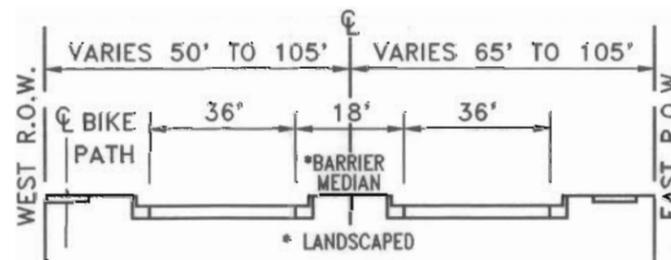


UNINCORPORATED KANE CO.

AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

* Median break with full access should be provided at Hill Road, Averhill Circle and Geneva Drive.



PROPOSED TYPICAL SECTION E-E
MATCHLINE C1-11 TO MATCHLINE C1-13

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
[+25']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

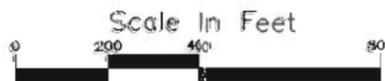
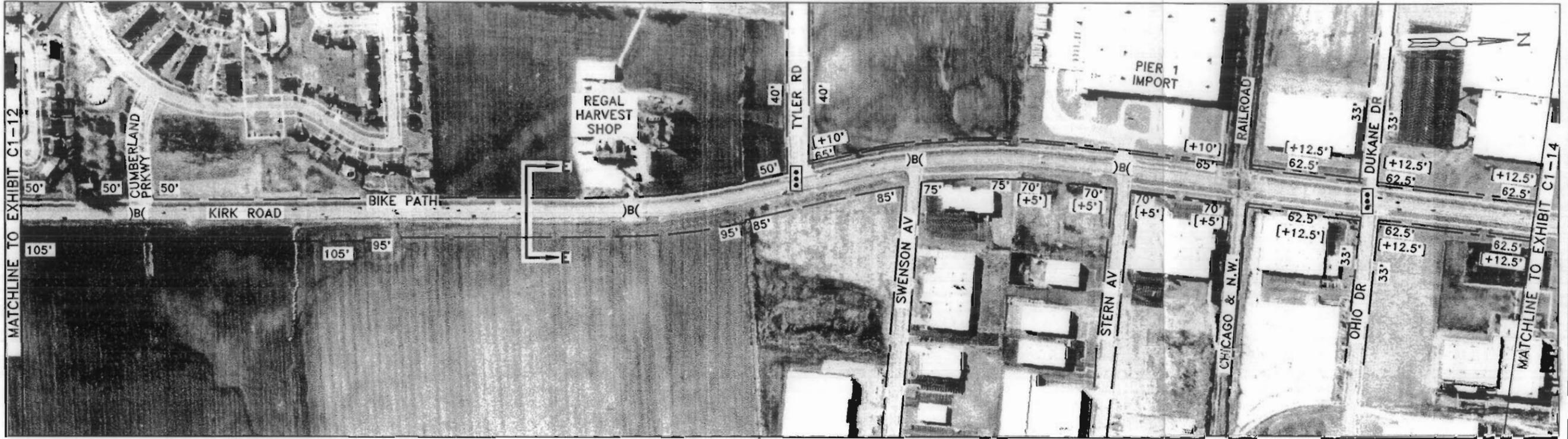
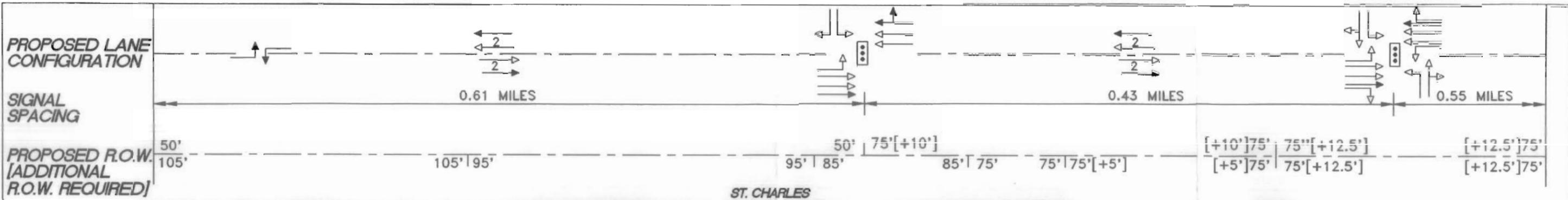
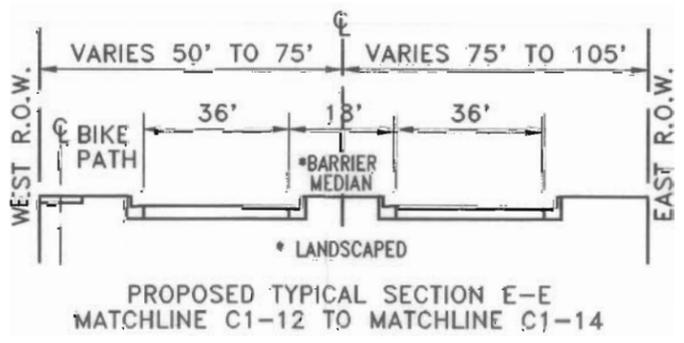


EXHIBIT C1-12



DESCRIPTION OF PROPOSED CONDITIONS:

* Median break with full access should be provided at Cumberland Parkway, Swenson Avenue and Stern Avenue.



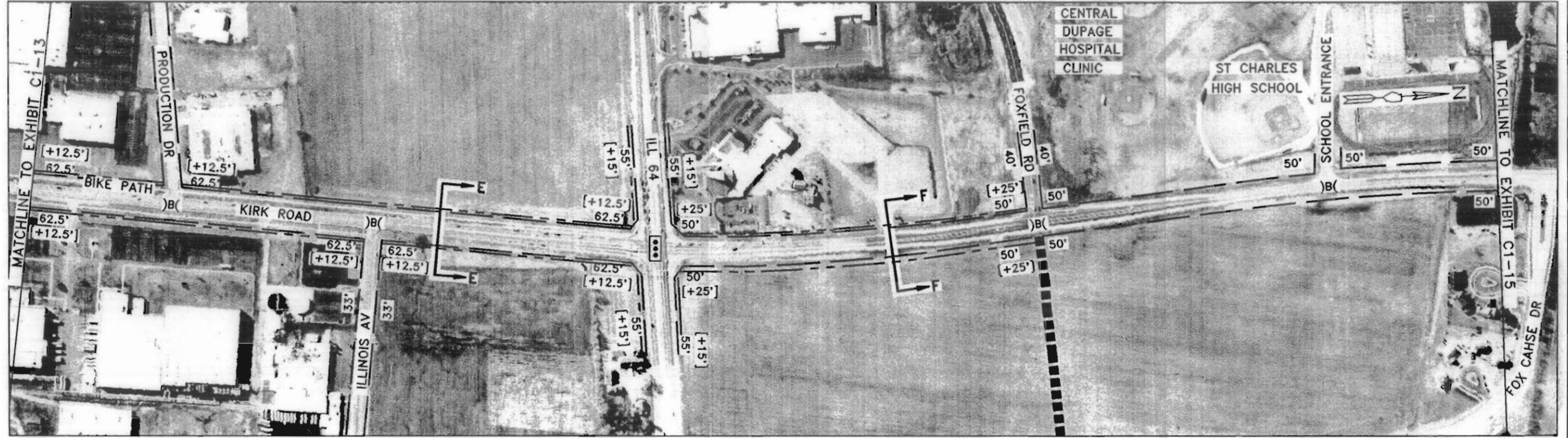
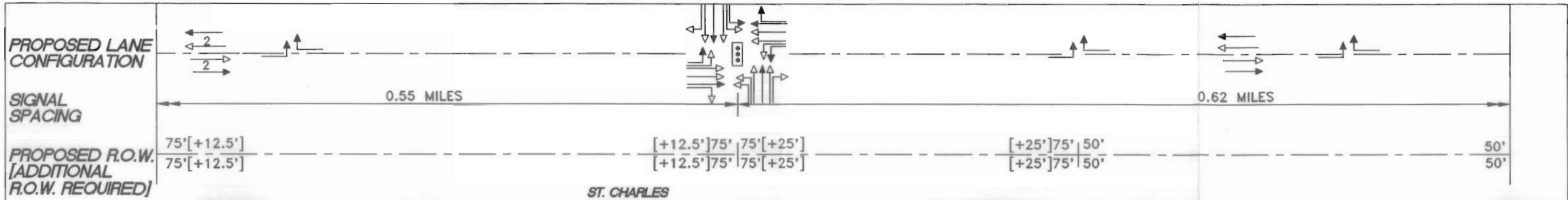
LEGEND

- = EXISTING RIGHT OF WAY
- - - = PROPOSED RIGHT OF WAY
- 95' = EXISTING RIGHT OF WAY DISTANCE
- [+12.5'] = PROPOSED RIGHT OF WAY DISTANCE
- ⊠ = EXISTING TRAFFIC SIGNAL
-)B(= MEDIAN BREAK
- = EXISTING TRAFFIC LANE CONFIGURATION
- = PROPOSED TRAFFIC LANE CONFIGURATION
- - - = CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

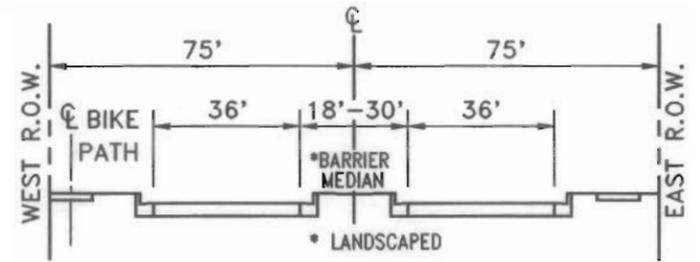




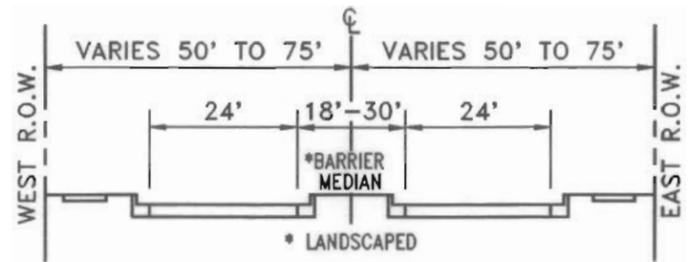
AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

* Median breaks with full access should be provided at Production Drive, Illinois Avenue, Foxfield Road and School Entrance.



PROPOSED TYPICAL SECTION E-E
MATCHLINE C1-13 TO ILLINOIS ROUTE 64



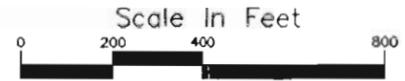
PROPOSED TYPICAL SECTION F-F
ILLINOIS ROUTE 64 TO MATCHLINE C1-15

LEGEND	
---	= EXISTING RIGHT OF WAY
- - -	= PROPOSED RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
[+25']	= PROPOSED RIGHT OF WAY DISTANCE
⊙	= EXISTING TRAFFIC SIGNAL
)B(	= MEDIAN BREAK
↔	= EXISTING TRAFFIC LANE CONFIGURATION
→	= PROPOSED TRAFFIC LANE CONFIGURATION
▬▬▬▬	= PROPOSED ROADWAY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



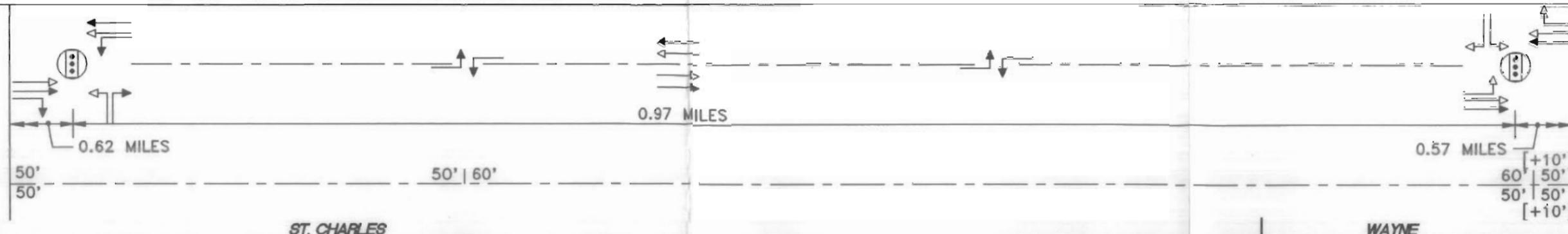
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]

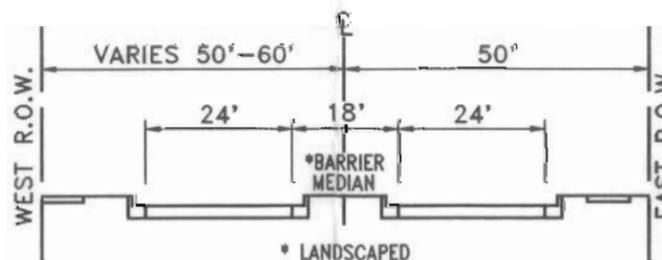


ST. CHARLES

WAYNE
AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

- * The intersection of Fox Chase Drive/Kirk Road and Dunham Road/Kirk Road have been identified as candidate sites for future signalization. The need for signalization should be evaluated as future development warrants.
- * Landscaped median should be considered for the section between Fox Chase Drive and Dunham Road.
- * Proposed ROW should be 120' where possible to provide a standard suburban SRA cross section.



PROPOSED TYPICAL SECTION F-F
MATCHLINE C1-14 TO MATCHLINE C1-16

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/TOWNSHIP BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

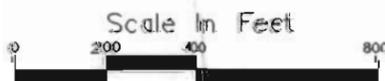
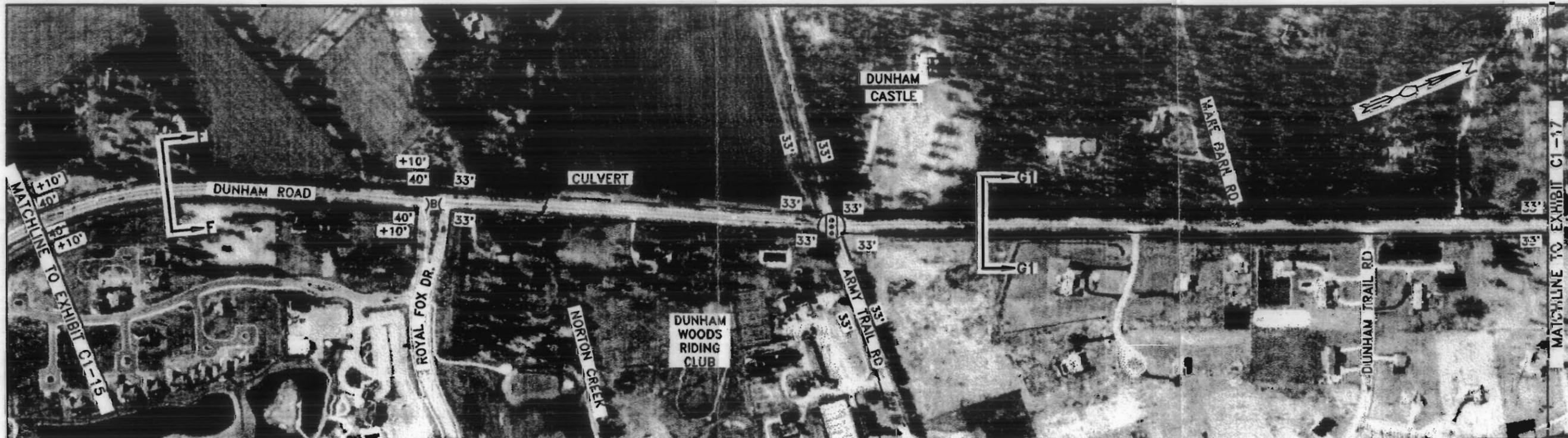
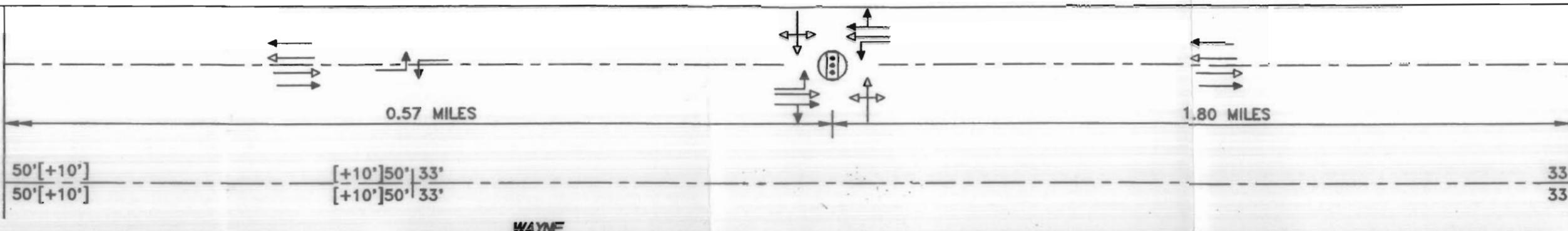


EXHIBIT C1-15

PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



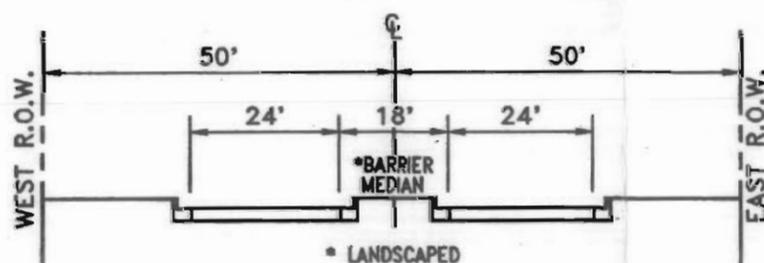
ST. CHARLES

WAYNE

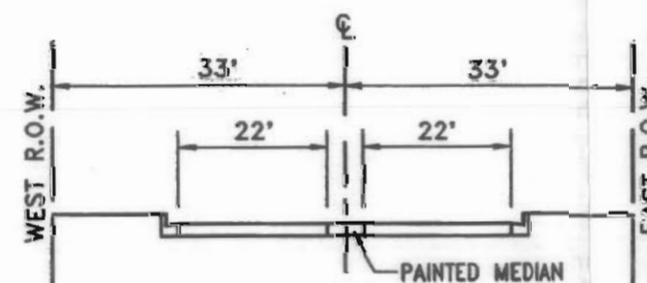
AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

- * Intersection of Dunham Road and Army Trail Road has been identified as a candidate site for future signalization. The need for signalization should be evaluated as future development warrants.
- * Provide full access to Royal Fox Drive.
- * Existing 66' R.O.W. is sufficient to provide 4-lane cross section with painted median. Additional R.O.W. be required at the intersection of Dunham Road & Army Trail Road to provide left turn lane in northbound & southbound direction.



PROPOSED TYPICAL SECTION F-F
MATCHLINE C1-15 TO ROYAL FOX DRIVE



PROPOSED TYPICAL SECTION G1-G1
ROYALE FOX DRIVE TO MATCHLINE C1-17

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
)BK	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/WCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

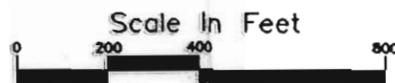
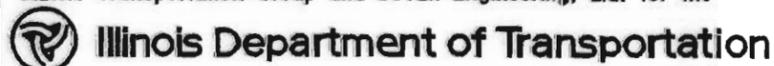
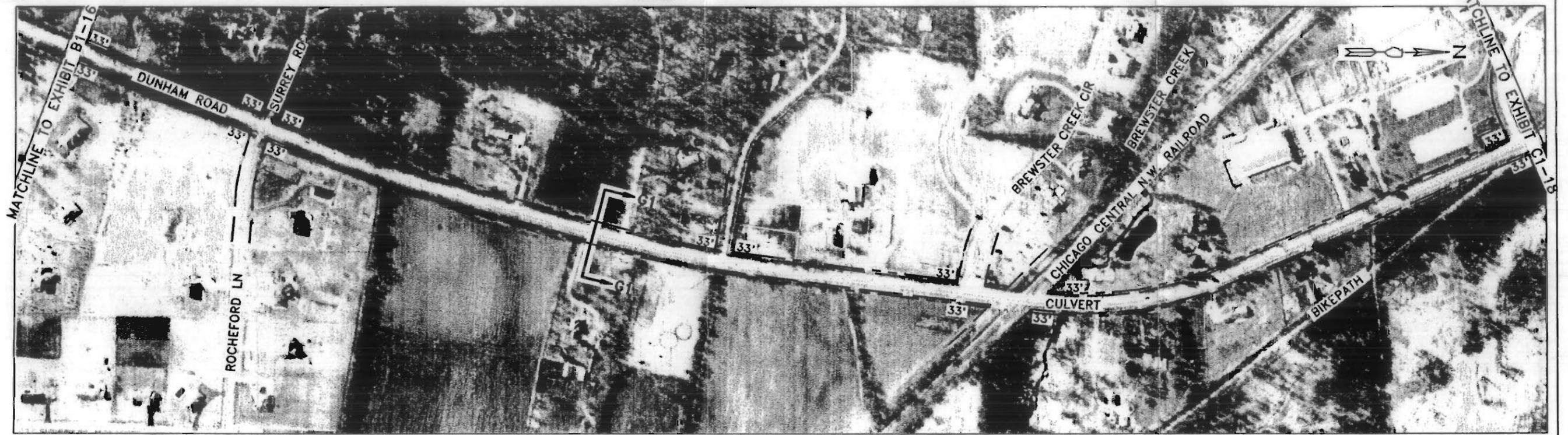
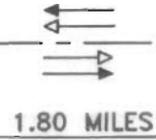


EXHIBIT C1-16

PROPOSED LANE CONFIGURATION

SIGNAL SPACING

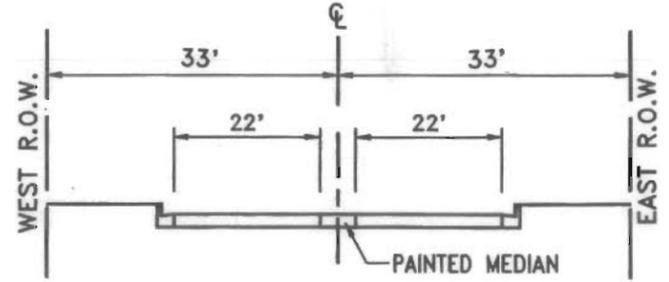
PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

* Existing 66' R.O.W. is sufficient to provide 4-lane cross section with painted median.



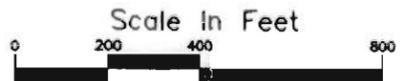
PROPOSED TYPICAL SECTION G1-G1
MATCHLINE C1-16 TO MATCHLINE C1-18

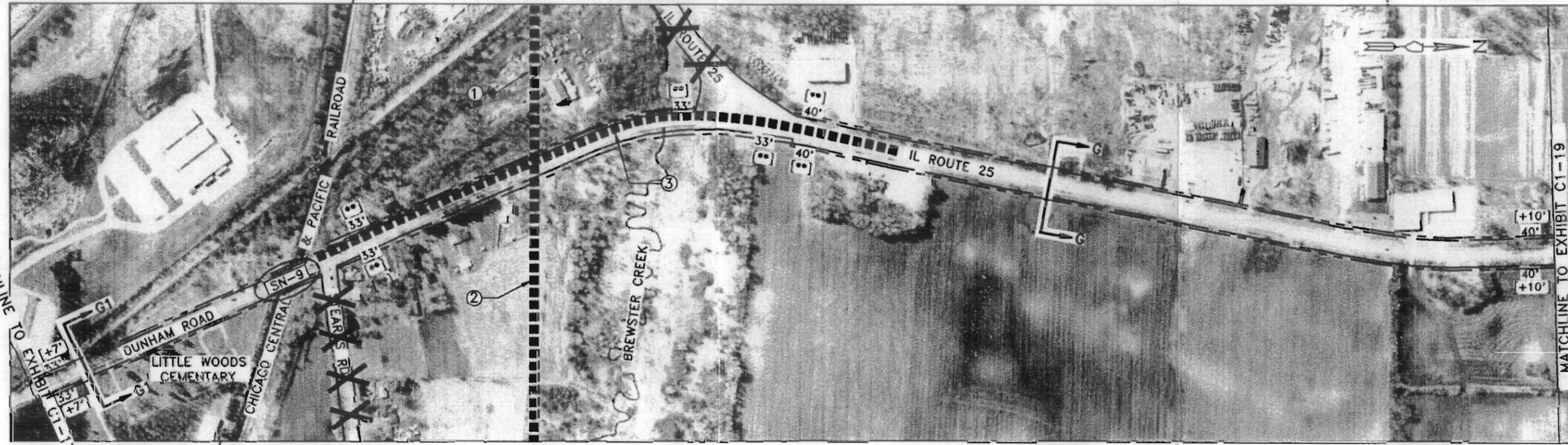
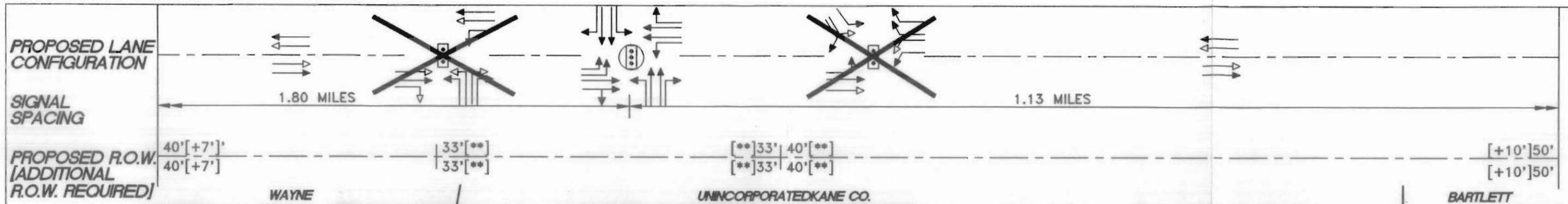
LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
	= MEDIAN BREAK
	= EXISTING RIGHT OF WAY DISTANCE
	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

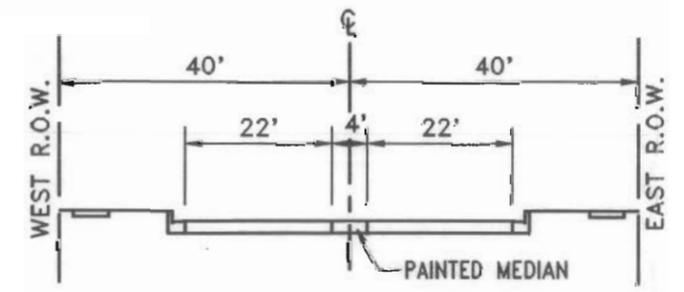




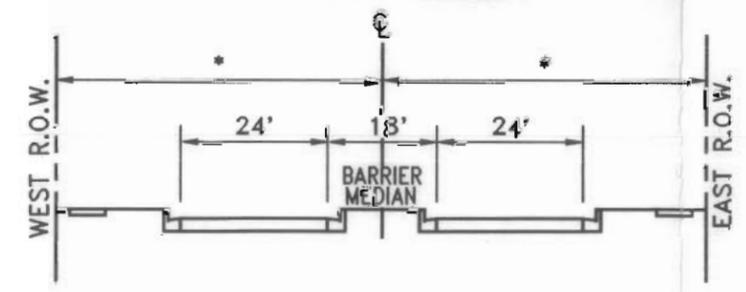
DESCRIPTION OF PROPOSED CONDITIONS:

- SN-9 = STRUCTURE NUMBER 045-3021
Modification of this structure will be necessary to accommodate the proposed roadway section.
- ① = PROPOSED IL ROUTE 25 REALIGNMENT
- ② = PROPOSED STEARNS ROAD REALIGNMENT
- ③ = PROPOSED DUNHAM ROAD REALIGNMENT
- [**] = PROPOSED R.O.W. BASED ON THE DUNHAM ROAD REALIGNMENT

NOTE: RECOMMENDATION FROM THE CURRENT STUDY BEING DONE BY KANE AND DUPAGE COUNTIES HAS BEEN INCORPORATED.



PROPOSED TYPICAL SECTION G1-G1
MATCHLINE C1-17 TO CHICAGO CENTRAL & PACIFIC RAILROAD



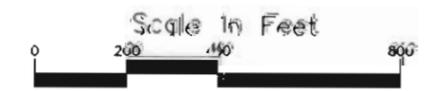
PROPOSED TYPICAL SECTION G-G
CHICAGO CENTRAL & PACIFIC RAILROAD TO MATCHLINE C1-19

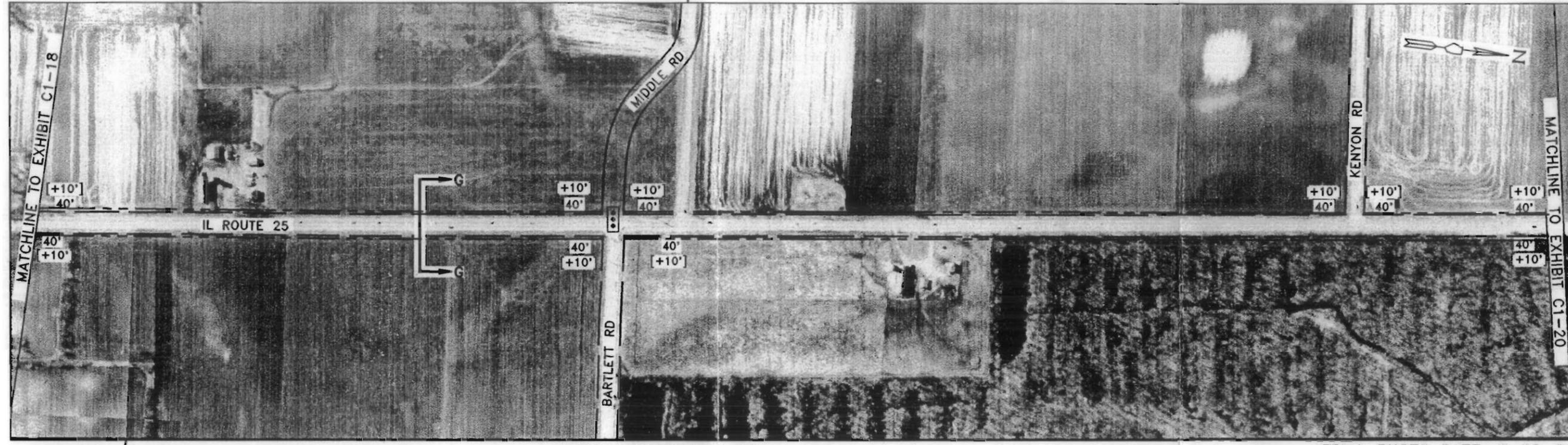
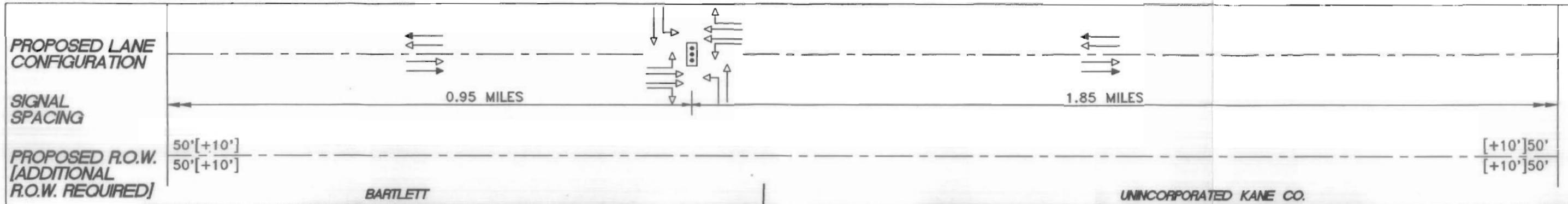
* PROPOSED R.O.W. IS BASED ON DUNHAM ROAD REALIGNMENT

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= PROPOSED TRAFFIC SIGNAL
	= MODIFY EXISTING STRUCTURE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY
	= PROPOSED ROAD REALIGNMENT

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

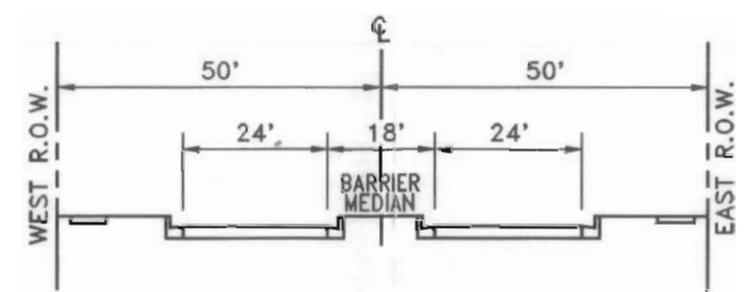
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:



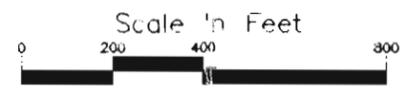
PROPOSED TYPICAL SECTION G-G
MATCHLINE C1-18 TO MATCHLINE C1-20

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
40'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



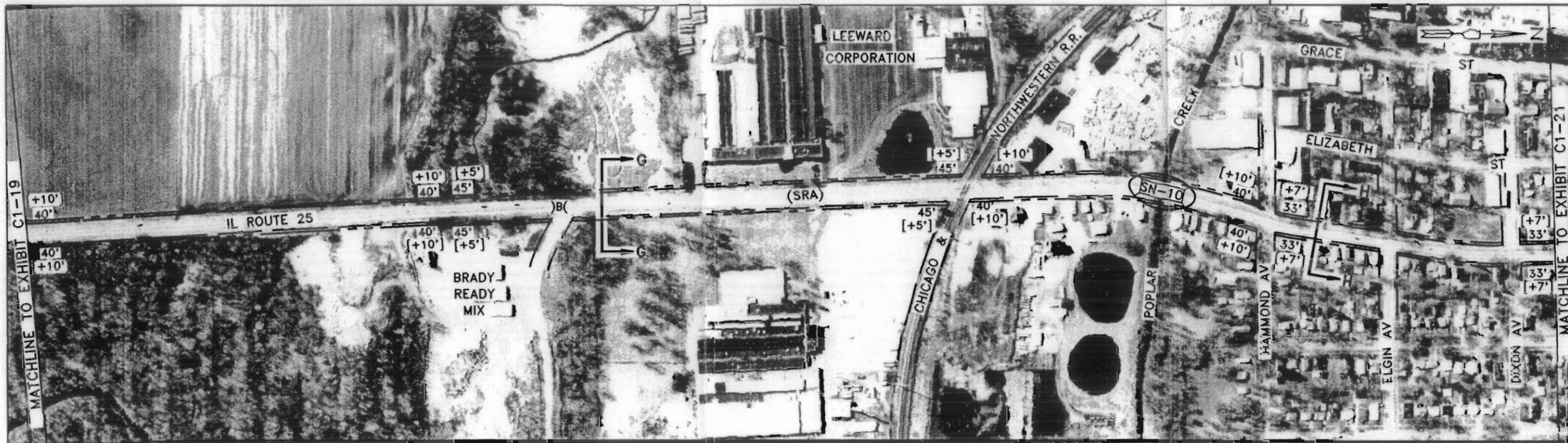
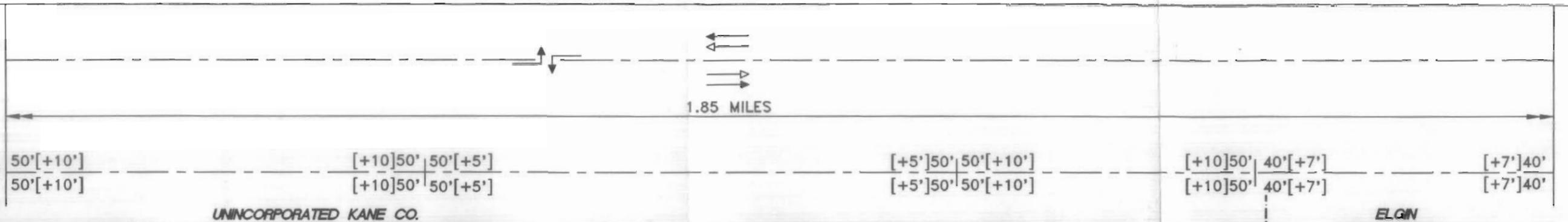
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

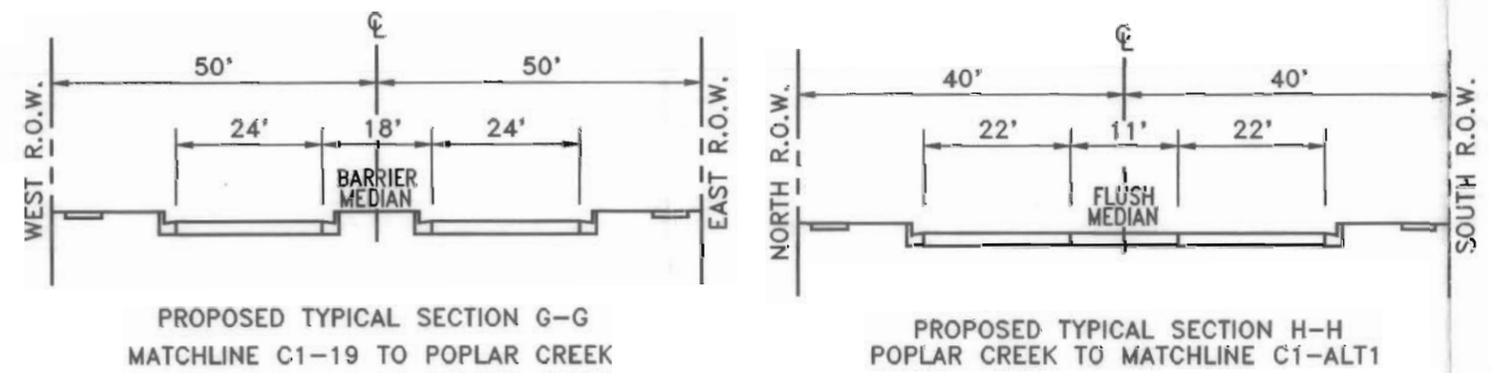
PROPOSED R.O.W. (ADDITIONAL R.O.W. REQUIRED)



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

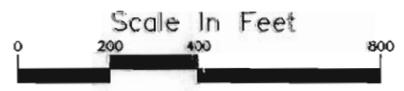
SN-10 = STRUCTURE NUMBER 045-5000
Modification of this structure will be necessary to accommodate the proposed roadway section.



LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
45'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= MODIFY EXISTING STRUCTURE
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

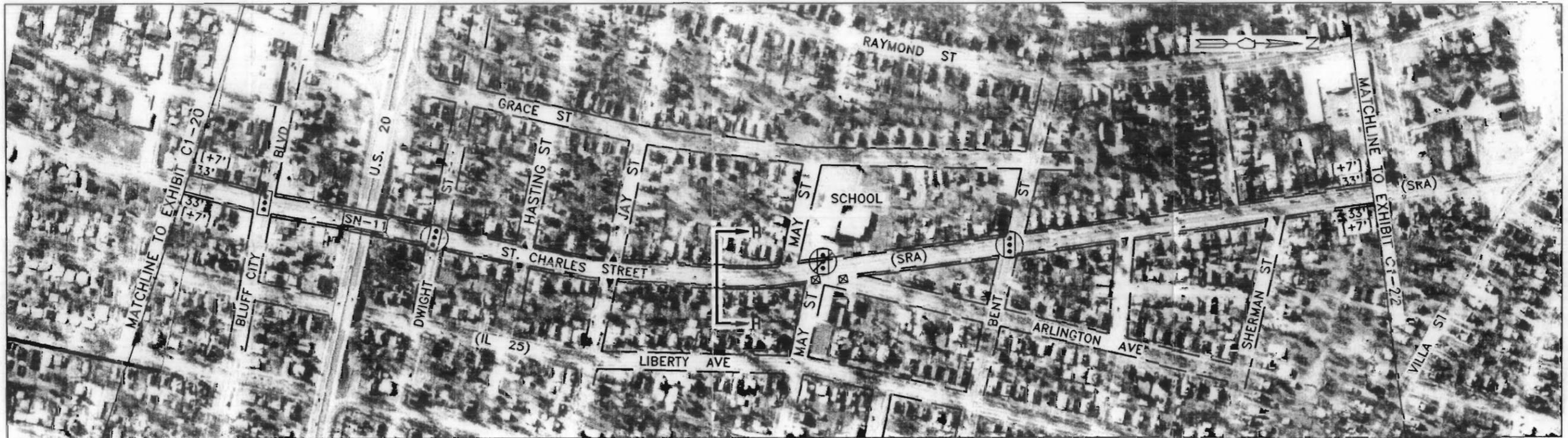
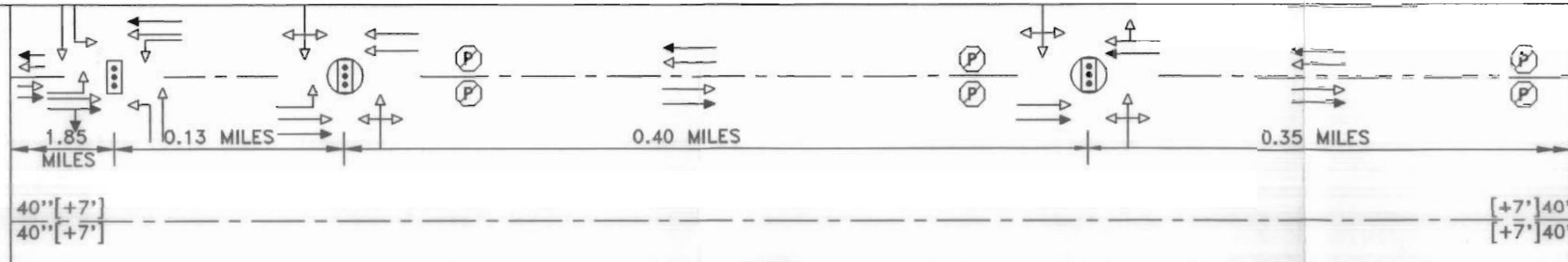
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]

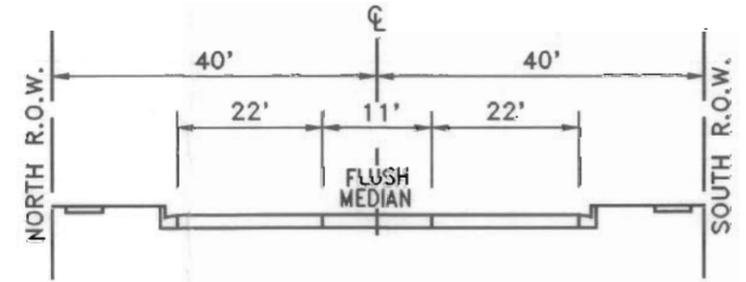


AERIAL PHOTO DATE: 02-09-92

DESCRIPTION OF PROPOSED CONDITIONS:

SN-11 = STRUCTURE NUMBER 045-0007

- * Remove all on-street parking from Dwight Street to Villa Street along St. Charles Street.
- * Limit all non-signalized intersections to right in/right out.
- * Close access to Arlington Avenue and May Street.
- * Dwight Street and Bent Street have been identified as candidate sites for future signalization. The need for signalization should be evaluated as future development warrants.



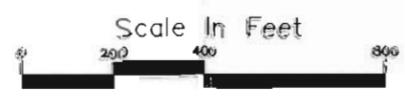
PROPOSED TYPICAL SECTION H-H
MATCHLINE C1-20 TO MATCHLINE C1-22

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
	= EXISTING RIGHT OF WAY DISTANCE
	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= PROPOSED TRAFFIC SIGNAL
	= REMOVE EXISTING TRAFFIC SIGNAL
	= EXISTING STRUCTURE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CLOSE ACCESS
	= RIGHT IN / RIGHT OUT
	= REMOVE PARKING

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



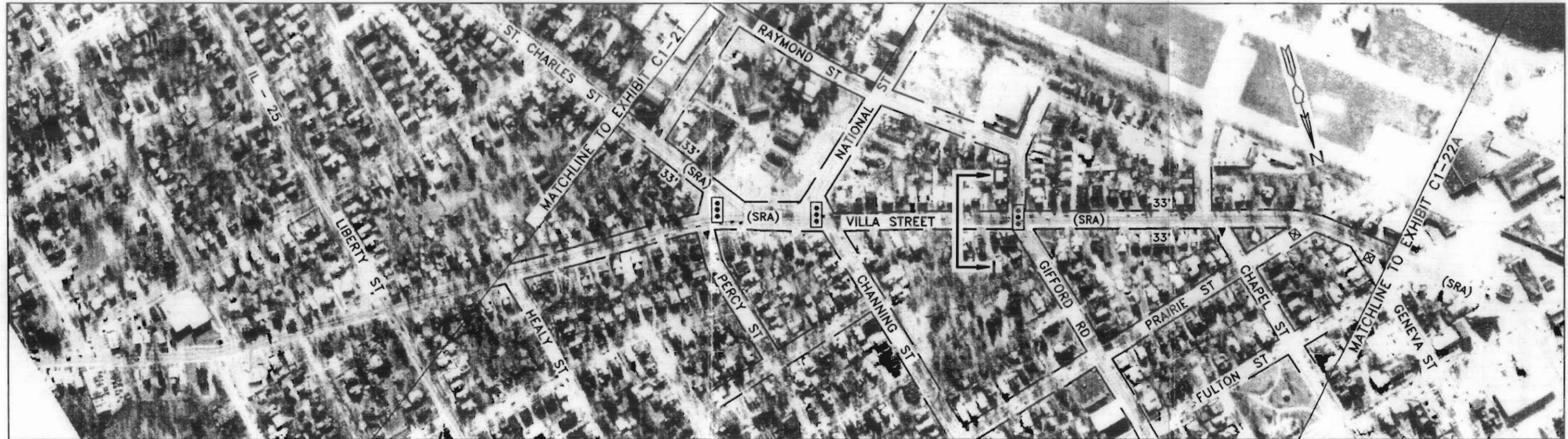
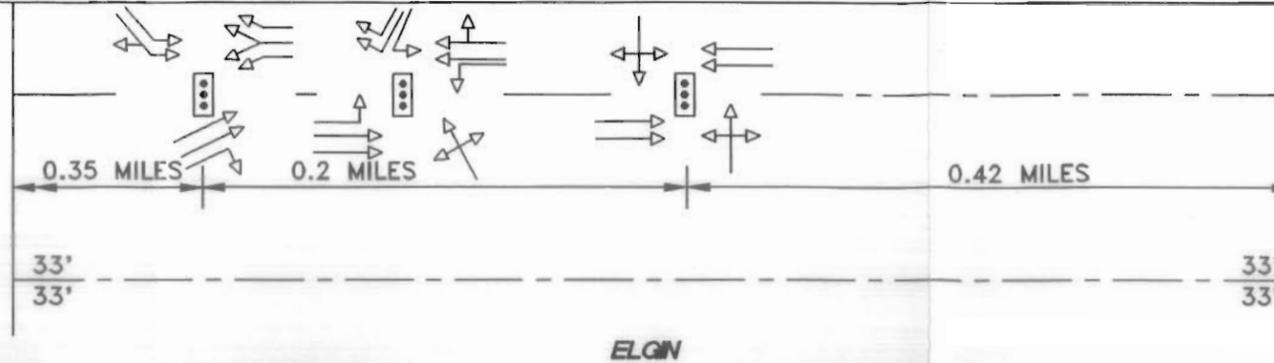
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

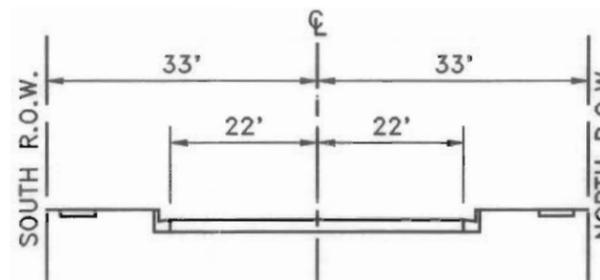
PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

- * Close access to Prairie Street
- * Limit Percy Street, and Chapel Street to right in / right out.



PROPOSED TYPICAL SECTION I-I
ST. CHARLES STREET TO MATCHLINE C1-22A
(ALONG VILLA STREET)

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= RIGHT IN / RIGHT OUT
	= CLOSE ACCESS

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

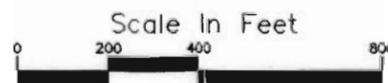
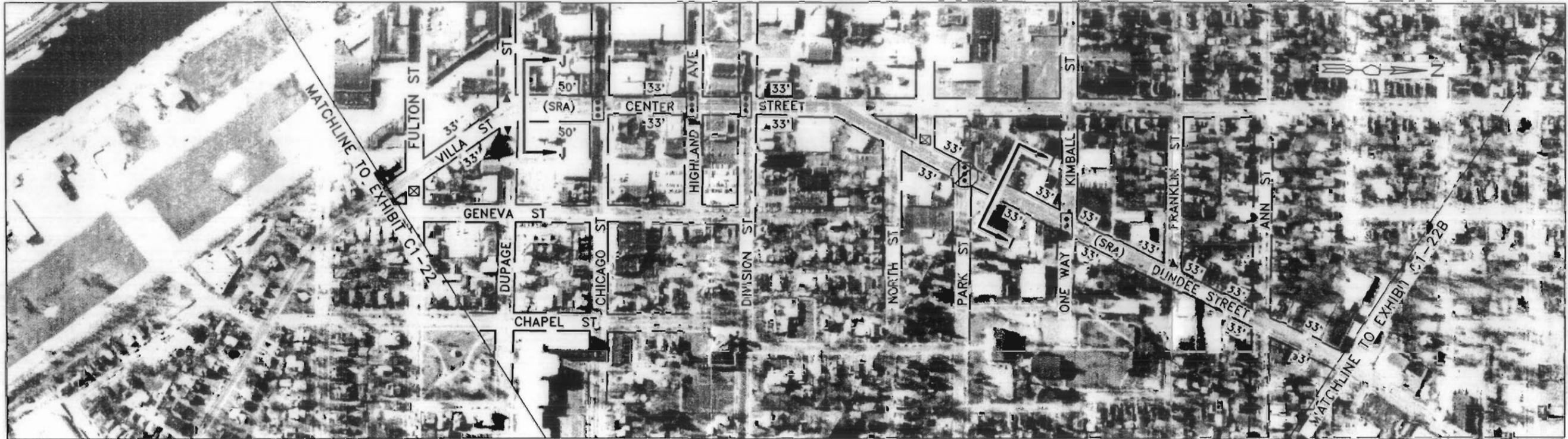
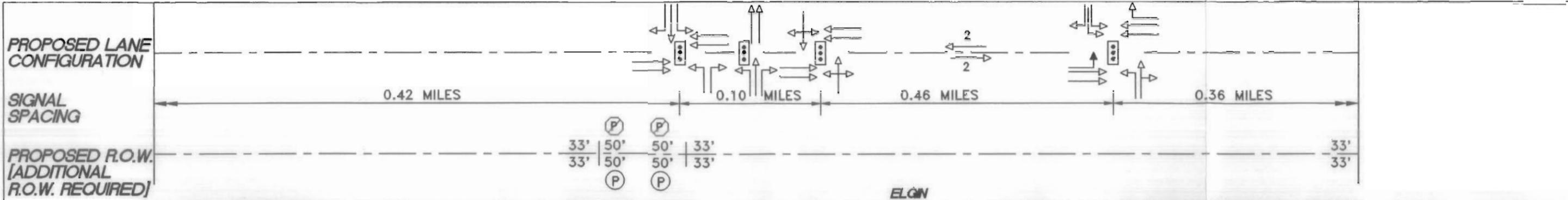


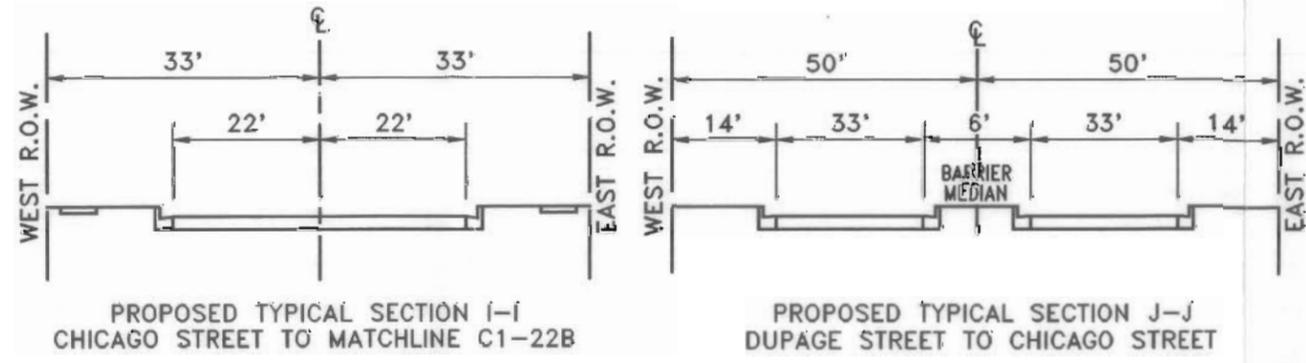
EXHIBIT C1-22



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

- * Close access to south Fulton Street, Geneva Street, and west of North Street.
- * Limit DuPage Street and Franklin Street to right in/right out
- * Remove traffic signals at Park Street.



LEGEND

	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC SIGNAL TO BE REMOVED
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= RIGHT IN / RIGHT OUT
	= CLOSE ACCESS
	= REMOVE PARKING

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



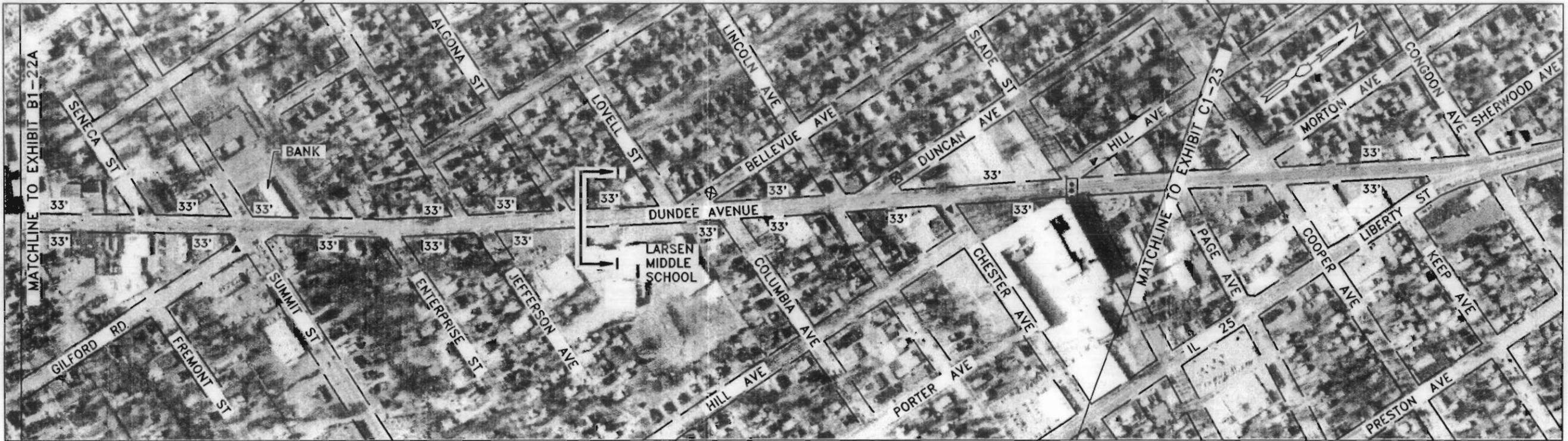
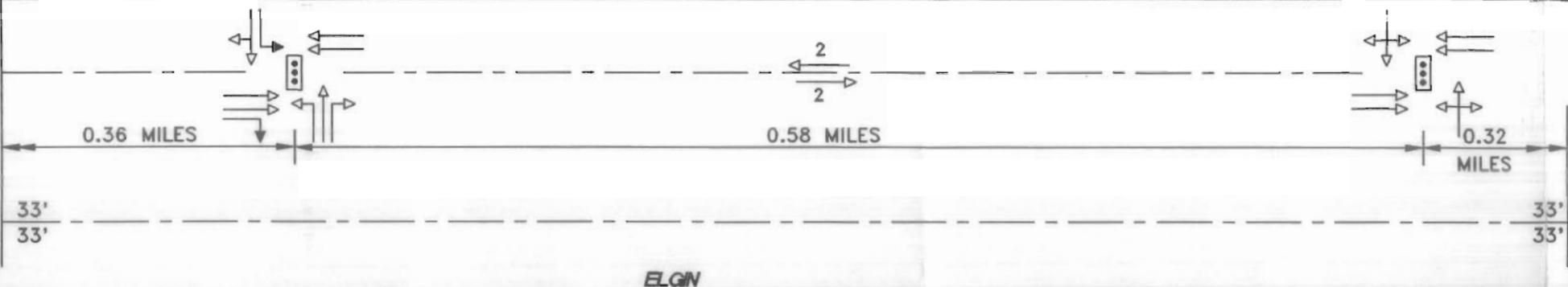
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



PROPOSED LANE CONFIGURATION

SIGNAL SPACING

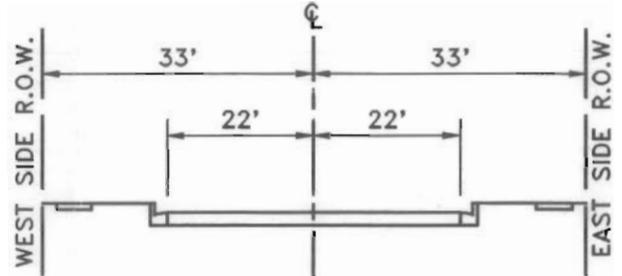
PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



AERIAL PHOTO DATE: 3-02-92

DESCRIPTION OF PROPOSED CONDITIONS:

- * Close access to Bellevue Avenue, and Duncan Avenue
- * Limit Seneca Street, Gilford Street, Alcona Street, Chester Avenue and Hill Avenue to right in right out.



PROPOSED TYPICAL SECTION I-I
MATCHLINE C1-22A TO SLADE STREET

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
	= EXISTING RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= RIGHT IN / RIGHT OUT
	= CLOSE ACCESS

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

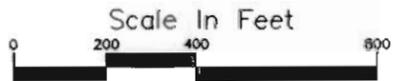
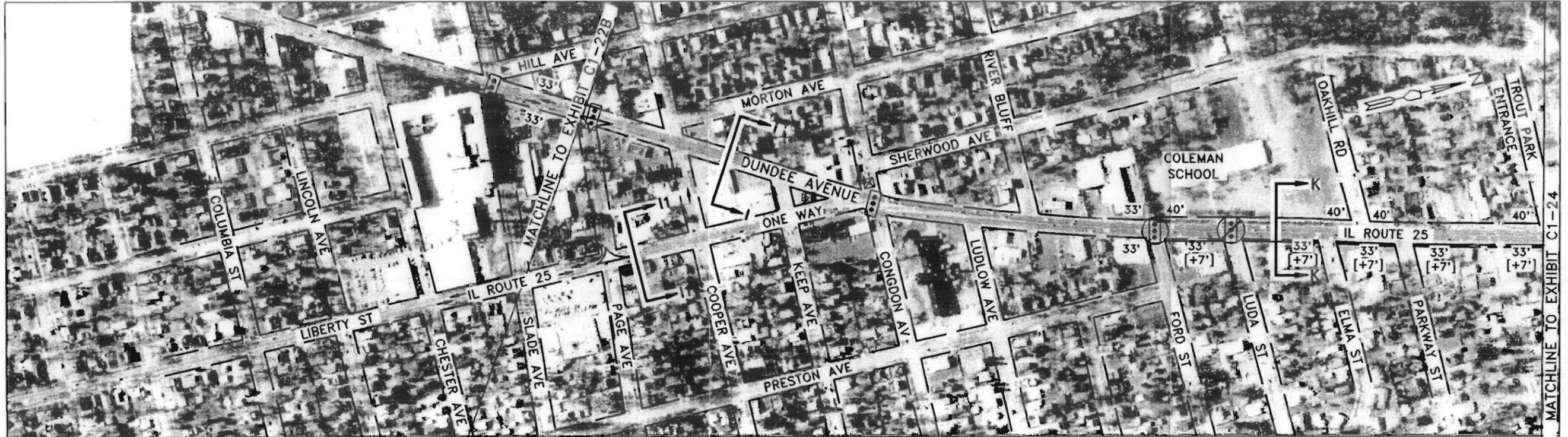
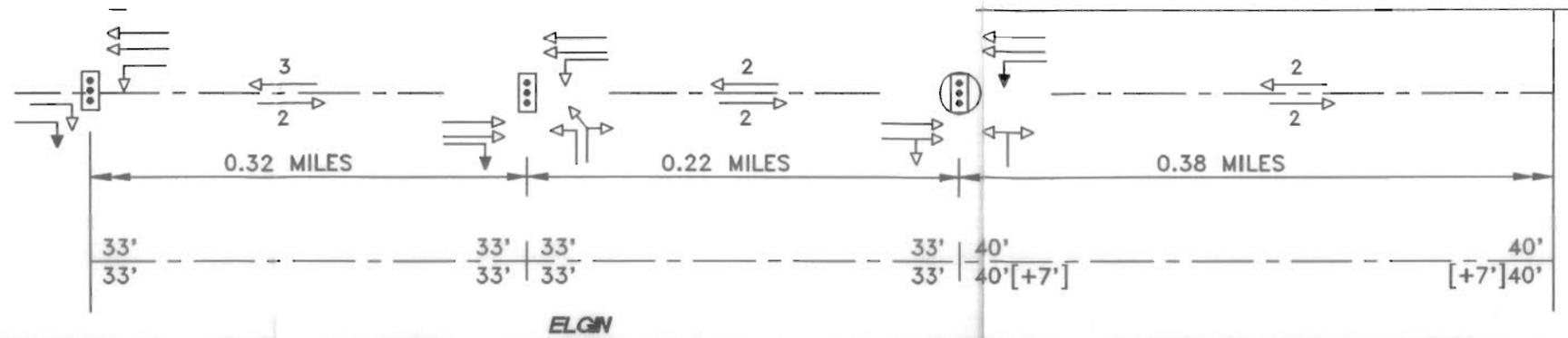


EXHIBIT C1-22B

PROPOSED LANE CONFIGURATION

SIGNAL SPACING

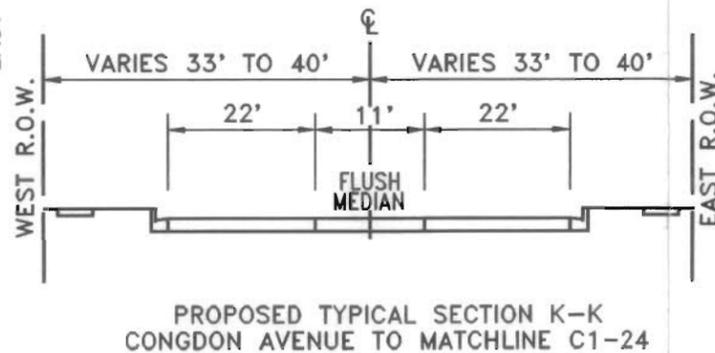
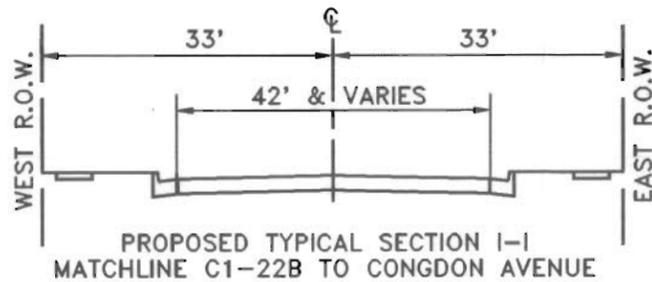
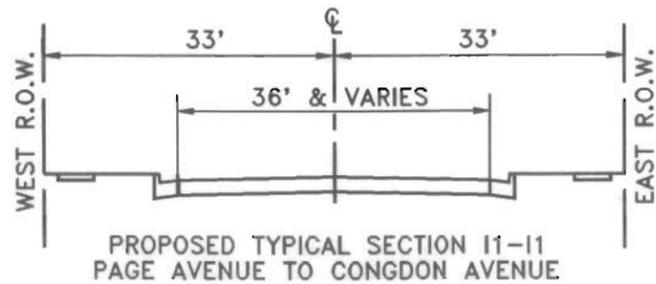
PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



AERIAL PHOTO DATE: 2-09-92

DESCRIPTION OF PROPOSED CONDITIONS:

- * Close access to west Congdon Avenue.
- * Limit Hill Avenue to right in / right out
- * IL Route 25 is one-way Northbound from Page Avenue to Congdon Avenue



LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
33'	= EXISTING RIGHT OF WAY DISTANCE
[7']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= PROPOSED TRAFFIC SIGNAL
	= EXISTING TRAFFIC SIGNAL TO BE REMOVED
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= RIGHT IN / RIGHT OUT
	= CLOSE ACCESS

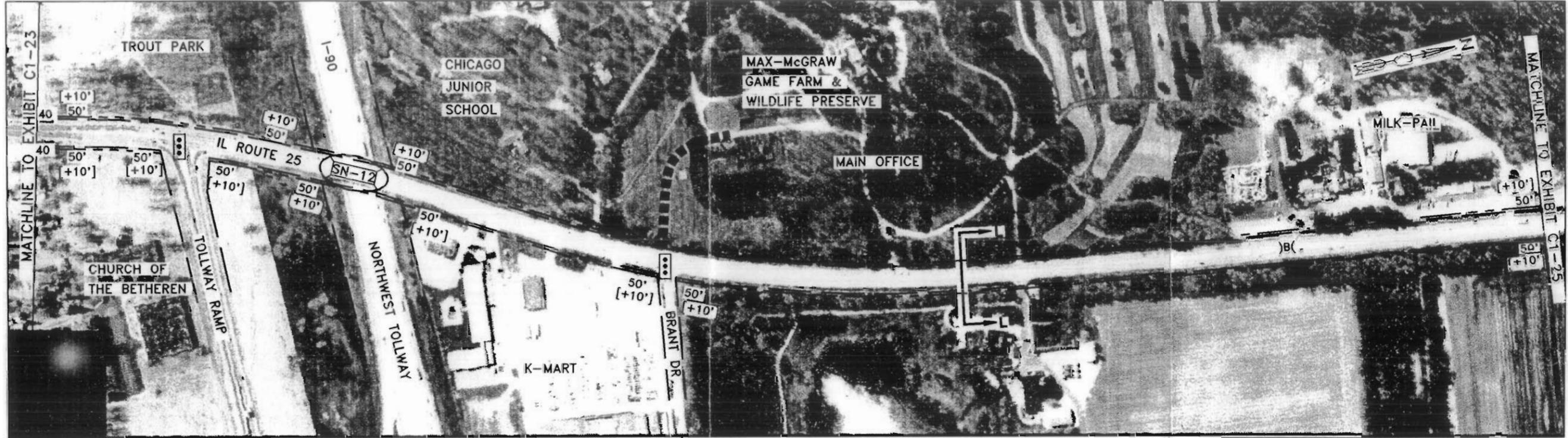
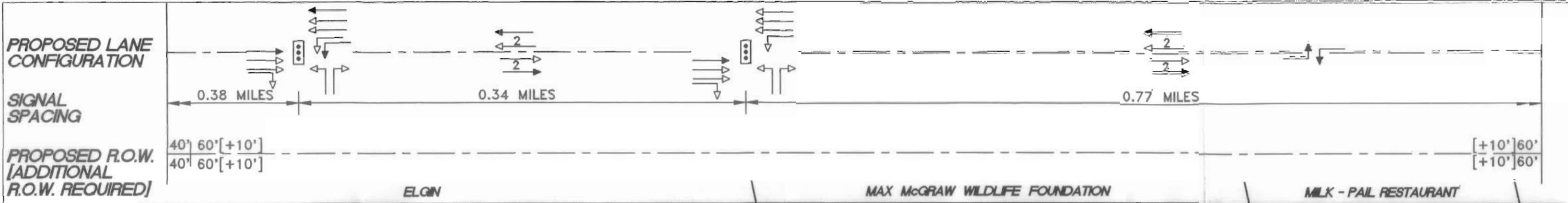
IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

Illinois Department of Transportation

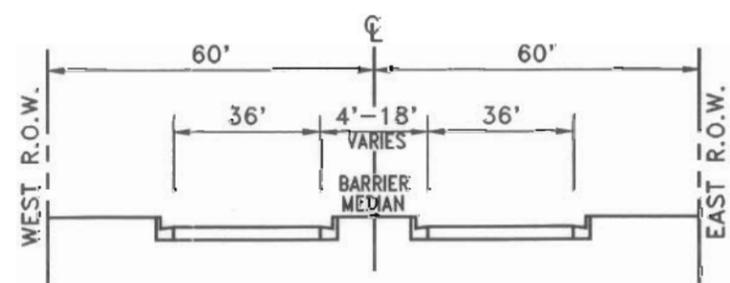


SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY



DESCRIPTION OF PROPOSED CONDITIONS:

- SN-12 = STRUCTURE NUMBER 045-0042
 Modification of this structure will be necessary to accommodate the proposed roadway section.
- * Median break with full access should be provided to Milk-Pail Restaurant.
 - * Max-McGraw Game Farm & Wildlife Preserve main entrance should be moved across from Brant Drive.

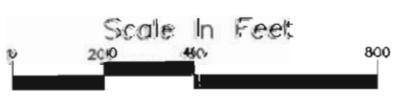


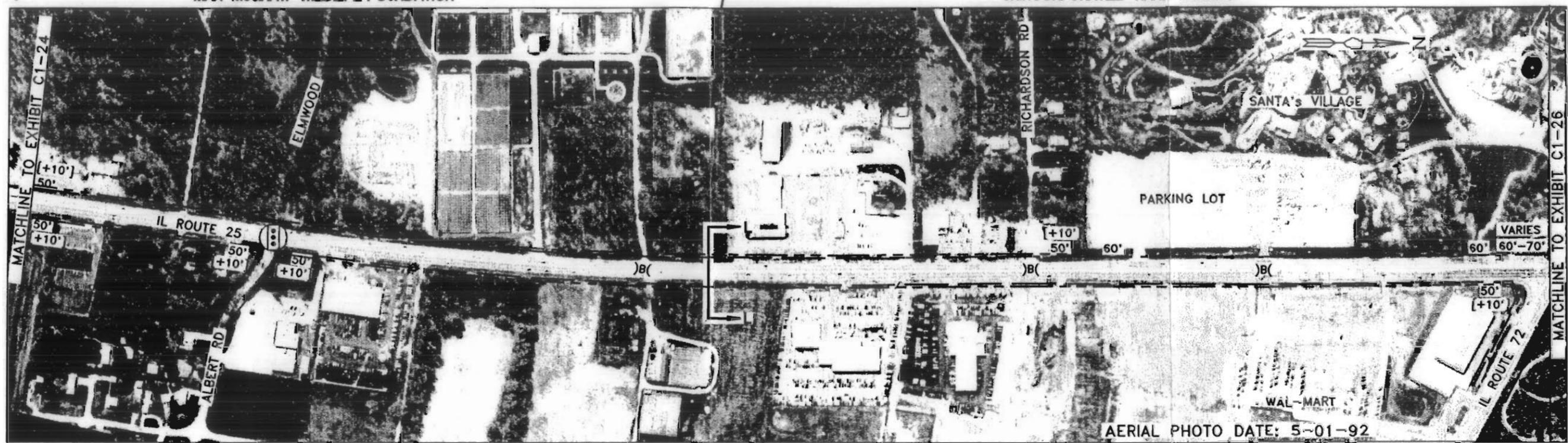
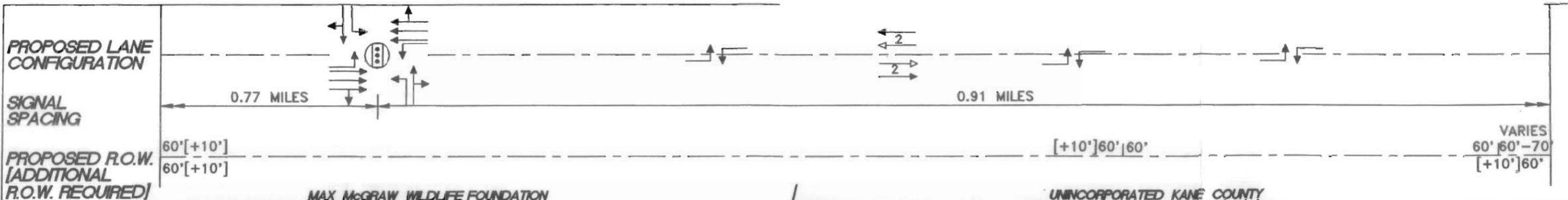
PROPOSED TYPICAL SECTION L-L
 MATCHLINE C1-23 TO MATCHLINE C1-25

LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
50'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= MEDIAN BREAK
	= MODIFY EXISTING STRUCTURE
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY
	= NEW ENTRANCE

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

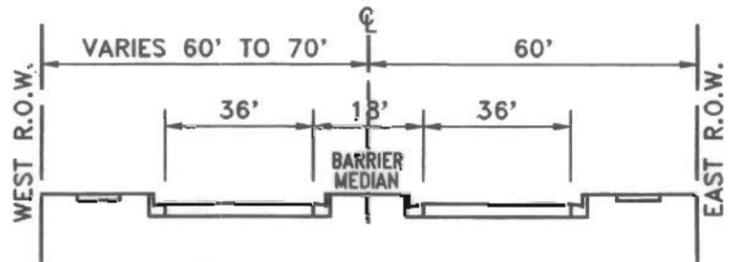
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF PROPOSED CONDITIONS:

- Median break with full access should be provided to Richardson Road and other major commercial entrances along IL Route 25.



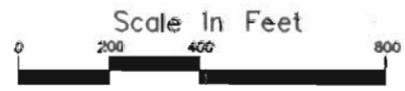
PROPOSED TYPICAL SECTION L-L
MATCHLINE C1-24 TO MATCHLINE C1-26

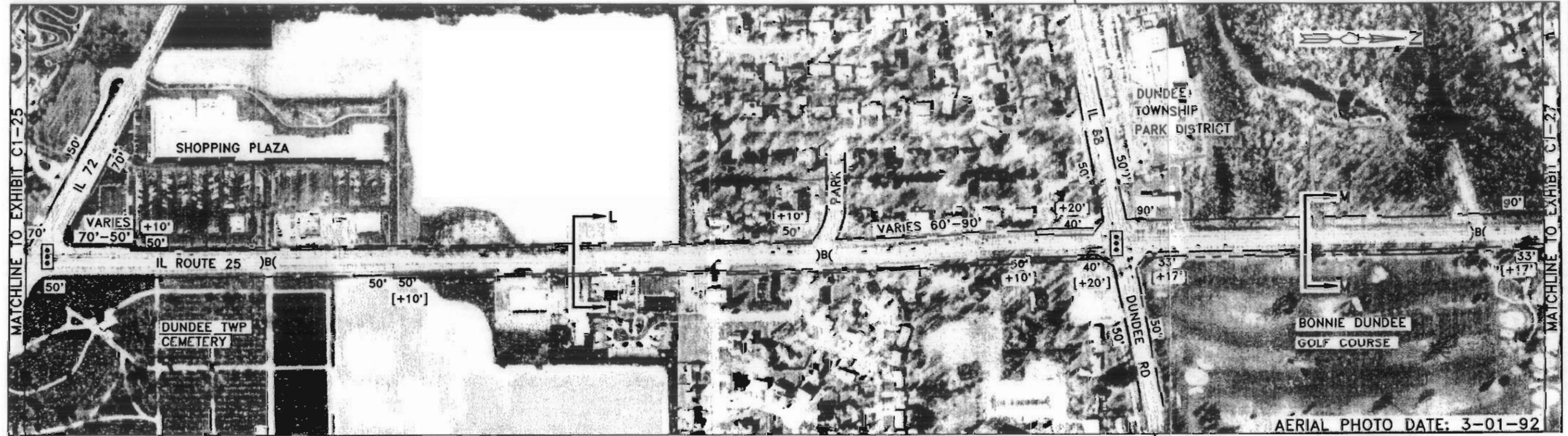
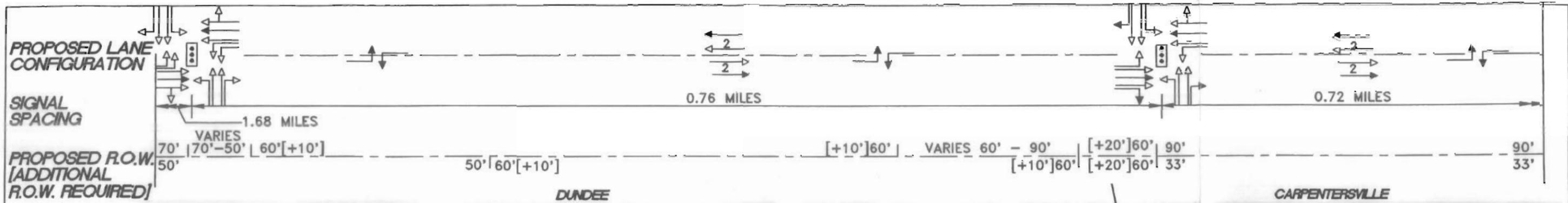
LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
60'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
	= PROPOSED TRAFFIC SIGNAL
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



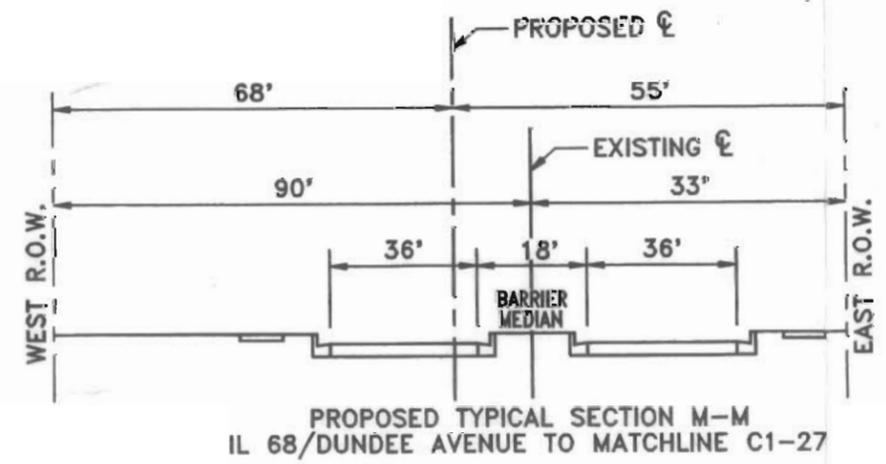
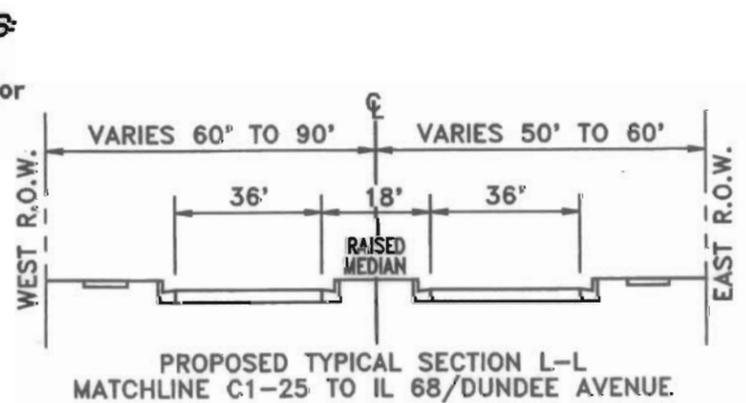
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





DESCRIPTION OF PROPOSED CONDITIONS:

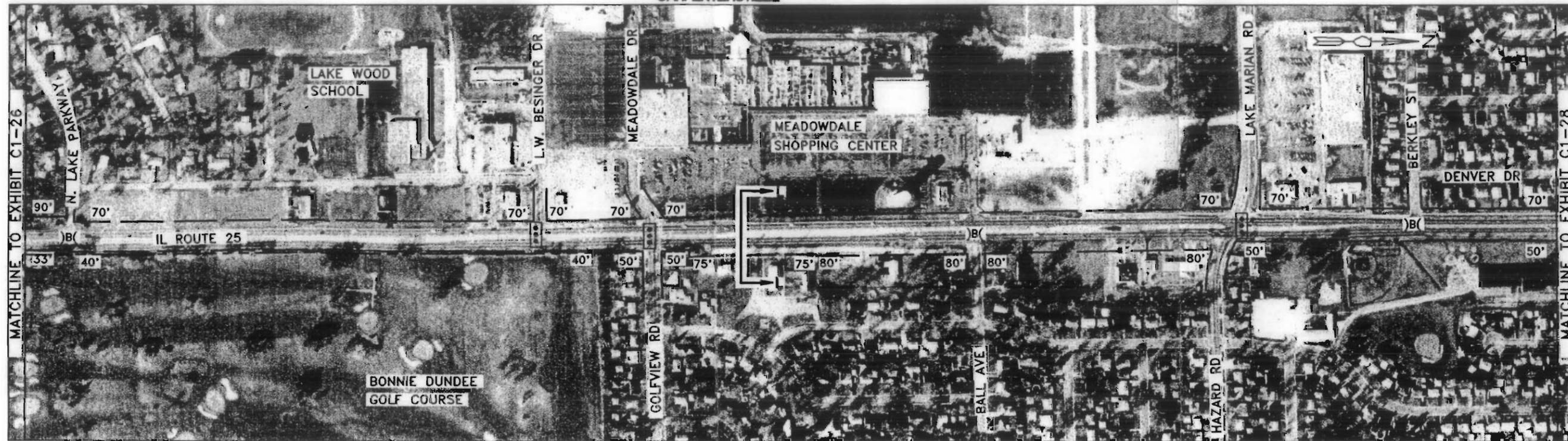
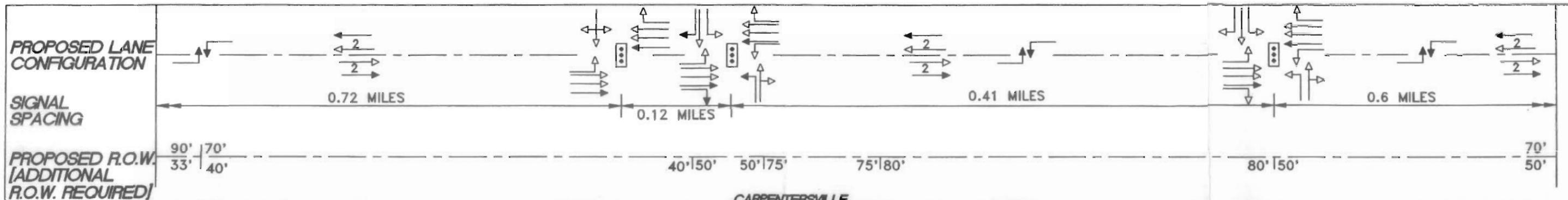
- * Provide median break for access to major commercial entrances, Park street, and Bonnie Dundee Golf Course entrance.



LEGEND	
	EXISTING RIGHT OF WAY
	PROPOSED RIGHT OF WAY
50'	EXISTING RIGHT OF WAY DISTANCE
[+10']	PROPOSED RIGHT OF WAY DISTANCE
	EXISTING TRAFFIC SIGNAL
)B(	MEDIAN BREAK
	EXISTING TRAFFIC LANE CONFIGURATION
	PROPOSED TRAFFIC LANE CONFIGURATION
	CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS

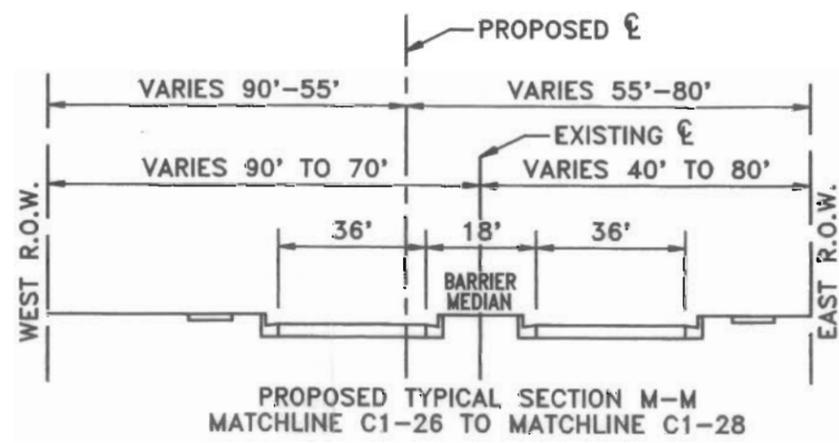




AERIAL PHOTO DATE: 3-01-92

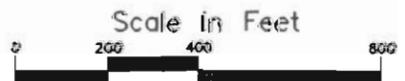
DESCRIPTION OF PROPOSED CONDITIONS:

* Provide median breaks with full access to N. Lake Parkway, Ball Avenue, and Berkley Street.



LEGEND	
	EXISTING RIGHT OF WAY
	PROPOSED RIGHT OF WAY
70'	EXISTING RIGHT OF WAY DISTANCE
[+10']	PROPOSED RIGHT OF WAY DISTANCE
	EXISTING TRAFFIC SIGNAL
)B(	MEDIAN BREAK
	EXISTING TRAFFIC LANE CONFIGURATION
	PROPOSED TRAFFIC LANE CONFIGURATION

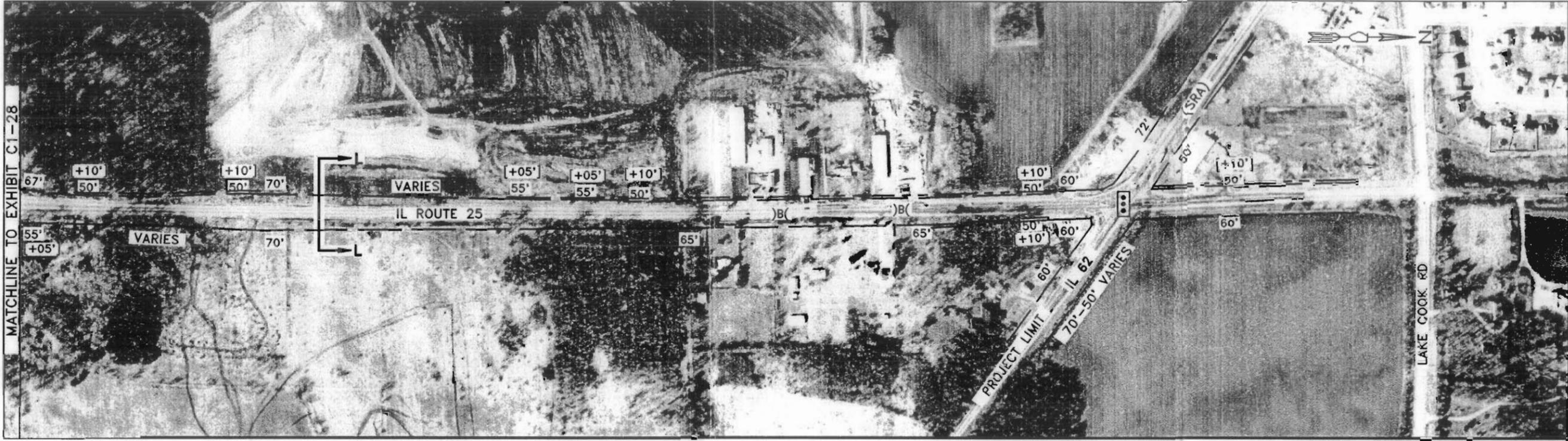
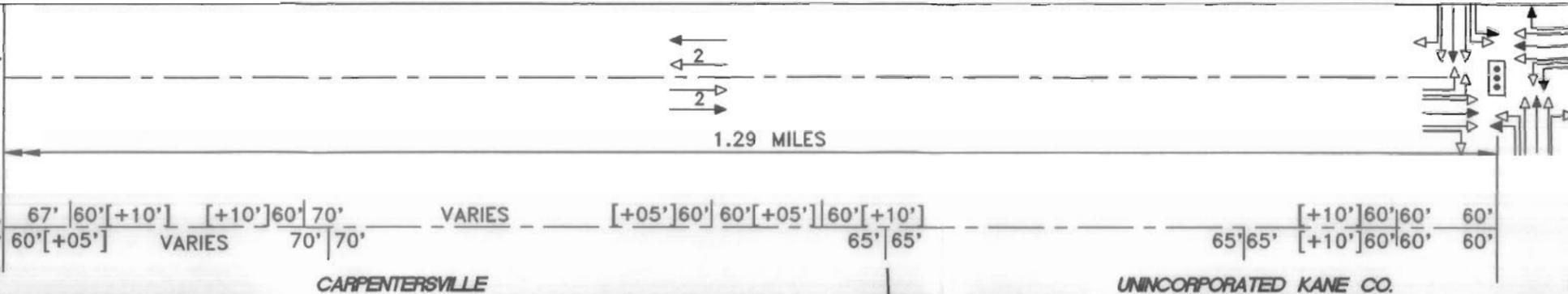
IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



PROPOSED LANE CONFIGURATION

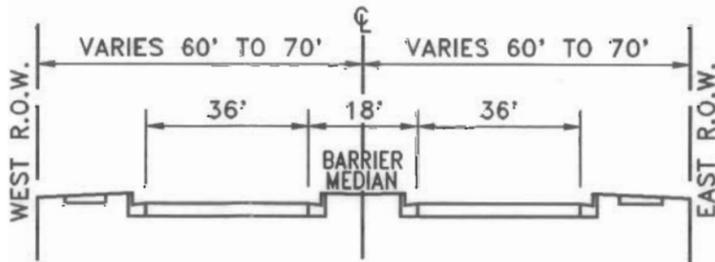
SIGNAL SPACING

PROPOSED R.O.W. [ADDITIONAL R.O.W. REQUIRED]



AERIAL PHOTO DATE: 3-01-92

DESCRIPTION OF PROPOSED CONDITIONS:



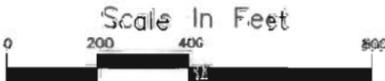
PROPOSED TYPICAL SECTION L-L
MATCHLINE C1-28 TO ILLINOIS ROUTE 62

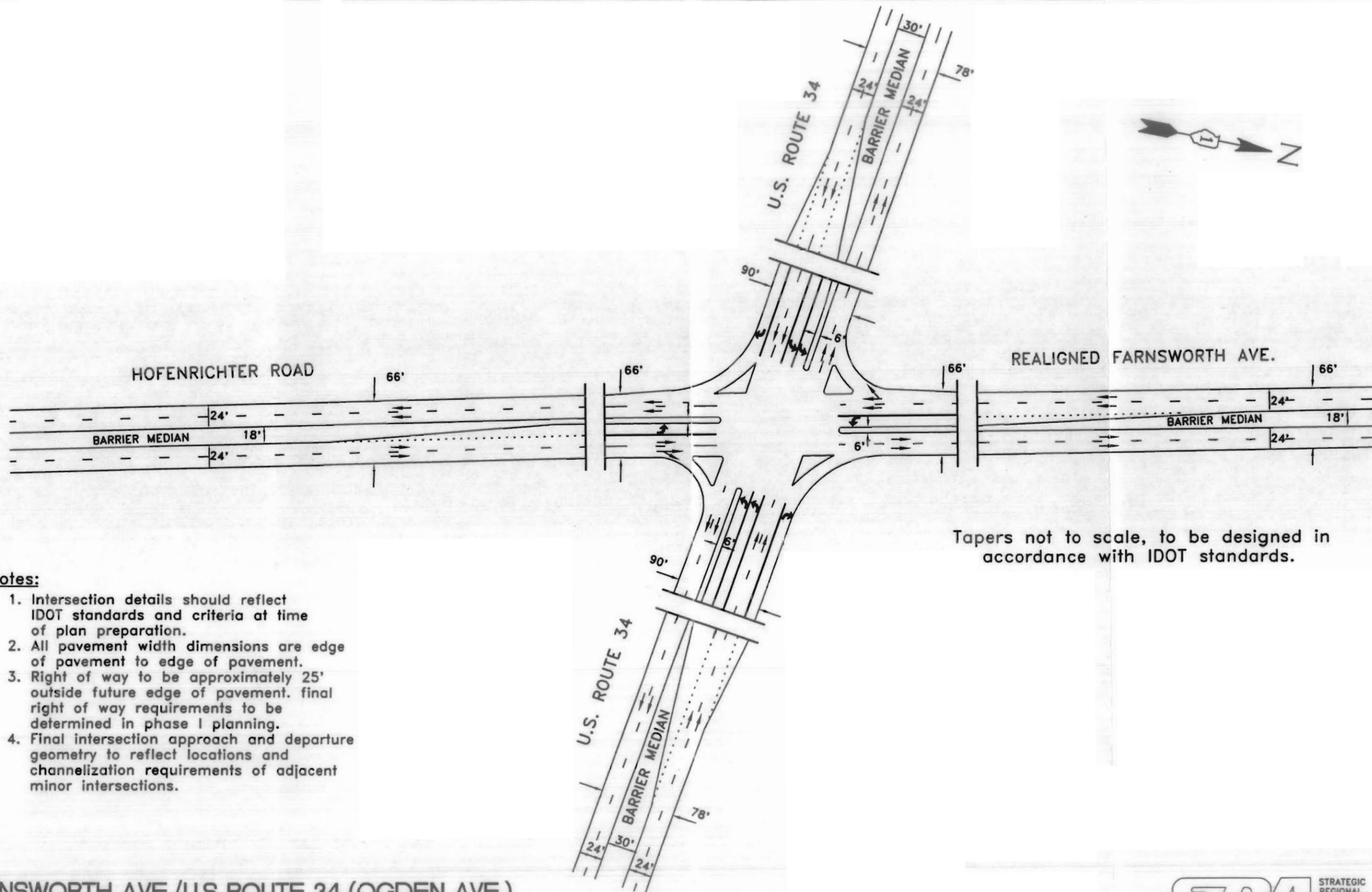
LEGEND	
	= EXISTING RIGHT OF WAY
	= PROPOSED RIGHT OF WAY
70'	= EXISTING RIGHT OF WAY DISTANCE
[+10']	= PROPOSED RIGHT OF WAY DISTANCE
	= EXISTING TRAFFIC SIGNAL
)B(	= MEDIAN BREAK
	= EXISTING TRAFFIC LANE CONFIGURATION
	= PROPOSED TRAFFIC LANE CONFIGURATION
	= CITY/VILLAGE BOUNDARY

IL 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH AVENUE - PROPOSED CONDITIONS



Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the





Tapers not to scale, to be designed in accordance with IDOT standards.

Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

FARNSWORTH AVE./U.S ROUTE 34 (OGDEN AVE.)

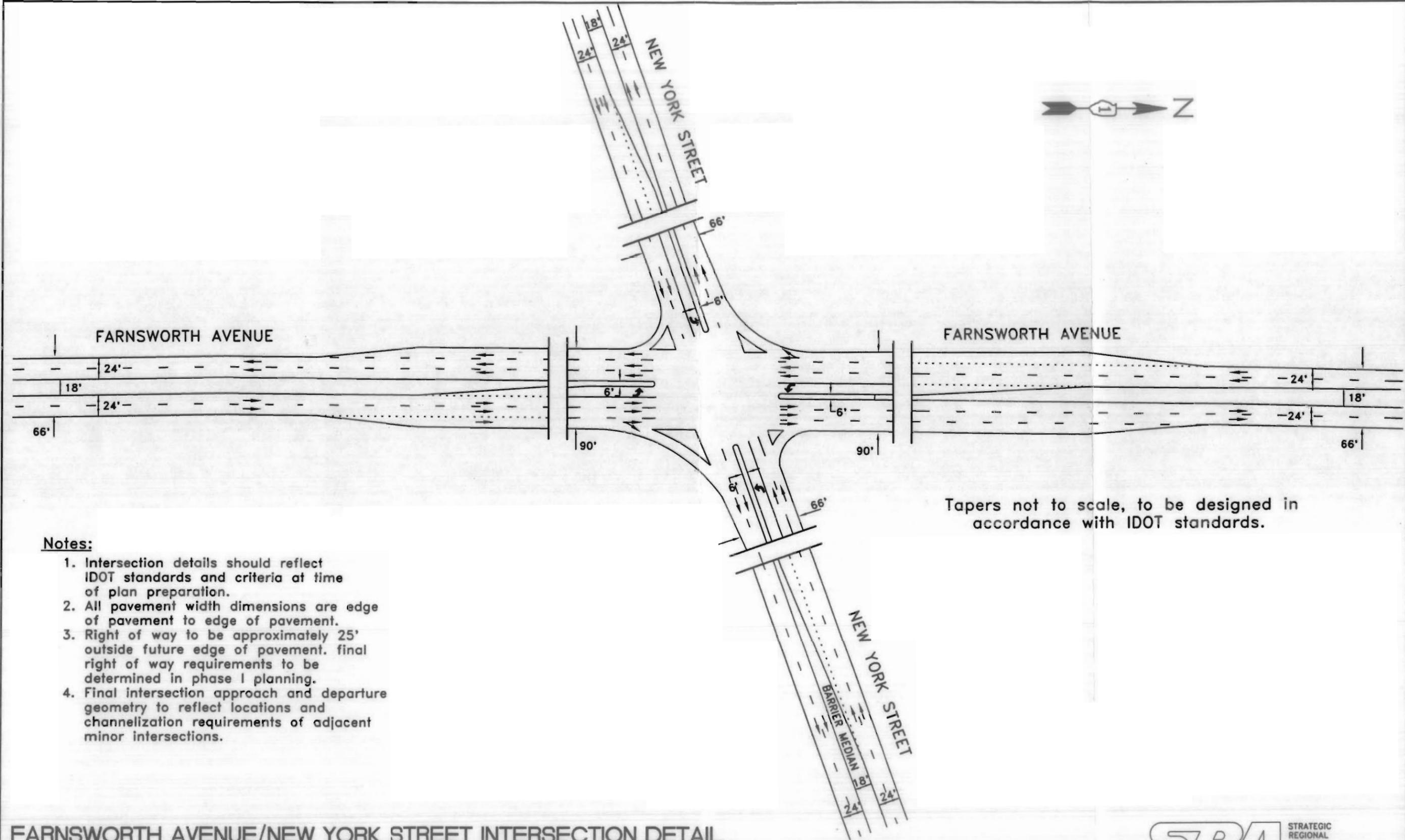
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the
 Illinois Department of Transportation

NOT TO SCALE



EXHIBIT D1-01

C:\PROJECTS\17049.016\GCOM-REV\FARN-340WG



Tapers not to scale, to be designed in accordance with IDOT standards.

Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

FARNSWORTH AVENUE/NEW YORK STREET INTERSECTION DETAIL

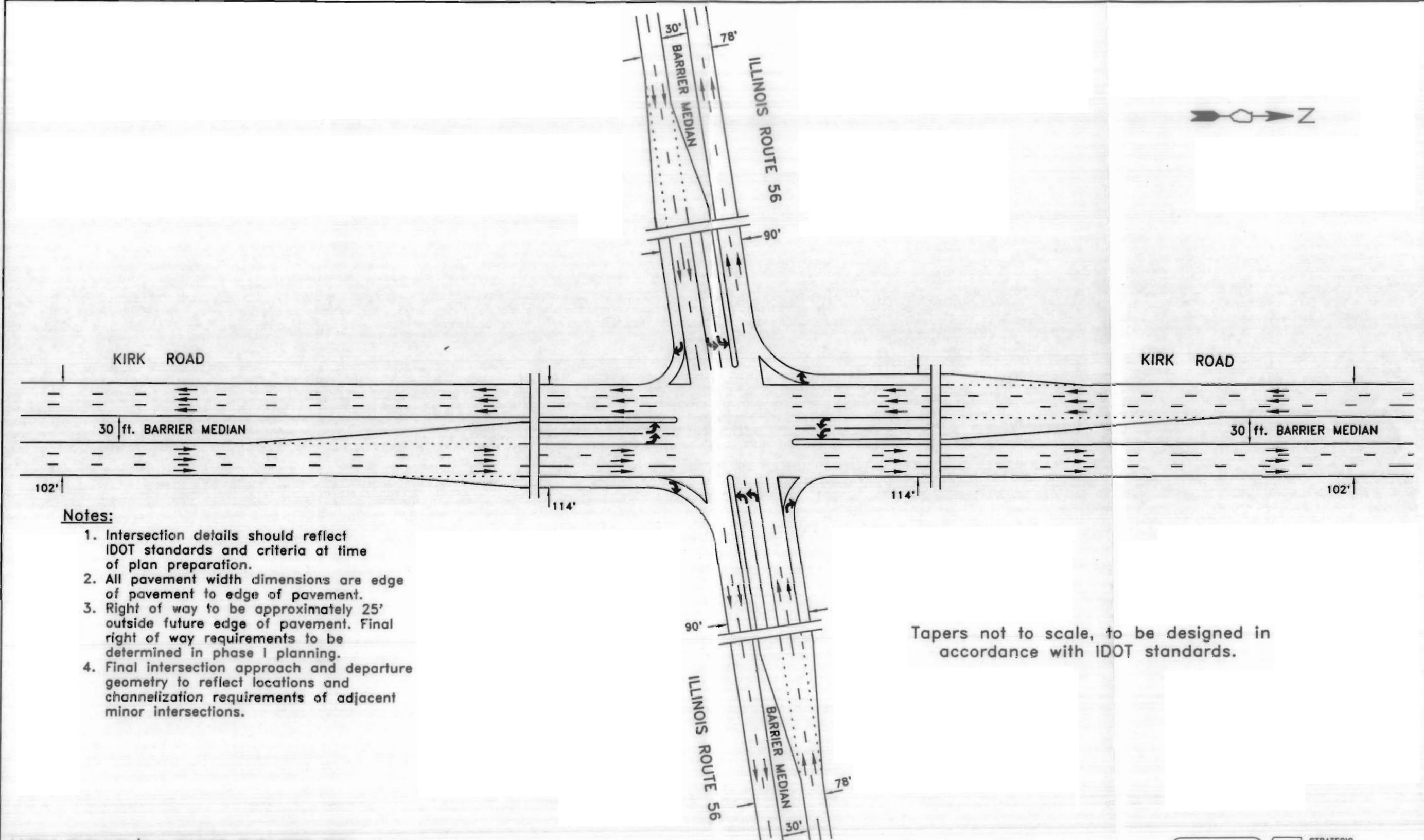
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the
 Illinois Department of Transportation

NOT TO SCALE



EXHIBIT D1-02

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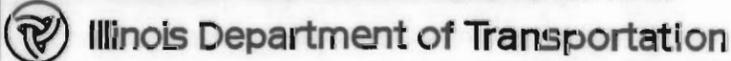
Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. Final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

Tapers not to scale, to be designed in accordance with IDOT standards.

KIRK ROAD/ILLINOIS ROUTE 56

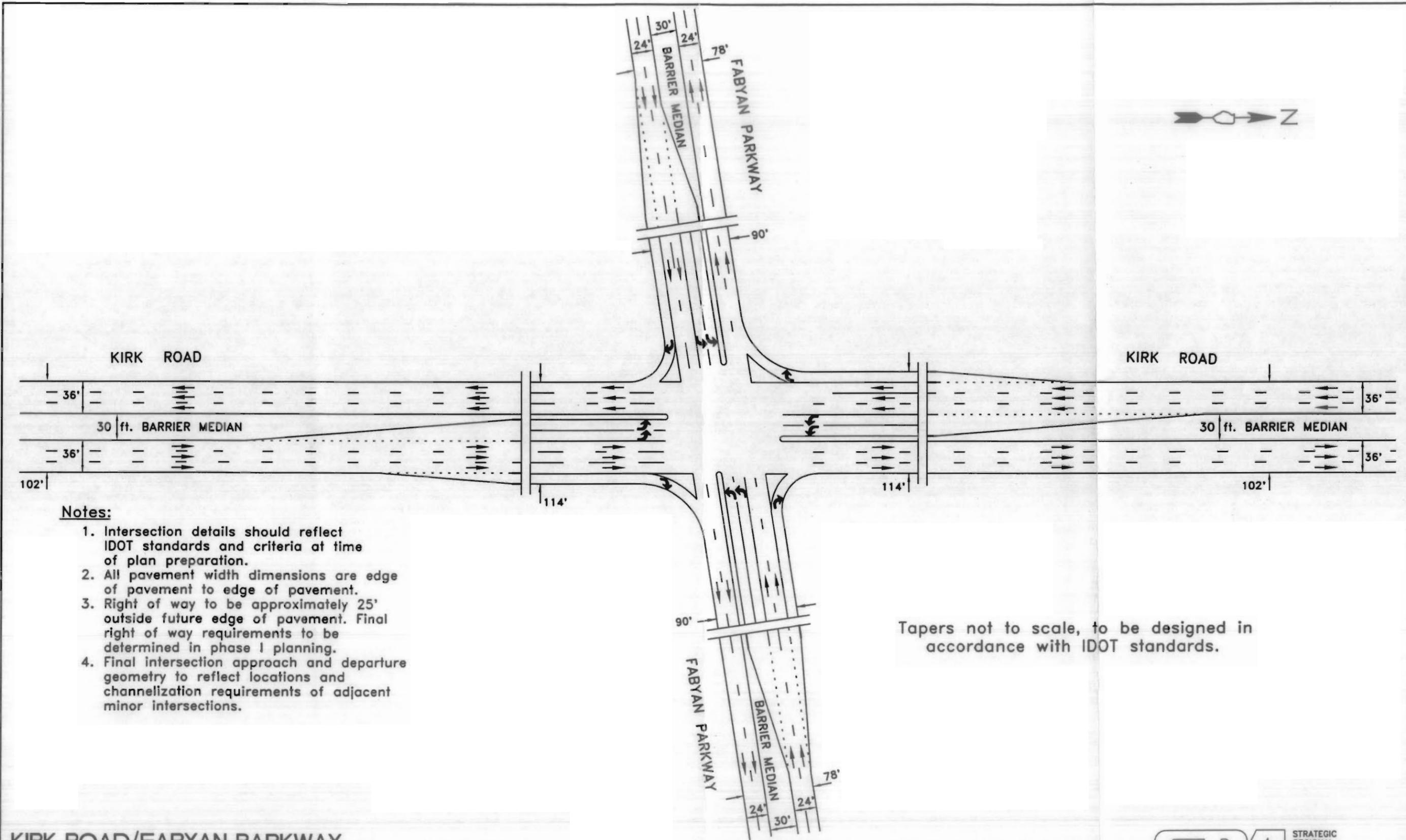
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



NOT TO SCALE



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Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. Final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

Tapers not to scale, to be designed in accordance with IDOT standards.

KIRK ROAD/FABYAN PARKWAY

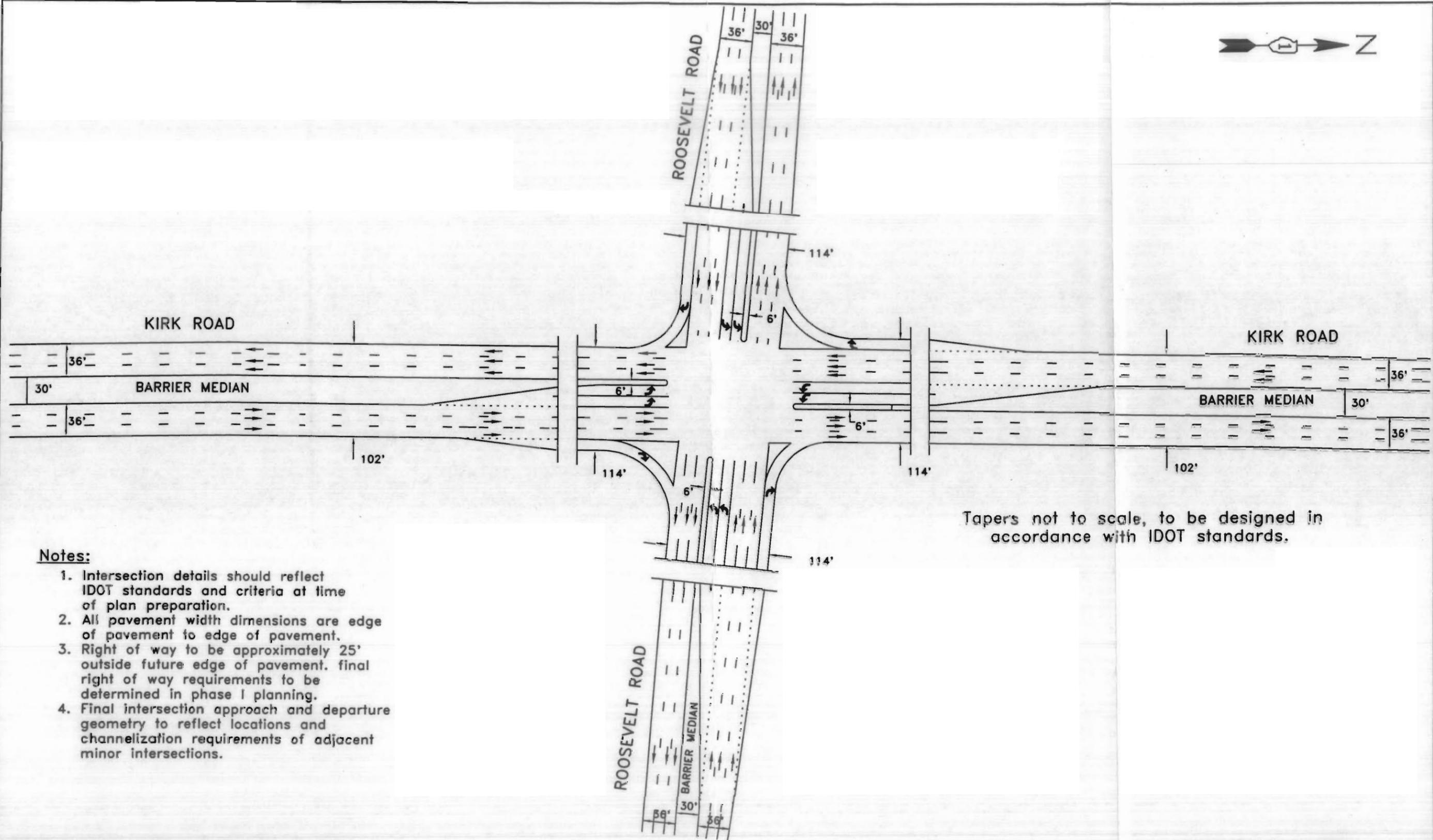
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



NOT TO SCALE



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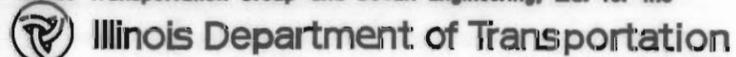
Tapers not to scale, to be designed in accordance with IDOT standards.

Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

KIRK ROAD/ROOSEVELT ROAD

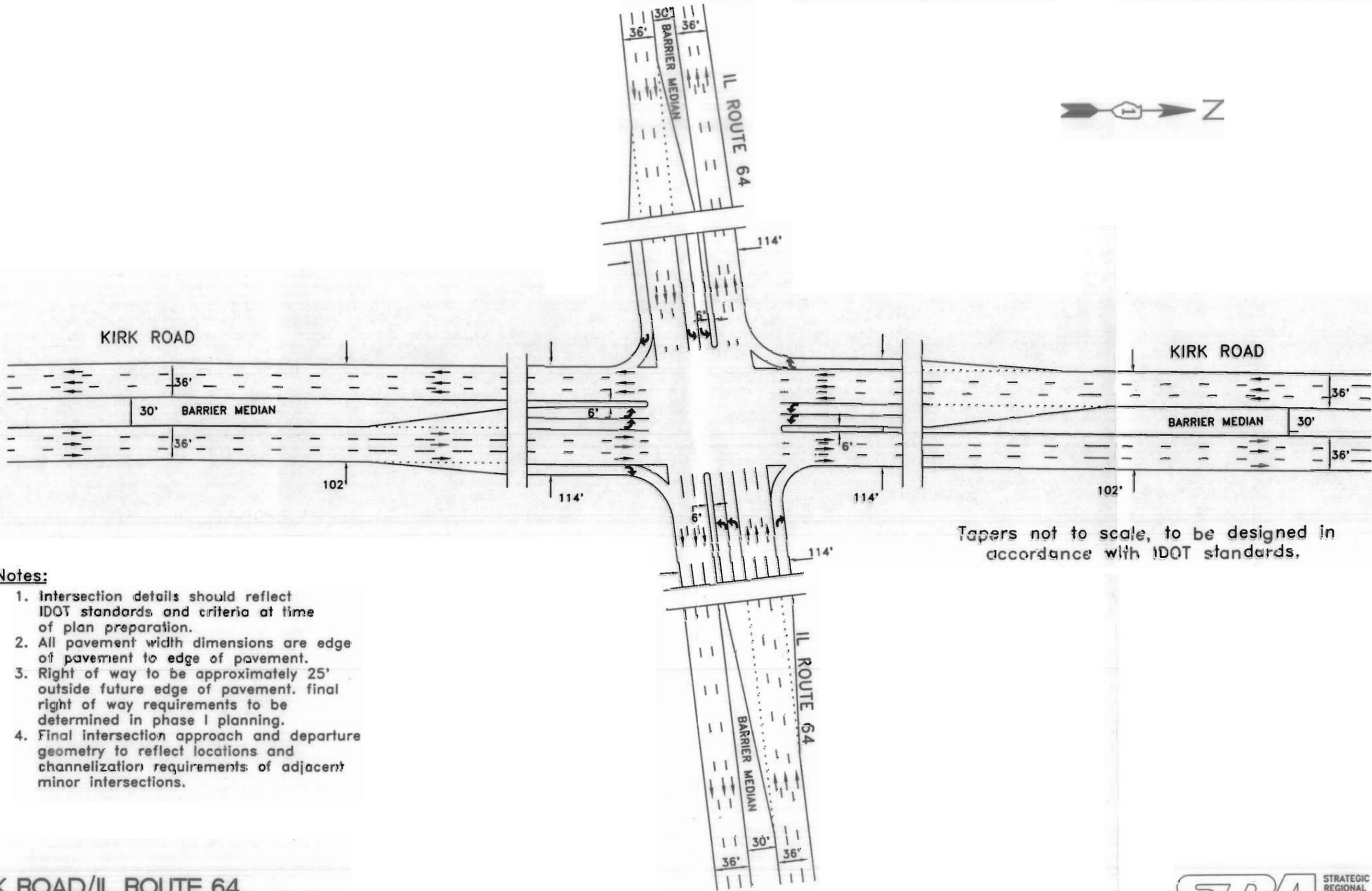
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the



NOT TO SCALE



EXHIBIT D1--05



Tapers not to scale, to be designed in accordance with IDOT standards.

Notes:

1. intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

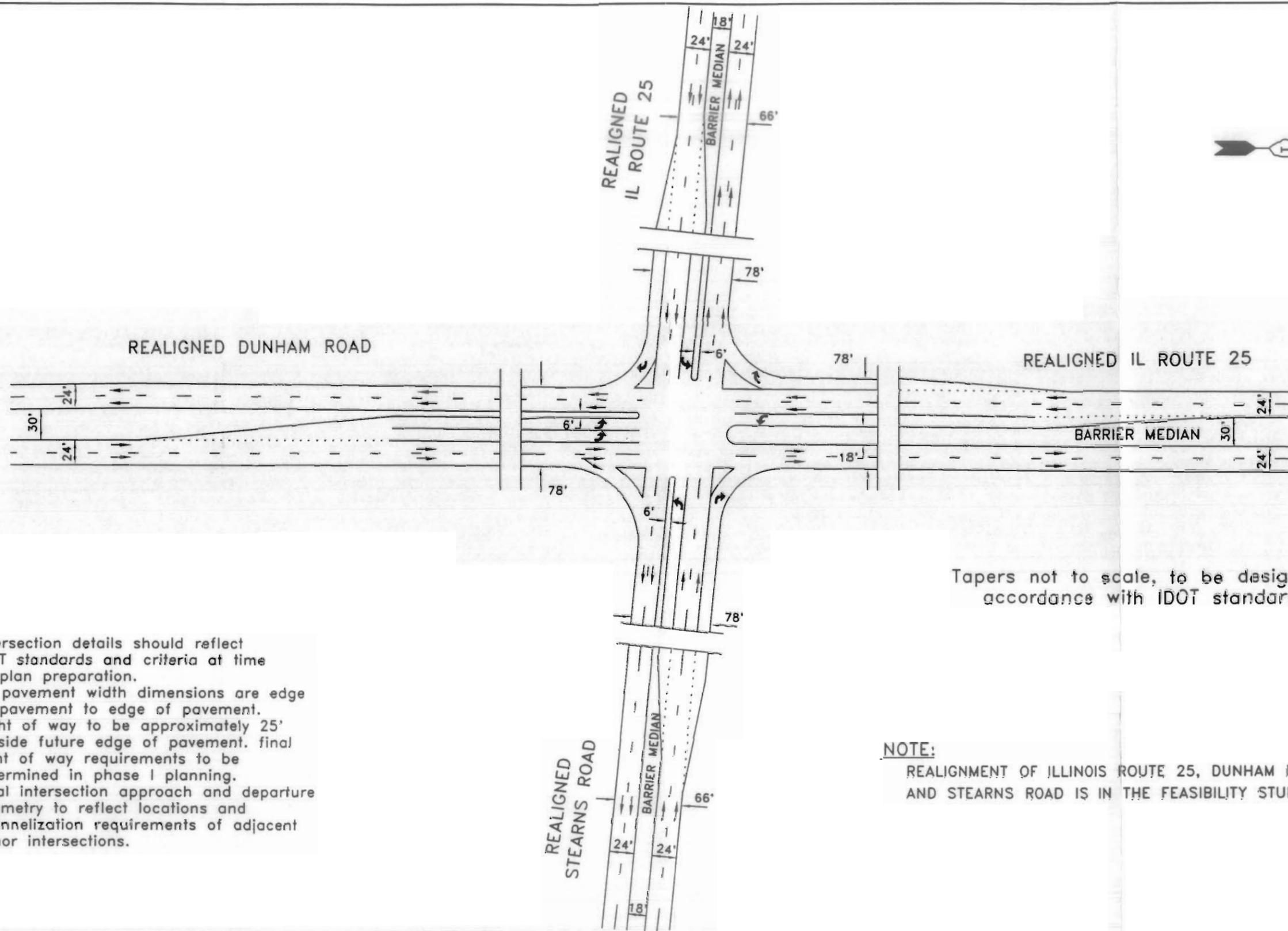
KIRK ROAD/IL ROUTE 64

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NOT TO SCALE



G:\PROJECTS\17049.016\GCOM-REV\KIRK-64



Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

Tapers not to scale, to be designed in accordance with IDOT standards.

NOTE:

REALIGNMENT OF ILLINOIS ROUTE 25, DUNHAM ROAD AND STEARNS ROAD IS IN THE FEASIBILITY STUDY PHASE.

REALIGNED IL ROUTE 25/DUNHAM ROAD/STEARNS ROAD

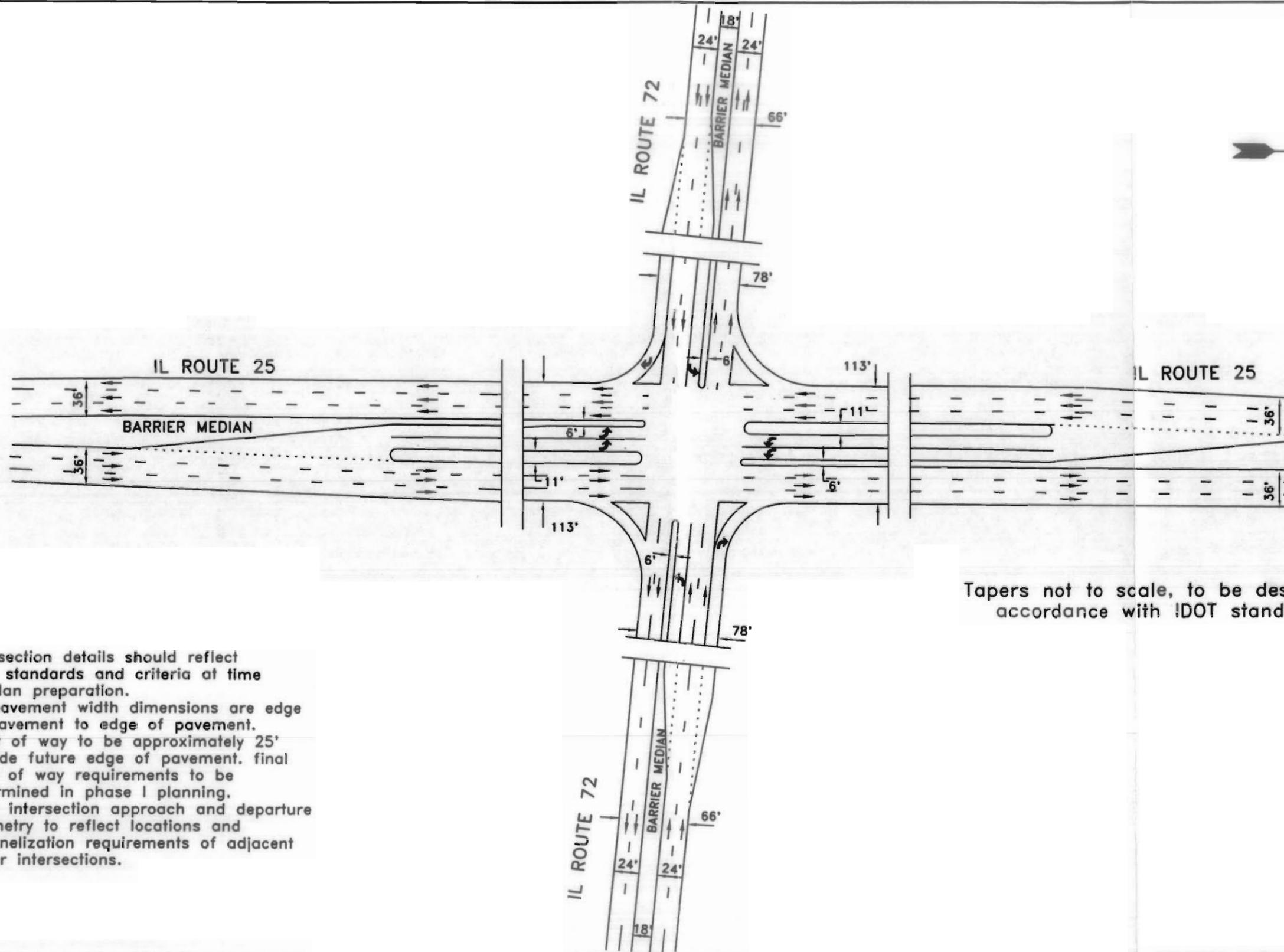
Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

 Illinois Department of Transportation

NOT TO SCALE

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

EXHIBIT D1-07



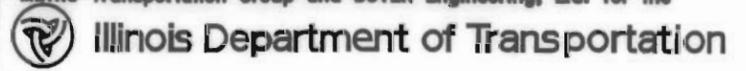
Tapers not to scale, to be designed in accordance with IDOT standards.

Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Right of way to be approximately 25' outside future edge of pavement. final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

IL ROUTE 25/IL ROUTE 72

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

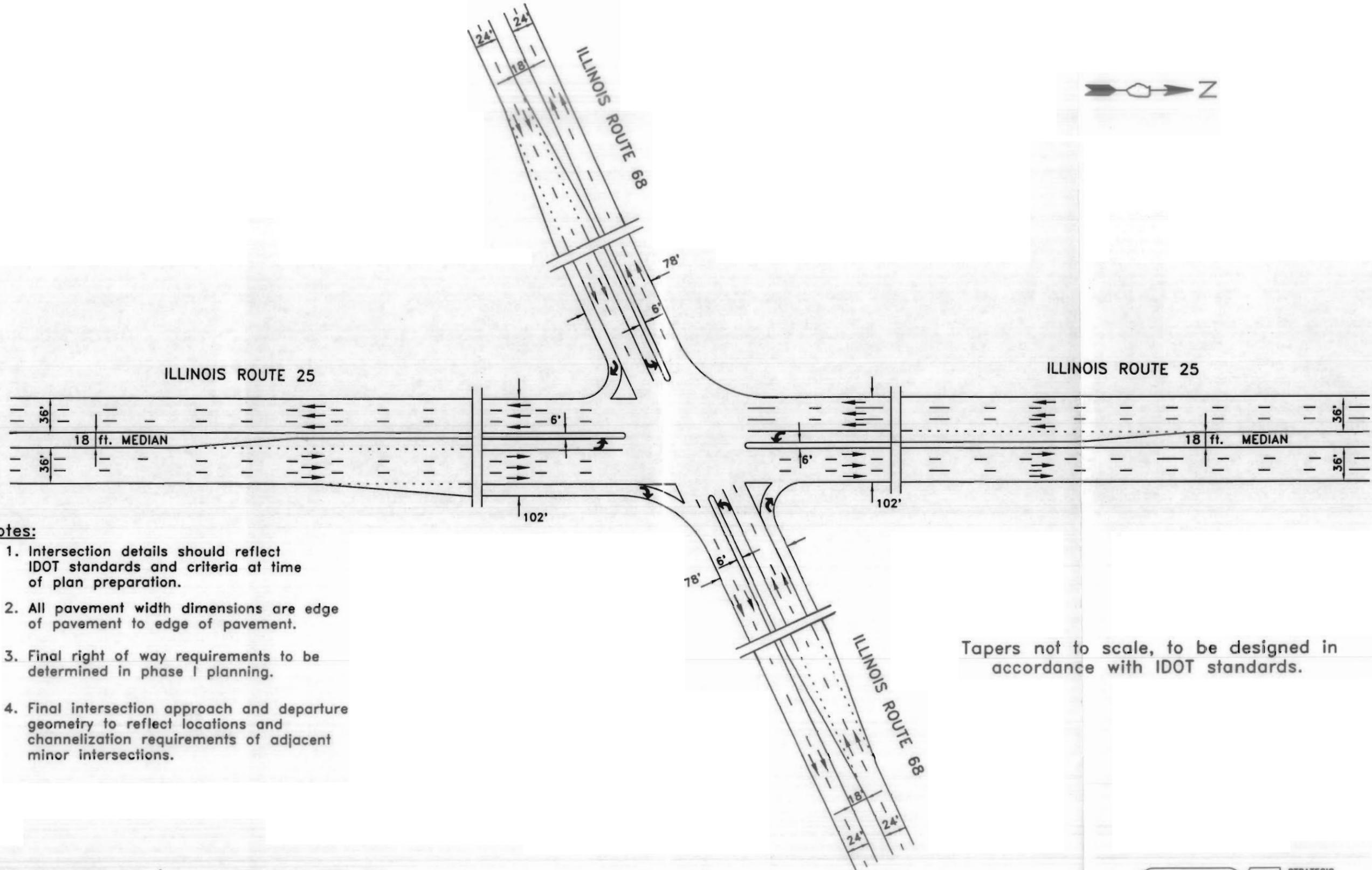


NOT TO SCALE



EXHIBIT D1-08

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Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

Tapers not to scale, to be designed in accordance with IDOT standards.

ILLINOIS ROUTE 25 / ILLINOIS ROUTE 68

Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the

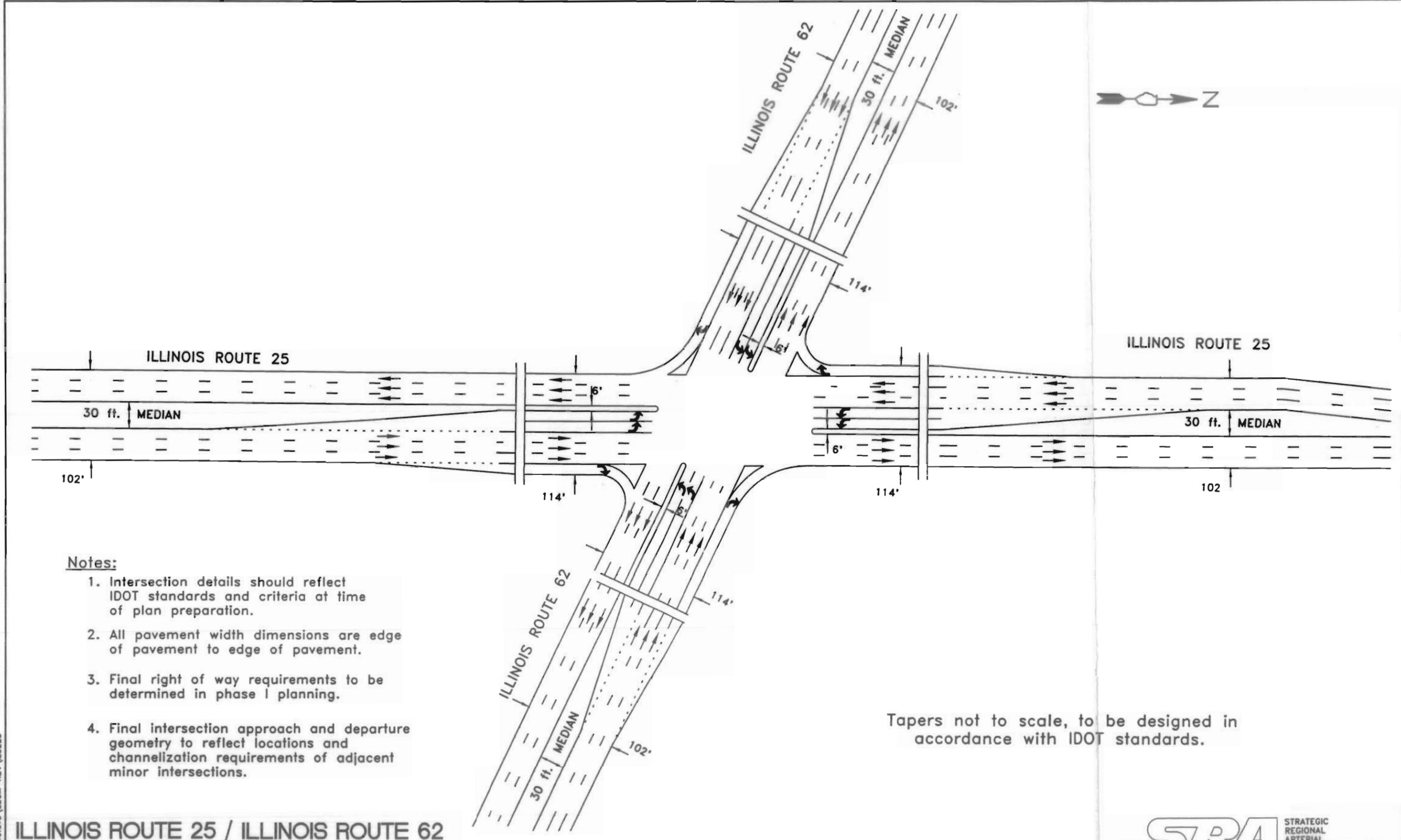
Illinois Department of Transportation

NOT TO SCALE

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

EXHIBIT D1-09

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Notes:

1. Intersection details should reflect IDOT standards and criteria at time of plan preparation.
2. All pavement width dimensions are edge of pavement to edge of pavement.
3. Final right of way requirements to be determined in phase I planning.
4. Final intersection approach and departure geometry to reflect locations and channelization requirements of adjacent minor intersections.

Tapers not to scale, to be designed in accordance with IDOT standards.

ILLINOIS ROUTE 25 / ILLINOIS ROUTE 62

NOT TO SCALE

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Prepared by DAMES & MOORE/MCE in association with METRO Transportation Group and BOYER Engineering, Ltd. for the
 Illinois Department of Transportation

SRA STRATEGIC REGIONAL ARTERIAL PLANNING STUDY

EXHIBIT D1-10

PUBLIC INVOLVEMENT

IL 25/DUNHAM RD/KIRK RD/

FARNSWORTH AVE



STRATEGIC
REGIONAL
ARTERIAL
PLANNING STUDY

STRATEGIC REGIONAL ARTERIAL STUDY

INDIVIDUAL COMMUNITY INTERVIEWS

CORRIDOR #1

IL ROUTE 25/DUNHAM ROAD/KIRK ROAD/FARNSWORTH ROAD

SUMMARY OF FINDINGS

April 8, 1994

Revised May 9, 1994

Summary of Interviews:

This "Individual Community Interview" process represents a significant departure from the past practices employed in conducting these studies of Strategic Regional Arterial Corridors. It acknowledges the principal role played by local government in determining not only the adjacent future land use but more so in influencing the eventual architecture of the roadway. It posits the axiom that working in concert with local political and professional leaders is the best way to assure the acceptance and eventual construction of these new "lifelines of transportation".

The 21 interviews with 45 individuals for this route were conducted over a two week period and comprised 15 municipalities, a county highway department, a major science research facility, a major general aviation airport, two wildlife and forest preserve organizations, a multi-municipal waste agency and the state department of conservation:

<u>Date:</u>	<u>Name:</u>	<u>Position:</u>	<u>Local Government:</u>
10/12/93	Donald Foster	City Administrator	City of West Chicago
10/13/93	Valerie Salmons James Plonczywski Dale Marting	Village Administrator Director of Community Development Village Engineer	Village of Bartlett
10/13/93	Jeffrey Schielke Gary Larson	Mayor Director of Public Works	City of Batavia
10/14/93	Edward Berry J. P. Della Maria Ruth Walker	Village President Village Trustee Village Trustee	Village of Wayne
10/14/93	G. Van De Voorde Robert Malm J. Kristiansen Jerry Deering	Mayor Acting City Manager Director of Public Works Senior Planner	City of Elgin
10/14/93	Jill A. Yucius David Smith	Village President Village Administrator	Village of East Dundee
10/15/93	Fred T. Norris C. J. Chismark	Mayor City Engineer	City of St. Charles
10/15/93	Susan Moylan Carol Thompson	President Executive Director	DuKane Valley Council
10/15/93	W. P. Otilie Phil Page	Mayor Director of Admin. Services	City of Geneva

10/19/93	Wayne Lampa	Planner	DuPage Forest Preserve District
10/20/93	Stan Koenig	Executive Director	Max McGraw Foundation
10/21/93	Maggie Cole	Heritage Director	IL Dept. of Conservation
10/21/93	James Kempe Robert Kosin	Village President Director of Admin. Services	Village of Barrington Hills
10/22/93	Mark Isackson Richard Zirk	Village Administrator Director of Public Works	Village of South Elgin
10/22/93	John Skillman Curt Carver Scott Killiger	Village President Village Manager Director of Public Works	Village of Carpentersville
10/22/93	Ted Spella William Ganek	Village President Village Administrator	Village of Algonquin
10/25/93	David L. Pierce J. Nanninga Rusty Erickson	Mayor Director of Public Works Director of Planning	City of Aurora
10/25/93	Thomas Fawell	Executive Director	DuPage County Airport Authority
10/26/93	Nabi Fakroddin Tom Cieslica Terry Heffron	Director of Transportation Deputy Director Planner	Kane County Highway Department
10/27/93	Dr. D. Theriot Dr. R. Stefanski M. F. Hansen	Associate Dir. Technology Associate Dir. Technology Project Manager	Fermi National Laboratory
10/27/93	Brooke Beal	Executive Director	Solid Waste Agency of Cook County

Summary Format:

Each final summary report is a distillation of the information garnered at the various interviews. In some cases, those interviewed disagreed with each other over a particular position. This report, therefore, attempts to portray the consensus of those responding and not unanimity. All respondents were unanimous, however, in their comments that this process of individual interviews was far superior to the panel approach utilized in previous SRA study efforts.

While this report attempts to summarize and reflect the local attitudes towards possible SRA improvements, it cannot guarantee that each articulated position can be accommodated. Its contents will

be forwarded to the Illinois Department of Transportation and the Corridor Design Engineering Team to be used as a guideline in evaluating the corridor.

Corridor Overview:

Like most corridors, this SRA reflects the diversity of both land uses and the political landscape. Its general character is best captured in a vision of distinct segments, each bearing little resemblance to one another. There is a sense that the road is not a major regional traffic arterial but rather a series of local arterials linked together by regional planners.

At its southern terminus, there is a strong desire to link U.S. Route 34 and U.S. Route 30 to I-88, and to see the Farnsworth Avenue SRA extended south to U.S. Route 30 so that various developments and improvement options could be implemented. Cooperation with Naperville is possible linking that community's 95th Street corridor with Farnsworth extending to U.S. Route 30. Aurora is already in the process of widening and improving sections of Farnsworth within its corporate limits.

As the road continues northward to I-88, there is significant potential for additional office and commercial activity along I-88 at the Farnsworth Avenue interchange. The affected municipalities are thus desirous of providing development opportunities north to IL Route 56.

From I-88 to north of North Avenue, the issue of possible future Fox River crossing locations complicates individual municipal views towards this corridor's improvement. In addition, the adjacent land uses reflect municipal attitudes toward possible widening opportunities. Communities with planned light industrial and commercial development along the corridors favorably view the typical SRA concept along Kirk Road in this section. Other communities have significant adjacent residential development and would oppose significant upgrading proposals. Safety enhancements on "Killer Kirk" and intersection improvements would, however, elicit area support.

Fermi Lab recognizes the need for improvements to Kirk Road. Representatives indicate a need for a left turn lane on southbound Kirk Road at the Pine Street entrance to the laboratory. They also indicated bike path safety and accessibility concerns, including the end for a Prairie Path overpass over Kirk Road near Butterfield Road and a Prairie Path extension along the east side of Kirk Road between Butterfield and Pine Street connecting to the bike paths located on the Fermilab site. Concerns regarding grade-separating bike path crossings of the corridor, particularly along the Prairie Path, were also expressed by the DuPage County Forest Preserve District. The proximity of this facility and the office-industrial complex to the north make this area a candidate for widening. The desire for industrial expansion in the immediate DuPage airport area remains strong despite the decision to abandon the Fox Valley Expressway. In fact, more attention seems fixed upon this SRA corridor as a direct fallout of that decision.

The North Avenue intersection is the scene of continued intense commercial retail development to serve the residential enclaves in the St. Charles, Batavia and Geneva areas. However, north of this area, there is significant upscale residential development and therefore significant resistance to widening proposals.

The Village of Wayne has taken significant steps to thwart any widening proposal through its area through the placement of conservation easements along the rights of way. Also, the Oak Lawn Farm Historic District, which is listed on the National Register of Historic Places, includes property from all four corners of the Dunham Road/Army Trail Road intersection. The Village is eager to discuss by-pass alternatives around their community. Politically, the opposition in this area necessitates a full discussion of alternatives. While most other areas can be mitigated to provide at least a four lane cross-section, this area poses the most significant obstacle to this route functioning as a regional SRA.

The proposed balefill at West Bartlett Road, along with the existing landfills south along this corridor, represent just some of the diverse land uses along IL Route 25 in the area immediately north of Wayne. Bartlett and South Elgin are expanding in this area and are thus supportive of corridor improvements. The balefill controversy, the recently established Tri-County Land Preserve and the South Elgin landfill site all heighten area environmental concerns in this section south of Elgin.

As IL Route 25 enters the City of Elgin on the south, there is heavy industrial use, gravel operations and some minor retail activity. The corridor enters older residential areas as it continues into the east side of the City, passing directly through a state-designated historical district. From US 20 north, the opportunities for even four lane widening are remote; such recommendations will rekindle previous fears and political responses. Either a four- or six-lane alternative will activate the strong opposition of the City of Elgin. Other options must be considered in this area.

An alternative of sorts is represented by some interest to see regional traffic swing west of the river to IL Route 31 and thus avoid the older residential neighborhoods. Access to downtown and the riverfront is desired due to the new Casino Complex and IL Route 31 serves as that linkage from the I-90 corridor to the north. A connection between this SRA corridor and this other state route could assist the City in serving its riverfront and downtown while avoiding substantial dislocation in the residential areas. A major drawback to this alternate is that it would take SRA regional traffic back and forth across the Fox River, where the lack of bridge crossing is already a problem.

North of Elgin, IL Route 25 crosses I-90 and serves, in conjunction with IL Route 72, as a conduit for the growing population in the northern Fox Valley communities. Widening alternatives in this region are limited given the presence and influence of the Max McGraw Foundation. However, sufficient right of way may exist for some improvement; the lack of existing curb cuts or intersections seems to keep traffic moving at a level of service that may preclude a six lane cross-section. Environmental concerns and sensitivities are keen in this section from I-90 to IL Route 72.

East Dundee and Carpentersville view IL Route 25 as currently adequate and effective as a transportation corridor. As this roadway cuts their communities in two further widening would be viewed with caution. The potential for greater right of way in this area exists, and discussions with these municipalities about other road tie-in possibilities and intersection enhancements could prove fruitful.

There was a suggestion to extend the SRA the few hundreds of feet to Lake-Cook Road from its present Route 62 terminus. However, some protective right of way purchasing has occurred to limit the corridor

from moving further north to a Fox River crossing. Also, there would appear to be considerable adjacent residential development that would make that alternative controversial.

Specific Area Assessments:

Specific areas that merited comments during the interviews were:

The City of Aurora plans to widen and improve Farnsworth to New York Avenue and beyond. They also desire to provide some form of U.S. Route 30 and 95th Street connection and/or bypass.

The industrial corridor south of North Avenue continues to foster needs for further corridor widening. However, the complexion north of this area changes dramatically with little desire for widening or other SRA type related improvements.

The Village of Wayne desires to study other options such as:

Kirk/North Avenue/Smith/Kirk or

Kirk/County Club Road/IL Route 25

Wayne will strongly oppose widening and other non-essential improvements in the immediate area around the Army Trail Road intersection.

The Prairie Path and other bike trails running parallel or over the corridor pose unique problems to substantial widening efforts and would therefore require substantial mitigation measures. However, a tie in with the proposed state park efforts and conservation areas proposed for the balefill site present some possible planning options.

Given the residential density and proximity in Elgin, no substantial widening is possible along IL Route 25. The possibility of cooperation with the city exists especially if the alternative of a connection west of the river is pursued.

Any improvement efforts north of I-90 in the Max McGraw area require the ultimate of planning and mitigation measures (i.e. limits on actual construction window, enhanced storm water management control). Further widening will be opposed and may not be justified.

The areas north of I-90 are focused on the intersection with IL Route 72 (Higgins) and the area's ability to serve that particular commercial-retail corridor. The increased traffic operations caused by the Sears and related developments are a cause of concern and the dominance of the Higgins Road corridor over IL Route 25.

East Dundee is desirous of a protected turning bay at Santa's Village and view the forty-one acres in this area as having prime development potential in the future. Further development to the East will necessitate a new road northeast to Higgins.

Next Steps:

As stated earlier, this report will be forwarded to the Illinois Department of Transportation and the Corridor Design Engineering Team for their use in evaluating possible improvements. The results of those efforts will be presented to a "corridor panel" comprised of elected officials from each community in the Summer of 1994.

The recommendations of the panel will be used to formulate the design reports which will be presented to the panels and eventually in a public hearing. In the interim, should there be any questions concerning this report or the progress of these evaluations, individuals are encouraged to contact us.

**ADVISORY PANEL I WORKSHOP
MEETING MINUTES
SRA Corridor #1
Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue**

Date: January 16 & 17, 1996

Location: Potwattamee Park Community Center, City of St. Charles
Elgin Community College Conference Center, City of Elgin

Subject: Strategic Regional Arterial, Subset #4
Corridor #1 - Illinois Route 25/Dunham/Kirk/Farnsworth

Attendees: Rich Starr, Illinois Department of Transportation
Debra Duerr, Dames & Moore/MCE
Thomas Ngo, Dames & Moore/MCE
Terry Heffron, Kane County Council of Mayors
Fred Norris, City of St. Charles
Bob Hupp, City of St. Charles
Ray Stefanski, Fermilab
Mark Koenen, City of St. Charles
Luis Guarin, City of Aurora
Tom Cieslica, Kane County Department of Transportation
Carol Schoenkart, Village of Wayne
David Smith, Village of East Dundee
Eileen Phipps, Village of Wayne
Tom Wheat, Kane County Department of Transportation
Jack Lamployl, Village of Wayne
Teresa Mask, Daily Herald
Robert J. Smith, Pavia-Marting & Company
Jim Plonczynski, Village of Bartlett
Kevin Kelly, City of Elgin

Copies To: Attendees

The purpose of these meetings was to present the concept for this corridor to the Panel members and solicit their comments. Ms. Duerr of Dames & Moore/MCE presented agenda for the Advisory Panel Workshop and gave a brief overview of the corridor. Mr. Starr gave an overview of the SRA system and stated that the SRA study will assist IDOT in prioritizing the routes for roadway improvement projects.

The route is justified as an SRA based on its use as the major north-south route to access eastern Fox River communities. The recommended concept is also consistent with Kane County plans to improve Kirk Road. The standard SRA cross-section can be achieved for approximately half of the corridor. Jurisdiction over the corridor is shared among the City of Aurora, Kane County, and IDOT. Some of the issues to be resolved include realignments at U.S. Route 34, Illinois Route 25, and Bartlett and Middle Roads; a new section is required to complete the corridor; potential R.O.W. impacts in Aurora, Wayne, and East Dundee; and the selection of a preferred alternative alignment near Wayne and Elgin.

Based on suggestions made through the Individual Community Interviews, several alternative routes were evaluated that would avoid areas of concern to Wayne and Elgin. No viable alternatives could be found in the Wayne vicinity. Through Elgin, Dames & Moore/MCE recommends use of an alternative that uses Villa Street, St. Charles Street, and Dundee Avenue. This alternative has an existing 4-lane cross-section, whereas designated Illinois Route 25 has only 2 lanes. It traverses a commercial area and provides access to the river boat casino, while Illinois Route 25 passes through an historic residential area.

SRA Corridor #1 is divided into six study sections. Mr. Ngo of Dames & Moore/MCE presented details of the recommended concepts for each section. A summary of the existing conditions and recommended concepts is presented below.

Section I - U.S. Route 34 to New York Street

Section I of Farnsworth Avenue begins at U.S. Route 34 (Ogden Avenue) in Kendall County, and continues north to New York Street in Kane County. This section passes through unincorporated Kendall and Kane Counties and the City of Aurora. Jurisdiction over this route segment is the City of Aurora.

Existing conditions:

- a. One lane in each direction with no median
- b. There is no existing roadway from Montgomery Road to Binder Street.

Recommended Concepts:

The concept includes realignment of the U.S. Route 34 (Ogden Avenue) intersection, a new roadway section between Montgomery Road and Binder Street, and 2 lanes in each direction with 12-foot flush median. From Binder Street to New York Street the recommended concept is two lanes in each direction with 11-foot flush median.

Section II - New York Street to Illinois Route 56

Section II of Farnsworth Avenue extends from New York Street to Illinois Route 56. This section passes through the City of Aurora and unincorporated Kane County. It intersects two major arterials, New York Street and Illinois Route 56. This section has several structures including the I-88 Tollway overpass. Jurisdiction over this section belongs to the City of Aurora.

Existing conditions:

Two lanes in each direction with no median - New York Street to Burlington Northern R.R.

Two lanes in each direction with varying median - Burlington Northern R.R. to Illinois Route 56

Recommended Concepts:

The recommended concept from New York Street to Burlington Northern Railroad is two lanes in each direction with no median. North of Burlington Northern RR to Reckinger Road the recommended concept is two lanes in each direction with 10 -12 foot flush median. From Reckinger Road to IL Route 56 three lanes in each direction with barrier median is recommended.

Section III - Illinois Route 56 to Illinois Route 64

Section III is now called Kirk Road extending from Illinois Route 56 (Butterfield Road) to Illinois Route 64 (North Avenue), both of which are SRA routes. This section passes through unincorporated Kane County, the Cities of Batavia, Geneva, and St. Charles, and Fermi National Laboratory. This section intersects two other SRA's, Fabyan Parkway and Illinois Route 38 (Roosevelt Road). Kane County and the City of St. Charles have jurisdiction over this section of Kirk Road. Kane County has completed a Phase I study for this section which will be implemented beginning in 1996. The plan calls for initial construction of 2 lanes in each direction with a shoulder that will be converted to a third lane as demand warrants.

Existing Conditions:

Two lanes in each direction with varying median.

Recommended Concepts:

Three lanes in each direction with 17-foot barrier median.

Section IV - Illinois Route 64 to Illinois Route 25

Section IV of this corridor extends from Illinois Route 64 to the intersection with Dunham Road, at which point the corridor follows Dunham Road to Illinois Route 25. This section passes through St. Charles, Wayne, Bartlett, and unincorporated Kane County. This section intersects a SRA route, Stearns Road. It is under Kane County's jurisdiction. In this section, Kirk Road merges into Dunham Road, which terminates at the intersection of Illinois Route 25. There are R.O.W. restrictions on Dunham Road, particularly through the Village of Wayne. Consequently, a 4-lane cross-section is proposed.

Existing Conditions:

Number of lanes varies from one-two in each direction with painted/barrier median.

Recommended Concepts:

Two lanes in each direction with 12 to 18 foot flush/painted median.

Section V - Illinois Route 25 to Congdon Avenue

Section V of Illinois Route 25 extends from the Dunham Road/Illinois Route 25 intersection to Bluff City Boulevard, where an alternate route uses St. Charles Street, Villa Street, Central Street, and Dundee Avenue in Elgin, and merges back to Illinois Route 25 to provide a continuous 4-lane cross-section. No additional R.O.W. would be required in the city. This section passes through Bartlett, Elgin, and unincorporated Kane County.

Existing Conditions:

Number of lanes varies from one-two lanes in each direction with varying width median.

Recommended Concepts:

Two lanes in each direction with 10 to 18 foot flush/barrier median - Illinois Route 25 to U.S. Route 20

Two lanes in each direction with 0 to 10 foot flush median

Section VI - Congdon Avenue to Illinois Route 62

Section VI merges back to Illinois Route 25 at Congdon Avenue and extends north to Illinois Route 62 (Algonquin Road). This section passes through Elgin, East Dundee, Carpentersville, Barrington, and unincorporated Kane County. A transition to 3 lanes in each direction is proposed from the I-90 Tollway north to Illinois Route 62 (Algonquin Road), another SRA corridor. R.O.W. impacts may occur at the intersection of Illinois Route 72 (Higgins Road).

Existing Conditions:

Number of lanes varies from one-two lanes in each direction with varying width median.

Recommended Concepts:

Three lanes in each direction with 18 to 30 foot barrier median.

Closing Statements

- Mr. Starr stated, the next steps are geometric review and draft reports. It was noted that vital communications between communities and IDOT and their consultants remain open for this project to be successful.

These meeting minutes are assumed to be accurate unless written comments are received within ten (10) days of receipt.

Sat Nagar, P.E.
Corridor Manager

ADVISORY PANEL II MEETING MINUTES

Date: May 13 & May 15, 1997

Location: Pottawattomee Park, St. Charles, Illinois
Elgin Community College, Elgin, Illinois

Subject: Strategic Regional Arterial Subset #4
IL Route 25/Dunham Road/Kirk Road/Farnsworth Avenue

Attendees: Rich Starr, Illinois Department of Transportation
Sat Nagar, Dames & Moore
Thomas Reckert, Kane County Department of Transportation
Jack Cook, Kane County Department of Transportation
Tom Wheat, Kane County Department of Transportation
Fred Norris, City of St. Charles
Bob Hupp, City of St. Charles
Ray Stefanski, Fermilab
Mark Koenen, City of St. Charles
Luis Guarin, City of Aurora
Carol Schoenkart, Village of Wayne
David Smith, Village of East Dundee
Eileen Phipps, Village of Wayne
Jack Lamployl, Village of Wayne
Teresa Mask, Daily Herald
Robert J. Smith, Pavia-Marting & Company
Jim Plonczynski, Village of Bartlett
Kevin Kelly, City of Elgin

Copies to: Attendees

The purpose of this meeting was to present the recommended improvements for the Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue Corridor to the panel members and solicit their comments. Rich Starr began the meeting an introduction and a general overview of the SRA project. Mr. Starr also stated that there is no funding available to build these projects at this time and IDOT will prioritize the projects based on recommendations from the consultants in various subsets. Sat Nagar

presented the existing conditions & recommended improvements to the Illinois Route 25/Dunham Road/Kirk Road/Farnsworth Avenue corridor. The following is a summary of comments for specific locations. A copy of the Issues Summary & Recommended Improvements is attached to these meeting minutes.

Section 1 - Farnsworth Avenue - U.S. Route 34 to New York Street

Section 1 of Farnsworth Avenue begins at U.S. Route 34 (Ogden Avenue) in northern Kendall County, and continues north to New York Street in Kane County. This section passes through unincorporated Kendall and Kane Counties and the City of Aurora.

Recommended cross-section consists of two through lanes in each direction with raised/flush median. Additional right-of-way has to be acquired for the roadway improvements. There were no comments on the recommended improvements for this section.

Section 2 - Farnsworth Avenue - New York Street to IL Route 56

Section 2 of Farnsworth Avenue extends from New York Avenue to Illinois Route 56. This section passes through the City of Aurora and unincorporated Kane County. The Farnsworth Avenue intersects two other SRA routes, US Route 34 and Illinois Route 56 in this section.

Recommended cross-section consists of 2-3 through lanes in each direction with raised/flush median. Additional right-of-way has to be acquired for the roadway improvements. There were no comments on the recommended improvements for this section.

Section 3 - Kirk Road - Illinois Route 56 to IL Route 64

Section 3 of what is now called Kirk Road extends from Illinois Route 56 to Illinois Route 64. This section passes through unincorporated Kane County, Batavia, Fermi National Laboratory, Geneva, and St. Charles.

Recommended cross-section consists of 3 through lanes in each direction with raised median. Additional right-of-way has to be acquired for the roadway improvements. Mr. Rickert noted that existing right-of-way information has some discrepancy at several locations along Kirk Road. Mr. Nagar stated that the existing right-of-way information was taken from the Kane County phase I plans. Mr. Rickert stated that the right-of-way shown on the phase I plans has not been acquired by Kane County completely and it will take several years to complete right-of-way acquisition. Mr. Rickert stated that he will get back to Dames & Moore about the right-of-way information to be shown in the final report.

- Mr. Talsma expressed concern about roadway drainage. Mr. Talsma noted that Kane County is not doing anything to control roadway drainage. No detention has been provided enough to Mr. Nagar explained that the SRA study is conceptual. Drainage, Lighting and many other issues will be analyzed during the Phase I & Phase II study. Mr. Starr expressed the importance of several aspects, but noted that it is difficult to address all these issues in SRA Planning Study.

Section 4 - Illinois Route 64 to Illinois Route 25

Section 4 of Kirk Road extends through Dunham Road from Illinois Route 64 (North Avenue) to Illinois Route 25. This section passes through St. Charles, Wayne, Bartlett, and unincorporated Kane County. This section intersects Stearns Road which is an SRA route.

The recommended cross-section consists of two through lanes in each direction with barrier/painted median. Additional right-of-way will be acquired to provide turn lanes at major cross streets. There was discussion about the CATS projected traffic numbers. Eileen Phipps noted that the projected traffic numbers are too high for this section of the corridor. Mr. Nagar stated that the projected numbers are based on a full SRA build out considering a standard Suburban SRA cross-section.

- Eileen Phipps stated that Village of Wayne is opposed to any kind of widening along Dunham Road through the Village of Wayne. Eileen Phipps noted that there are historic sites on both sides of the Dunham Road at the intersection of Army Trail Road. Mr. Starr suggested that a separate meeting should be held involving the Village of Wayne, IDOT, Dames & Moore and Kane County to finalize the recommended roadway improvements for this section.

Section 5 - Illinois 25/Dunham Road to Congdon Avenue

Section 5 of Illinois Route 25 extends from Dunham Road to Bluff City Boulevard, north of Bluff City Boulevard the SRA route continues along St. Charles Street, Villa Street, Center Street and Dundee Avenue, and merges back to Illinois Route 25 (Exhibit B1-23) at Page Avenue south of Congdon Avenue. This section passes through Bartlett, Elgin and unincorporated Kane County.

- Mr. Nagar stated that this section passes through downtown Elgin and there are existing residential developments on both sides of the route. Mr. Nagar noted that an SRA cross-section can be provided with major improvements at the intersection. The recommended cross-section consists of two through lanes in each direction with painted median.
- The City of Elgin supported the recommended improvements for this section and there were no comments on the recommendations.

Section 6 - Congdon Avenue to Illinois Route 62 (6.60 miles)

Section 6 extends from Congdon Avenue as Dundee Avenue and continues north as Illinois Route 25 to Illinois Route 62 (Algonquin Road). This section passes through Elgin, East Dundee, Carpentersville, Barrington, and unincorporated Kane County.

Closing Statements

Mr. Nagar thanked the panel members for attending the Advisory Panel II Workshop. Mr. Nagar stated that the Public Hearing on this corridor has been scheduled for May 19, 1997 at Campbell House, Kane County Cougar Stadium.

These meeting minutes are assumed to be correct unless written comments are received within ten (10) days.

Sat Nagar, P.E.
Project Engineer

PUBLIC HEARING REGISTER

Project: IL 25/Dunham/Kirk/Farnsworth from IL 62 to US 34

Location: Kane County Events Center **Date:** 5/19/97 **Time:** 2-7 PM

Campbell House

To be added to the mailing list for this project, please provide your complete address below

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Name	Address	Representing
1 ARRY COULSON	P.O. BOX 500 BATAVIA IL Zip 60510	Self _____ Other FERMILAB
2 Martha Nuttall	Box 207 Wayne, IL Zip 60184	Self <u>x</u> Other _____
3 Tom Dorc	_____ _____ Zip _____	Self _____ Other _____
4 Gene McArdle	35 W. 4th PINELANDS ST. CHARLES IL Zip 60174	Self _____ Other FACTS ABOUT BRIDGES
5 RICHARD & HUDREX MORONEY	45266 FARNSWORTH HURON Zip 60504	Self <u>✓</u> Other _____
6 Jon S Dvorn Hane Co. For. Pres.	719 Batavia Ave. Geneva IL 60124 Zip	Self _____ Other For. Pres. Hane Co.
7 Tom Panski	_____ _____ Zip _____	Self _____ Other Beacon News
8 Ted Bergeson	100 NO. FARMER AVE. BATAVIA ILLINOIS Zip 60510	Self _____ Other City of Batavia
9 Jim Plonczynski	228 S. ALLEN ST. BARTLETT ILL Zip 60103	Self _____ Other Village of Bartlett
10 CAROL SCHENKMAN	P.O. Box 532 Wayne Zip 60184	Self _____ Other Village of Wayne
11 Debbie Burrichter	10 Woodland Hills Rd Batavia Zip 60510	Self <u>✓</u> Other Homeowners Batavia
12	_____ _____ Zip _____	Self _____ Other _____

PUBLIC HEARING REGISTER

Project: IL 25/Dunham/Kirk/Farnsworth from IL 62 to US 34

Location: Kane County Events Center

Date: 5/19/97

Time: 2-7 PM

Campbell House

To be added to the mailing list for this project, please provide your complete address below

	Name	Address	Representing
P	1 Audrey Troyer	BOX 409 Wayne IL Zip 60184	Self <input checked="" type="checkbox"/> Other
L	2 KELLY MULLIGAN	BOX 1269 WAYNE Zip 60184	Self <input checked="" type="checkbox"/> Other <input checked="" type="checkbox"/> SPOUSE
E	3 Billie Higham	33 W 112 Stoughton Ave Wayne Zip	Self <input checked="" type="checkbox"/> Other
A	4 Drake Letke	33 W 262 Ashtoy Lane Wayne IL Zip 60184	Self <input checked="" type="checkbox"/> Other
S	5 Barbara Neplek	33 W 262 Ashtoy Lane Wayne IL Zip 60184	Self <input checked="" type="checkbox"/> Other
E	6 Jeanne Weiss	1629 Derby Dr. Batavia Zip 60510	Self <input checked="" type="checkbox"/> Other
	7 HOWARD LEVINE	Box 754 Wayne IL Zip 60184	Self <input checked="" type="checkbox"/> Other
P	8 Richard E. Hart	1034 Woodland Hills Rd Batavia IL Zip 60510	Self <input checked="" type="checkbox"/> Other
A	9 AIZEL WEILER	P.O. 1224 WAYNE Zip 60184	Self <input checked="" type="checkbox"/> Other
I	10 MARK WEILER	P.O. Box 1224 WAYNE Zip 60184	Self _____ Other
N	11 Sully Jones	307 W. + field Dr Geneva IL Zip 60132	Self <input checked="" type="checkbox"/> Other
T	12	_____ _____ Zip	Self _____ Other

PUBLIC COMMENT

PROJECT: Ill Rt 25 / Dunham / Kirk Farmworth IL62-1L34
DATE: 5-19-97

This is a poor use of tax dollars and I will fight this tooth and nail. This road runs through residential and historic areas and it would create a more hazardous roadway to expand this road in the way you describe here.

Putting money into Rt. 59 and Randall Road would solve a lot of the problems you ~~are~~ currently seem to think are gargantuan. The current traffic flow of Blue Page and Kane Counties could be solved in a variety of other ways such as utilizing one-way streets and helping develop viable public transportation so fewer cars are used. This is also a more ecologically sound solution.



Illinois Department of Transportation

NAME: Audrey Trope

ADDRESS: Box 409-33W260 Woodmere, Wayne IL 60184

PUBLIC COMMENT

PROJECT: St. Rt. 25 / Durham / Kirk / Farmwest Tr. 62 to 34
DATE: May 19, 1997

This seems to be a very ambitious project that applies past solutions to present problems. In other words — a short term solution to a long term problem.

The better the highways, the more traffic and the more development of houses, businesses, etc.

I would like to see some of our bright, imaginative minds working toward new solutions to transporting people. Perhaps if our public transportation was more dependable and user-friendly (could take your bike with you, or your dog, etc.) or park a car and take the public transportation.

The intersection at Army Trail and Durham Rd. concerns me. Widening that intersection and adding the lights seems like over-kill. People are smart enough to use stop signs. How does this signal help the people of the community? The houses and ~~businesses~~ ^{all this} that use that corner and removing the trees, etc., definitely affects the character of Wayne. This is an historic area and should be considered an asset to the area, as well as a bit of diversity, instead of a "problem" for IDOT. (2) SRAs can't have stop signs, "grandfather" these in.)



Illinois Department of Transportation

NAME: Martha Vatchell

ADDRESS: Bd 207 - Wayne, Il. 60154

PUBLIC COMMENT

PROJECT: OPERATION GREENLIGHT
DATE: 5/19/97

Put THIS ON THE STATE ROUTE (25) OR (31)

DO NOT RUIN ONE OF THE FEW BEAUTIFUL COMMUNITIES LEFT IN THE MIDWEST. THIS IS AN "EQUESTRIAN" COMMUNITY AND WE PAY VERY HIGH TAXES FOR THE UNIQUENESS IT PROVIDES. THERE ARE LESS AND LESS PLACES LEFT LIKE THIS. WAYNE IS A RARITY IN A WORLD THAT IS BECOMING LESS DESIRABLE EVERY DAY. THERE IS A LOW DENSITY POPULATION IN WAYNE AND WE DON'T DESERVE THIS KIND OF BLATENT DISREGARD FOR OUR LIFESTYLE AND OUR HOMES. WE ARE AN EPICENTER FOR EQUESTRIAN EVENTS & THE WELFARE OF COMPETITORS AND THEIR ANIMALS WILL BE GREATLY JEOPARDIZED BY THE PROPOSED VOLUME OF TRAFFIC. WE NOW HAVE THE ROAD (DUNHAM) MANNED BY PATROL OFFICERS AND TRAFFIC SHUT DOWN WHEN A RIDER FALLS OFF TO PREVENT DISASTER. THE WAYNE DUPAGE HUNT WOULD CEASE TO EXIST. THESE TWO THINGS ARE WHAT WAYNE IS BUILT ON - WHY IT EXISTS!!! GO AWAY!!!!



Illinois Department of Transportation

NAME: ALEXANDRIA WEILER

ADDRESS: PO. 1224 WAYNE, IL 60184

PUBLIC COMMENT

PROJECT: SECTION #4

DATE: 5/19/97

There are two major state routes that parallel the proposed roadway on Dunham Road. These state routes should be used instead of destroying the rural community of Wayne. Please do not ruin the only rural community left in eastern Kane county



Illinois Department of Transportation

NAME: MARK WEILER

ADDRESS: P.O. BOX 1224
WAYNE ILL 60184-1224

IN RE:)
)
STRATEGIC REGIONAL ARTERIAL)
)
OPERATION GREENLIGHT)
)
ILLINOIS ROUTE 25/DUNHAM/)
KIRK/FARNSWORTH FROM ILLINOIS)
ROUTE 62 TO U.S. 34)

GENEVA, KANE COUNTY, ILLINOIS, PUBLIC HEARING

REPORT of comments made at the Public Hearing of the above-captioned study and summary of recommendations, taken before Joan M. Kenny, C. S. R., a Notary Public in and for the County of DuPage, State of Illinois, at the Campbell House, 33 W 848 Cherry Lane, Geneva, Illinois, on Monday, the 19th day of May, A. D. 1997, between the hours of 2:00 and 7:00 P. M.

LARRY COULSON: Larry Coulson from Fermi Lab.

The documents presented us had Fermi Lab marked on drawing C-7 and that is not Fermi Lab. Over here it is marked properly. It is on C-08 and C-09.

It is between the bike path on C-8 and Wilson Street on C-9. So I just want to make sure it gets mark on that properly.

* * * * *

RICHARD MORONEY: Richard Moroney, 9 S 886 Farnsworth Avenue, Aurora Township.

They want more right-of-away on us. They want another 17 feet on our side. We got a six-foot cyclone fence on the front and a full set of pines that -- oh, I don't know how tall.

AUDREY MORONEY: They must be 30 feet some of them.

RICHARD MORONEY: They would take those things right out.

AUDREY MORONEY: And across the road it is undeveloped.

RICHARD MORONEY: I think it is Inland that owns it, undeveloped. So I would sooner have them take it off of that.

Like I was telling the guy, we had -- they put in two bridges. The county put in two bridges two years ago. There was nothing wrong with the culverts that were there, big, circular corrugated culverts.

When they took them out, they couldn't even crush them, they were so strong. But they put new bridges in. And the Township Commission put a bronze plaque on the breach bridge with his name on it. And the county did the work.

When they did that, put the bridge in, the county exposed the four-inch and two-inch gas mains in our creek. They are out of the water. I told the gas company about it and they will not do a thing about it unless they are leaking.

I worked for the gas company for thirty-nine years. I put the two mains in down the road, drew them up and everything; so I know. I got the two permits for the creek; and, according to state law, they have to be below grade, under the creek. The county is letting them stay right above the

water.

Then in the back of us the City of Aurora put in a sewer line down for this new subdivision, Summer Lake.

AUDREY MORONEY: Summer Lane

RICHARD MORONEY: Summer Lane, at the south end of the road, a brand new subdivision. They came down to the neighbor's place, across the creek, and down all the way and tied in at Waubensee. They put culverts in about this big around over the creek.

(Indicating)

Now, this is a creek. And when the water is heavy, it runs high. We have gotten water twice so far since this happened. You can't restrict a creek to a pipe like this.

AUDREY MORONEY: How big would you say that pipe is?

RICHARD MORONEY: I don't know. Eddy was telling me it was yea-big around. (Indicating)

I haven't seen it so I really can't say.

AUDREY MORONEY: The pipe is too small to stop the flow.

RICHARD MORONEY: All the debris comes up to it, because Ed told me that and it overflowed and,

bingo, we got water.

AUDREY MORONEY: And they also want to put Farnsworth in back of the farm at the end of the road and then come back to our road, which would make it a giant curve, which is ridiculous, where they could go in back of that farm, where it is supposed to connect, and completely eliminate our road.

RICHARD MORONEY: So that is circling right around that farm and coming down those bridges. There are only two-lane bridges, but they cost the taxpayers a lot of money. It is really dumb.

* * * * *

AUDREY TROYER: My name is Audrey Troyer and and I live in Wayne, 33 W 260 Woodmere Lane, Box 409.

I am very much opposed to this expansion. I think there are other ways to solve the problems; such as, continuing with the Randall Road expansion and Route 59 and utilizing the state roads more rather than, you know, the local roads that go through residential areas.

And I, also, think that our tax dollars would be better spent on working on better trans-

portation, which there is almost none in this area. And that would help solve some of the problems and take some of the cars off the road.

And it would solve a lot of problems if people would do car pooling or do some other form of shared transportation.

So I would rather see the taxpayers' money used that way rather than just building more roads. We should be expanding the roads that are already there.

I also think, because I live in Wayne, that it would ruin the character of the village and I would fight this tooth and nail.

* * * * *

DEBRA BURRICHTER: Debra Burrichter, 10 Woodland Hills Road, Batavia, Illinois. I am a spokesperson for the Batavia Woodland Hills Homeowners' Association.

On behalf of approximately two hundred homeowners in the Batavia Woodland Hills Homeowners' area, we would like to express our disapproval with the IDOT proposal to expand Kirk Road to six lanes

