

DISTRICT 2 STANDARDS

FULL SIZE

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- 3.1 MAILBOX TURNOUT IN CURB AND GUTTER SECTION (DIST STD 3.1)
- 4.1 PC CONCRETE ISLANDS AND MEDIANS ACCESSIBLE TO THE DISABLED
- 20.1 HOT-MIX ASPHALT APPROACHES AND MAILBOX RETURNS (DIST STD 20.1)
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- 32.1 SEWER AND WATER MAIN CROSSINGS (DIST STD 32.1)
- 33.1 CONCRETE COLLARS FOR PIPE OR BOX CULVERT EXTENSIONS (DIST STD 33.1)
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- 37.1 TRAFFIC CONTROL FOR THREE LANE SECTION (DIST STD 37.1)
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- 40.1a TRAFFIC CONTROL FOR ROAD CLOSURE WITH SIDE ROAD WITHIN 150' OF CLOSURE (DIST STD 40.1a)
- 41.1 TYPICAL PAVEMENT MARKINGS (DIST STD 41.1)
- 44.1 PAINTING DETAILS (DIST STD 44.1)
- 53.1 REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL (DIST STD 53.1)
- 54.1 TRAFFIC BARRIER TERMINAL, TYPE 2 (27" HEIGHT) (DIST STD 54.1)
- 68.1 SLOTTED DRAIN PIPE (VARIABLE HEIGHT) (DIST STD 68.1)
- 71.1 DETAIL OF FLOOD GATE (DIST STD 71.1)
- 72.1 40' SINGLE LANE MEDIAN CROSSOVER (45 mph WORK ZONE SPEED LIMIT)
- 73.1 50' SINGLE LANE MEDIAN CROSSOVER (45 mph WORK ZONE SPEED LIMIT)
- 74.1 64' SINGLE LANE MEDIAN CROSSOVER (45 mph WORK ZONE SPEED LIMIT)
- 75.1 40' SINGLE LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 76.1 50' SINGLE LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 77.1 64' SINGLE LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 78.1 88' SINGLE LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 79.1 40' TWO LANE MEDIAN CROSSOVER (45 mph WORK ZONE SPEED LIMIT)
- 80.1 50' TWO LANE MEDIAN CROSSOVER (45 mph WORK ZONE SPEED LIMIT)
- 81.1 64' TWO LANE MEDIAN CROSSOVER (45 mph WORK ZONE SPEED LIMIT)
- 82.1 40' TWO LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 83.1 50' TWO LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 84.1 64' TWO LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 85.1 88' TWO LANE MEDIAN CROSSOVER (55 mph WORK ZONE SPEED LIMIT)
- 86.1 BEVELED PIPE & GUARD DETAIL FOR MEDIAN CROSSOVER
- 90.1 TRAFFIC BARRIER TERMINAL, TYPE 6B (SPECIAL) (DIST STD 90.1)
- 92.1 DETAILS OF PLANTING AND BRACING TREES (DIST STD 92.1)

District 2 Standards Designer Notes

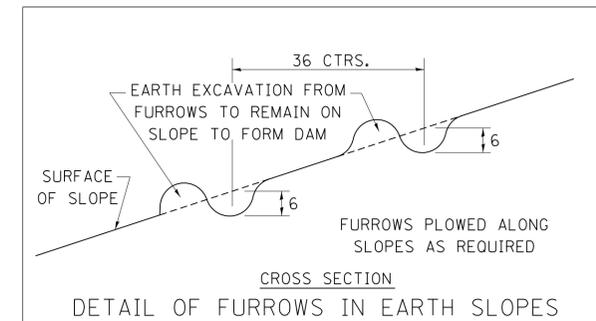
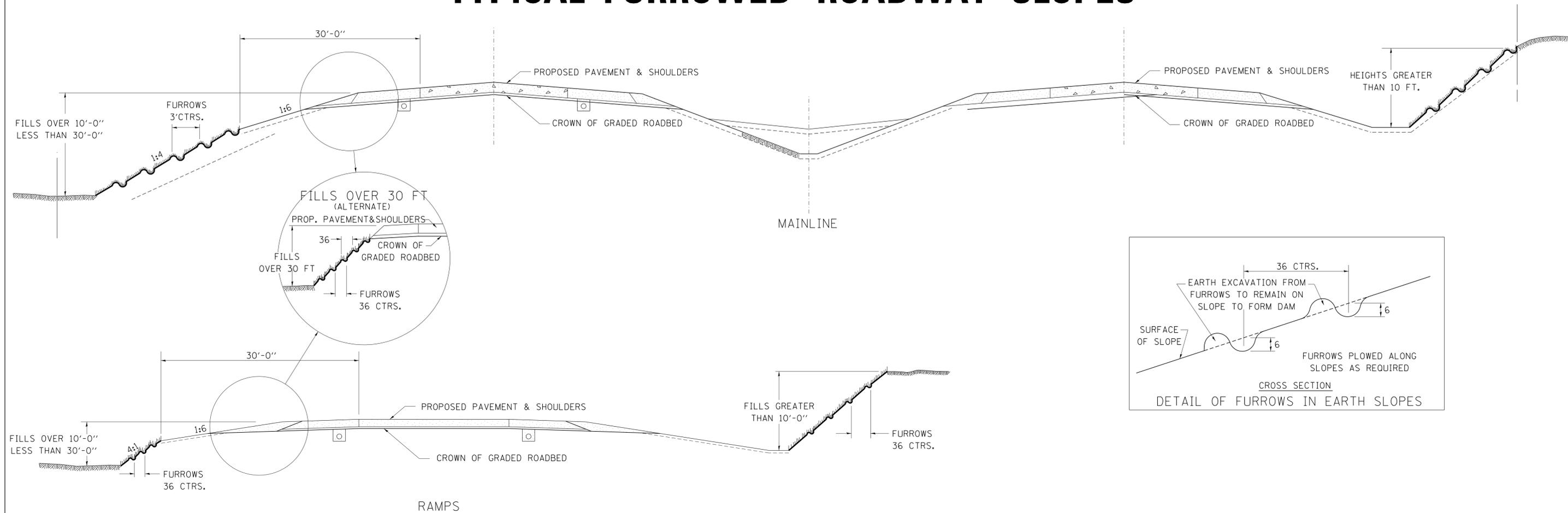
Full Size District 2 Standards

- 1.1 Include when foreslopes and/or backslopes are more than 10' in height.
- 3.1 Use when a mailbox turnout is needed in a curb & gutter section and there isn't a parking lane or a mail delivery lane.
- 4.1 Use this when there are cross walks that will go through an island or median. Specify which option the contractor is required to use when building the Concrete Median (Special).
- 20.1 Include for rural entrances and sideroads on 3R projects, reconstruction projects, or for new entrances. Do not include on 3P or Smart resurfacing projects.
- 25.1 Include for urban entrances with curb & gutter on 3R projects, reconstruction projects, or for new entrances. Do not include on 3P or Smart resurfacing projects.
- 32.1 Include in urban projects with proposed storm sewers or water mains.
- 33.1 Use this for pipe or box culvert extensions. Fill in the information in the table for the Bill of Materials.
- 34.1 Use this when you have box culvert end sections.
- 36.1 Use this district standard for any short term closure of an expressway at a diamond interchange.
- 37.1 Use this district standard for work that will require a lane closure in a three lane section such as a truck climbing lane.
- 38.1 Use this district standard when there is a transition from a four lane section that transitions to a two lane section.
- 39.1 Include on 4 lane highways where the contractor may change a portion of the work to the opposite lane.
- 40.1 Include for a mainline road closure.
- 40.1a Include for a mainline road closure when a sideroad is within 150' of the mainline closure.
- 41.1 Include in projects with pavement marking or raised reflective pavement markers.
- 44.1 Include in projects with pavement marking on entrance and exit ramps & cloverleafs.
- 53.1 Use this to remove and re-erect an old type steel plate beam guardrail which has 6" block outs and a 27½" rail height.
- 54.1 Use this when installing a Traffic Barrier Terminal, Type 2 on the old type of

District 2 Standards Designer Notes

- 68.1 steel plate beam guardrail with a 27½" rail height.
This can be used to increase drainage in curb & gutter with very flat grades (less than 0.3%). Also include this when constructing median crossovers.
- 71.1 Use if a property owner has a fenced field with livestock and a stream or river. The flood gate will be placed near the right-of-way to prevent livestock from leaving the field through the waterway. During high water, the flood gate will open to let water and debris through.
- 72.1, 73.1, 74.1, 75.1, 76.1, 77.1, 78.1 Use on single lane median crossovers of the median width specified and for the work zone speed limit. Include District Standard 86.1.
- 79.1, 80.1, 81.1, 82.1, 83.1, 84.1, 85.1 Use on two lane median crossovers of the median width specified and for the work zone speed limit. Include District Standard 86.1.
- 86.1 Include this on median crossovers, District Standards 72.1, 73.1, 74.1, 75.1, 76.1, 77.1, 78.1, 79.1, 80.1, 81.1, 82.1, 83.1, 84.1, & 85.1.
- 90.1 Use this on 4-lane highways that go under dual structures and the piers required shielding. The outside of the piers are shielded with impact attenuators. The gap between the piers is shielded using Traffic Barrier Terminal Type 6B (Special). The Traffic Barrier Terminal Type 6B (Special) is required on both sides of the piers.
Design Note: The **length** of the double thrie beam between the piers **must be added on the elevation on the District Standard.**
- 92.1 Include when planting new ball & burlapped trees.

TYPICAL FURROWED ROADWAY SLOPES



GENERAL NOTES

IN GENERAL, THE ENTIRE EARTH SURFACE WITHIN THE RIGHT-OF-WAY SHALL BE SEEDING AND MULCHED.

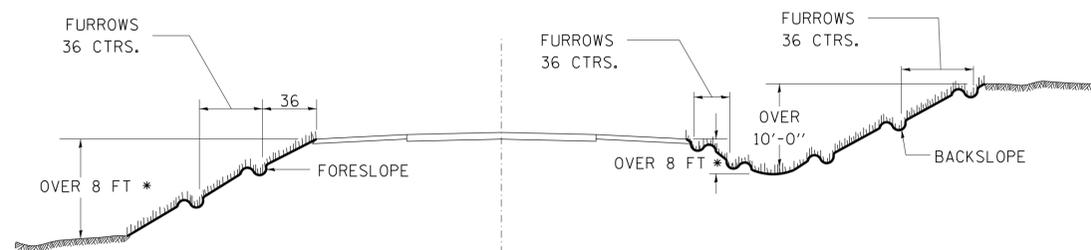
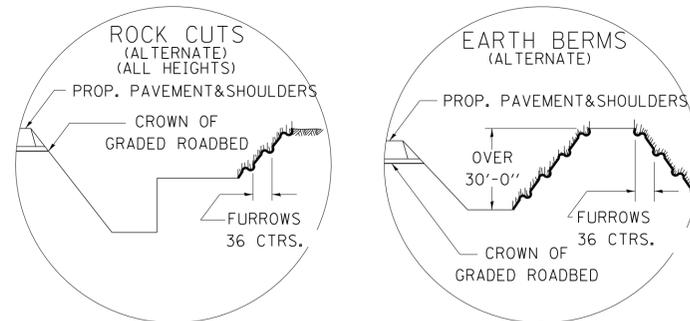
NO AGRICULTURAL GROUND LIMESTONE SHALL BE APPLIED TO THE GRADED ROADBED.

FORESLOPES AND/OR BACKSLOPES 10 FT. OR LESS IN HEIGHT WILL NOT REQUIRE FURROWING UNLESS OTHERWISE NOTED IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

FORESLOPES AND/OR BACKSLOPES OVER 10 FT. IN HEIGHT SHALL BE FURROWED. THE OPERATION SHALL INCLUDE FINISHING THE SLOPES TO FINAL LINE AND GRADE, AS SHOWN ON THE CROSS SECTIONS BEFORE FURROWING IS DONE. FURROWS SHALL BE PLOWED ALONG A LEVEL LINE CONFORMING TO THE CONTOURS OF THE SLOPE. THE COST OF FURROWING SHALL BE CONSIDERED INCLUDED IN THE PROJECT COST AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

SEQUENCE AND OPERATION FOR SEEDING, MULCHING AND FURROWING OF ROADWAY SLOPES:

1. SPREAD FERTILIZER.
2. PERFORM THE OPERATION OF GROUND PREPARATION.
3. PLOW FURROWS.
4. PERFORM THE OPERATION OF SEEDING. THE SEED SHALL BE SOWN ON THE SURFACE OF THE PREPARED GROUND AFTER FURROWING.
5. THE OPERATION OF COVERING THE SEED, BY HARROWING OR OTHER MEANS, SHALL BE PERFORMED ONLY IF SO DIRECTED BY THE ENGINEER AND SHALL BE INCLUDED TO THE ITEM OF SEEDING.
6. SECTION 250 AND 251 OF THE STANDARD SPECIFICATIONS SHALL GOVERN THIS WORK EXCEPT AS NOTED HEREIN.



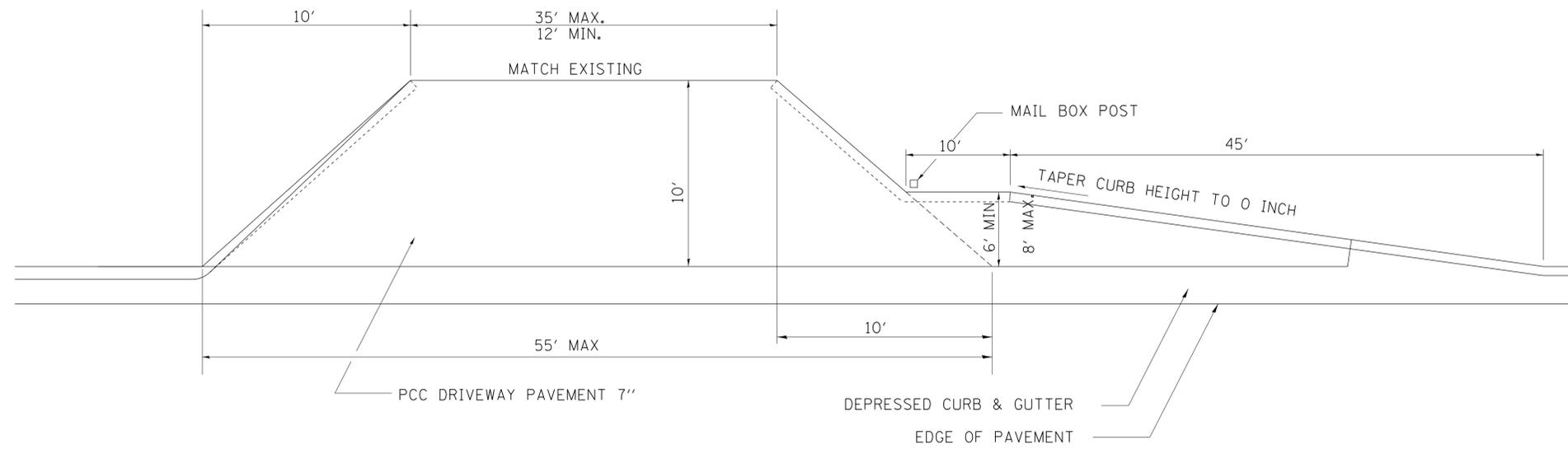
* IF FORESLOPES ARE SIMILAR TO MAINLINE OR RAMP CONFIGURATION, FURROW AS INDICATED FOR THOSE SLOPES.

CROSSROAD GRADE SEPERATIONS

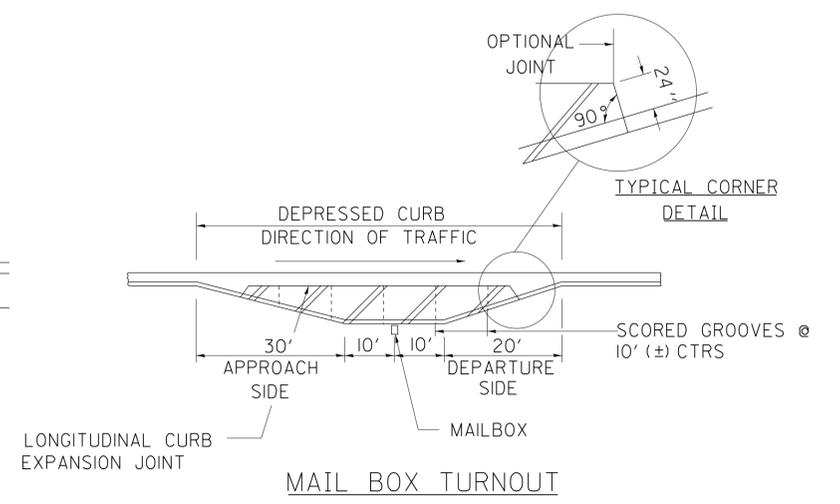
ALL DIMENSIONS ARE IN INCHES
UNLESS OTHERWISE NOTED.

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| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 10-17-11 REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:12 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |
| TYPICAL FURROWED ROADWAY SLOPES | | | | | | | | | | | | | |

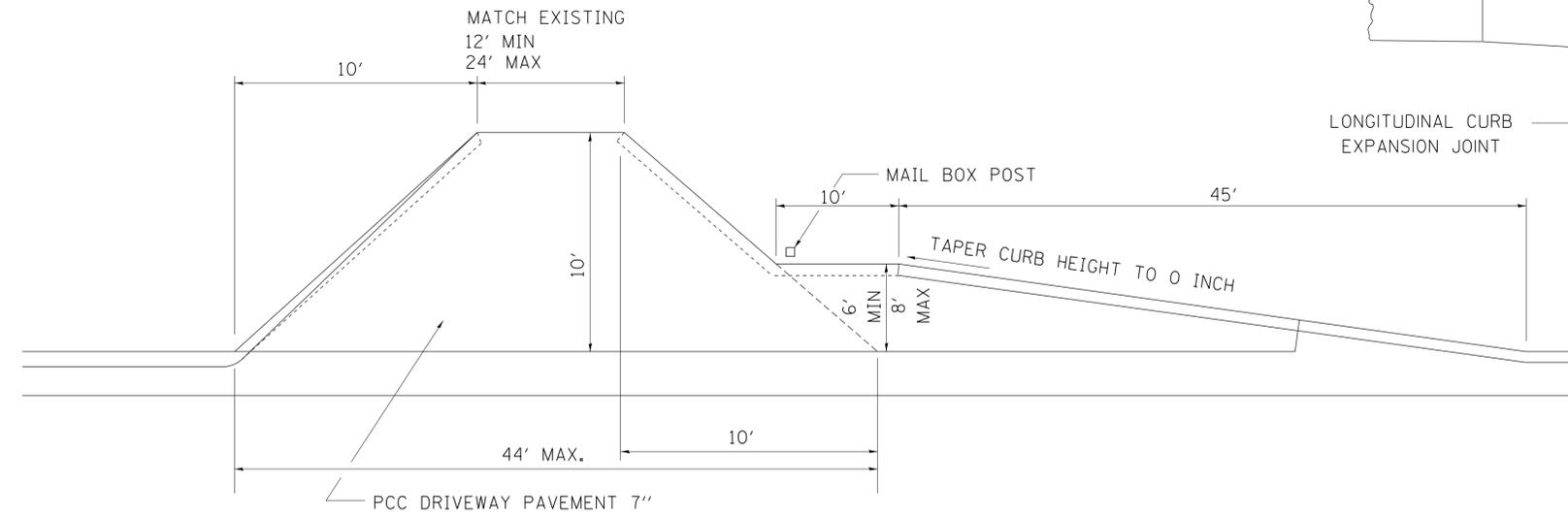
MAILBOX TURNOUT IN CURB AND GUTTER SECTION



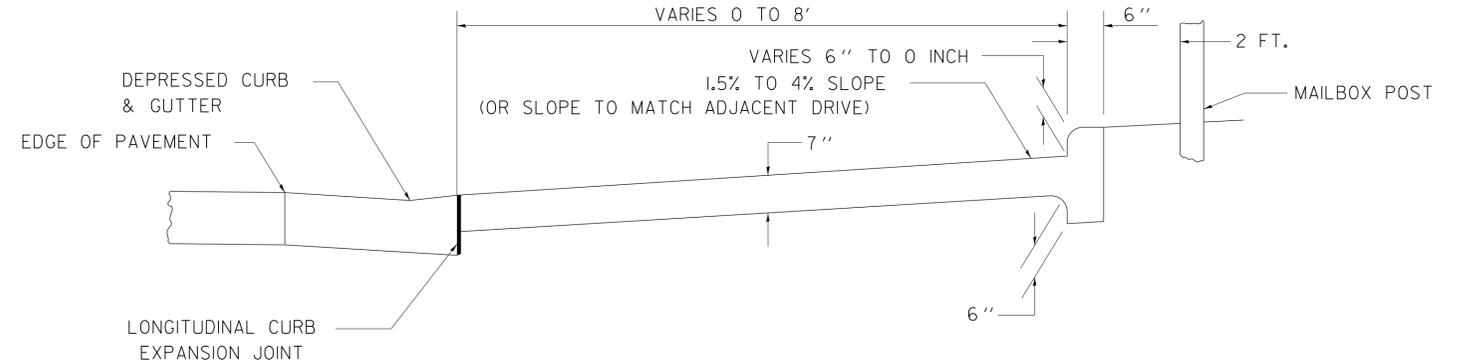
COMMERCIAL ENTRANCE WITH MAIL BOX TURNOUT



MAIL BOX TURNOUT



PRIVATE ENTRANCE WITH MAIL BOX TURNOUT



TYPICAL CROSS SECTION

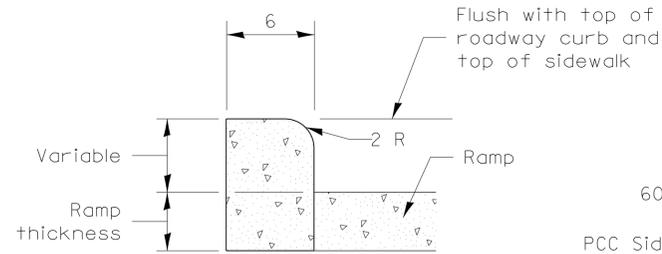
GENERAL NOTES

- 1.) THE LONGITUDINAL CURB EXPANSION JOINT SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECIFICATIONS.
- 2.) THE MAILBOX TURNOUT CROSS SLOPE WILL BE AS SHOWN ABOVE, AS SHOWN ON THE STATION CROSS SECTIONS OR AS DIRECTED BY THE ENGINEER.
- 3.) THE MAILBOX TURNOUT SHALL BE CONSTRUCTED WITH SCORED GROOVES, AS SPECIFIED IN ARTICLE 423.06 OF THE STANDARD SPECIFICATIONS, AT APPROXIMATELY 10 FT. CENTERS. IN THE EVENT THERE IS EXISTING OR PROPOSED SIDEWALK PRESENT, THESE SCORED GROOVES SHALL BE PLACED IN LINE WITH EVERY OTHER JOINT IN THE ADJACENT SIDEWALK.
- 4.) THE WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS WHICH PRICE SHALL INCLUDE THE LONGITUDINAL CURB EXPANSION JOINT, MONOLITHIC CURB AS SHOWN, SCORED GROOVES, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 5.) SEE THE DISTRICT STANDARD 25.1 FOR ADDITIONAL DETAILS.

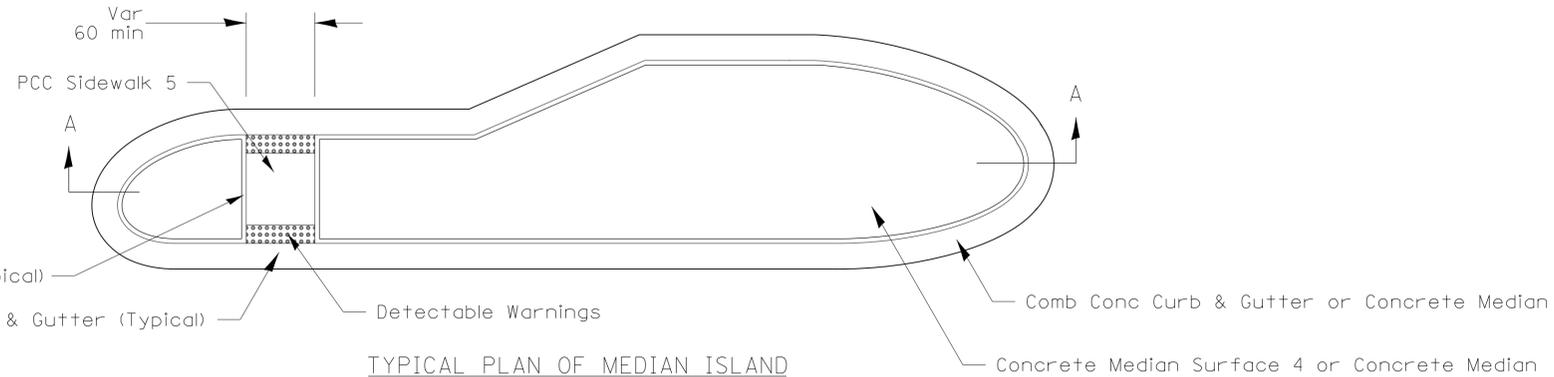
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

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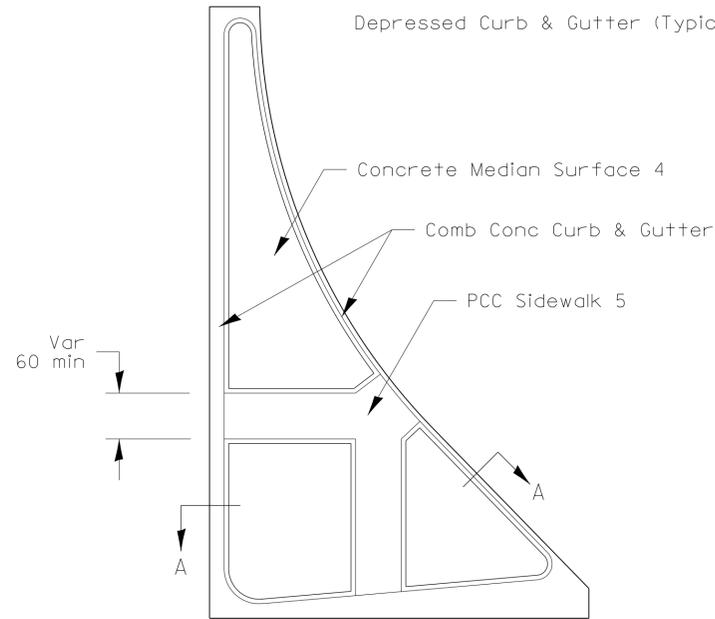
PC CONCRETE ISLANDS AND MEDIANS ACCESSIBLE TO THE DISABLED



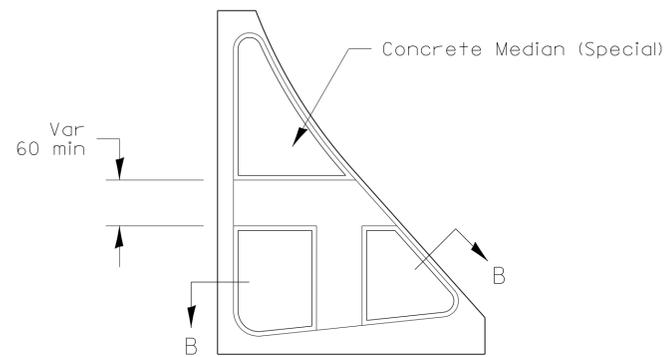
SIDE CURB DETAIL



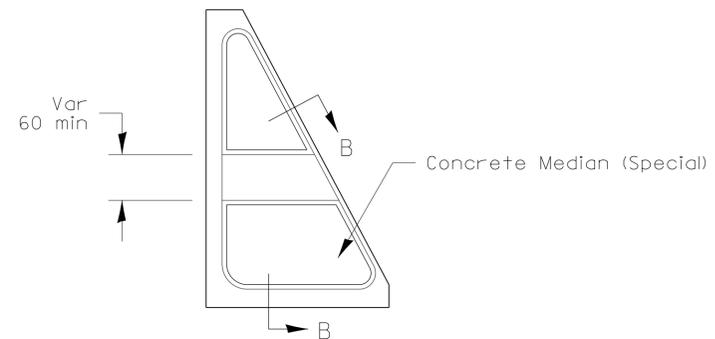
TYPICAL PLAN OF MEDIAN ISLAND



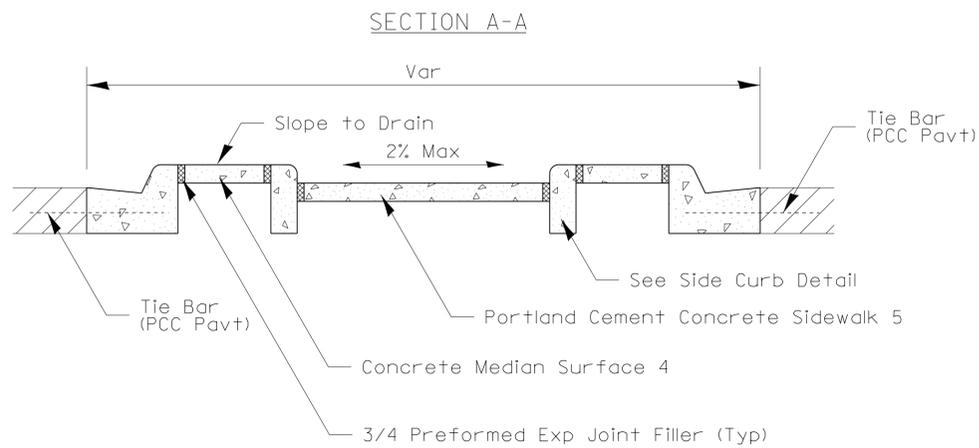
LARGE ISLAND
(Free Flow Design)



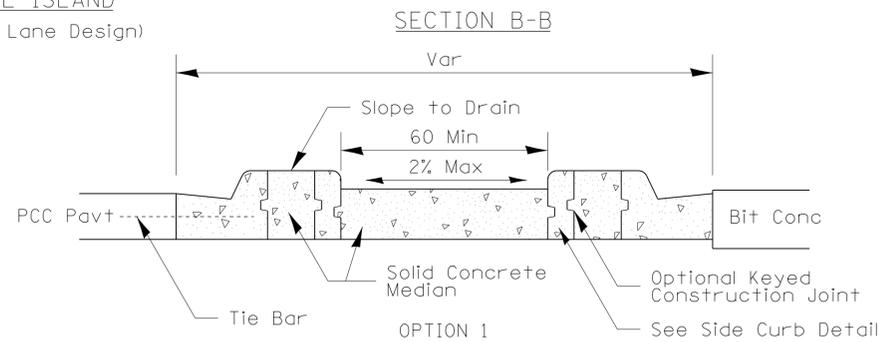
INTERMEDIATE ISLAND
(For Right Turn Lane Design)



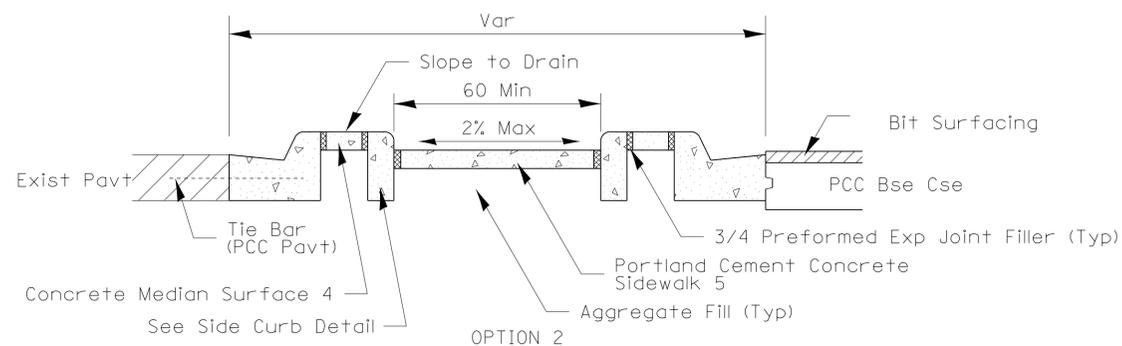
SMALL ISLAND
(For Typical Design)



SECTION A-A



SECTION B-B



OPTION 2

General Notes:
See Standard 606301 and plan sheets for station & offsets, radii, dimensions, and details not shown.

See Standard 424031 for sidewalk details not shown.

The sidewalk should drain to the low side of the island. If necessary the sidewalk shall be sloped to drain at a maximum 2% grade.

See the plan general notes for the type of curb & gutter to be used on islands.

Curb & gutter adjacent to the walkway in the interior of the island shall have 6 gutter flags.

The sidewalk should not be closer than 36 from the corner of the island.

Keyed longitudinal construction joints shall be constructed without tie bars.

Medians and large islands shall consist of PCC Sidewalk 5, Concrete Median Surface 4, and Combination Concrete Curb & Gutter, Type M or B of the size specified. Median island can also be solid concrete medians.

Locations, layouts, and widths of the flush sidewalk area, shall be determined by the designer and shown on the plans.

The intermediate and small islands will be measured for payment from E.O.P. to E.O.P. using either option 1 or option 2, as directed by the Engineer, and will be paid for at the contract unit price per SQ FT for CONCRETE MEDIAN (SPECIAL) which shall include the combination curb & gutter, sidewalk, aggregate fill, concrete median surface, and solid concrete median.

Omit detectable warnings when distance between back of curbs is less than 6'.

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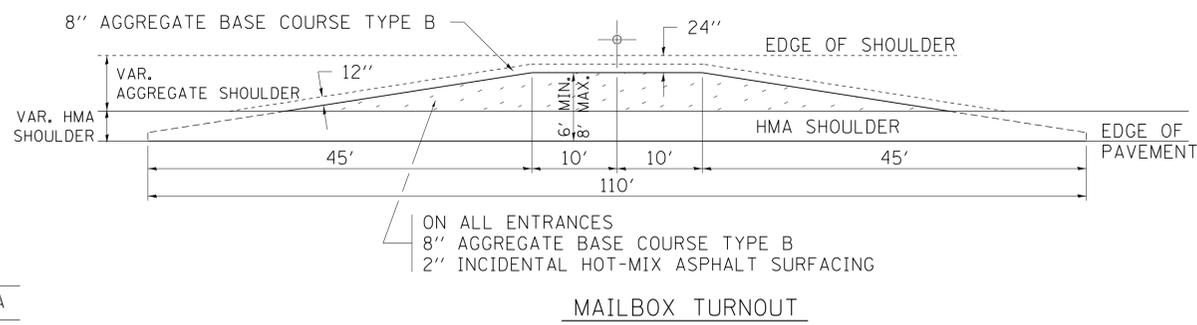
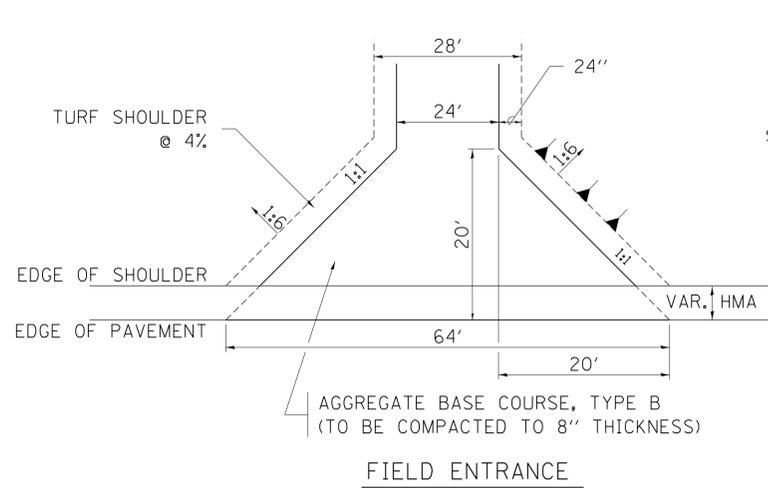
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

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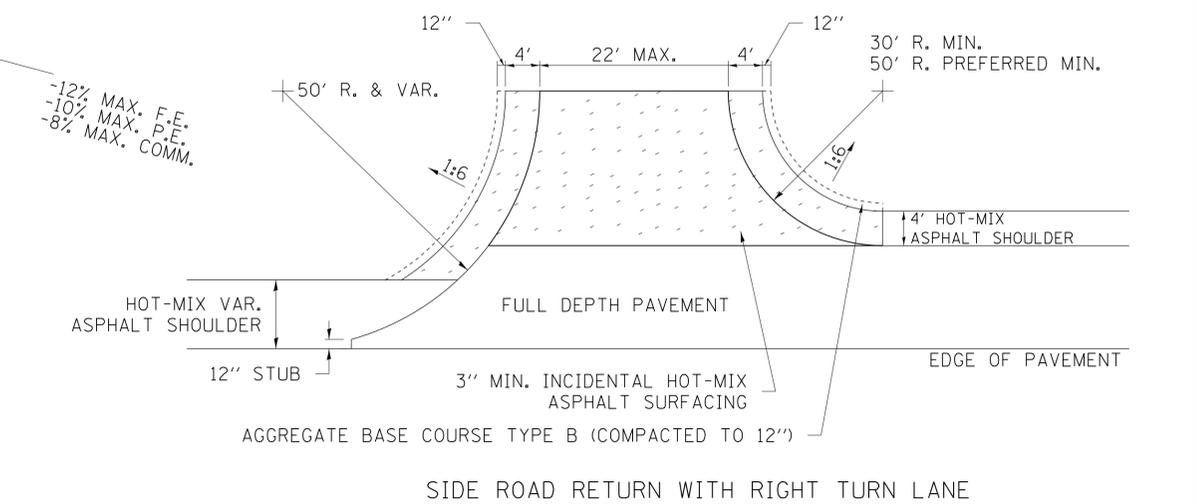
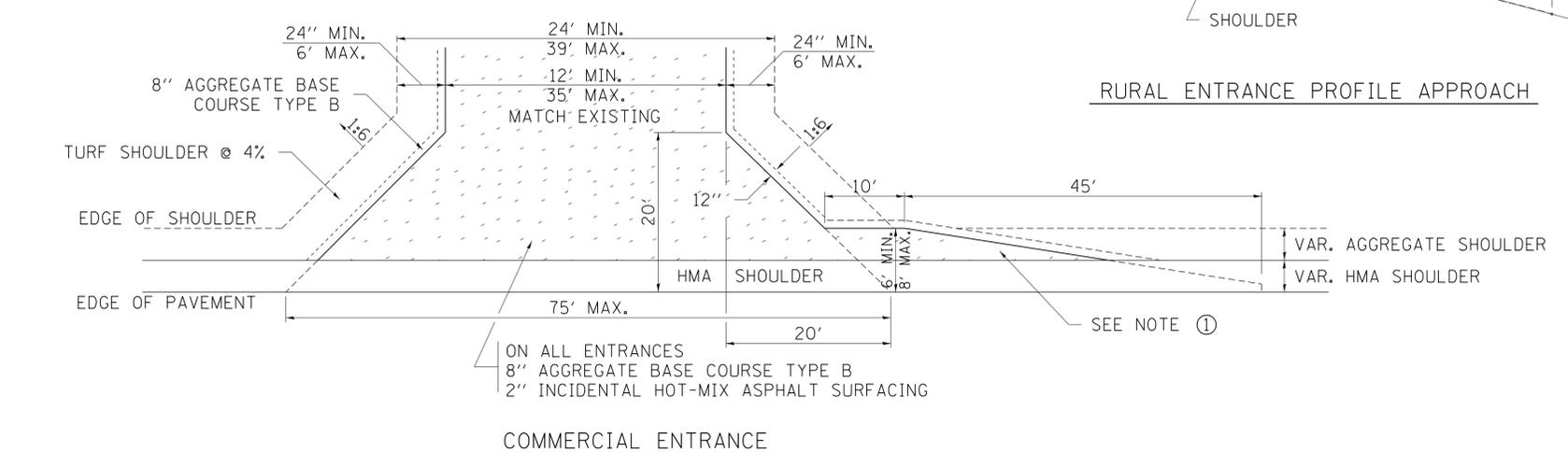
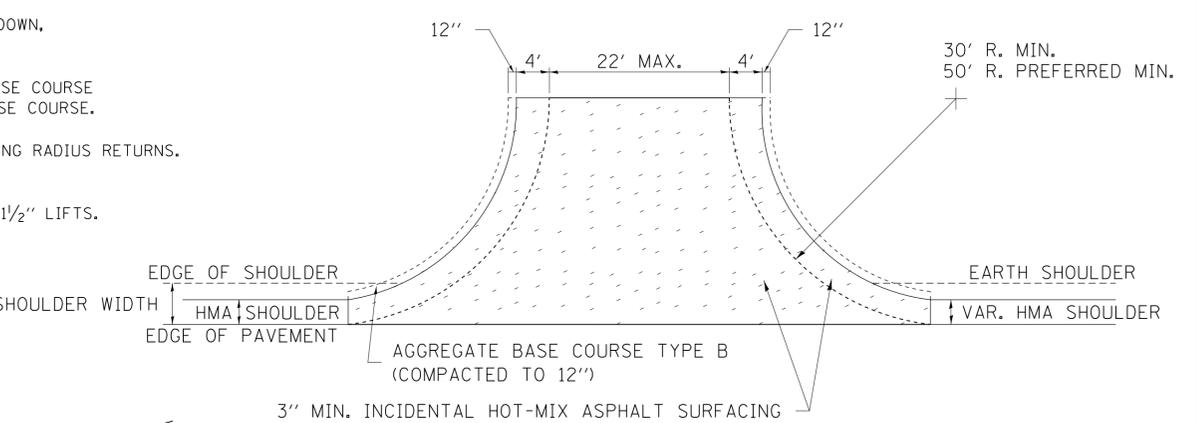
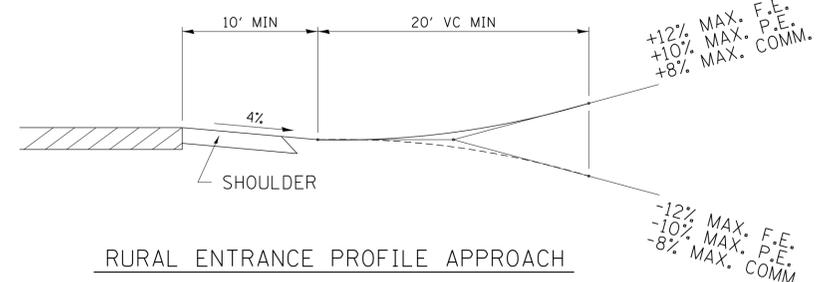
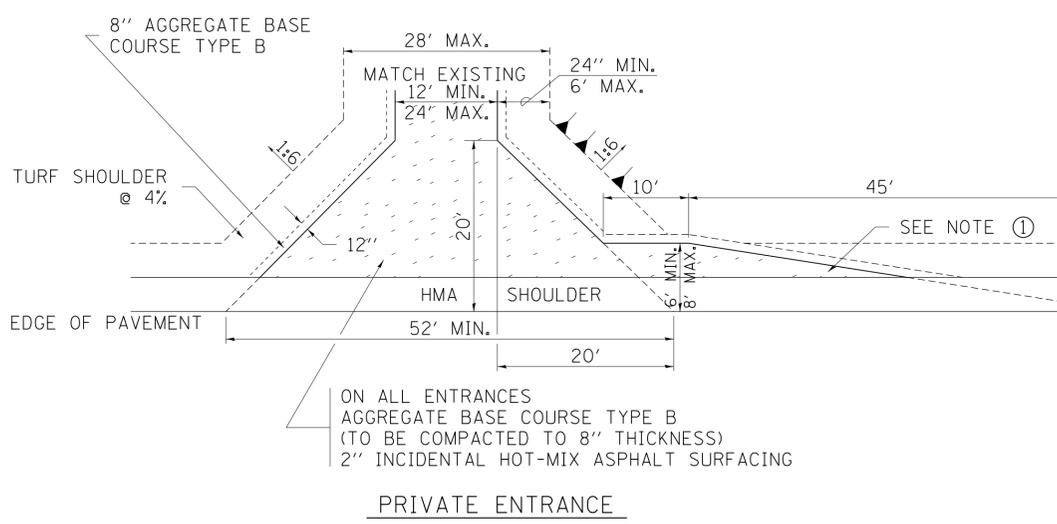
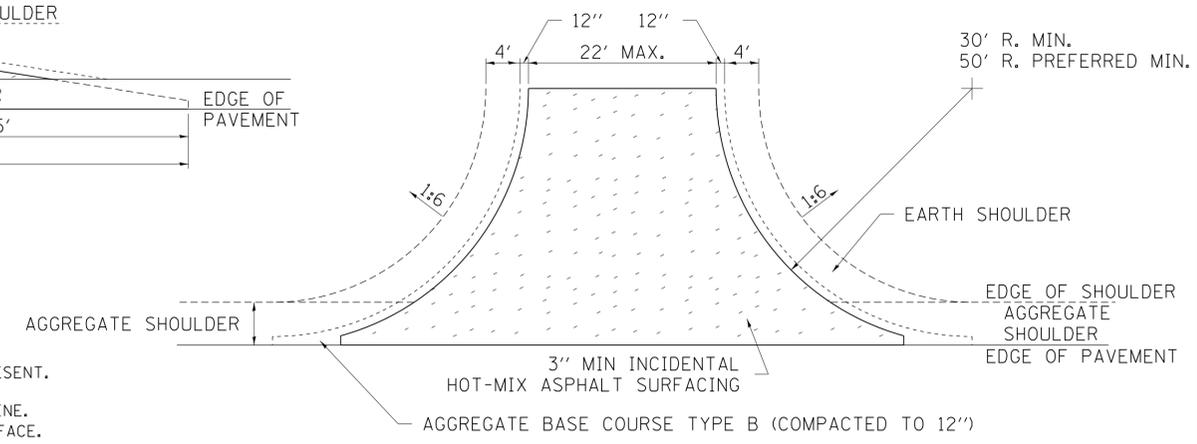
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HOT-MIX ASPHALT APPROACHES AND MAILBOX RETURNS



NOTE

- ① TURNOUTS ARE TO BE CONSTRUCTED ON THE APPROACH SIDE OF ALL PE & CE REGARDLESS IF A MAILBOX IS PRESENT.
- ② ALL PE & CE ARE TO BE SURFACED TO RIGHT OF WAY LINE. AREA BEHIND RIGHT OF WAY SHALL MATCH EXISTING SURFACE.
- ③ ALL PE & CE TO BE CONSTRUCTED WITH AN 8" AGGREGATE BASE COURSE, TYPE B AND WITH A 2" INCIDENTAL HOT-MIX ASPHALT SURFACING, UNLESS OTHERWISE NOTED.
- ④ FE ARE TO BE AGGREGATE TO RIGHT OF WAY OR TOUCH DOWN, WHICH EVER IS GREATEST.
- ⑤ EXCAVATION REQUIRED FOR PLACEMENT OF AGGREGATE BASE COURSE SHALL BE INCLUDED IN THE COST OF THE AGGREGATE BASE COURSE.
- ⑥ ON ENTRANCES THE CONTRACTOR HAS THE OPTION OF USING RADIUS RETURNS. USE RADII OF 20' TO 60'.
- ⑦ SIDE ROADS SHALL HAVE 3" INCIDENTAL PLACED IN TWO 1 1/2" LIFTS.



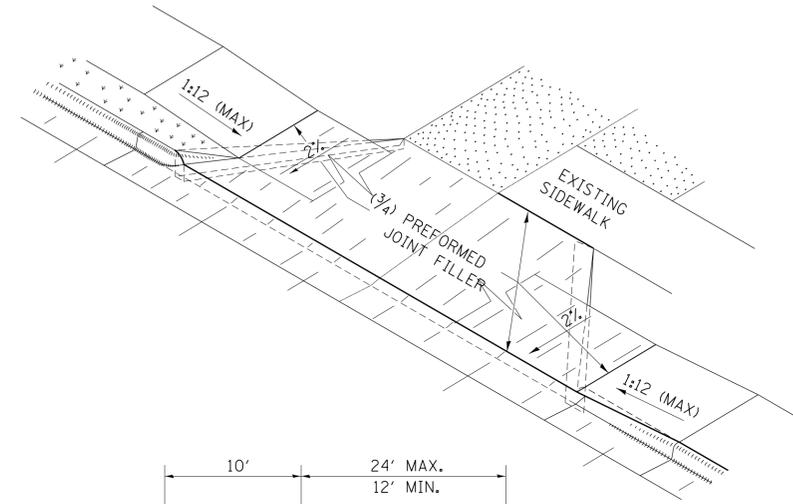
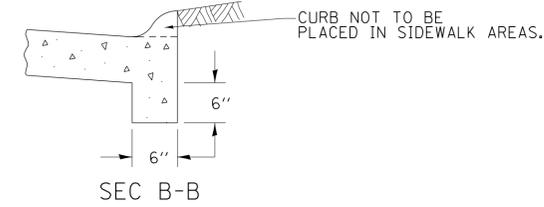
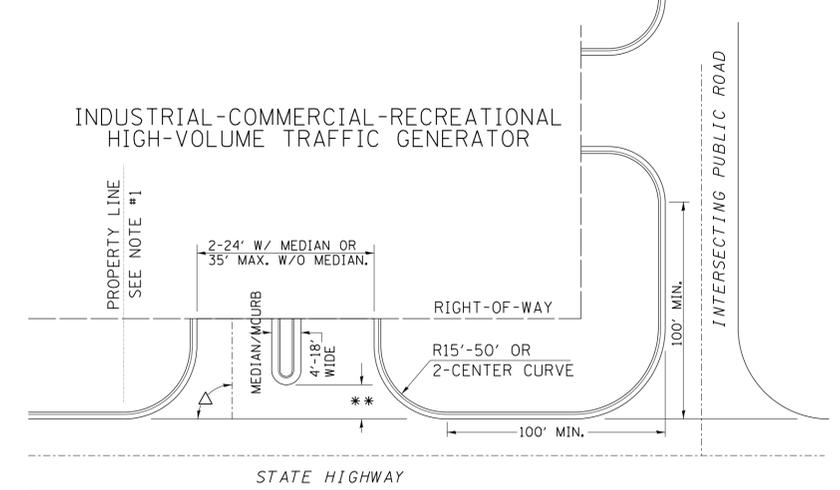
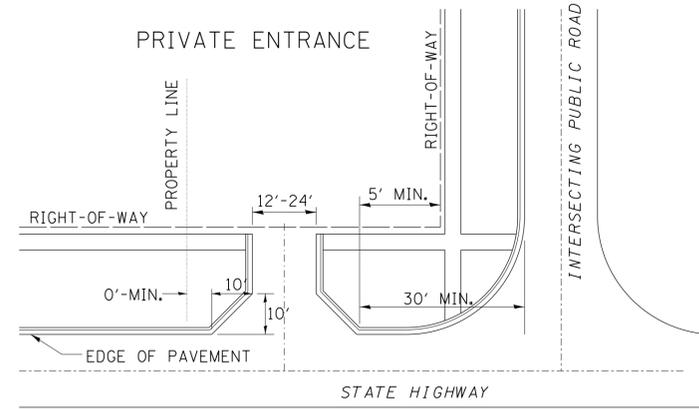
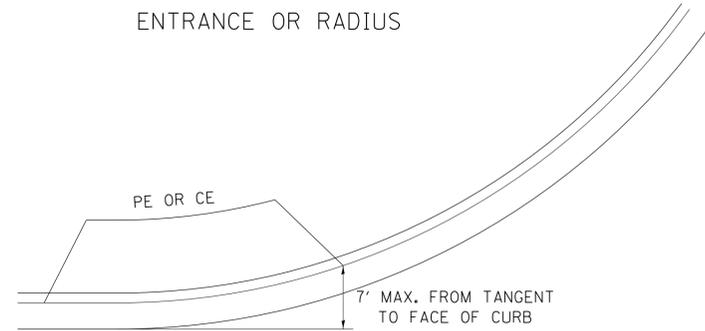
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| | | DRAWN - | REVISED - 12-07-10 |
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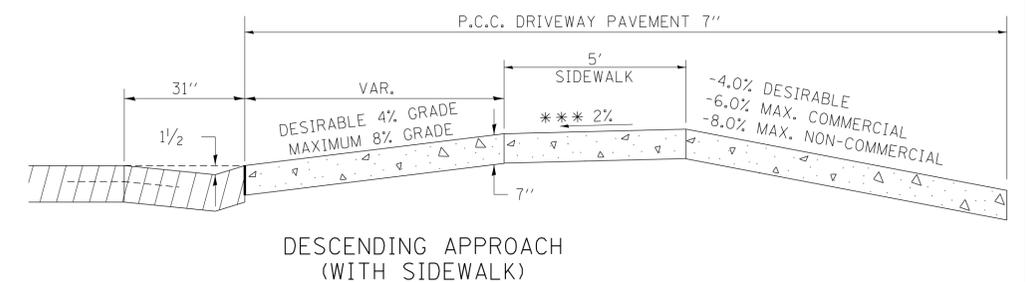
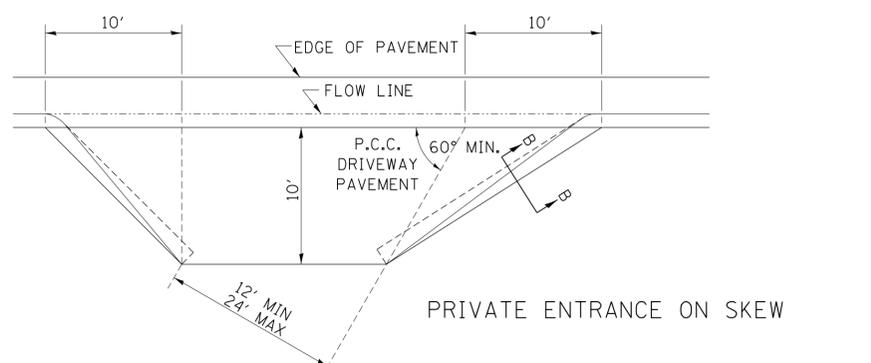
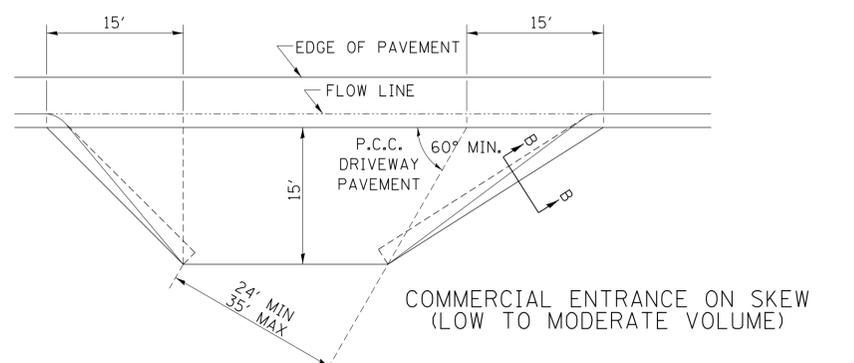
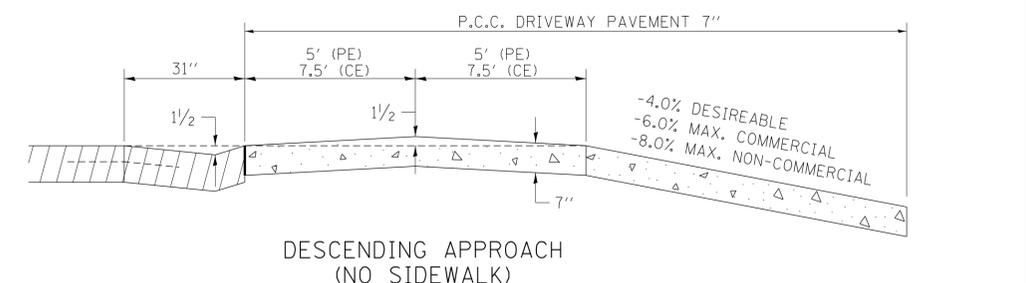
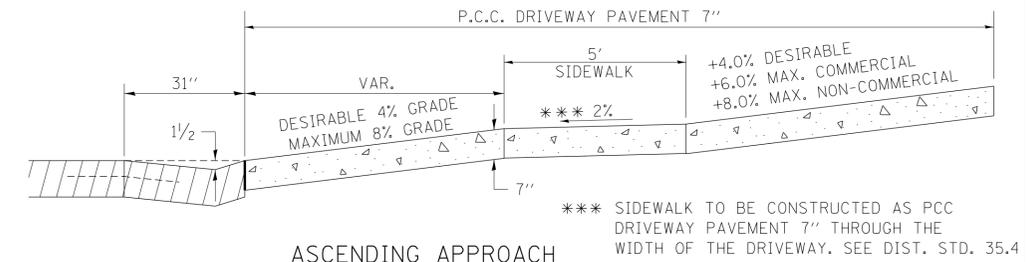
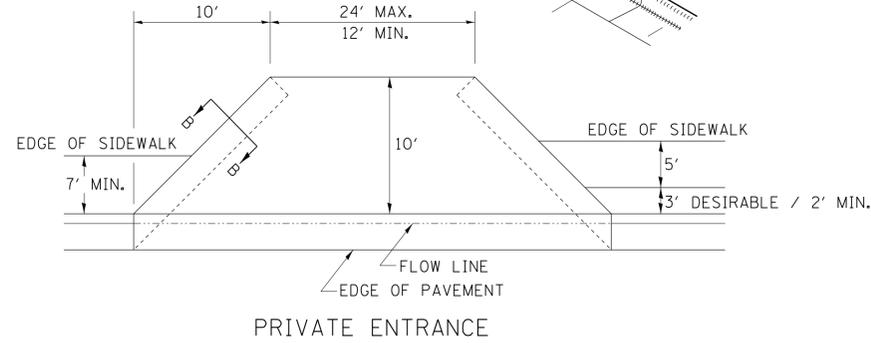
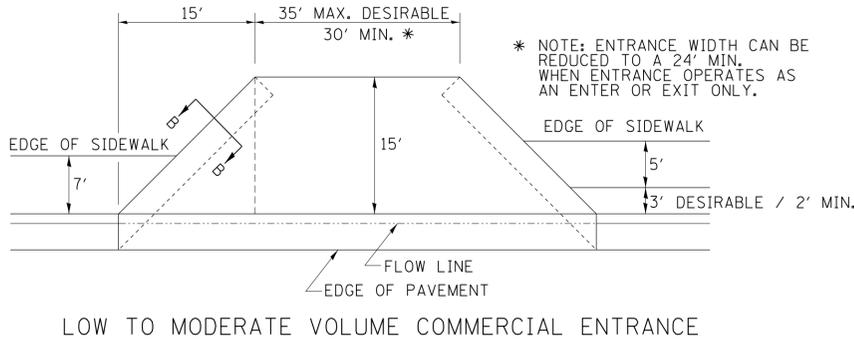
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| REGION 2 / DISTRICT 2 STANDARD | | | |
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ENTRANCE APPROACHES – URBAN AREA



A MINIMUM OF 440 FEET SHALL BE MAINTAINED BETWEEN CENTER LINES OF ADJACENT DRIVEWAYS.
 Δ 90° DESIRABLE, 45° MIN. ANGLE PERMITTED ONLY FOR ONE-WAY DRIVEWAYS.
 60° MIN. ANGLE FOR TWO-WAY DRIVEWAYS.
 NOTE: #1 ENCROACHMENT ON THE ADJACENT PROPERTY OWNER LAND REQUIRES HIS OR HER WRITTEN APPROVAL.
 ** 4'-10' IF HIGHWAY CURBED.
 AT EDGE OF SHOULDER IF HIGHWAY UNCURBED.



NOTE: CURVED ENTRANCE RETURNS MAY BE USED FOR LOW TO MODERATE VOLUME LOCATIONS WITH REVIEW ON A CASE-BY-CASE BASIS.

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| FILE NAME = District 2 Standard | USER NAME = ID07/District 2 | DESIGNED - DRAWN - | REVISED - 12-07-10 REVISED - |
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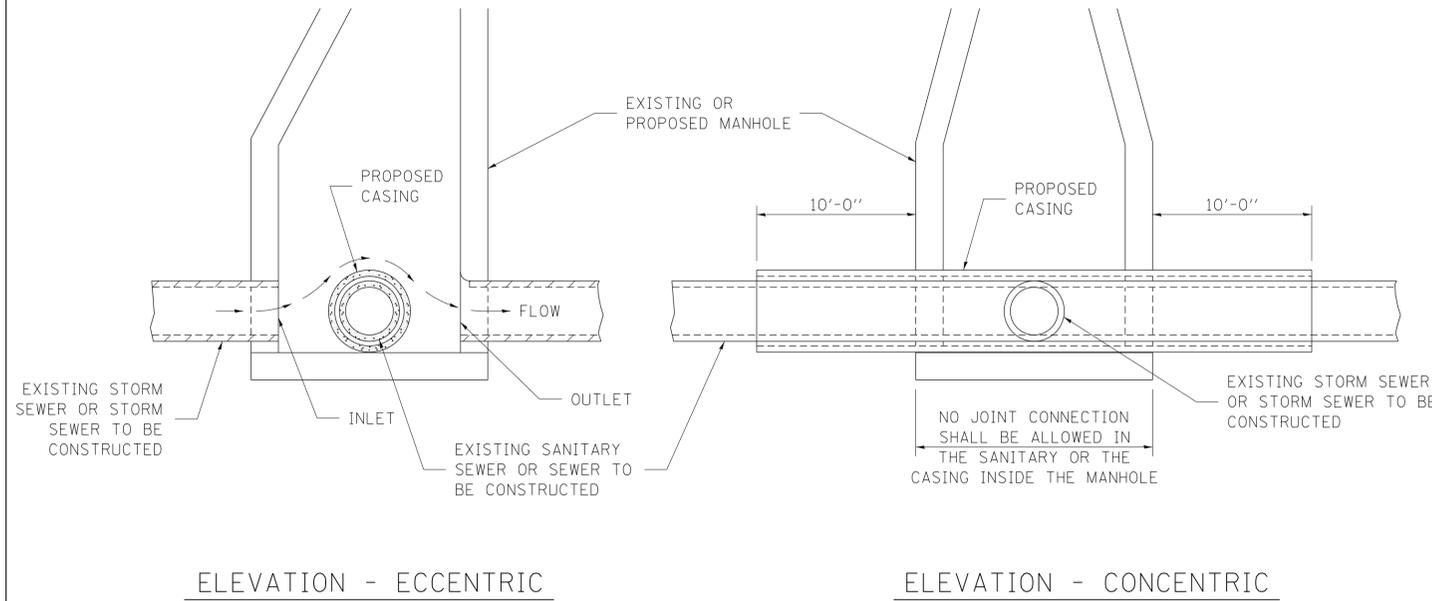
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|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

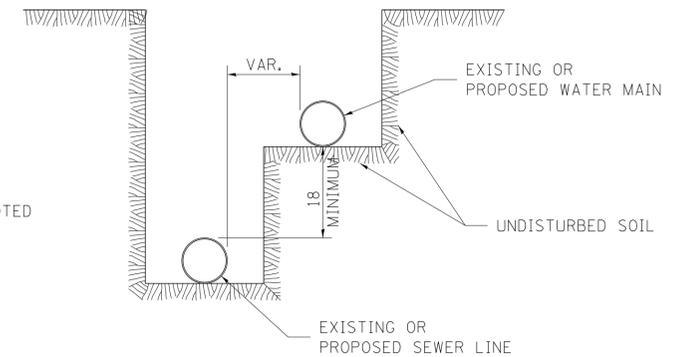
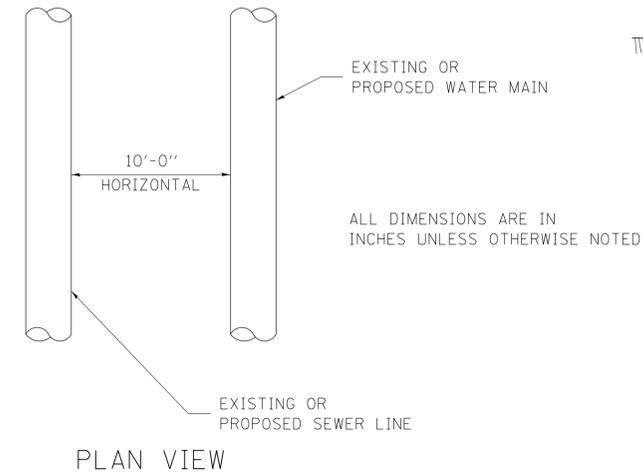
SEWER AND WATER MAIN CROSSINGS



THIS DETAIL IS FOR UNKNOWN UTILITIES UNLESS QUANTITIES ARE INCLUDED IN THE PLANS THE EXTRA WORK WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04.

WHEN PROPOSED SEWER (OR WATER) IS LOCATED 10'-0" OR MORE FROM EXISTING WATER (OR SEWER) NO SPECIAL CONSTRUCTION REQUIRED.

WHEN PROPOSED SEWER (OR WATER) IS LOCATED LESS THAN 10'-0" FROM EXISTING WATER (OR SEWER) DETAILS BELOW SHALL APPLY.

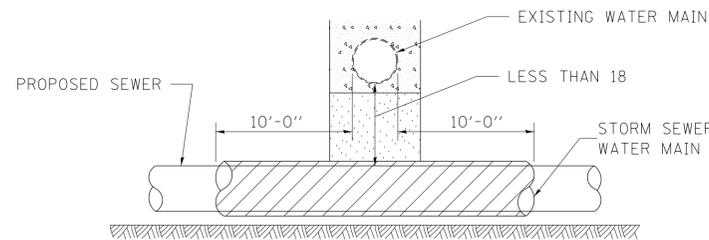


CASING SHALL BE CAST IRON WITH AN INSIDE DIAMETER 2" LARGER IN DIAMETER THAN ENCASED PIPE OUTSIDE DIAMETER WITH BOTH ENDS OF CASING SEALED

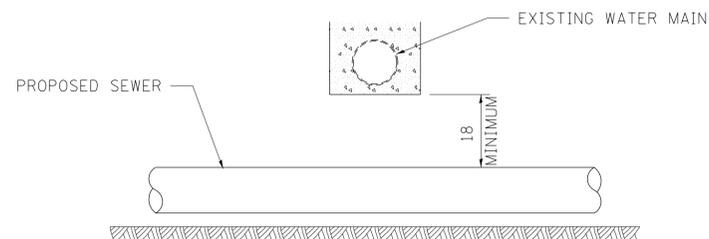
AT GRADE CROSSING OF SANITARY AND STORM SEWER

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

POINT LOADS SHALL NOT BE ALLOWED BETWEEN SEWER OR SEWER CASING AND WATER MAIN
 PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH



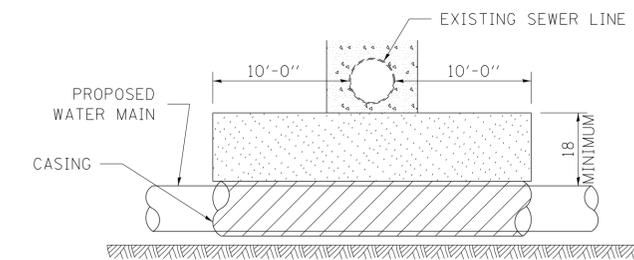
PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH
 MAINTAIN 18 MINIMUM VERTICAL SEPARATION FOR 10 FT. HORIZONTALLY



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

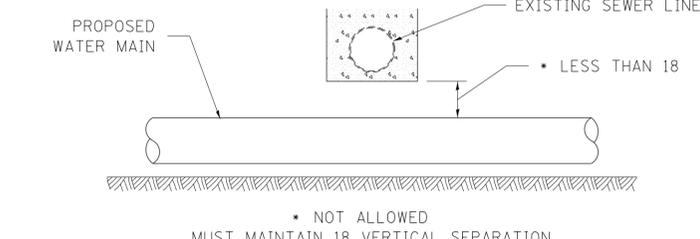
PROPOSED SEWER LINE BELOW EXISTING WATER MAIN

PROVIDE ADEQUATE SUPPORT FOR EXISTING SEWER LINE TO PREVENT DAMAGE DUE TO SETTLEMENT
 PLACE TRENCH BACKFILL FOR 10 FT. ON EITHER SIDE OF SEWER LINE



CASING SHALL BE CAST IRON WITH AN INSIDE DIAMETER 2" LARGER IN DIAMETER THAN ENCASED PIPE OUTSIDE DIAMETER WITH BOTH ENDS OF CASING SEALED

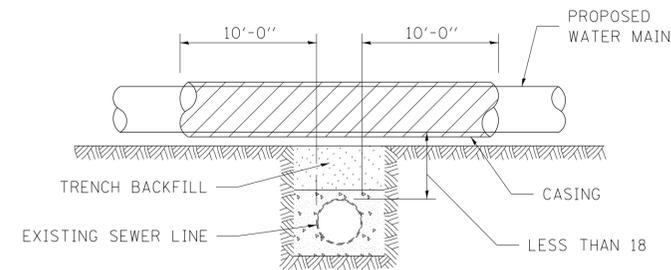
PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH
 MAINTAIN 18 MINIMUM VERTICAL SEPARATION FOR 10 FT. HORIZONTALLY



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

PROPOSED WATER MAIN BELOW EXISTING SEWER LINE

POINT LOADS SHALL NOT BE ALLOWED BETWEEN WATER MAIN OR WATER MAIN CASING AND SEWER

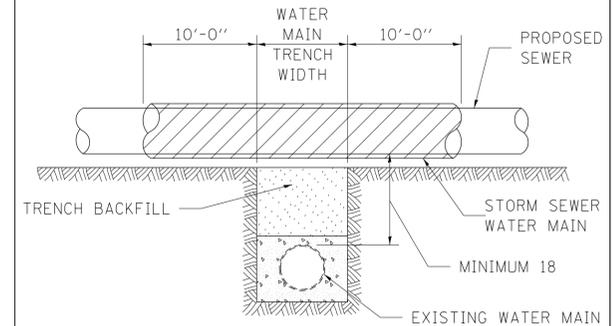


CASING SHALL BE CAST IRON WITH AN INSIDE DIAMETER 2" LARGER IN DIAMETER THAN ENCASED PIPE OUTSIDE DIAMETER WITH BOTH ENDS OF CASING SEALED

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

PROPOSED WATER MAIN ABOVE EXISTING SEWER LINE

PROVIDE ADEQUATE SUPPORT FOR SEWER TO PREVENT SETTLING AND BREAKING THE WATER MAIN.



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED

EXISTING WATER MAIN BELOW PROPOSED SEWER LINE WITH MINIMUM 18 VERTICAL SEPARATION

| | | | |
|------------------------------------|--------------------------------------|-----------------------|---------------------------------|
| FILE NAME = District 2 Standard | USER NAME = 1007/District 2 | DESIGNED - DRAWN - | REVISED - 10-17-11 REVISED - |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:16 2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

BOX CULVERT END SECTIONS

GENERAL NOTES

Box Culvert End Sections shall be constructed according to the requirements of Section 540 of the Standard Specifications except as modified herein. This work will be measured for payment as each, with each end of each culvert being one each. End sections will be paid for at the contract unit price per each for Box Culvert End Sections of the culvert number specified.

Typical box section dimensions, materials, and reinforcement details for Box Culvert End Sections shall be according to the requirements of ASTM C 1577 as required for the design of the portion of the culvert within the limits of Precast Concrete Box Culverts except as modified herein.

Number of segments shown in Side Elevation is for example only. Length and number of precast box sections required to construct Box Culvert End Sections shall be determined by the Contractor.

**See roadway plans for embankment slope (V:H). The Slope Must Match.

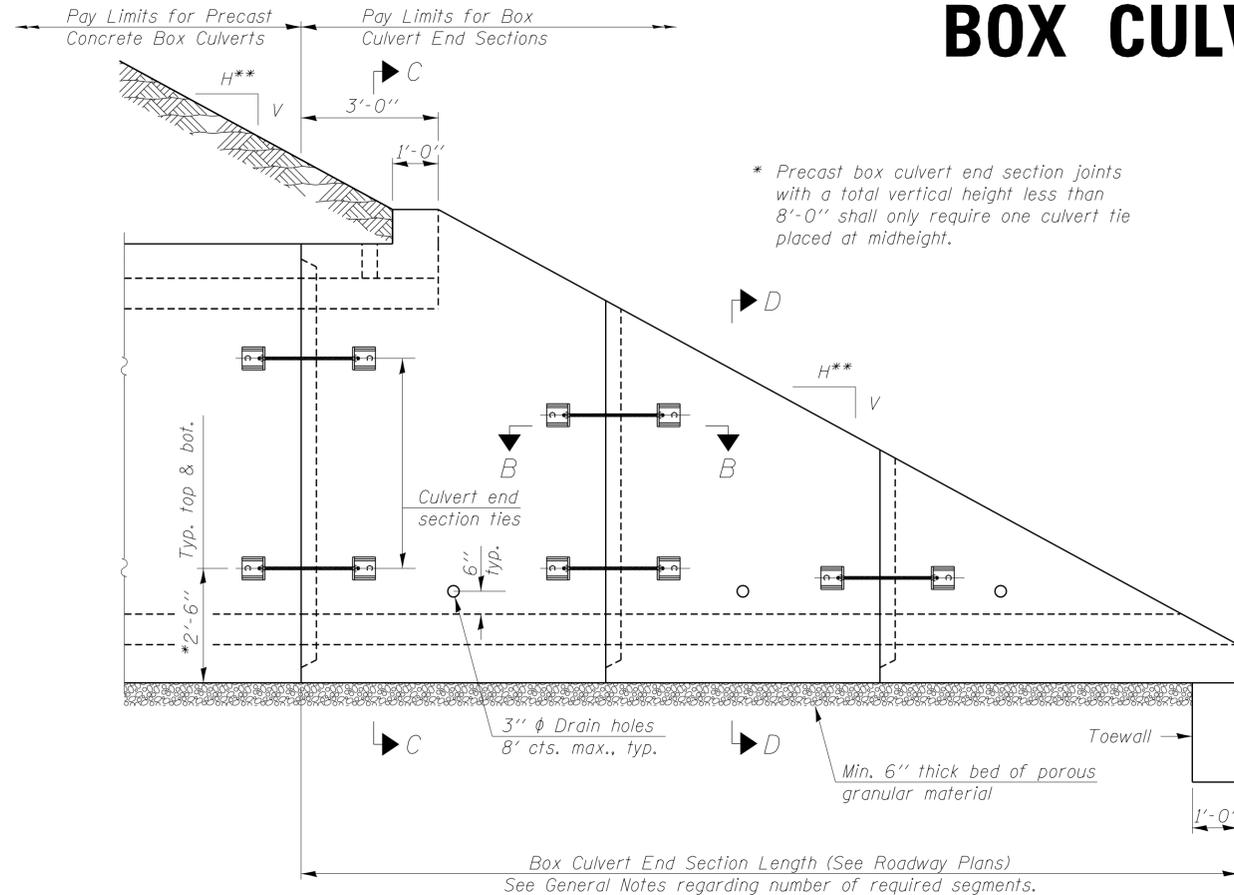
1" ϕ anchor rods for the culvert ties shall conform to the requirements of ASTM F1554, Grade 105. Structural steel for tie plate and restraint angle shall conform to the requirements of Article 1006.04 of the Standard Specifications. All components of the culvert tie detail shall be galvanized according to the requirements of AASHTO M 111 or M 232 as applicable. 2 1/4" x 2 1/4" x 5/16" plate washers shall be provided under each nut required for the anchor rods. Anchor rods connecting precast sections shall be brought to a snug tight condition followed by an additional 1/2 turn on one of the nuts. Match marks shall be provided on the bolt and nut to verify relative rotation between the bolt and the nut. Holes in the walls for the culvert tie assembly may be drilled using core bits in lieu of using formed holes.

All costs associated with furnishing and installing or constructing the geotextile fabric, toewall, and culvert ties will not be measured for payment but shall be included in the contract unit price for Box Culvert End Sections of the culvert number specified.

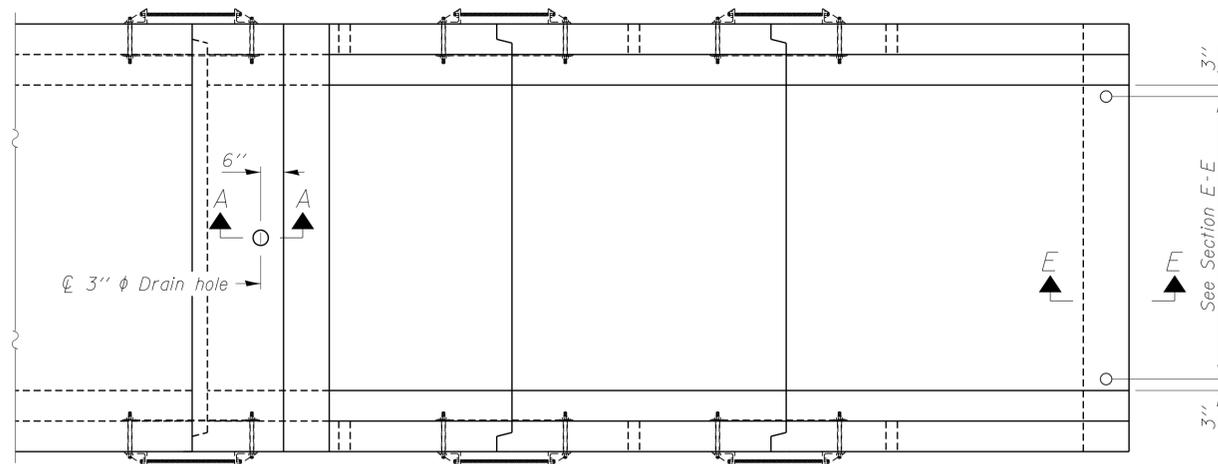
Reinforcement bars designated (E) shall be epoxy coated. Drain holes shall conform to the requirements of Article 503.11 of the Standard Specifications unless noted otherwise.

Nonwoven geotextile fabric shall conform to the requirements of Article 1080.01. The minimum weight of the fabric shall be 6 oz. / sq. yd.

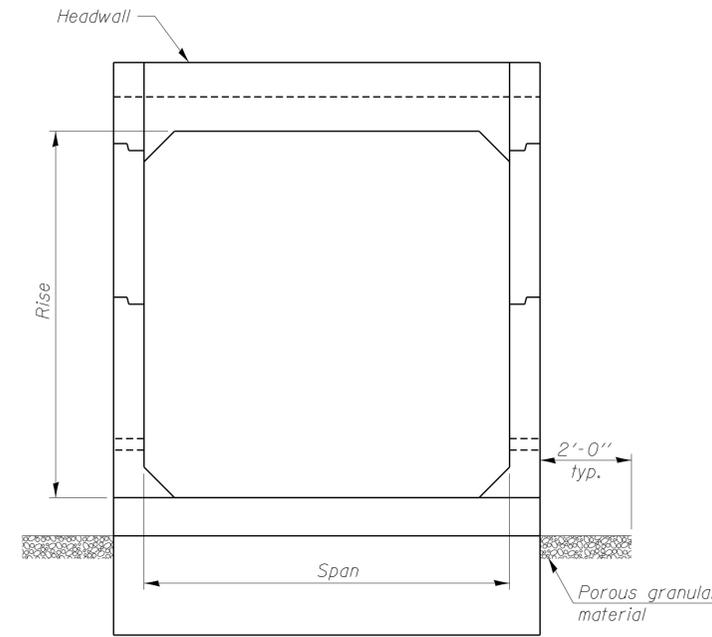
For end sections with traversable pipe grate systems, see Highway Standard 542311 for required modifications.



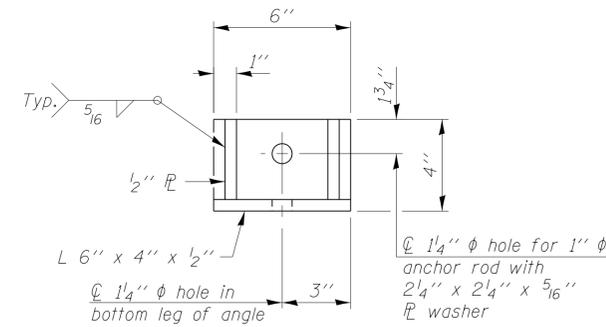
ELEVATION



PLAN



END VIEW



RESTRAINT ANGLE DETAIL

12" x 12" block of CA5, CA7, or CA11 coarse aggregate placed over drain opening. Block of aggregate shall be completely wrapped in nonwoven geotextile fabric.

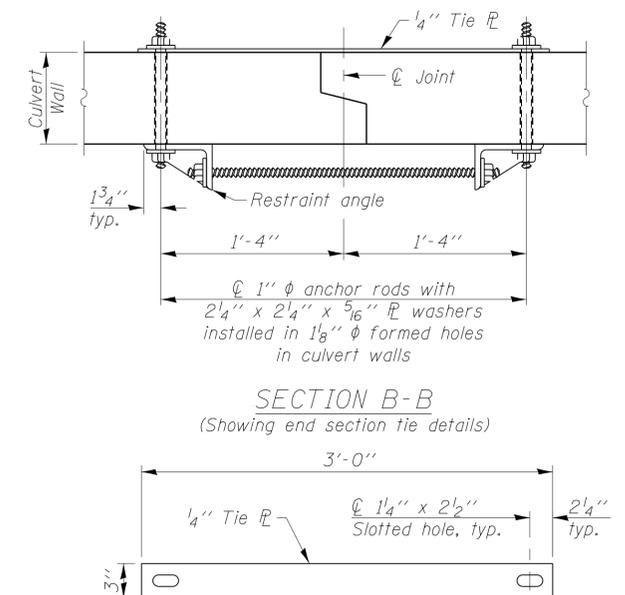
Provide a double layer of 12" x 12" nonwoven geotextile fabric centered over the drain hole. Fabric shall be sealed to the concrete with mastic.

3" ϕ PVC drain cast with the concrete (Adjust location to clear reinforcement).

1/2" Square foam blockout around PVC drain (to be removed with formwork)

SECTION A-A

(All costs associated with furnishing and constructing the above drain details will not be measured for payment but shall be included in the contract unit price for the end section.)



SECTION B-B
(Showing end section tie details)

TIE PLATE DETAIL

| | | | |
|------------------------------------|--------------------------------------|------------|-------------------|
| FILE NAME = District 2 Standard | USER NAME = ID07/District 2 | DESIGNED - | REVISED - 2-10-14 |
| | | DRAWN - | REVISED - |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:17 2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

BOX CULVERT END SECTIONS

| Rise (ft.) T(in.), T _s (in.) | Reinforcing Steel A _{s1m} (in. ² /ft.) | | | | | | | | | | |
|--|--|------|------|------|------|------|------|------|------|------|------|
| | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 |
| 4 | 0.19 | 0.17 | | | | | | | | | |
| 5 | 0.26 | 0.21 | 0.18 | | | | | | | | |
| 6 | | 0.26 | 0.23 | 0.22 | | | | | | | |
| 7 | | 0.33 | 0.59 | 0.27 | 0.28 | | | | | | |
| 8 | | | 0.43 | 0.39 | 0.36 | 0.34 | 0.40 | | | | |
| 9 | | | | 0.43 | 0.40 | 0.37 | 0.36 | 0.48 | | | |
| 10 | | | | 0.47 | 0.44 | 0.41 | 0.38 | 0.42 | 0.56 | | |
| 11 | | | 0.54 | | 0.46 | | 0.41 | | 0.50 | 0.65 | |
| 12 | | | 0.58 | | 0.50 | | 0.45 | | 0.46 | | 0.75 |

(A_{s1m} reinforcement based upon welded wire fabric conforming to AASHTO M 55 or M 221).

ℓ₁ DIMENSION

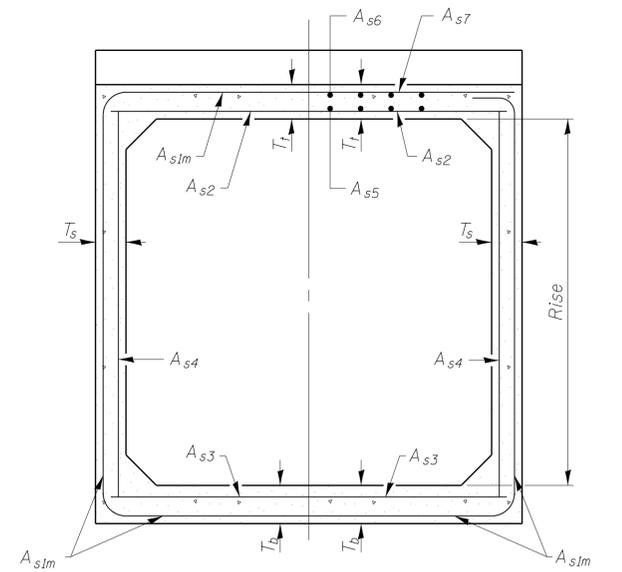
- #3 bar = 2'-0"
- #4 bar = 2'-8"
- #5 bar = 3'-4"
- #6 bar = 3'-11"

Notes:

Alternate Section D-D is provided to allow the Contractor the option of casting the bottom slab of the end section first followed by construction of the sidewalls using conventional forming methods. Shop drawings that detail slab thickness and reinforcement layout shall be submitted to the Engineer for review and approval when using Alternate Section D-D.

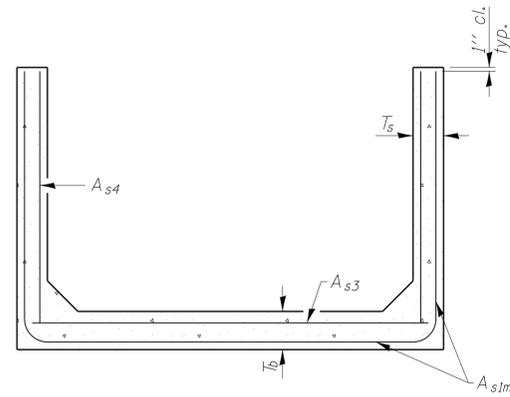
The size and spacing of the v₂(E) bars shall provide a minimum reinforcement area along each face of the walls (in.²/ft.) equal to 1.10*(A_{s1m}). v₂(E) bars may consist of #3 thru #6 size reinforcement bars and the longitudinal spacing shall not exceed the lesser of the wall thickness or 8 inches.

Bonded construction joints shall be prepared according to Article 503.09 of the Standard Specifications.

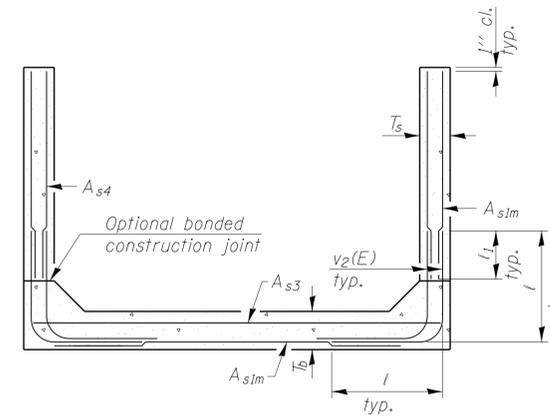


(Design Earth Cover < 2 ft.) (Design Earth Cover ≥ 2 ft.)

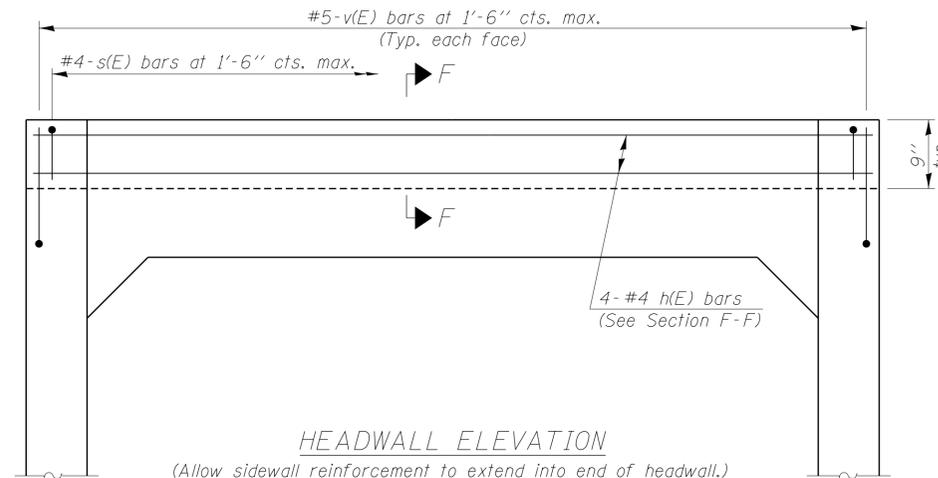
SECTION C-C



SECTION D-D

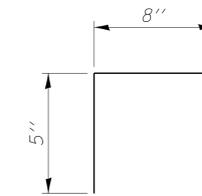


ALTERNATE SECTION D-D

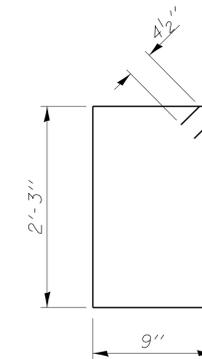


(Allow sidewall reinforcement to extend into end of headwall.)

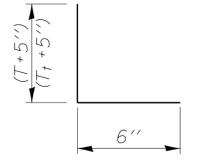
HEADWALL ELEVATION



BAR s(E)



BAR s₁



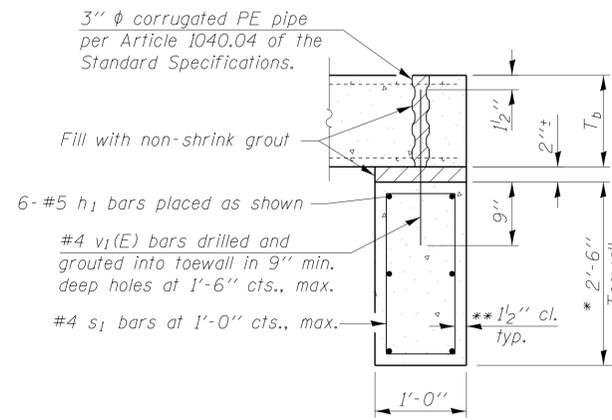
BAR v(E)

TOEWALL CONSTRUCTION SEQUENCE

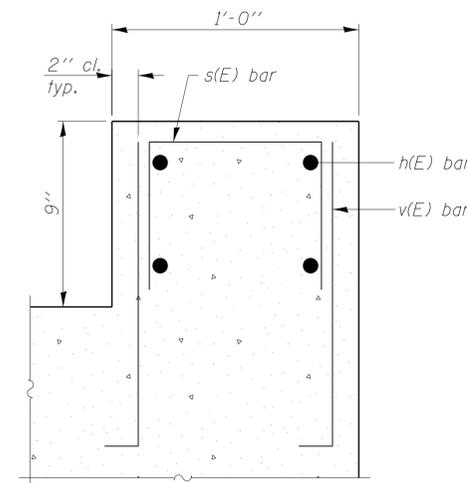
1. Perform excavation and construct toewall.
2. Backfill according to the applicable paragraphs of Article 502.10 of the Standard Specifications and place bedding for precast box culvert end sections.
3. Set precast box culvert end section.
4. Drill and grout reinforcement in toewall using approved chemical adhesive in accordance with Section 1027 of the Standard Specifications.
5. Pressure grout voids using non-shrink grout conforming to Section 1024 of the Standard Specifications.

* The Contractor may furnish a precast or cast-in-place toewall. The Contractor shall be responsible for the strength and stability of the precast toewall during handling. Additional lifting points may be required depending upon the length of the toewall or the Contractor may need to modify the design of the toewall for the proposed handling the method.

** If soil conditions permit, the sides of the toewall may be poured directly against the soil. The clear cover on the sides of the toewall shall be increased to 3" by increasing the thickness of the toewall.



SECTION E-E



SECTION F-F

| | | | | | | | | | | | | | |
|------------------------------------|-----------------------------|-----------------------|--------------------------------|---|---------------------------------------|-----------|-----------|------|--------------|---|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 2-10-14 REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | | | | | | CONTRACT NO. | | | | |
| | | | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | |

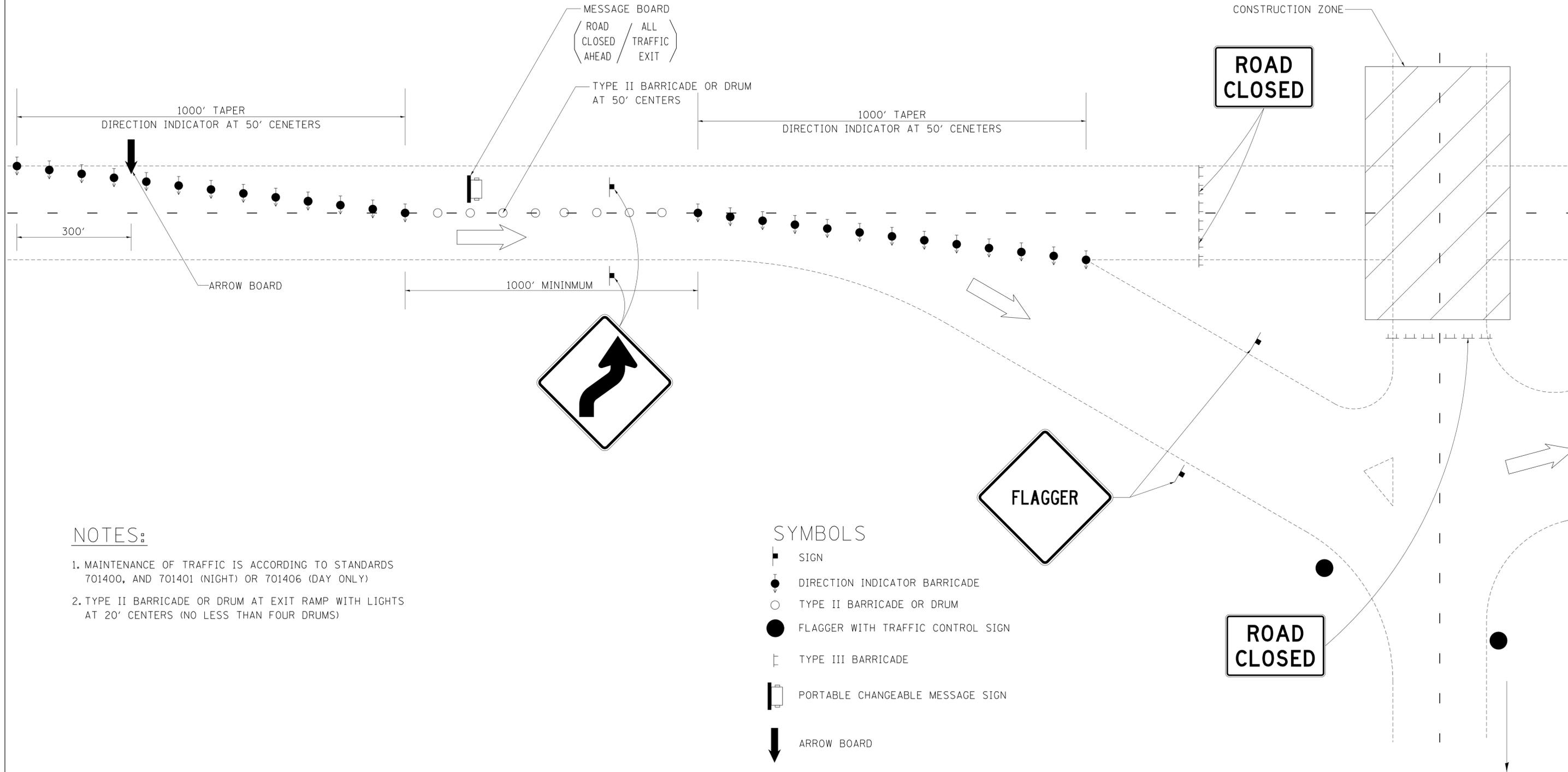
BOX CULVERT END SECTIONS

PIPE-GRADE SCHEDULE FOR BOX CULVERT END SECTIONS

| Precast Box Culvert Dimensions | | | Slope of End Section | | | | | | | | |
|-----------------------------------|--------|---------------------|---------------------------|------------------------------|-------------------------|---------------------------|------------------------------|-------------------------|---------------------------|------------------------------|-------------------------|
| | | | 1:3 | | | 1:4 | | | 1:6 | | |
| | | | Main Pipe No. / Length | Int. Support No. / Length | Total Length of Pipe | Main Pipe No. / Length | Int. Support No. / Length | Total Length of Pipe | Main Pipe No. / Length | Int. Support No. / Length | Total Length of Pipe |
| S (ft) | R (ft) | T ₁ (in) | | | | | | | | | |
| 4 | 2 | 7.5 | 1 @ 8'-10" | N/A | 8'-10" | 1 @ 11'-7" | N/A | 11'-7" | 1 @ 17'-2" | N/A | 17'-2" |
| 4 | 2 | 5 | 1 @ 8'-2" | N/A | 8'-2" | 1 @ 10'-8" | N/A | 10'-8" | 1 @ 15'-11" | N/A | 15'-11" |
| 4 | 3 | 7.5 | 1 @ 12'-0" | N/A | 12'-0" | 1 @ 15'-8" | N/A | 15'-8" | 1 @ 23'-3" | 1 @ 3'-7" | 26'-10" |
| 4 | 3 | 5 | 1 @ 11'-4" | N/A | 11'-4" | 1 @ 14'-10" | N/A | 14'-10" | 1 @ 22'-0" | 1 @ 3'-7" | 25'-7" |
| 4 | 4 | 7.5 | 1 @ 15'-2" | N/A | 15'-2" | 1 @ 19'-10" | 1 @ 3'-7" | 23'-5" | 1 @ 29'-4" | 2 @ 3'-7" | 36'-6" |
| 4 | 4 | 5 | 1 @ 14'-6" | N/A | 14'-6" | 1 @ 18'-11" | N/A | 18'-11" | 1 @ 28'-1" | 2 @ 3'-7" | 35'-3" |
| 5 | 2 | 8 | 1 @ 8'-11" | N/A | 8'-11" | 1 @ 11'-9" | N/A | 11'-9" | 1 @ 17'-5" | N/A | 17'-5" |
| 5 | 2 | 6 | 1 @ 8'-5" | N/A | 8'-5" | 1 @ 11'-1" | N/A | 11'-1" | 1 @ 16'-5" | N/A | 16'-5" |
| 5 | 3 | 8 | 1 @ 12'-1" | N/A | 12'-1" | 1 @ 15'-10" | N/A | 15'-10" | 1 @ 23'-6" | 1 @ 4'-7" | 28'-1" |
| 5 | 3 | 6 | 1 @ 11'-7" | N/A | 11'-7" | 1 @ 15'-2" | N/A | 15'-2" | 1 @ 22'-6" | 1 @ 4'-7" | 27'-1" |
| 5 | 4 | 8 | 1 @ 15'-3" | N/A | 15'-3" | 1 @ 20'-0" | 1 @ 4'-7" | 24'-7" | 1 @ 29'-7" | 2 @ 4'-7" | 38'-9" |
| 5 | 4 | 6 | 1 @ 14'-9" | N/A | 14'-9" | 1 @ 19'-3" | N/A | 19'-3" | 1 @ 28'-7" | 2 @ 4'-7" | 37'-9" |
| 5 | 5 | 8 | 1 @ 18'-5" | N/A | 18'-5" | 1 @ 24'-1" | 2 @ 4'-7" | 33'-3" | 1 @ 35'-8" | 3 @ 4'-7" | 49'-5" |
| 5 | 5 | 6 | 1 @ 17'-11" | N/A | 17'-11" | 1 @ 23'-5" | 1 @ 4'-7" | 28'-0" | 1 @ 34'-8" | 2 @ 4'-7" | 43'-10" |
| 6 | 2 | 8 | 2 @ 8'-11" | N/A | 17'-10" | 2 @ 11'-9" | N/A | 23'-6" | 2 @ 17'-5" | N/A | 34'-10" |
| 6 | 2 | 7 | 2 @ 8'-8" | N/A | 17'-4" | 2 @ 11'-5" | N/A | 22'-10" | 2 @ 16'-11" | N/A | 33'-10" |
| 6 | 3 | 8 | 2 @ 12'-1" | N/A | 24'-2" | 2 @ 15'-10" | N/A | 31'-8" | 2 @ 23'-6" | 1 @ 5'-7" | 52'-7" |
| 6 | 3 | 7 | 2 @ 11'-10" | N/A | 23'-8" | 2 @ 15'-6" | N/A | 31'-0" | 2 @ 23'-0" | 1 @ 5'-7" | 51'-7" |
| 6 | 4 | 8 | 2 @ 15'-3" | N/A | 30'-6" | 2 @ 20'-0" | 1 @ 5'-7" | 45'-7" | 2 @ 29'-7" | 2 @ 5'-7" | 70'-4" |
| 6 | 4 | 7 | 2 @ 15'-0" | N/A | 30'-0" | 2 @ 19'-8" | 1 @ 5'-7" | 44'-11" | 2 @ 29'-1" | 2 @ 5'-7" | 69'-4" |
| 6 | 5 | 8 | 2 @ 18'-5" | N/A | 36'-10" | 2 @ 24'-1" | 2 @ 5'-7" | 59'-4" | 2 @ 35'-8" | 3 @ 5'-7" | 88'-1" |
| 6 | 5 | 7 | 2 @ 18'-2" | N/A | 36'-4" | 2 @ 23'-9" | 2 @ 5'-7" | 58'-8" | 2 @ 35'-2" | 2 @ 5'-7" | 81'-6" |
| 6 | 6 | 8 | 2 @ 21'-7" | 1 @ 5'-7" | 48'-9" | 2 @ 28'-3" | 2 @ 5'-7" | 67'-8" | 2 @ 41'-9" | 3 @ 5'-7" | 100'-3" |
| 6 | 6 | 7 | 2 @ 21'-4" | 1 @ 5'-7" | 48'-3" | 2 @ 27'-11" | 2 @ 5'-7" | 67'-0" | 2 @ 41'-3" | 3 @ 5'-7" | 99'-3" |
| 7 | 2 | 8 | 2 @ 8'-11" | N/A | 17'-10" | 2 @ 11'-9" | N/A | 23'-6" | 2 @ 17'-5" | N/A | 34'-10" |
| 7 | 3 | 8 | 2 @ 12'-1" | N/A | 24'-2" | 2 @ 15'-10" | N/A | 31'-8" | 2 @ 23'-6" | 2 @ 6'-7" | 60'-2" |
| 7 | 4 | 8 | 2 @ 15'-3" | N/A | 30'-6" | 2 @ 20'-0" | 2 @ 6'-7" | 53'-2" | 2 @ 29'-7" | 3 @ 6'-7" | 78'-11" |
| 7 | 5 | 8 | 2 @ 18'-5" | N/A | 36'-10" | 2 @ 24'-1" | 3 @ 6'-7" | 67'-11" | 2 @ 35'-8" | 4 @ 6'-7" | 97'-8" |
| 7 | 6 | 8 | 2 @ 21'-7" | 2 @ 6'-7" | 56'-4" | 2 @ 28'-3" | 3 @ 6'-7" | 76'-3" | 2 @ 41'-9" | 5 @ 6'-7" | 116'-5" |
| 7 | 7 | 8 | 2 @ 24'-9" | 3 @ 6'-7" | 69'-3" | 2 @ 32'-4" | 4 @ 6'-7" | 91'-0" | 2 @ 47'-10" | 6 @ 6'-7" | 135'-2" |
| 8 | 2 | 8 | 3 @ 8'-11" | N/A | 26'-9" | 3 @ 11'-9" | N/A | 35'-3" | 3 @ 17'-5" | N/A | 52'-3" |
| 8 | 3 | 8 | 3 @ 12'-1" | N/A | 36'-3" | 3 @ 15'-10" | N/A | 47'-6" | 3 @ 23'-6" | 2 @ 7'-7" | 85'-8" |
| 8 | 4 | 8 | 3 @ 15'-3" | N/A | 45'-9" | 3 @ 20'-0" | 2 @ 7'-7" | 75'-2" | 3 @ 29'-7" | 3 @ 7'-7" | 111'-6" |
| 8 | 5 | 8 | 3 @ 18'-5" | N/A | 55'-3" | 3 @ 24'-1" | 3 @ 7'-7" | 95'-0" | 3 @ 35'-8" | 4 @ 7'-7" | 137'-4" |
| 8 | 6 | 8 | 3 @ 21'-7" | 2 @ 7'-7" | 79'-11" | 3 @ 28'-3" | 3 @ 7'-7" | 107'-6" | 3 @ 41'-9" | 5 @ 7'-7" | 163'-2" |
| 8 | 7 | 8 | 3 @ 24'-9" | 3 @ 7'-7" | 97'-0" | 3 @ 32'-4" | 4 @ 7'-7" | 127'-4" | 3 @ 47'-10" | 6 @ 7'-7" | 189'-0" |
| 8 | 8 | 8 | 3 @ 27'-11" | 3 @ 7'-7" | 106'-6" | 3 @ 36'-6" | 4 @ 7'-7" | 139'-10" | 3 @ 53'-11" | 6 @ 7'-7" | 207'-3" |
| 9 | 2 | 9 | 3 @ 9'-3" | N/A | 27'-9" | 3 @ 12'-1" | N/A | 36'-3" | 3 @ 17'-11" | N/A | 53'-9" |
| 9 | 3 | 9 | 3 @ 12'-4" | N/A | 37'-0" | 3 @ 16'-2" | N/A | 48'-6" | 3 @ 24'-0" | 3 @ 8'-7" | 97'-9" |
| 9 | 4 | 9 | 3 @ 15'-6" | N/A | 46'-6" | 3 @ 20'-4" | 2 @ 8'-7" | 78'-2" | 3 @ 30'-1" | 3 @ 8'-7" | 116'-0" |
| 9 | 5 | 9 | 3 @ 18'-8" | N/A | 56'-0" | 3 @ 24'-5" | 3 @ 8'-7" | 99'-0" | 3 @ 36'-2" | 4 @ 8'-7" | 142'-10" |
| 9 | 6 | 9 | 3 @ 21'-10" | 2 @ 8'-7" | 82'-8" | 3 @ 28'-7" | 3 @ 8'-7" | 111'-6" | 3 @ 42'-3" | 5 @ 8'-7" | 169'-8" |
| 9 | 7 | 9 | 3 @ 25'-0" | 3 @ 8'-7" | 100'-9" | 3 @ 32'-8" | 4 @ 8'-7" | 132'-4" | 3 @ 48'-4" | 6 @ 8'-7" | 196'-6" |
| 9 | 8 | 9 | 3 @ 28'-2" | 3 @ 8'-7" | 110'-3" | 3 @ 36'-10" | 4 @ 8'-7" | 144'-10" | 3 @ 54'-5" | 6 @ 8'-7" | 214'-9" |
| 9 | 9 | 9 | 3 @ 31'-4" | 3 @ 8'-7" | 119'-9" | 3 @ 40'-11" | 5 @ 8'-7" | 165'-8" | 3 @ 60'-6" | 7 @ 8'-7" | 241'-7" |
| 10 | 2 | 10 | 3 @ 9'-6" | N/A | 28'-6" | 3 @ 12'-5" | N/A | 37'-3" | 3 @ 18'-5" | N/A | 55'-3" |
| 10 | 3 | 10 | 3 @ 12'-8" | N/A | 38'-0" | 3 @ 16'-6" | N/A | 49'-6" | 3 @ 24'-6" | 3 @ 9'-7" | 102'-3" |
| 10 | 4 | 10 | 3 @ 15'-10" | N/A | 47'-6" | 3 @ 20'-8" | 2 @ 9'-7" | 81'-2" | 3 @ 30'-7" | 3 @ 9'-7" | 120'-6" |
| 10 | 5 | 10 | 3 @ 19'-0" | N/A | 57'-0" | 3 @ 24'-9" | 3 @ 9'-7" | 103'-0" | 3 @ 36'-8" | 4 @ 9'-7" | 148'-4" |
| 10 | 6 | 10 | 3 @ 22'-1" | 2 @ 9'-7" | 85'-5" | 3 @ 28'-11" | 3 @ 9'-7" | 115'-6" | 3 @ 42'-9" | 5 @ 9'-7" | 176'-2" |
| 10 | 7 | 10 | 3 @ 25'-3" | 3 @ 9'-7" | 104'-6" | 3 @ 33'-0" | 4 @ 9'-7" | 137'-4" | 3 @ 48'-10" | 6 @ 9'-7" | 204'-0" |
| 10 | 8 | 10 | 3 @ 28'-5" | 3 @ 9'-7" | 114'-0" | 3 @ 37'-2" | 4 @ 9'-7" | 149'-10" | 3 @ 54'-11" | 6 @ 9'-7" | 222'-3" |
| 10 | 9 | 10 | 3 @ 31'-7" | 4 @ 9'-7" | 133'-3" | 3 @ 41'-3" | 5 @ 9'-7" | 171'-8" | 3 @ 61'-0" | 7 @ 9'-7" | 250'-1" |
| 10 | 10 | 10 | 3 @ 34'-9" | 4 @ 9'-7" | 142'-7" | 3 @ 45'-5" | 5 @ 9'-7" | 184'-2" | 3 @ 67'-1" | 8 @ 9'-7" | 277'-11" |
| 11 | 2 | 11 | 4 @ 9'-9" | N/A | 39'-0" | 4 @ 12'-9" | N/A | 51'-0" | 4 @ 18'-11" | N/A | 75'-8" |
| 11 | 3 | 11 | 4 @ 12'-11" | N/A | 51'-8" | 4 @ 16'-11" | N/A | 67'-8" | 4 @ 25'-0" | 3 @ 10'-7" | 131'-9" |
| 11 | 4 | 11 | 4 @ 16'-1" | N/A | 64'-4" | 4 @ 21'-0" | 2 @ 10'-7" | 105'-2" | 4 @ 31'-1" | 3 @ 10'-7" | 156'-1" |
| 11 | 6 | 11 | 4 @ 22'-5" | 2 @ 10'-7" | 110'-10" | 4 @ 29'-3" | 3 @ 10'-7" | 148'-9" | 4 @ 43'-3" | 5 @ 10'-7" | 225'-11" |
| 11 | 8 | 11 | 4 @ 28'-9" | 3 @ 10'-7" | 146'-9" | 4 @ 37'-6" | 4 @ 10'-7" | 192'-4" | 4 @ 55'-5" | 6 @ 10'-7" | 285'-2" |
| 11 | 10 | 11 | 4 @ 35'-0" | 4 @ 10'-7" | 182'-4" | 4 @ 45'-9" | 5 @ 10'-7" | 235'-11" | 4 @ 67'-7" | 8 @ 10'-7" | 355'-0" |
| 11 | 11 | 11 | 4 @ 38'-2" | 4 @ 10'-7" | 195'-0" | 4 @ 49'-10" | 6 @ 10'-7" | 262'-10" | 4 @ 73'-8" | 9 @ 10'-7" | 389'-11" |
| 12 | 2 | 12 | 4 @ 10'-0" | N/A | 40'-0" | 4 @ 13'-1" | N/A | 52'-4" | 4 @ 19'-5" | N/A | 77'-8" |
| 12 | 3 | 12 | 4 @ 13'-2" | N/A | 52'-8" | 4 @ 17'-3" | N/A | 69'-0" | 4 @ 25'-6" | 3 @ 11'-7" | 136'-9" |
| 12 | 4 | 12 | 4 @ 16'-4" | N/A | 65'-4" | 4 @ 21'-4" | 2 @ 11'-7" | 108'-6" | 4 @ 31'-7" | 4 @ 11'-7" | 172'-8" |
| 12 | 6 | 12 | 4 @ 22'-8" | 2 @ 11'-7" | 113'-10" | 4 @ 29'-7" | 3 @ 11'-7" | 153'-1" | 4 @ 43'-9" | 5 @ 11'-7" | 232'-11" |
| 12 | 8 | 12 | 4 @ 29'-0" | 3 @ 11'-7" | 150'-9" | 4 @ 37'-10" | 4 @ 11'-7" | 197'-8" | 4 @ 55'-11" | 7 @ 11'-7" | 304'-9" |
| 12 | 10 | 12 | 4 @ 35'-4" | 4 @ 11'-7" | 187'-8" | 4 @ 46'-1" | 5 @ 11'-7" | 242'-3" | 4 @ 68'-1" | 8 @ 11'-7" | 365'-0" |
| 12 | 12 | 12 | 4 @ 41'-8" | 5 @ 11'-7" | 224'-7" | 4 @ 54'-4" | 6 @ 11'-7" | 286'-10" | 4 @ 80'-3" | 10 @ 11'-7" | 436'-10" |

| | | | | | | | | | | |
|--------------------------------------|-----------------------------|-----------------------|--------------------------------|---|---------------------------------------|-----------|-----------|--------|--------------|---------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 2-10-14 REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | CONTRACT NO. | | | | | | | |
| PLOT DATE = Mon Feb 10 13:44:19 2014 | DATE - | REVISED - | SCALE: | | | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. |

TEMPORARY ROAD CLOSURE EXPRESSWAY



NOTES:

1. MAINTENANCE OF TRAFFIC IS ACCORDING TO STANDARDS 701400, AND 701401 (NIGHT) OR 701406 (DAY ONLY)
2. TYPE II BARRICADE OR DRUM AT EXIT RAMP WITH LIGHTS AT 20' CENTERS (NO LESS THAN FOUR DRUMS)

SYMBOLS

- ▬ SIGN
- DIRECTION INDICATOR BARRICADE
- TYPE II BARRICADE OR DRUM
- FLAGGER WITH TRAFFIC CONTROL SIGN
- ▬ TYPE III BARRICADE
- ▬ PORTABLE CHANGEABLE MESSAGE SIGN
- ▬ ARROW BOARD

SIGNING ACCORDING TO SPECIFIED STANDARDS

| | | | |
|------------------------------------|--------------------------------------|------------|-------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 |
| | | DRAWN - | REVISED - 4-04-11 |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:20 2014 | DATE - | REVISED - |

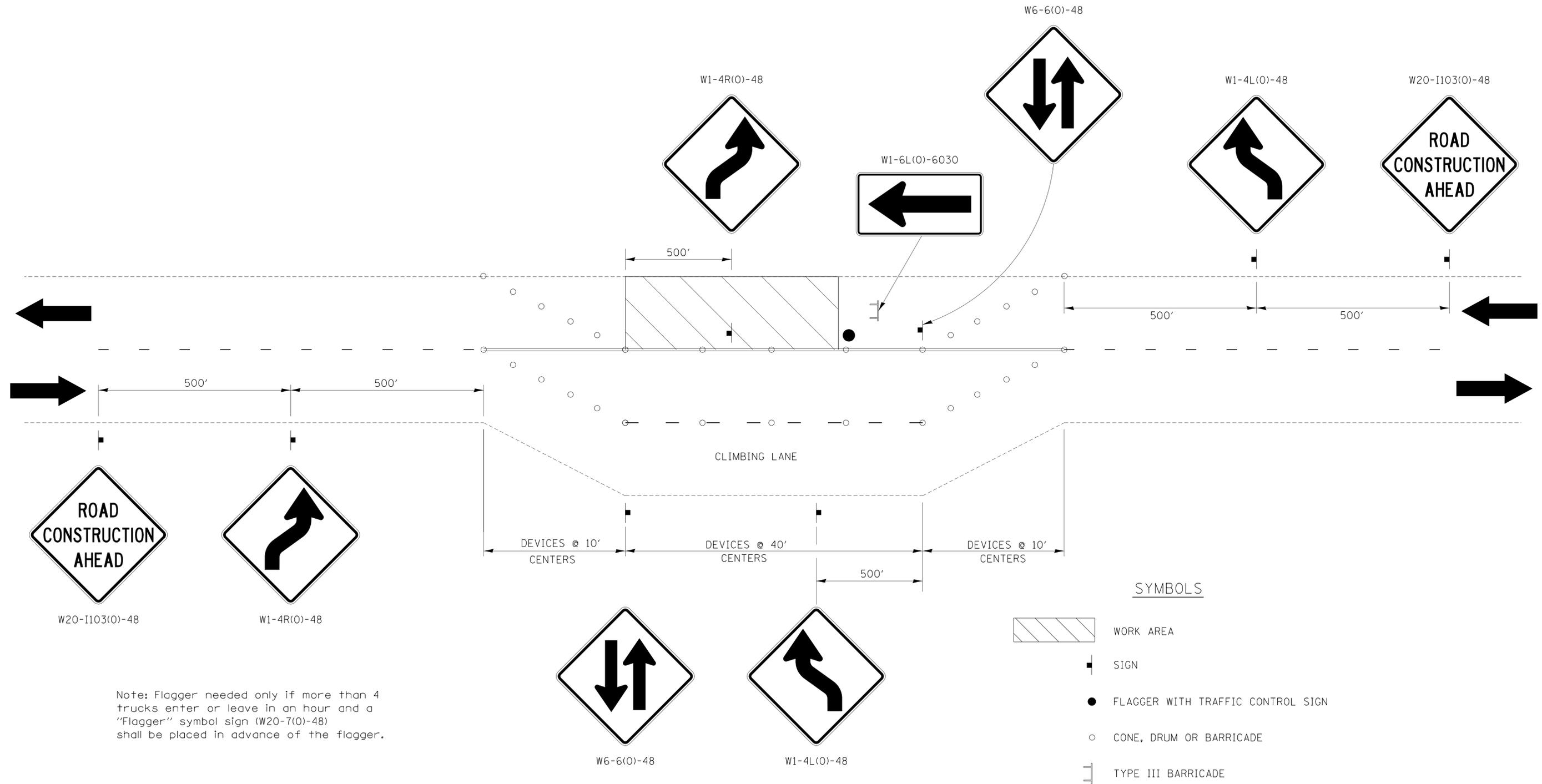
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC CONTROL FOR THREE LANE SECTION CASE 1



Note: Flagger needed only if more than 4 trucks enter or leave in an hour and a "Flagger" symbol sign (W20-7(0)-48) shall be placed in advance of the flagger.

THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

| | | | |
|------------------------------------|--------------------------------------|------------|-------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 |
| | | DRAWN - | REVISED - 7-30-13 |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:20 2014 | DATE - | REVISED - |

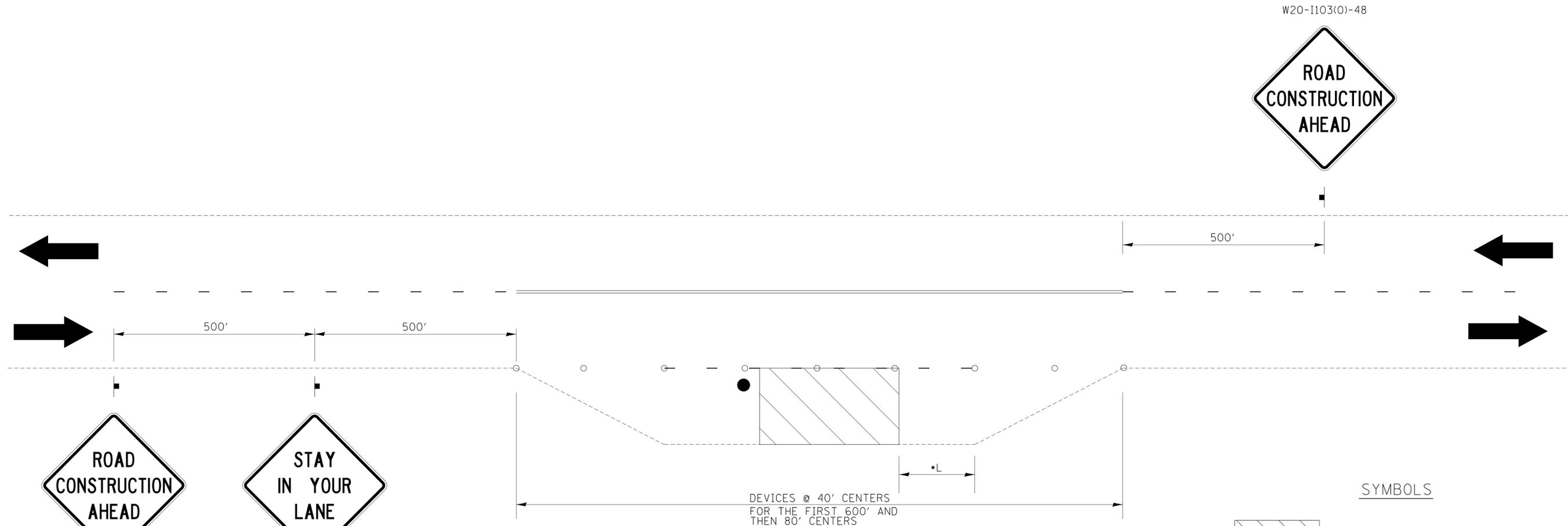
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC CONTROL FOR THREE LANE SECTION CASE 2



W20-1103(0)-48

Note: Flagger needed only if more than 4 trucks enter or leave in an hour and a "Flagger" symbol sign (W20-7(0)-48) shall be placed in advance of the flagger.

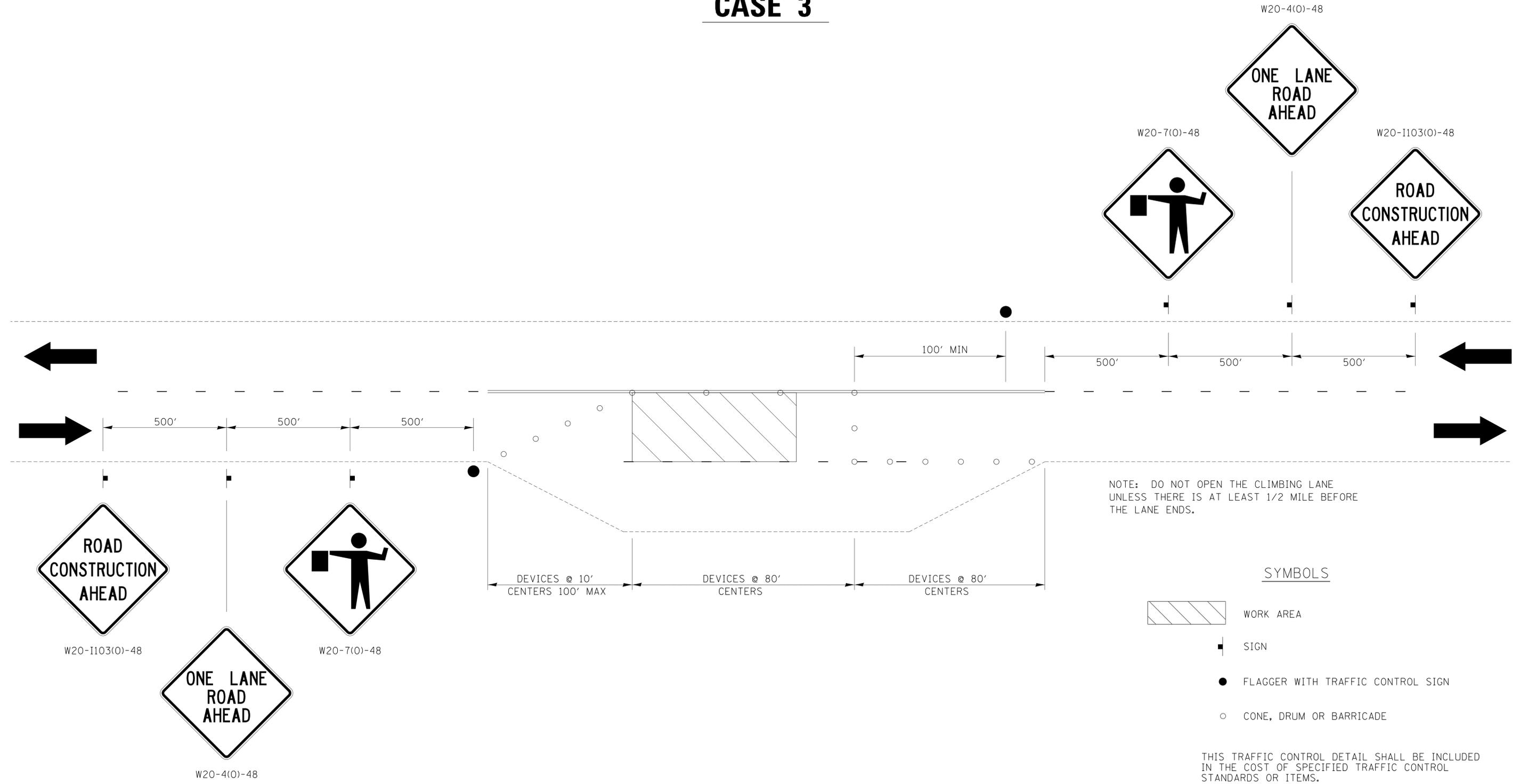
- SYMBOLS
- WORK AREA
 - SIGN
 - FLAGGER WITH TRAFFIC CONTROL SIGN
 - CONE, DRUM OR BARRICADE

THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

*IF THE WORK ENDS WITHIN 2500 FEET OF THE TRANSITION WHEN THE SPEED IS > 40 MPH OR 1500 FEET FOR ALL OTHER SPEEDS, THE CLIMBING LANE SHALL REMAIN CLOSED AS SHOWN.

| | | | | | | | | | | | | | |
|------------------------------------|-----------------------------|------------|-------------------|--|---------------------------------------|--|--|--|---|---------|--------|-----------------|--------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - 7-30-13 | | | | | | | | | | |
| | | CHECKED - | REVISED - | | | | | | CONTRACT NO. | | | | |
| | | DATE - | REVISED - | | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |
| | | | | SCALE: SHEET NO. OF SHEETS STA. TO STA. | | | | | | | | | |

TRAFFIC CONTROL FOR THREE LANE SECTION CASE 3



| | | | |
|------------------------------------|--------------------------------------|------------|-------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 |
| | | DRAWN - | REVISED - 7-30-13 |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:21 2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

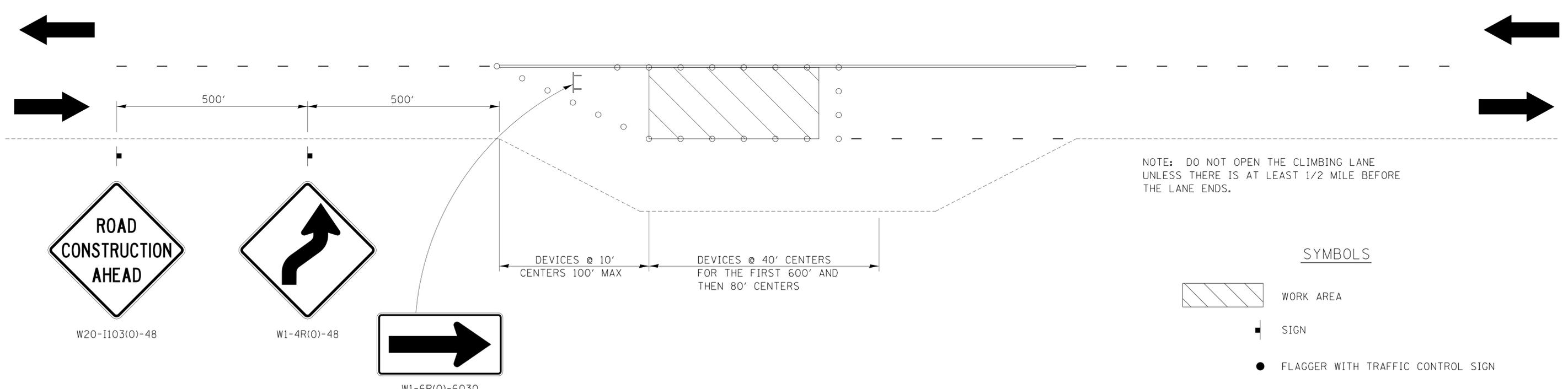
| | | | |
|---------------------------------------|---------------------|--------------|--|
| REGION 2 / DISTRICT 2 STANDARD | | | |
| SCALE: | SHEET NO. OF SHEETS | STA. TO STA. | |

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC CONTROL FOR THREE LANE SECTION

CASE 4

W20-I103(O)-48



NOTE: DO NOT OPEN THE CLIMBING LANE UNLESS THERE IS AT LEAST 1/2 MILE BEFORE THE LANE ENDS.

SYMBOLS

- WORK AREA
- SIGN
- FLAGGER WITH TRAFFIC CONTROL SIGN
- CONE, DRUM OR BARRICADE
- TYPE III BARRICADE

THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

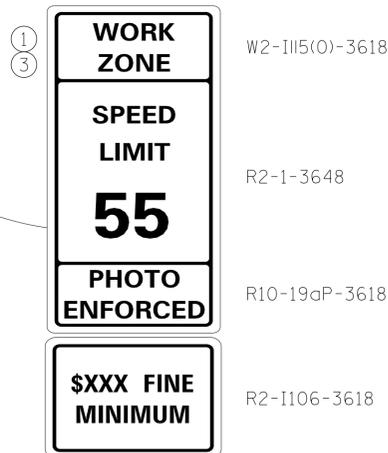
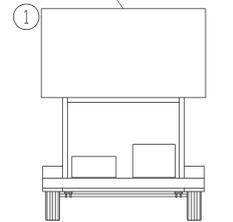
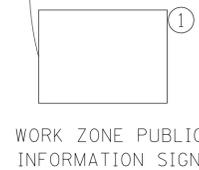
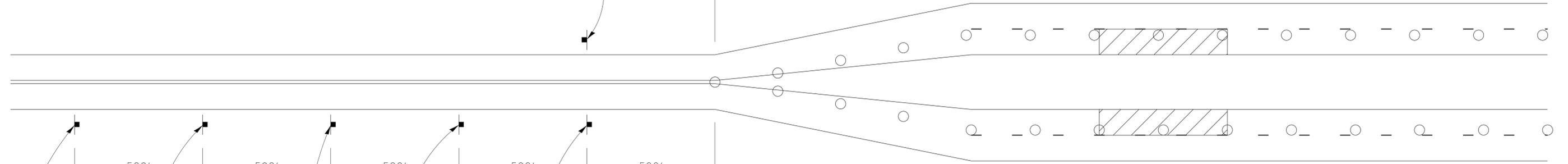
CASE 4 APPLIES WHEN NO WORKERS ARE PRESENT. WHEN WORKERS ARE PRESENT, TWO LANES SHALL BE CLOSED AND TRAFFIC CONTROL SHALL BE ACCORDING TO CASE 3.

| | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|------------|-------------------|---|---------------------------------------|--------|-----------|-----------|---------|---------|---------------------|---------------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - 7-30-13 | | | | | | | | | |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - | | | | | | | | | |
| | PLOT DATE = Mon Feb 10 13:44:22 2014 | DATE - | REVISED - | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |

TRAFFIC CONTROL FOR TRANSITION AREAS

CASE 1

SIGNS, DEVICES & FLAGGERS
ACCORDING TO APPLICABLE
TRAFFIC CONTROL STANDARDS



GENERAL NOTES
THIS DETAIL IS TO BE USED IN CONJUNCTION
WITH THE APPLICABLE MULTILANE TRAFFIC
CONTROL AND PROTECTION STANDARD.

1. If applicable, Use speed limit as shown on applicable multilane Traffic Control and Protection Standard.
2. If the work is within 2500 feet of the transition when the speed is > 40 mph, or 1500 feet for all other speeds, the detail shall be used.
3. WORK ZONE SPEED LIMIT 55 SHALL be replaced with WORK ZONE SPEED LIMIT 45 BEGINS where the workers are within 500 feet of the transition.

THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED
IN THE COST OF SPECIFIED TRAFFIC CONTROL
STANDARDS OR ITEMS.

NOTE: STANDARDS 701301 AND 701306
SHALL NOT BE USED WITHIN 500 FEET
OF THE TRANSITION.

* DEPENDS ON GEOMETRICS
OF THE TRANSITION. MAY
SWITCH THE "STAY IN YOUR
LANE" AND "WEAVE SIGNS"

○ DRUMS OR BARRICADES

▨ WORK AREA

| | | | |
|------------------------------------|--------------------------------------|------------|-------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 |
| | | DRAWN - | REVISED - 1-16-13 |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:22 2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

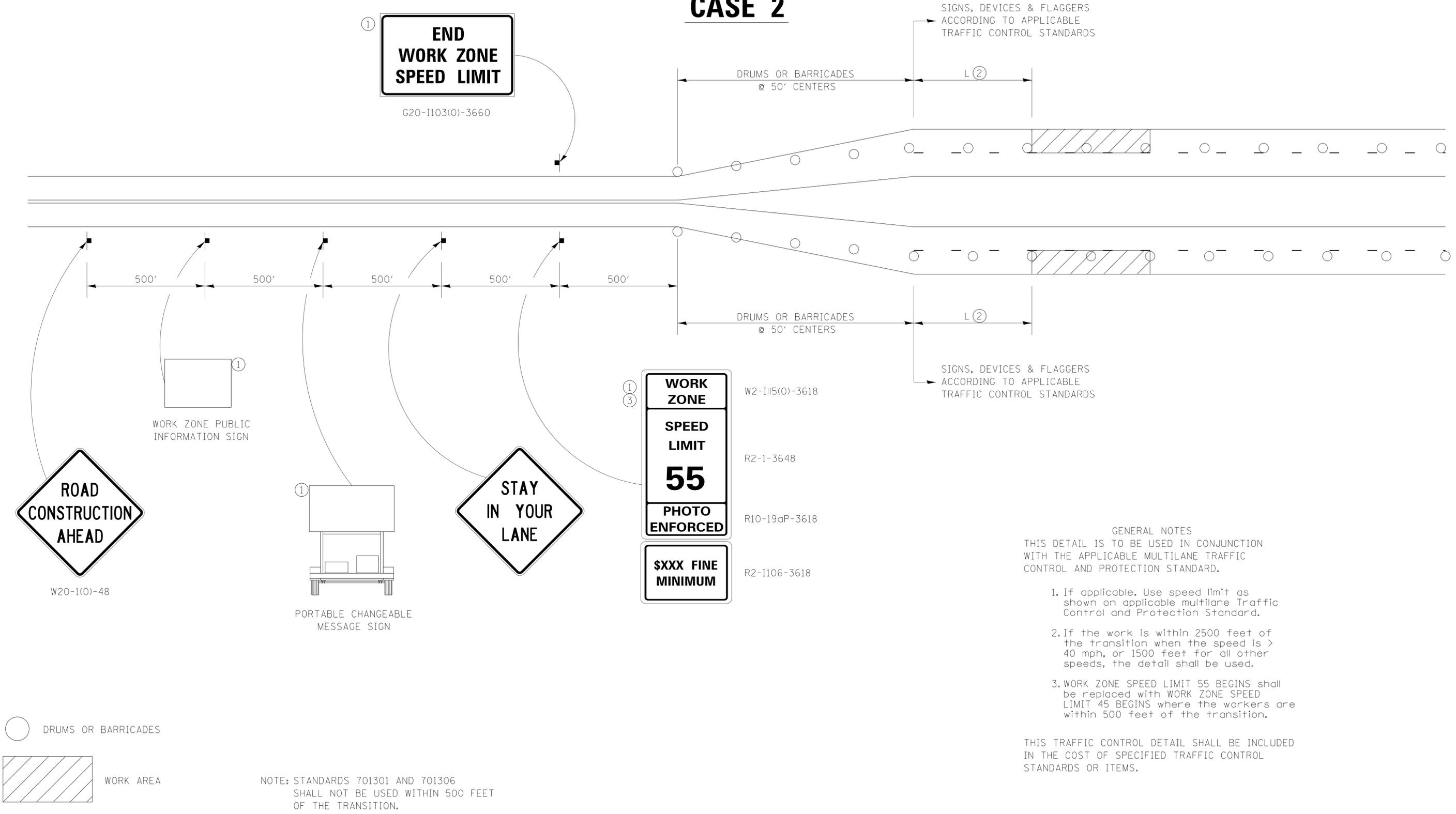
REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC CONTROL FOR TRANSITION AREAS

CASE 2



SIGNS, DEVICES & FLAGGERS ACCORDING TO APPLICABLE TRAFFIC CONTROL STANDARDS

SIGNS, DEVICES & FLAGGERS ACCORDING TO APPLICABLE TRAFFIC CONTROL STANDARDS

GENERAL NOTES
THIS DETAIL IS TO BE USED IN CONJUNCTION WITH THE APPLICABLE MULTILANE TRAFFIC CONTROL AND PROTECTION STANDARD.

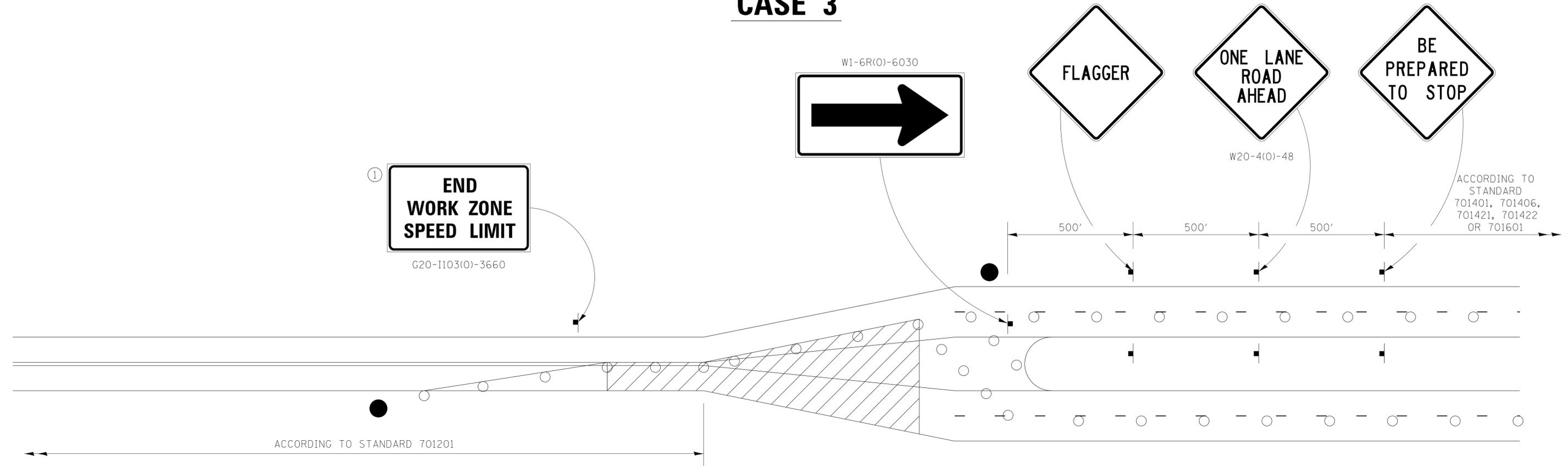
1. If applicable, Use speed limit as shown on applicable multilane Traffic Control and Protection Standard.
2. If the work is within 2500 feet of the transition when the speed is > 40 mph, or 1500 feet for all other speeds, the detail shall be used.
3. WORK ZONE SPEED LIMIT 55 SHALL BE REPLACED WITH WORK ZONE SPEED LIMIT 45 BEGINS where the workers are within 500 feet of the transition.

THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

| | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|--|---|---------------------------------------|-----------|-----------|---------------------------|---------|--------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 1-16-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | |
| | PLOT DATE = Mon Feb 10 13:44:23 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. | | | ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC CONTROL FOR TRANSITION AREAS

CASE 3



GENERAL NOTES
THIS DETAIL IS TO BE USED IN CONJUNCTION WITH THE APPLICABLE MULTILANE TRAFFIC CONTROL AND PROTECTION STANDARD.

1. If applicable, Use speed limit as shown on applicable multilane Traffic Control and Protection Standard.
2. If the work is within 2500 feet of the transition when the speed is > 40 mph, or 1500 feet for all other speeds, the detail shall be used.
3. WORK ZONE SPEED LIMIT 55 BEGINS shall be replaced with WORK ZONE SPEED LIMIT 45 BEGINS where the workers are within 500 feet of the transition.

THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

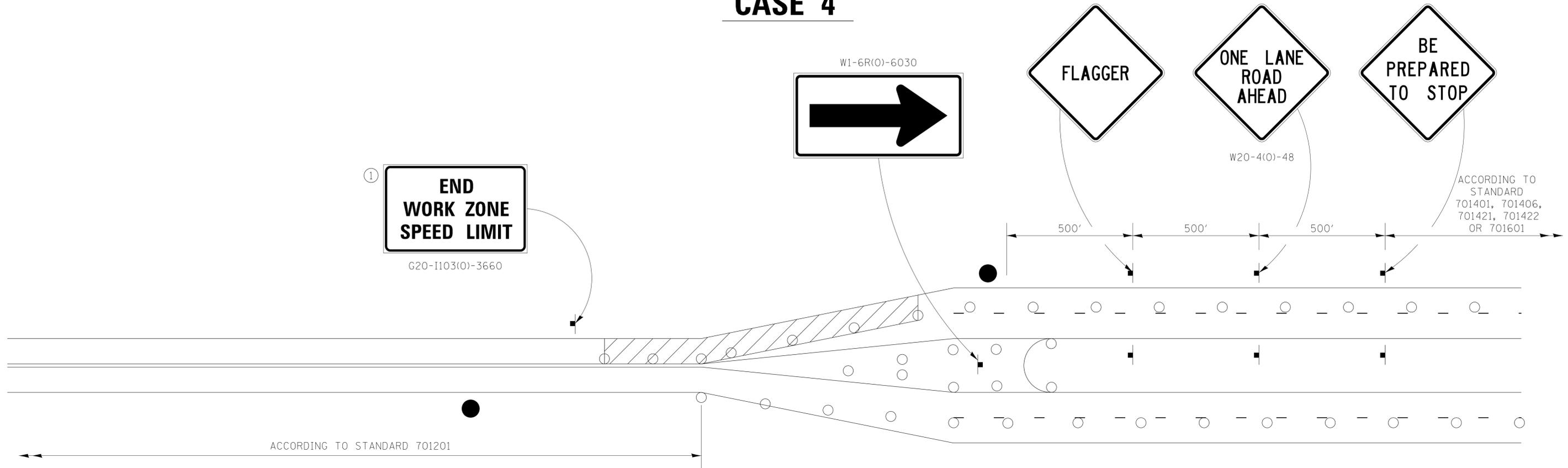
- FLAGGER WITH TRAFFIC CONTROL SIGN
- DRUMS OR BARRICADES
- ▨ WORK AREA

NOTE: STANDARDS 701301 AND 701306 SHALL NOT BE USED WITHIN 500 FEET OF THE TRANSITION.

| | | | | | | | | | | | | |
|------------------------------------|-----------------------------|------------|-------------------|---|---------------------------------------|--|--|---|---------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - 3-05-12 | | | | | | | | | |
| | | CHECKED - | REVISED - | | | | | CONTRACT NO. | | | | |
| | | DATE - | REVISED - | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |
| | | | | SCALE: SHEET NO. OF SHEETS STA. TO STA. | | | | | | | | |

TRAFFIC CONTROL FOR TRANSITION AREAS

CASE 4



GENERAL NOTES
THIS DETAIL IS TO BE USED IN CONJUNCTION WITH THE APPLICABLE MULTILANE TRAFFIC CONTROL AND PROTECTION STANDARD.

1. If applicable, Use speed limit as shown on applicable multilane Traffic Control and Protection Standard.
2. If the work is within 2500 feet of the transition when the speed is > 40 mph, or 1500 feet for all other speeds, the detail shall be used.
3. WORK ZONE SPEED LIMIT 55 BEGINS shall be replaced with WORK ZONE SPEED LIMIT 45 BEGINS where the workers are within 500 feet of the transition.

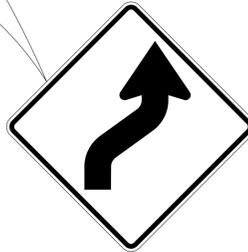
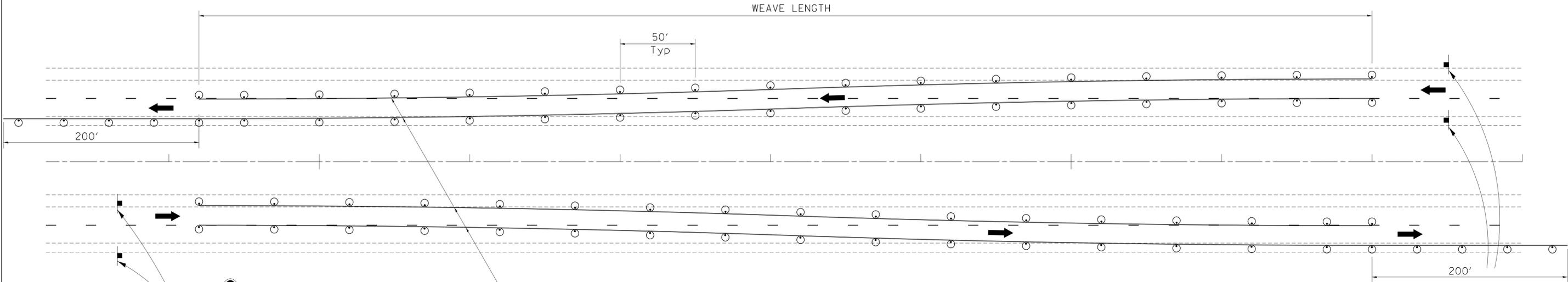
THIS TRAFFIC CONTROL DETAIL SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

- FLAGGER WITH TRAFFIC CONTROL SIGN
- DRUMS OR BARRICADES
- ▨ WORK AREA

NOTE: STANDARDS 701301 AND 701306 SHALL NOT BE USED WITHIN 500 FEET OF THE TRANSITION.

| | | | | | | | | | | | | |
|------------------------------------|-----------------------------|--------------------------------------|-------------------|---|---------------------------------------|--------|---------------------|--------------|---------------------|---------------------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - 3-05-12 | | | | | | | | | |
| | | PLOT SCALE = 1.0000' / in. | CHECKED - | | REVISED - | | | | | | | |
| | | PLOT DATE = Mon Feb 10 13:44:24 2014 | DATE - | | REVISED - | SCALE: | SHEET NO. OF SHEETS | STA. TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

TRAFFIC CONTROL TYPICAL WEAVE



W1-4R(O)-48



W13-1(O)-2424

DESIGNER NOTE:

1. USE ON LONG 4-LANE PROJECTS WHERE THE CONTRACTOR MAY CHANGE A PORTION OF THE WORK TO THE OPPOSITE LANE.
2. USE WHERE THE PROJECT IS ADJACENT TO ANOTHER AND THE CONTRACTOR COULD BE WORKING ON DIFFERENT LANES.
3. TEMPORARY PAVEMENT MARKING SHALL BE USED WHEN TYPICAL WEAVE IS USED FOR 14 DAYS OR MORE.
4. TRAFFIC CONTROL TYPICAL WEAVE SHALL BE INCLUDED IN THE COST OF THE SPECIFIC TRAFFIC CONTROL STANDARDS OF ITEMS.

Temporary Pavement Marking required if Typical Weave is used for 14 days or more.

LEGEND

- ⊙ DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHTS
- ▬ SIGN ON PERMANENT MOUNT

STANDARD WEAVE CONDITIONS FOR DIFFERENT SPEED LIMITS

| POSTED SPEED LIMIT | ADVISORY SPEED LIMIT | WEAVE LENGTH |
|--------------------|----------------------|--------------|
| 65 MPH | 45 MPH | 780 FT. |
| 55 MPH | 35 MPH | 660 FT. |
| 45 MPH | 25 MPH | 540 FT. |



W1-4L(O)-48

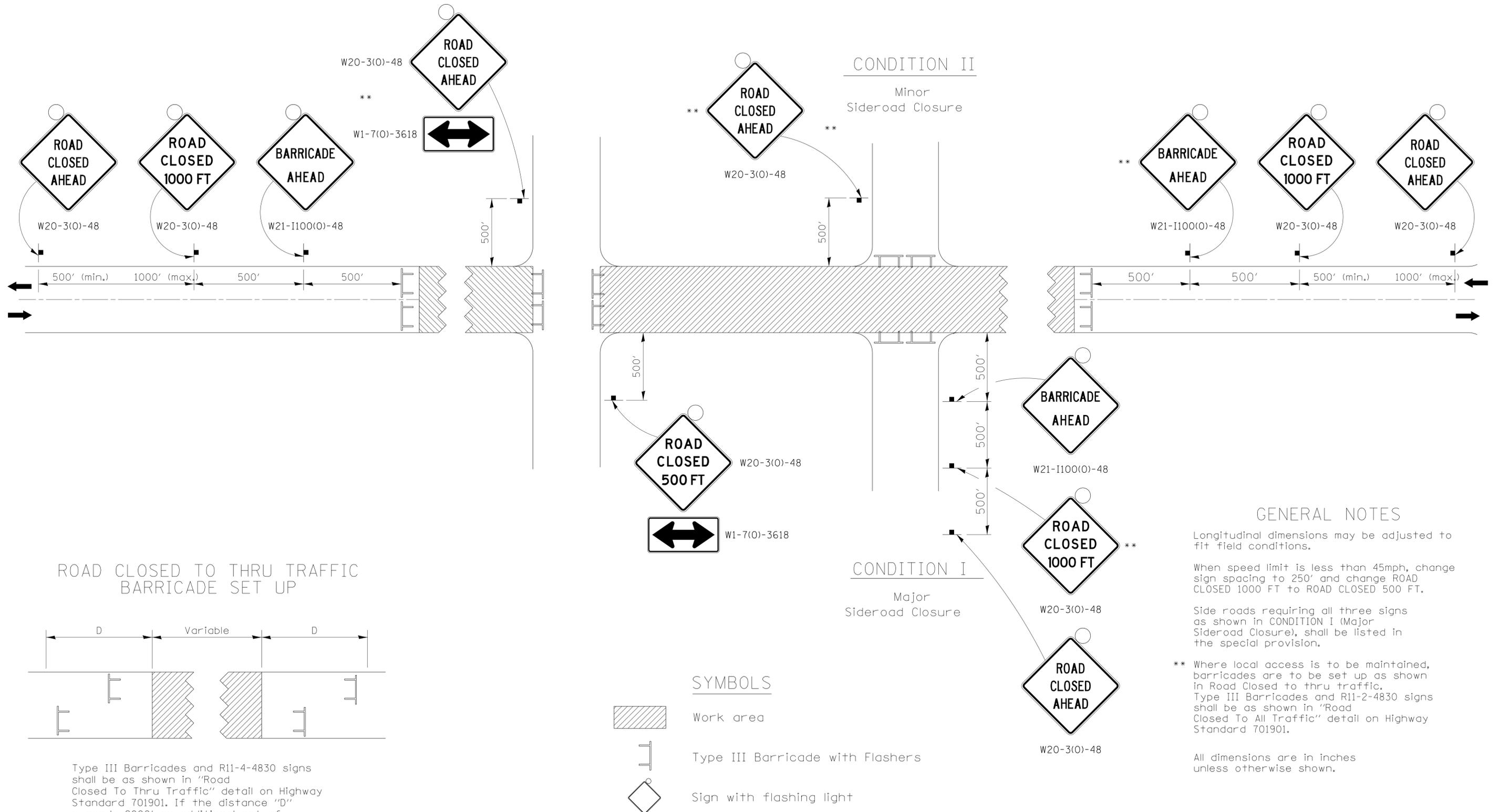


W13-1(O)-2424

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

| | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---------------------------------------|-----------|-----------|---|---------|--------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 10-17-11 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1,0000' / in. | CHECKED - DATE - | REVISED - REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | |
| | PLOT DATE = Mon Feb 10 13:44:25 2014 | | | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

TRAFFIC CONTROL FOR ROAD CLOSURE



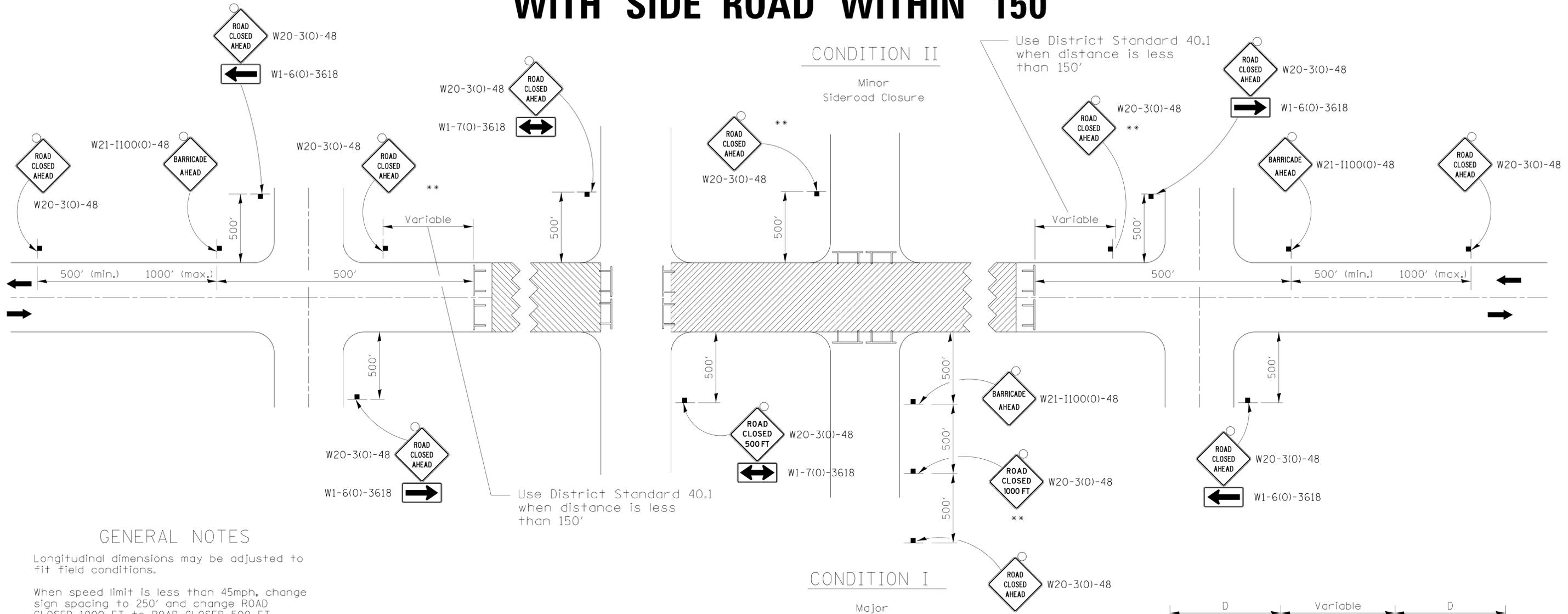
GENERAL NOTES

- Longitudinal dimensions may be adjusted to fit field conditions.
- When speed limit is less than 45mph, change sign spacing to 250' and change ROAD CLOSED 1000 FT to ROAD CLOSED 500 FT.
- Side roads requiring all three signs as shown in CONDITION I (Major Sideroad Closure), shall be listed in the special provision.
- ** Where local access is to be maintained, barricades are to be set up as shown in Road Closed to thru traffic. Type III Barricades and R11-2-4830 signs shall be as shown in "Road Closed To All Traffic" detail on Highway Standard 701901.
- All dimensions are in inches unless otherwise shown.

TYPICAL APPLICATION FOR ROAD CLOSURE

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---|-----------|-----------|------|-----------|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 10-17-11 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:25 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |

TRAFFIC CONTROL FOR ROAD CLOSURE WITH SIDE ROAD WITHIN 150'



GENERAL NOTES

Longitudinal dimensions may be adjusted to fit field conditions.

When speed limit is less than 45mph, change sign spacing to 250' and change ROAD CLOSED 1000 FT to ROAD CLOSED 500 FT.

When the distance between the barricade and the intersection is between 1500' and 2000', the advance sign shall be placed at the intersection. When the distance between the barricade and the intersection is over 2000', an additional sign shall be placed at the intersection. The additional sign shall give the distance to the barricade in miles or fractions of a mile.

Side roads requiring all three signs as shown in CONDITION I (Major Sideroad Closure), shall be listed in the special provision.

** Where local access is to be maintained, barricades are to be set up as shown above in Road Closed to thru traffic.

Type III Barricades and R11-2-4830 signs shall be as shown in "Road Closed To All Traffic" detail on Highway Standard 701901.

SYMBOLS



Work area

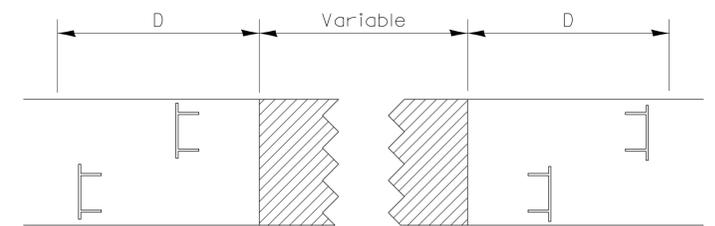


Type III Barricade with Flashers



Sign with flashing light

All dimensions are in inches unless otherwise shown.



Type III Barricades and R11-4-4830 signs shall be as shown in "Road Closed To Thru Traffic" detail on Highway Standard 701901. If the distance "D" exceeds 2000' an additional set of barricades and R11-4-4830 shall be placed at each end of the work area.

TYPICAL APPLICATION FOR ROAD CLOSURE WITH SIDE ROAD WITHIN 150' OF CLOSURE

| | | | |
|------------------------------------|--------------------------------------|-----------------------|---------------------------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED 10-17-11 |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED |
| | PLOT DATE = Mon Feb 10 13:44:26 2014 | DATE - | REVISED |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

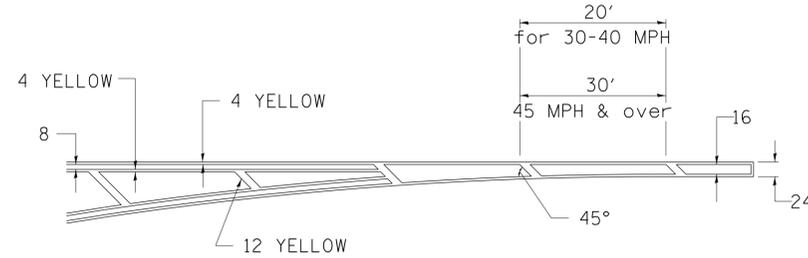
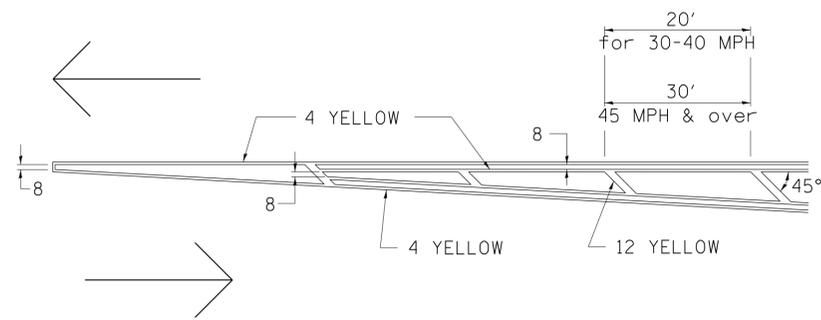
REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

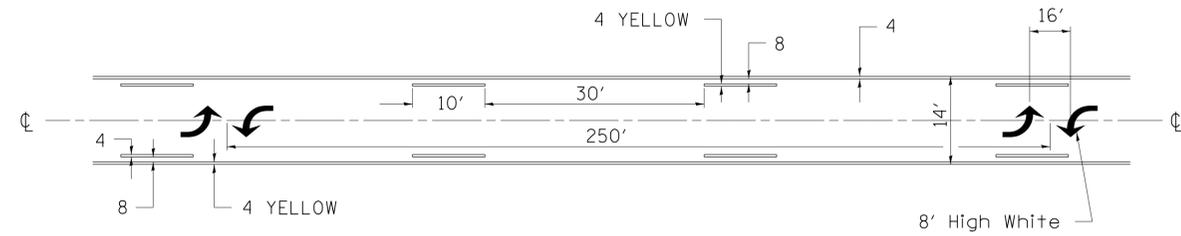
| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

TYPICAL PAVEMENT MARKINGS

TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN AT LEFT TURN LANE

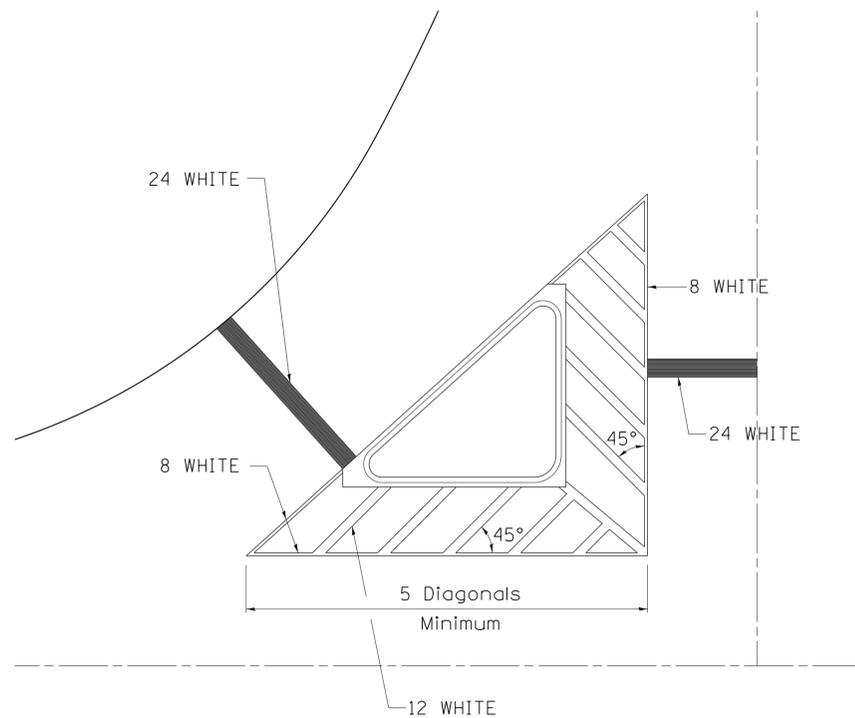


MEDIAN PAVEMENT MARKING

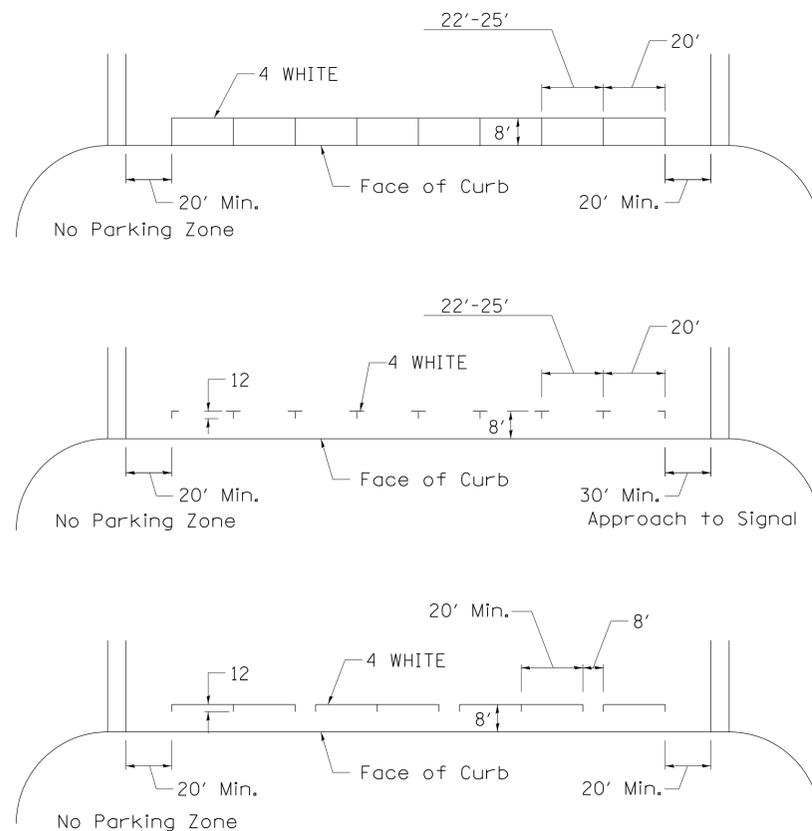


** ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

TYPICAL ISLAND OFFSET SHOULDER WIDTH

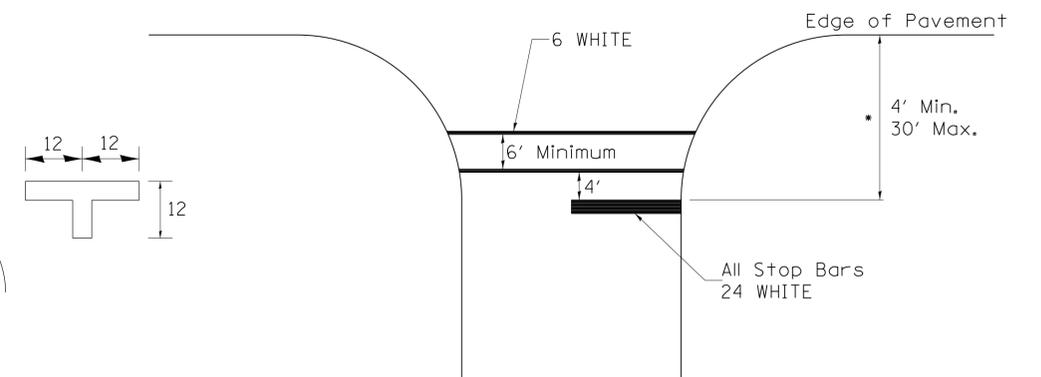


TYPICAL PARKING SPACING



STANDARD CROSSWALK MARKING

See Schedules for Locations

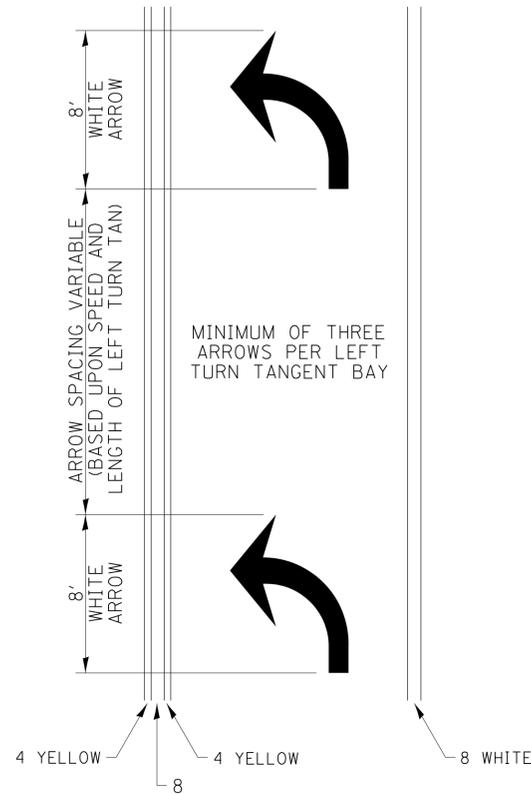


• Distance to the nearest edge of the intersecting roadway in the absence of a marked crosswalk.

| | | | | | | | | | | | | | |
|--|--------------------------------------|-----------------------|--------------------------------|---|---|-----------|-----------|------|-----------|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 3-05-12 REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:27 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |
| TYPICAL PAVEMENT MARKINGS SHEET 1 OF 3 41.1 | | | | | | | | | | | | | |

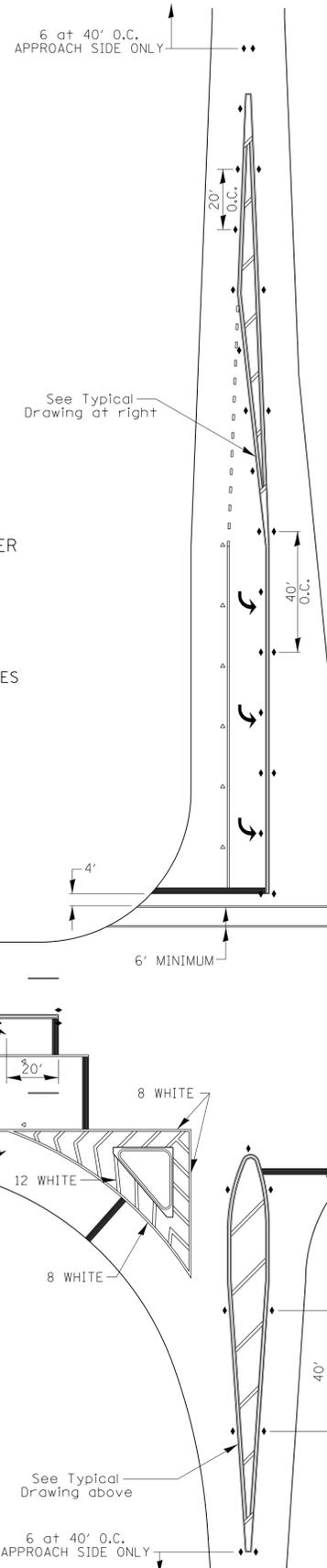
TYPICAL PAVEMENT MARKINGS

ARROW LAYOUT

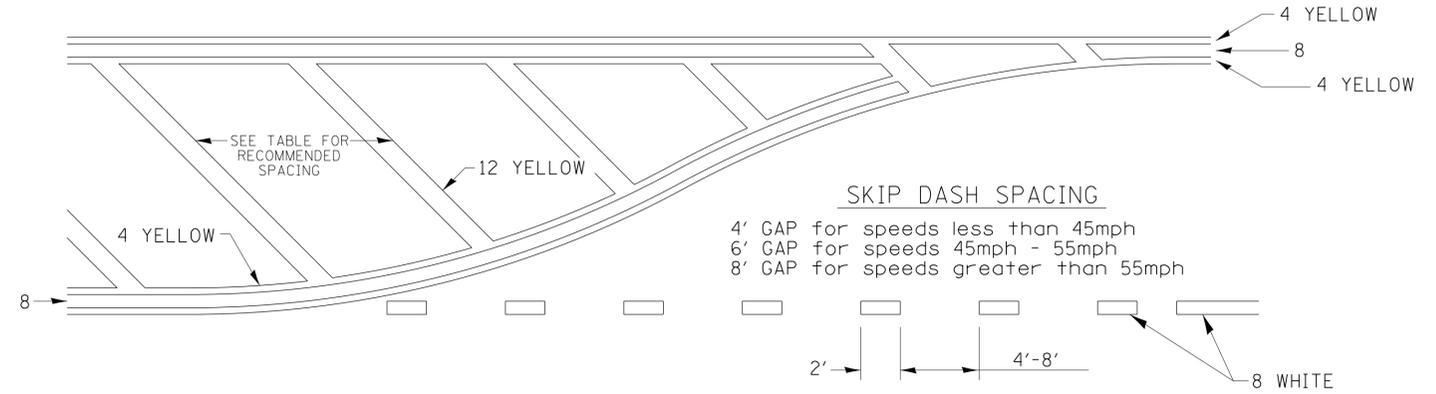


- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER
- ◆ TWO-WAY AMBER MARKER

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.



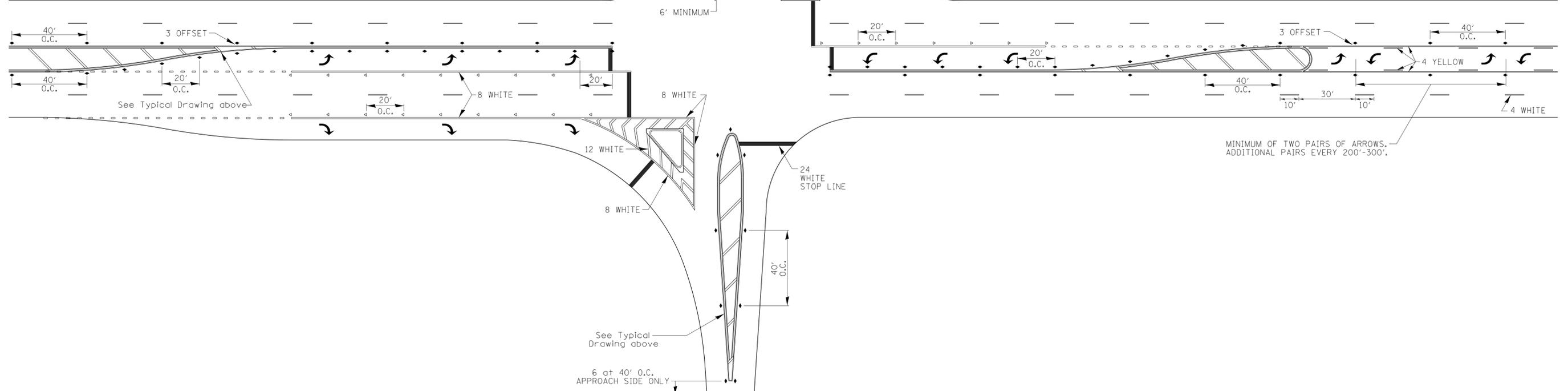
TYPICAL PAVEMENT MARKING FOR FLUSH MEDIAN



RECOMMENDED SPACING BETWEEN DIAGONALS (IN FEET)

| Speed Limit Range | Continuous Median Area | Intersection Channelization | Objects (Islands) |
|-------------------|------------------------|-----------------------------|-------------------|
| less than 30MPH | 50' | 15' | 10' |
| 30-40MPH | 75' | 20' | 15' |
| 45MPH & over | 75' | 30' | 20' |

NOTE: if the spacing recommended in the Table does not permit at least five diagonal lines in the area being marked, the spacing from the next lowest speed range should be used. The recommended spacing is measured parallel to the pavement center line.



| | | | | |
|------------------------------------|--------------------------------------|-----------------------|------------------------|---------|
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| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - | |
| | PLOT DATE = Mon Feb 10 13:44:27 2014 | DATE - | REVISED - | |

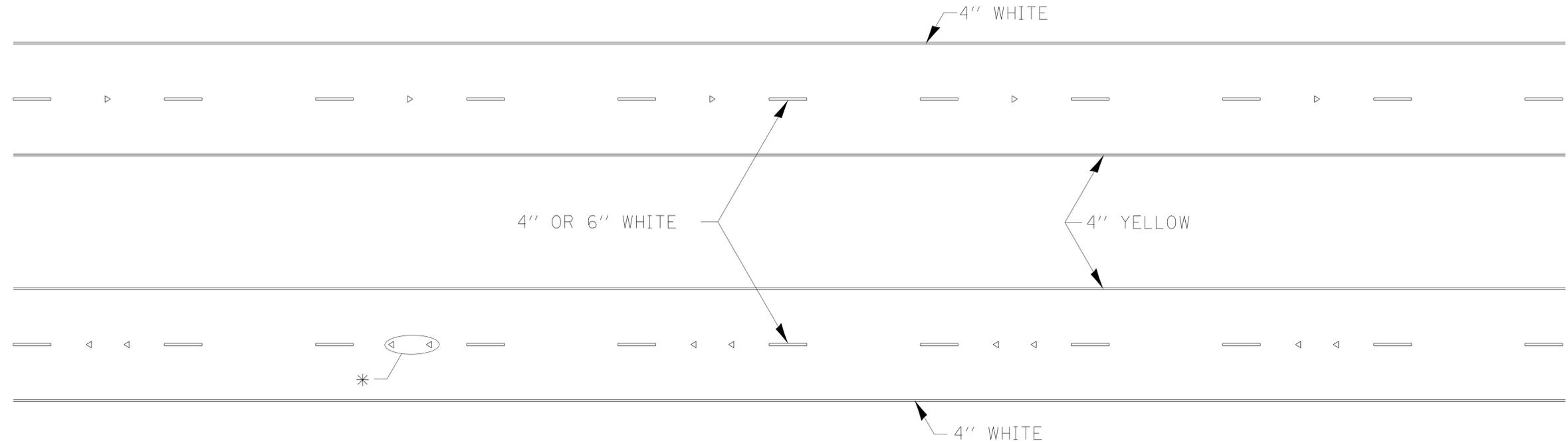
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

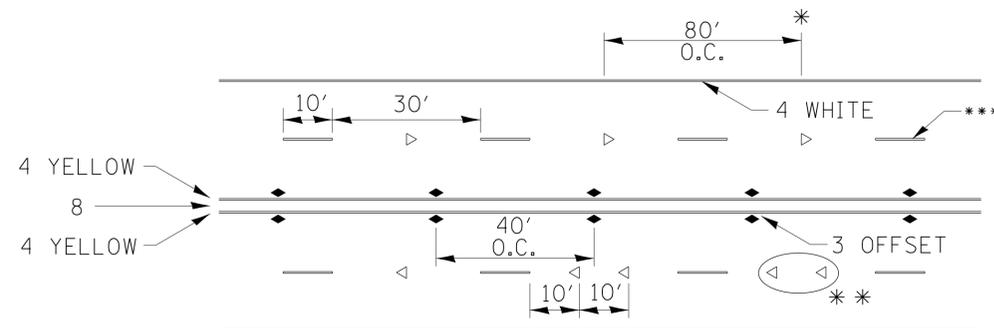
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|---------------------|---------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

TYPICAL PAVEMENT MARKINGS



* SEE HIGHWAY STANDARD 781001 FOR SPACING DETAILS.
USE DOUBLE MARKERS WHEN ADT \geq 20,000.

MULTI-LANE / DIVIDED



* REDUCE TO 40' O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH LOWER THAN POSTED SPEEDS.

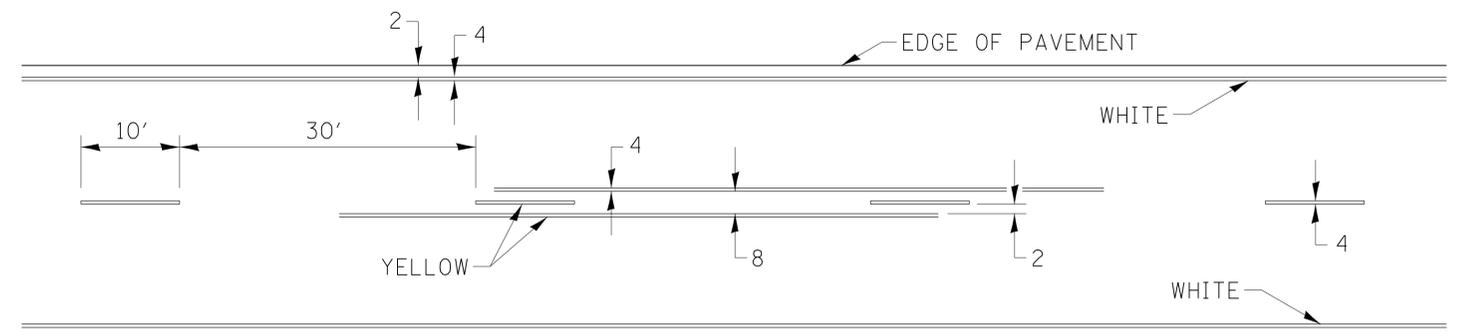
** USE DOUBLE MARKERS WHEN ADT \geq 20,000

*** CENTERLINE SKIP DASH PAVEMENT MARKING SPEED LIMIT LESS THAN 40 MPH USE 4" LINE. SPEED LIMIT 40 MPH AND OVER USE 6" LINE.

MULTI-LANE / UNDIVIDED & ONE WAY

(FOR MULTI-LANE UNDIVIDED HIGHWAYS USE THIS
DETAIL NOT HIGHWAY STANDARD 781001)

TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION – NO PASSING ZONES



SYMBOLS

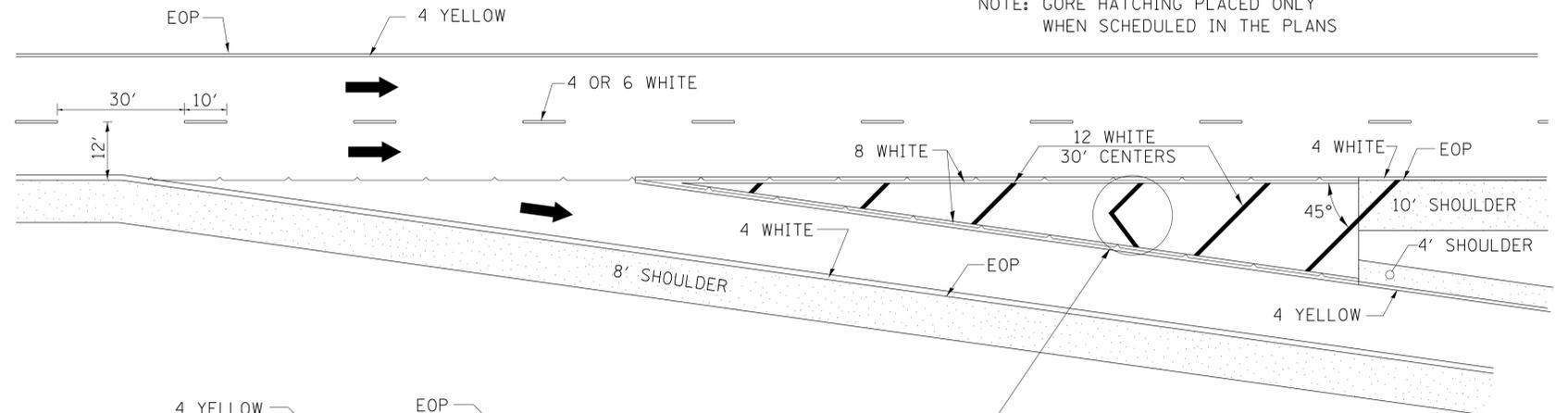
| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---------------------------------------|-----------|-----------|------|--------------|--------------|---------------------------|-----------------|--------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 11-28-12 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:28 2014 | DATE - | REVISED - | | | | | | | | ILLINOIS FED. AID PROJECT | | |

PAINING DETAILS

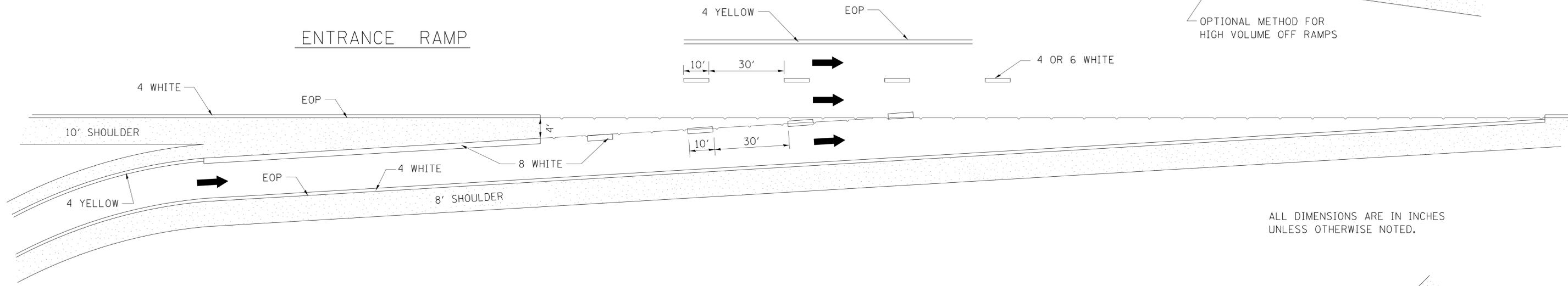
CENTERLINE SKIP DASH PAVEMENT MARKING WIDTH SHALL BE 4" WHEN THE POSTED SPEED LIMIT IS UNDER 40 MPH AND 6" WHEN THE POSTED SPEED LIMIT IS 40 MPH AND OVER.

EXIT RAMP

NOTE: GORE HATCHING PLACED ONLY WHEN SCHEDULED IN THE PLANS

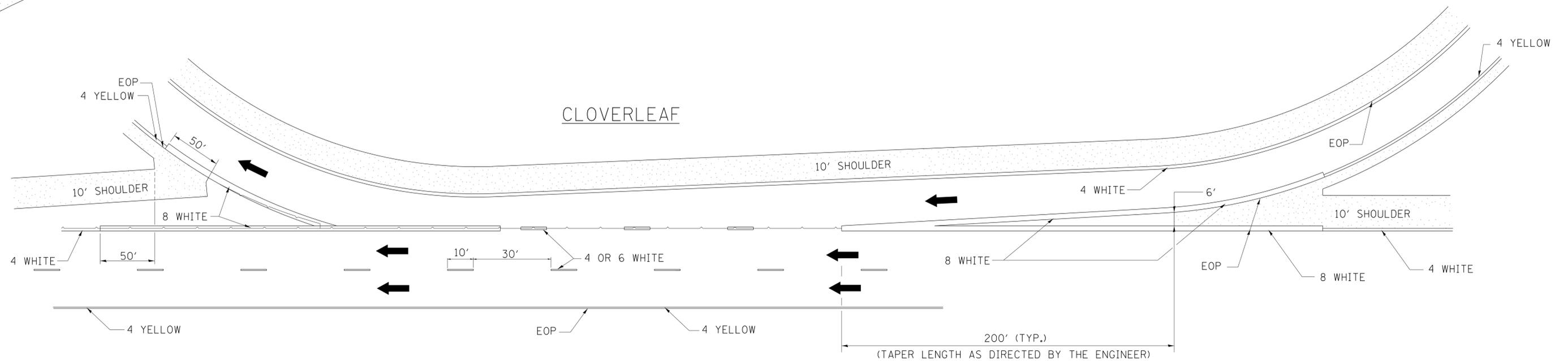


ENTRANCE RAMP



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

CLOVERLEAF



FILE NAME = District 2 Standard

USER NAME = IDOT/District 2
 PLOT SCALE = 1,0000' / in.
 PLOT DATE = Mon Feb 10 13:44:29 2014

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED - 8-27-13
 REVISED - 10-18-11
 REVISED -
 REVISED -

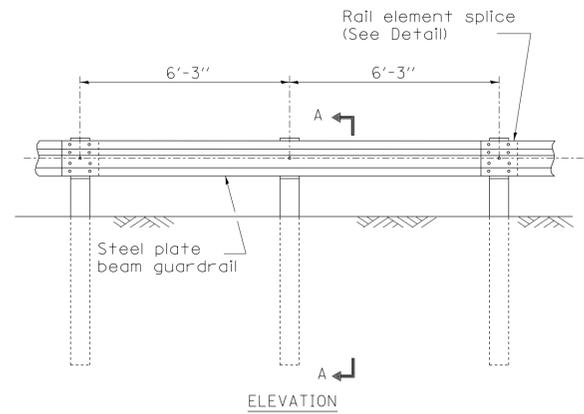
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

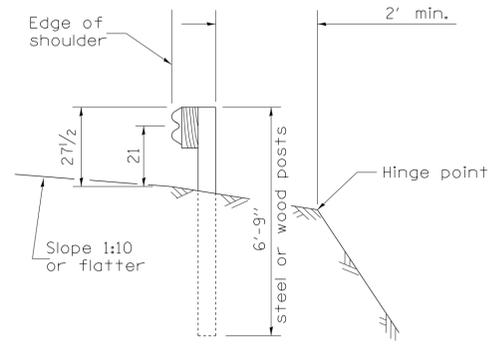
SCALE: SHEET NO. OF SHEETS STA. TO STA.

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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

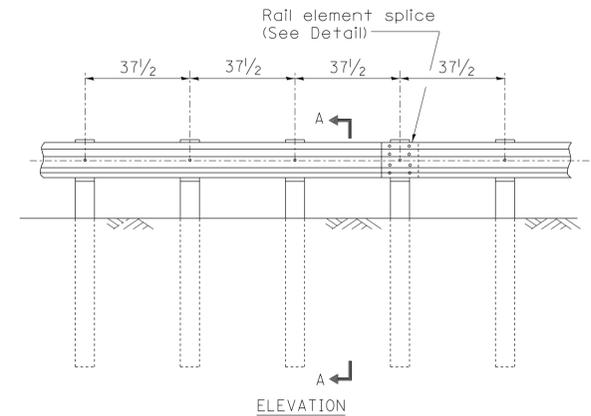
REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL



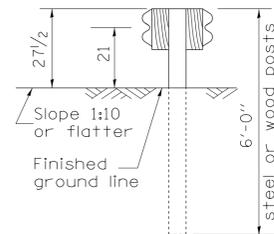
TYPE A
6'-3" Typical post spacing



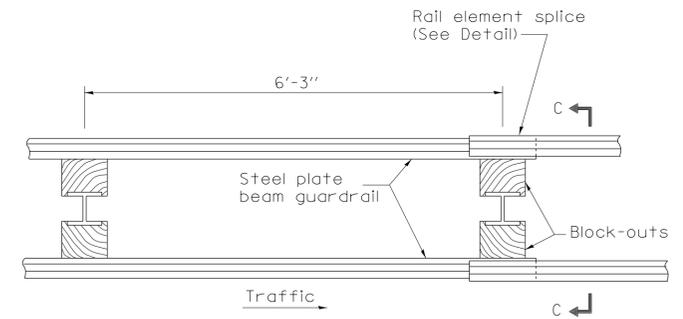
SECTION A-A



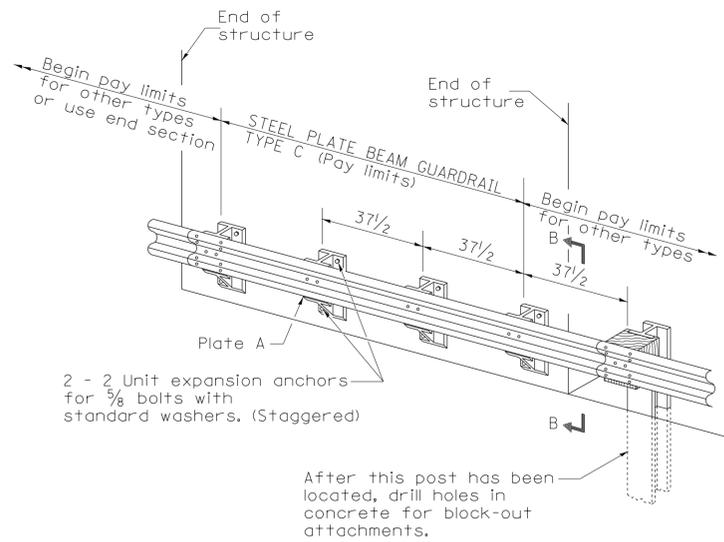
TYPE B
37 1/2" Closed post spacing



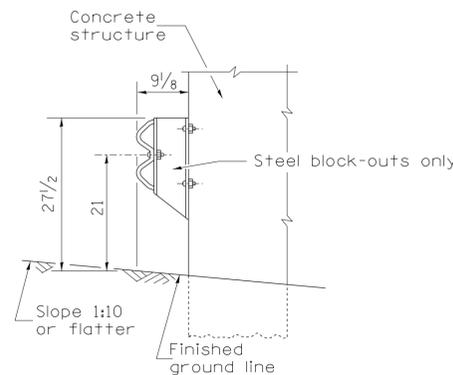
SECTION C-C



TYPE D
Double steel plate beam guardrail
6'-3" typical post spacing



TYPE C
37 1/2" Block-out spacing



SECTION B-B

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
All dimensions are in inches unless otherwise shown.
The existing steel posts may be drilled to match the bolt pattern shown herein for the wood block-out, or a new steel post shall be provided.
This detail is applicable to the guardrail system used prior to January 1, 2007. For details on the Midwest Guardrail System, see Standard 630001.

| | | | |
|------------------------------------|--------------------------------------|-----------------------|---------------------------------|
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| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:29 2014 | DATE - | REVISED - |

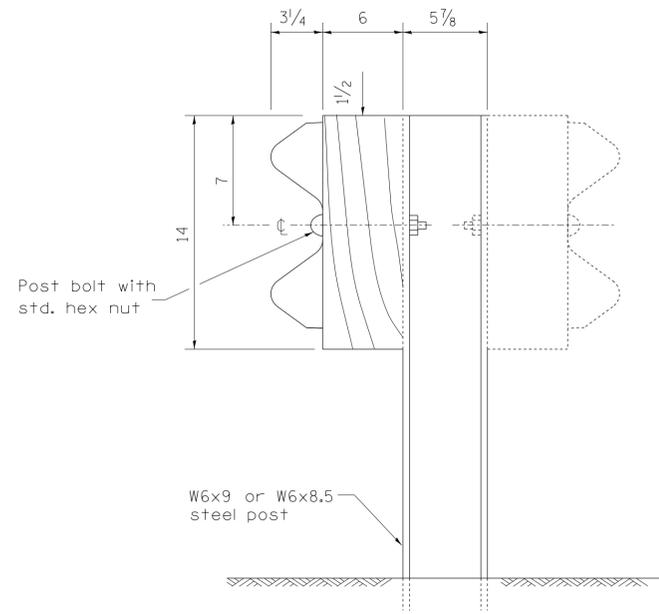
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REGION 2 / DISTRICT 2 STANDARD

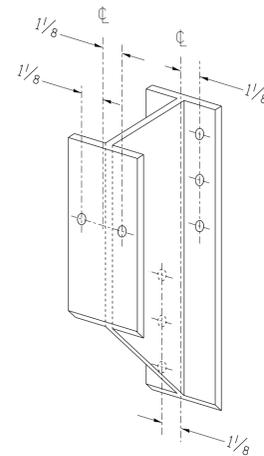
SCALE: SHEET NO. OF SHEETS STA. TO STA.

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|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

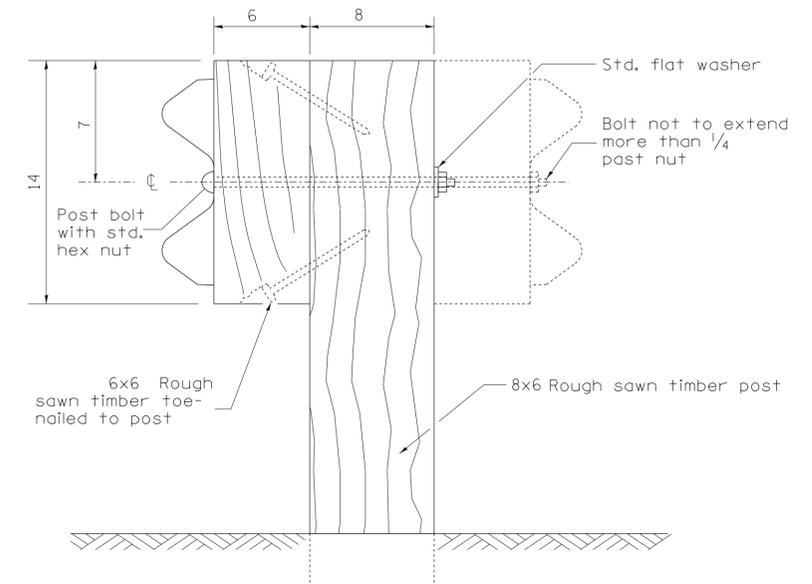
REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL



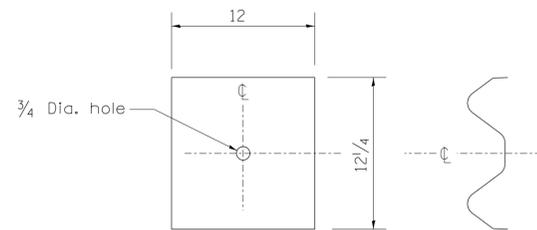
STEEL POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



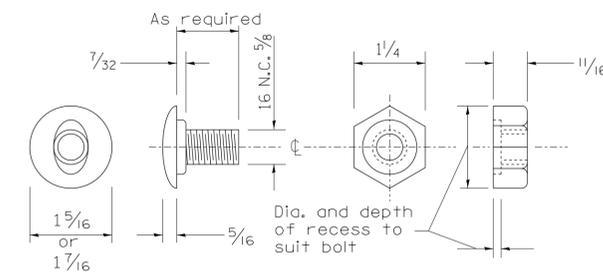
WOOD POST CONSTRUCTION



NOTE

Plate A shall be placed between rail element and block-out at non-splice mounting points only when steel block-outs are used.

PLATE A



POST OR SPLICE BOLT & NUT

| | | | |
|------------------------------------|--------------------------------------|------------|--------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 10-18-11 |
| | | DRAWN - | REVISED - |
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

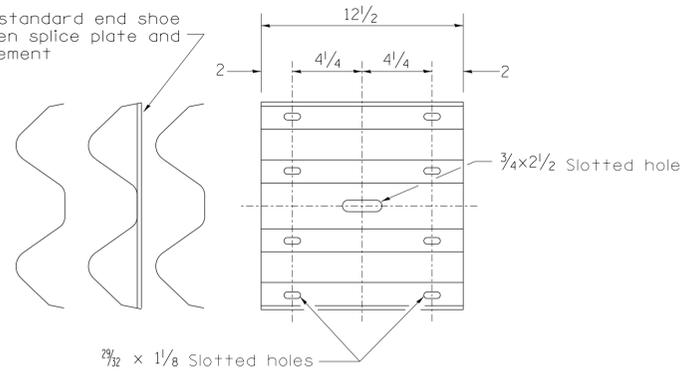
REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

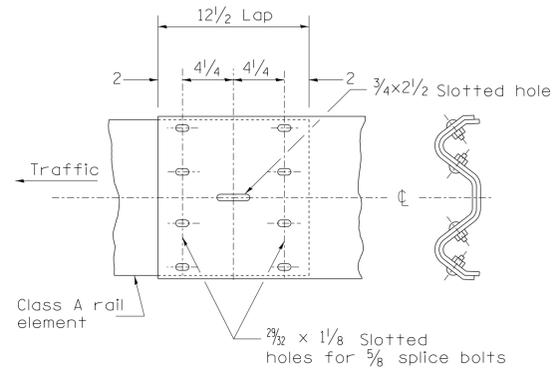
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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL

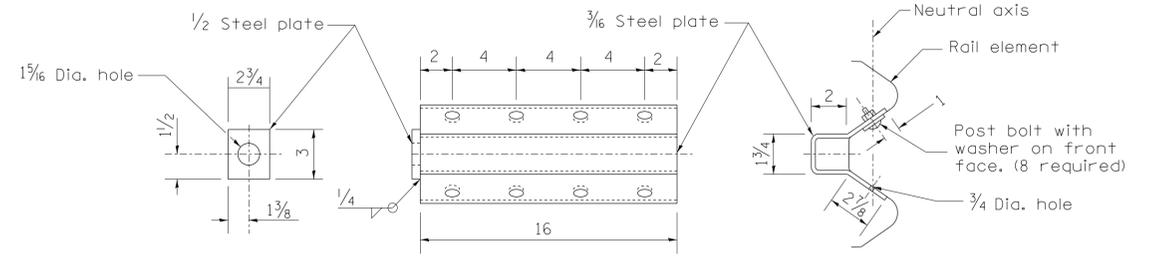
Place standard end shoe between splice plate and rail element



SPLICE PLATE



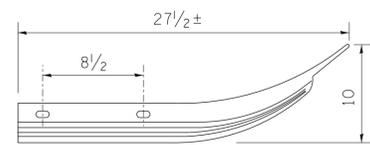
RAIL ELEMENT SPLICE



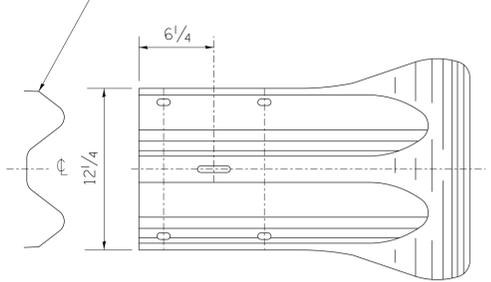
NOTE

Anchor plate T shall be used to attach cable assembly to guardrail when required on traffic barrier terminals.

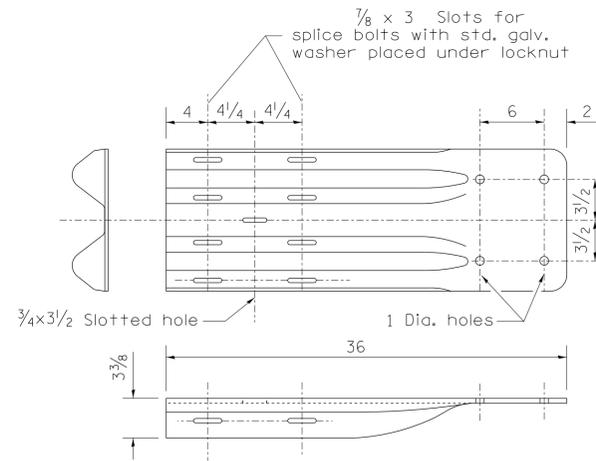
ANCHOR PLATE T DETAILS



Class A rail element



END SECTION



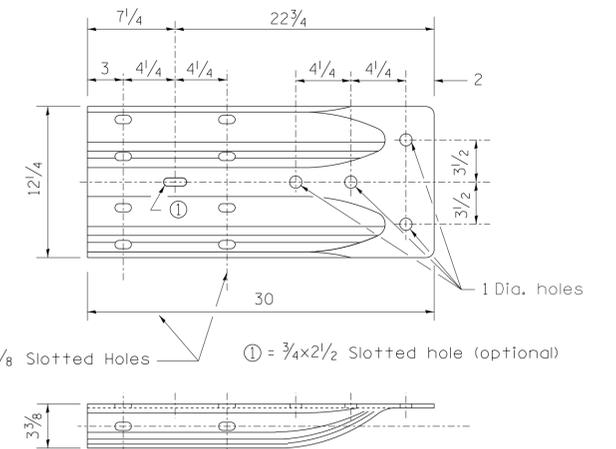
NOTE

When end shoe is attached to a bridge parapet which has an expansion joint, the bolts shall be provided with a locknut or double nut and shall be tightened only to a point that will allow guardrail movement.

The standard end shoe shall be attached to the concrete with pre-drilled or self-drilling anchor bolts. The anchor cone shall be set flush with the surface of the concrete.

Externally threaded studs protruding from the surface of the concrete will not be permitted.

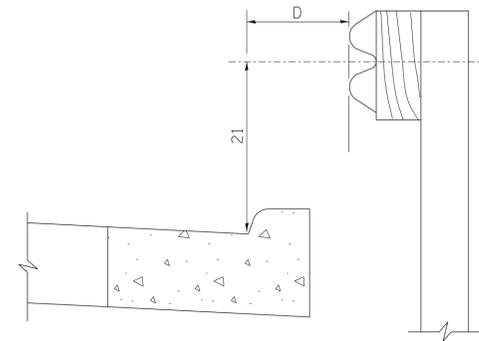
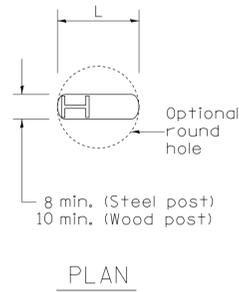
END SHOE



ALTERNATE END SHOE

| | | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---------------------------------|---|---------------------------------------|--|--|---|---------|--------|--------------|-----------|--|--|
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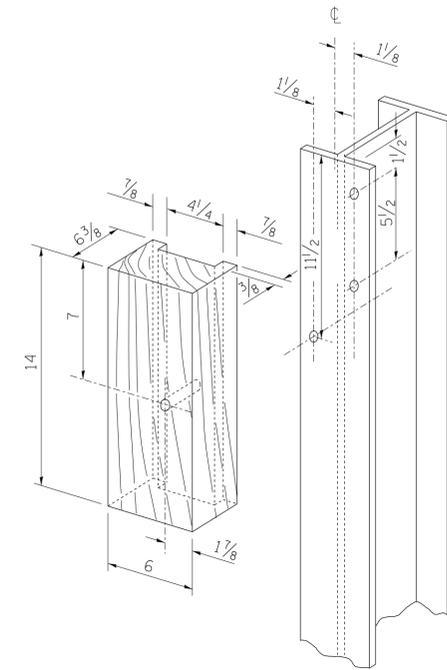
REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL



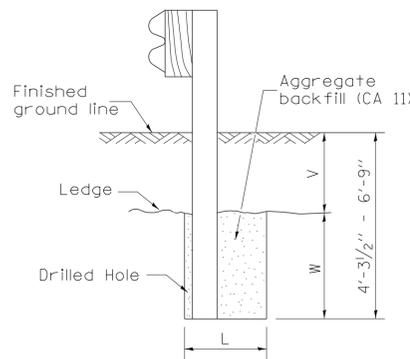
Note:
If it is necessary for D to be more than 12 and less than 10'-0" type M-2 curb and gutter (Std. 606001) shall be used in front of and in advance of the guardrail.

GUARDRAIL PLACED BEHIND CURB

(D = 0 desirable to 12 maximum)



WOOD BLOCK-OUT AND STEEL POST DETAILS

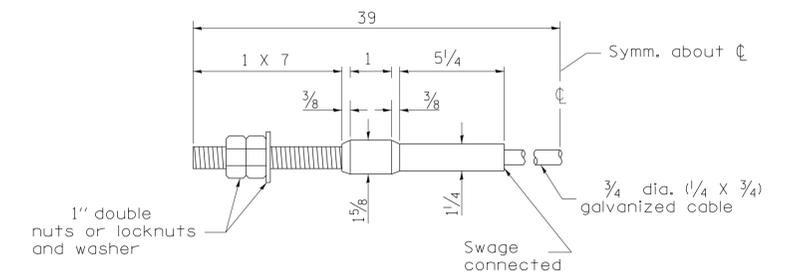


Note:
Ledge line is top of rock ledge or hard slag fill.

ELEVATION

FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED

| V | W | L | |
|--------------|--------|------------|-----------|
| | | Steel Post | Wood Post |
| 0 - 18 | 24 | 21 | 23 |
| >18 - 41.5 | 12 | 8 | 10 |
| >41.5 - 53.5 | 12 - 0 | 8 | 10 |



CABLE ASSEMBLY
(40,000 lbs. min. breaking strength)
Tighten to fault tension.

| | | | |
|------------------------------------|--------------------------------------|------------|--------------------|
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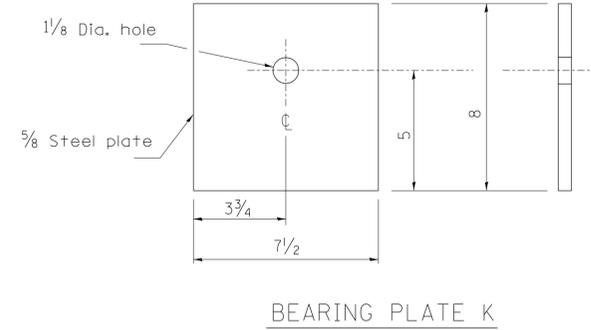
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

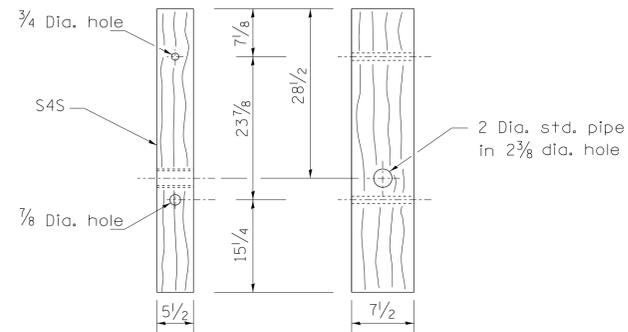
SCALE: SHEET NO. OF SHEETS STA. TO STA.

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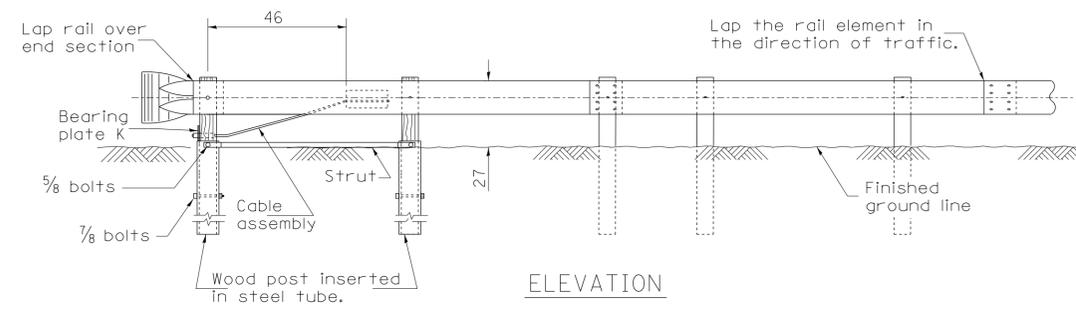
TRAFFIC BARRIER TERMINAL, TYPE 2 (27" HEIGHT)



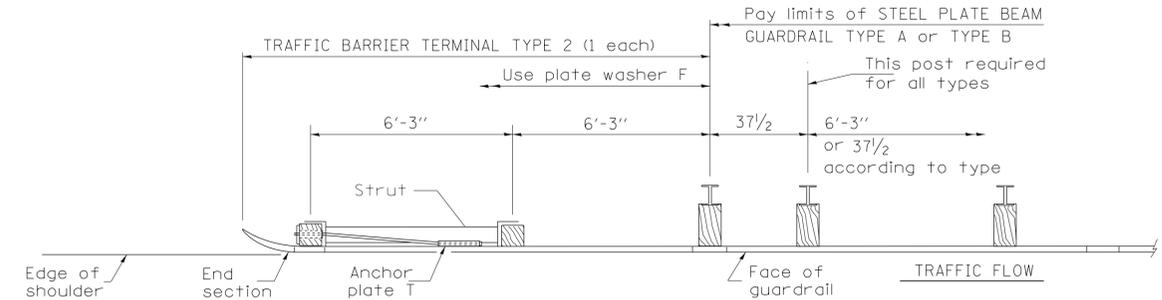
BEARING PLATE K



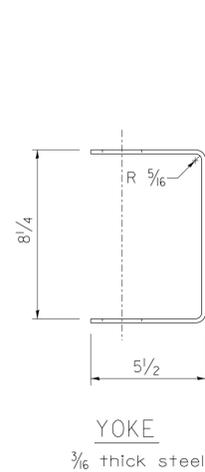
WOOD POST



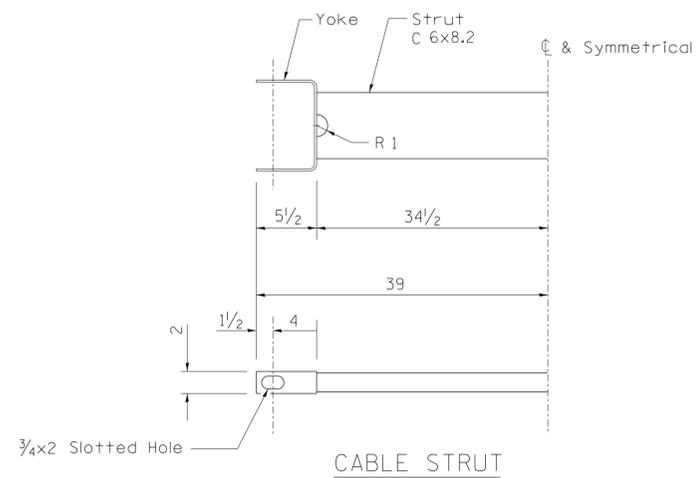
ELEVATION



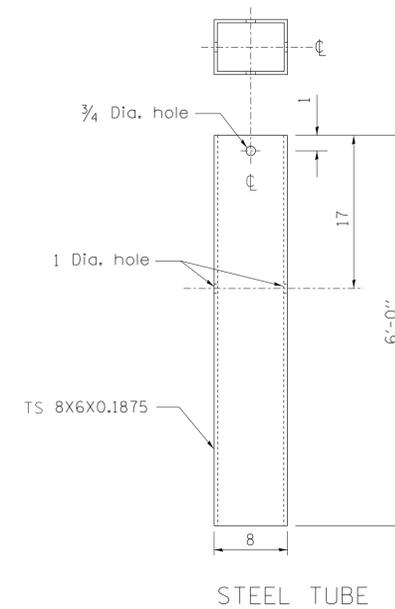
PLAN



YOKE



CABLE STRUT



STEEL TUBE

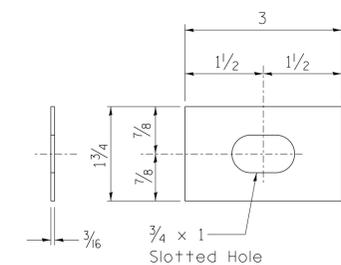


PLATE WASHER F

GENERAL NOTES

See Standard 630001 for details of guardrail not shown.

The bearing plate K shall be held in position by (2) two eight penny nails driven into the post and bent over the top of the plate.

All dimensions are in inches unless otherwise shown.

| | | | |
|------------------------------------|--------------------------------------|-----------------------|---------------------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 10-18-11 REVISED - |
| | PLOT SCALE = 1:0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:32 2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

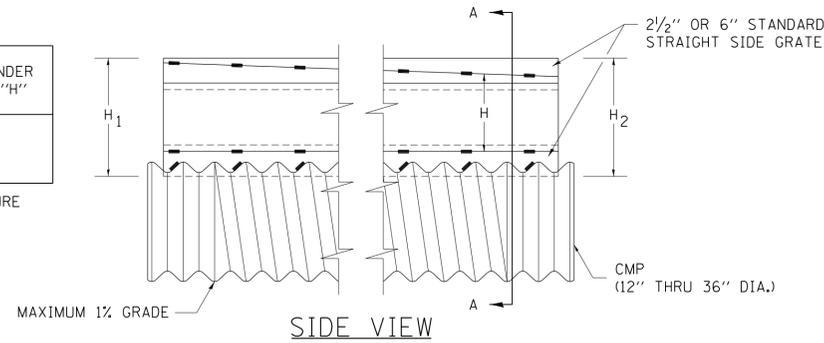
REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

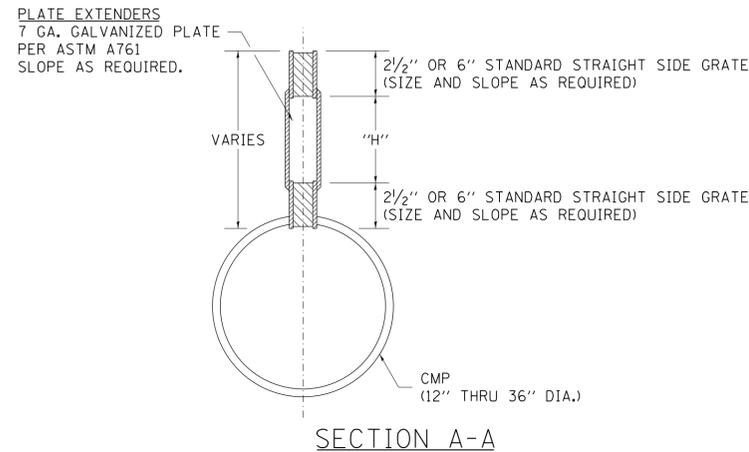
| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

SLOTTED DRAIN PIPE

| LOADING CONDITION | MAX. EXTENDER HEIGHT - "H" |
|--|----------------------------|
| H20/H25 • 750 PSI CONCRETE • 125 PSI TIRE PRESSURE | 19" |



SIDE VIEW
DETAIL WITH VARIABLE HEIGHT GRATE



SECTION A-A

GENERAL

Class SI Concrete shall be used throughout. This specification covers Slotted Drain used for the removal of water as shown on the plans. The Slotted Drain shall be Corrugated Pipe Culvert with Integral Slotted Drains. Before placing the concrete adjacent to the pipe, the slot shall be covered by either thin, flat metal sheeting or by a board notched to fit over the grate bars. This covering must fit closely in the slot to prevent entry of concrete into the pipe. Paving over the slotted drain will then be one continuous operation over the protected drain. The protection for the drain slot shall then be removed. The pipe shall drain into the side of the inlet. The opening where the slot is removed shall be covered to prevent concrete from entering the pipe. The Corrugated Steel Pipe used in the Slotted Drain shall meet the requirements of AASHTO M36/ASTM A760. The CMP shall be ALUMINIZED STEEL Type 2. The diameter shall be as shown on the plans. Steel grating shall meet the galvanizing requirements of AASHTO M111. This work will be paid for at the contract unit price per foot for SLOTTED DRAIN of the pipe diameter specified WITH VARIABLE SLOT, or SLOTTED DRAIN, of the pipe diameter specified, WITH 6" SLOT, and shall include concrete and grating for depth specified on plans. Use approved end cap to prevent concrete entry into the pipe during gutter construction on the upstream end of the pipe.

CONNECTIONS

The Corrugated Steel Pipe shall have a minimum of two rerolled annular ends. The Slotted Drain bands shall be modified HUGGER Bands to secure the pipe and prevent infiltration of the backfill. When the Slotted Drain is banded together, the adjacent grates shall have a maximum 3" gap.

GRATES

The grates shall be manufactured from ASTM A670, Grade 36 steel. The spacers and bearing bars (sides) shall be 3/16" material ±0.008". The spacers shall be on 6" centers and welded on both sides to each bearing bar (sides) with four (4) 1-1/4" long 3/16" fillet welds on each side of the bearing bar. The plate extender shall be 7 gage steel meeting ASTM A761. The engineer may call for tensile strength tests on the grate if the grate is not in compliance with the above spacer specifications. If tensile strength tests are called for, minimum results for an in-place spacer pulled perpendicular to the bearing bar shall be:
T = 12,000 pounds for 2-1/2" grate
T = 15,000 pounds for 6" grate

GALVANIZING

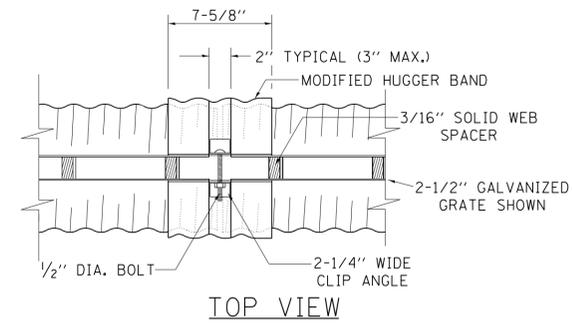
The grate and plate extenders shall be galvanized in accordance with ASTM A123 except with a 2 oz. galvanized coating.

GRATE ATTACHED TO CSP

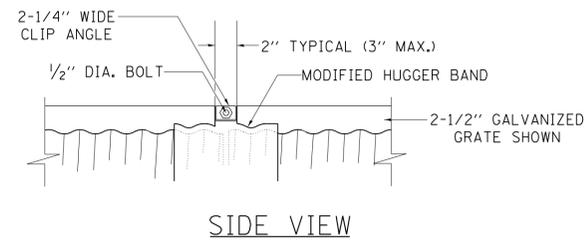
The grate shall be fillet welded with a minimum weld 1" long to the CSP on each side of the grate at every other corrugation.

TOLERANCES - FINISHED SLOTTED DRAIN - 20' LENGTHS

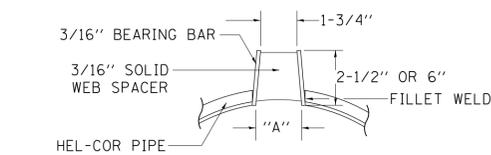
Vertical Bow = ± 3/8"
Horizontal Bow = ± 5/8"
Twist = ± 1/2"



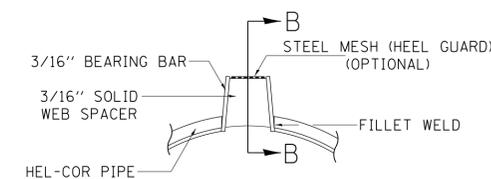
TOP VIEW



SIDE VIEW



SECTION A-A
STANDARD DETAIL



SECTION A-A
DETAIL WITH MESH
(TRAPEZOIDAL GALVANIZED GRATE SHOWN)

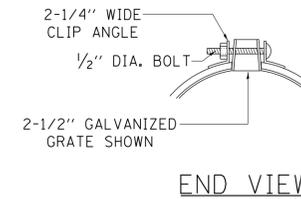
| GAGE OF PIPE | STANDARD SIZES | | | | | |
|--------------|------------------|------|------|------|-----|-----|
| | DIAMETER OF PIPE | | | | | |
| | 12" | 15" | 18" | 24" | 30" | 36" |
| 16 | X | X | X | X | X | X |
| 14 | X | X | X | X | X | X |
| 12 | N.A. | N.A. | N.A. | N.A. | X | X |

| GRATE TYPE | "A" | |
|------------|--------|--------|
| VERT | 2-1/2" | 1-3/4" |
| VERT TRAP | 6" | 1-3/4" |
| TRAP | 2-1/2" | 2-1/4" |
| TRAP | 6" | 3" |

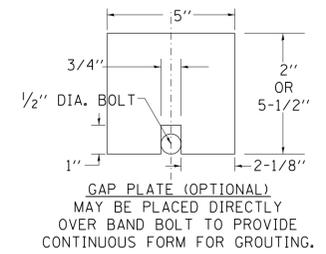
VERT = VERTICAL
TRAP = TRAPEZOIDAL

SLOTTED DRAIN NOTES

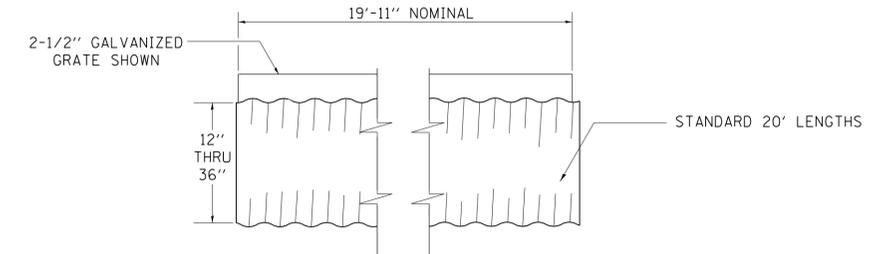
- GRATING IS AVAILABLE IN DEPTHS OF 2-1/2" AND 6".
- VERTICAL GRATING (STRAIGHT SIDES) WITH VERTICAL SPACERS IS ALSO AVAILABLE.
- FOR 6" VERTICAL & TRAPEZOIDAL REQUIREMENTS, THE SLOTTED DRAIN BAND MAY BE FURNISHED WITH THE 4" TECHCO BAND ANGLE.
- DIMENSIONS ARE SUBJECT TO MANUFACTURING TOLERANCES.
- DIMENSIONS FOR H₁ AND H₂ AS REQUIRED.
- H₁ AND H₂ MEASURED FROM TOP OF GRATE TO BOTTOM OF GRATE.



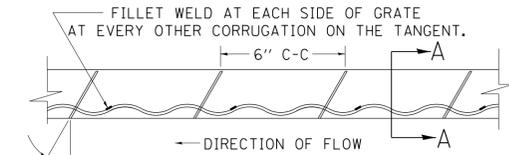
END VIEW



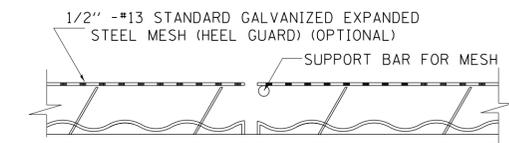
GAP PLATE (OPTIONAL) MAY BE PLACED DIRECTLY OVER BAND BOLT TO PROVIDE CONTINUOUS FORM FOR GROUTING.



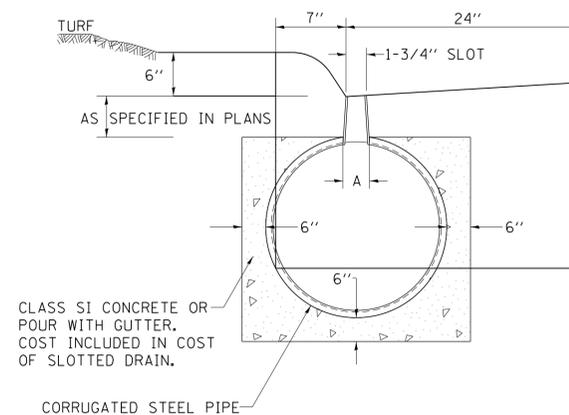
TYPICAL PIPE SECTION



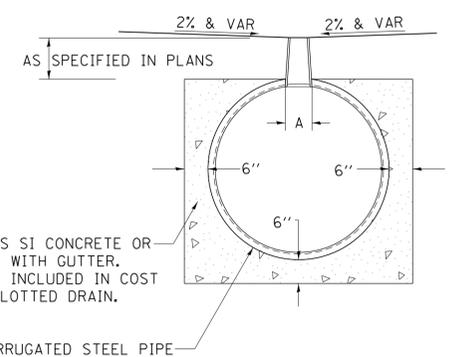
GRATE WELDING DETAIL



SECTION B-B



DETAIL FOR CURB & GUTTER



DETAIL FOR CROSSOVERS,
DRIVEWAYS, OR PARKING LOTS

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

| | | | |
|---------------------------------|--------------------------------------|--------------------|--------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 10-18-11 |
| | PLOT SCALE = 1:10000 ' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:32 2014 | DATE - | REVISED - |

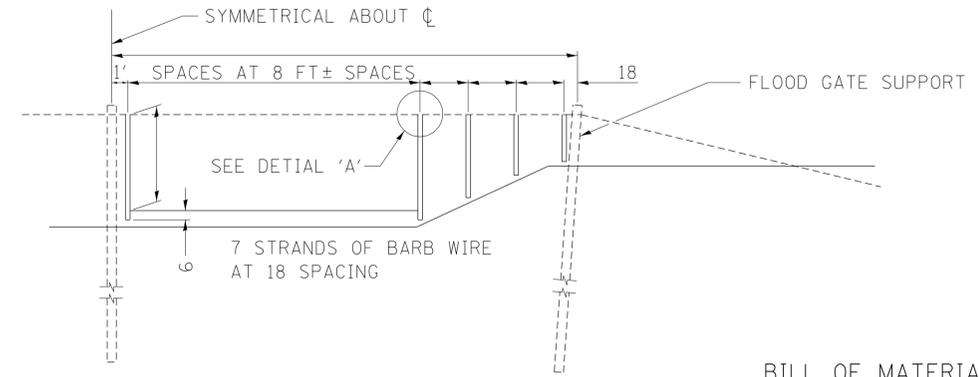
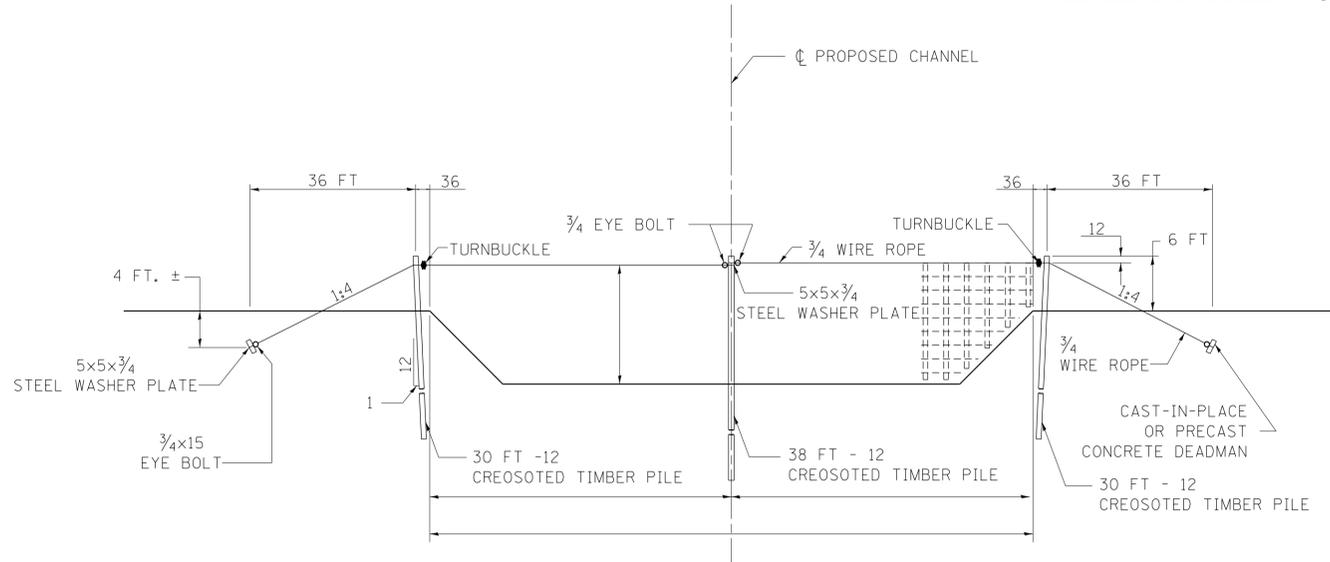
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------|-----------|
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

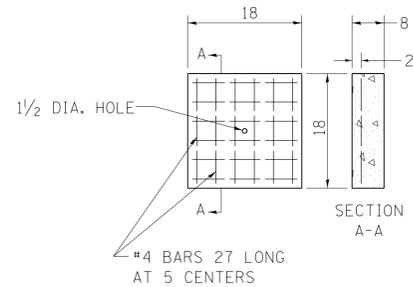
DETAIL OF FLOOD GATE



BILL OF MATERIALS

| QUANTITY | UNIT | ITEM |
|----------|------|---|
| | FT | 2 x 4 TREATED LUMBER |
| | FT | GALVANIZED BARBED WIRE |
| | FT | SPECIAL STEEL CHANNEL SC 25, 2 1/2 x 2 1/2 |
| | EACH | 3/8 x 3 GALVANIZED STEEL BOLTS AND WASHERS |
| | EACH | 3/8 x 2 GALVANIZED STEEL U-BOLTS |
| | EACH | GALVANIZED CABLE CLAMPS |
| | LBS | GALVANIZED FENCE STAPLES |

NOTE:
THIS WORK WILL BE PAID FOR AT THE
CONTRACT UNIT PRICE PER EACH FOR

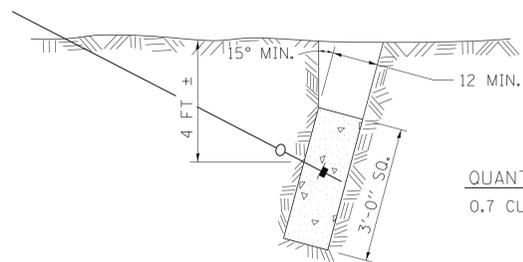


QUANTITIES FOR 2 DEADMEN
0.3 CU.YD CLASS S1 CONCRETE
36 LBS REINFORCEMENT BARS

DETAIL OF PRECAST CONCRETE DEADMAN

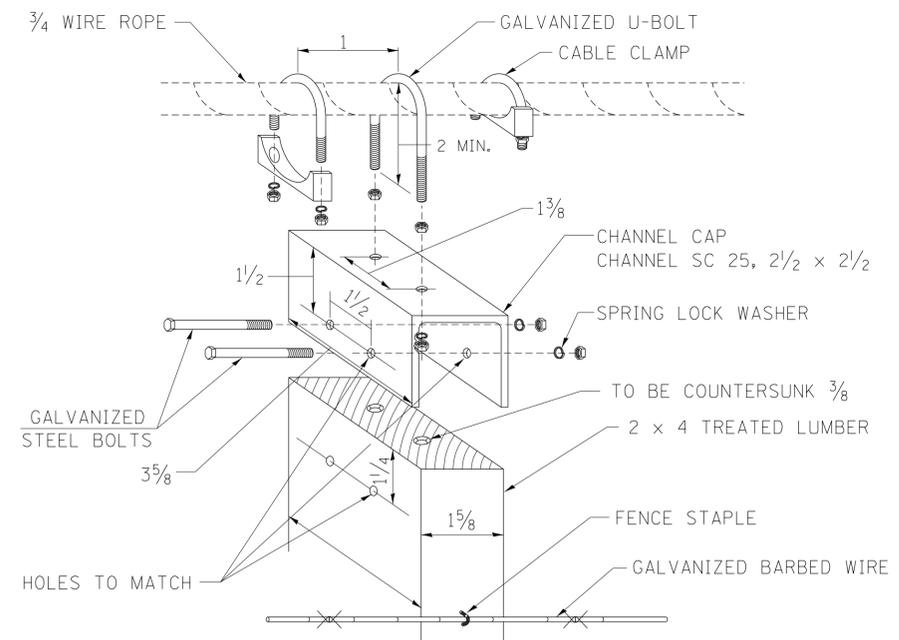
BILL OF MATERIALS

| QUAN. | UNIT | ITEM |
|-------|------|---|
| FT | | 3/4 DIA. GALVANIZED WIRE ROPE |
| EACH | | 3/4 DIA. x 15 GALVANIZED EYE BOLTS WITH NUTS AND WASHERS |
| EACH | | 3/4 DIA. GALVANIZED TURNBUCKLES |
| EACH | | 3/4 DIA. GALVANIZED CABLE CLAMPS |
| EACH | | 5x5x3/4 STEEL WASHER PLATES |
| EACH | | PRECAST CONCRETE DEADMEN OR CAST-IN-PLACE CONCRETE DEADMEN |
| FT | | 12 CREOSOTED TIMBER PILE (INCLUDES FURNISHING AND DRIVING TIMBER PILE) |



QUANTITIES FOR 2 DEADMEN
0.7 CU. YD. CLASS S1 CONCRETE

DETAIL OF CAST-IN-PLACE CONCRETE DEADMAN



DETAIL 'A'

EXPLODED VIEW OF FLOOD GATE TO CABLE

NOTE:
PRECAST CONCRETE DEADMEN AND CAST-IN-PLACE CONCRETE
DEADMEN SHALL BE CONSTRUCTED OF CLASS S1 CONCRETE

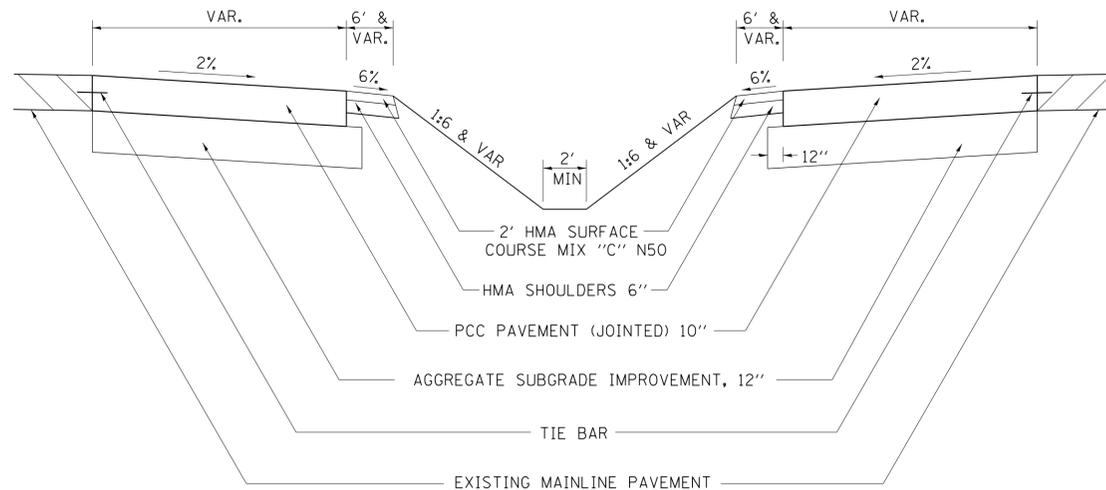
ALL DIMENSIONS ARE IN INCHES UNLESS
OTHERWISE NOTED.

| | | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|--------------------------------|---|---------------------------------------|--|--|--------------|-----------|--------|-----------------|--------------|---------|---------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-09-12 REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - | | | | | CONTRACT NO. | | | | | | |
| | PLOT DATE = Mon Feb 10 13:44:33 2014 | DATE - | REVISED - | | | | | SCALE: | SHEET NO. | OF | SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. |

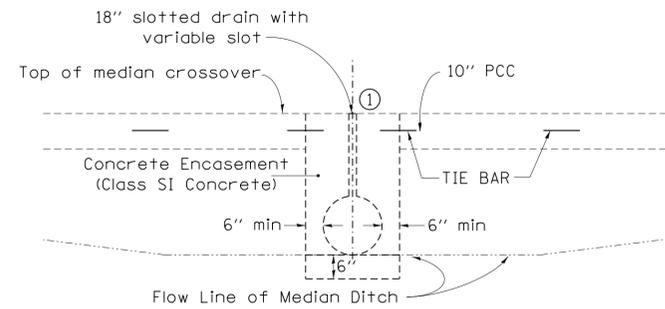
40' SINGLE LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 55 MPH, WORK ZONE SPEED LIMIT 45 MPH)

TYPICAL SECTION

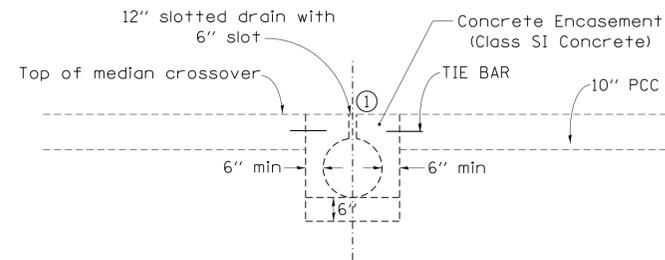


| TABLE OF OFFSETS AND DROPS | | | | | | |
|---|------|------|--------|--------|-------|---------|
| Distance feet from location station | 0 | 60' | 75' | 100' | 125' | 143.73' |
| | | ⓑ | | | | ⓒ |
| Offsets feet from inside edge of pavement | 20' | 18' | 15.32' | 11.37' | 8.06' | 6.00' |
| | ⓓ | | | | | |
| Drop feet from inside edge of pavement | 0.0' | 0.0' | 0.0' | 0.0' | 0.0' | 0.0' |



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (1340.88 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (1250.80 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (45.1 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (402.52 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

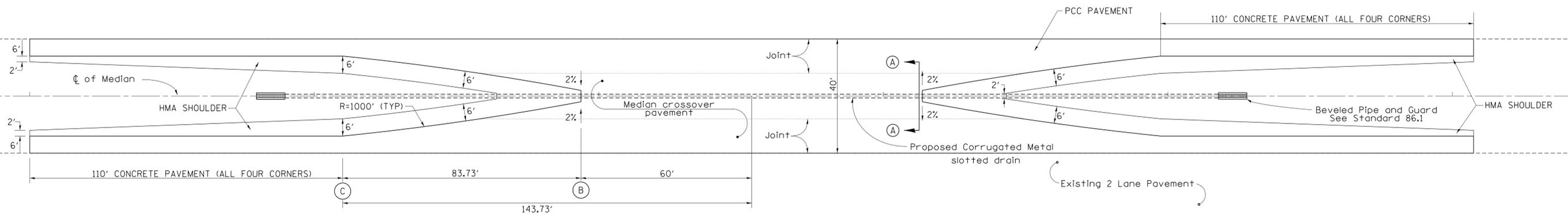
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 45mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

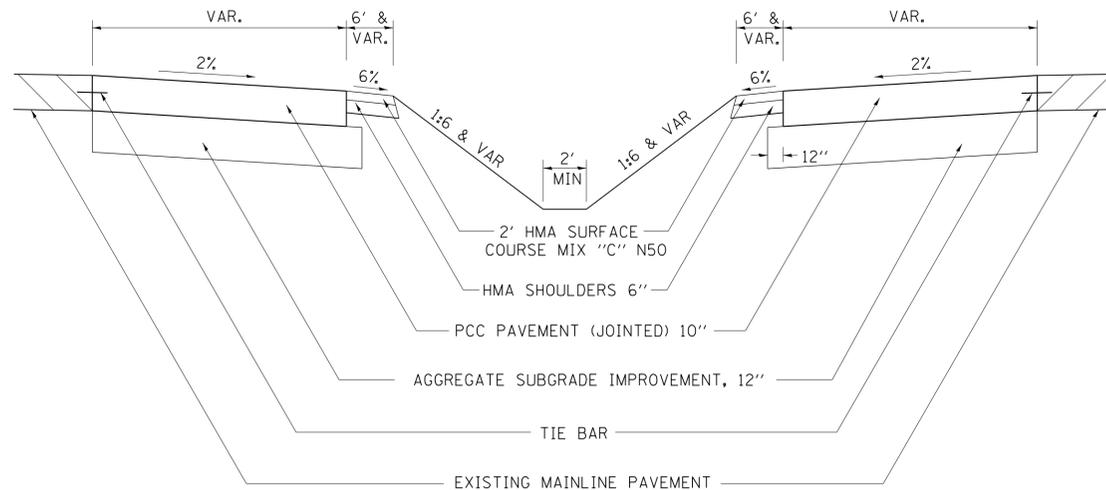
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---|-----------|-----------|------|-----------|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 12-07-10 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:34 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |

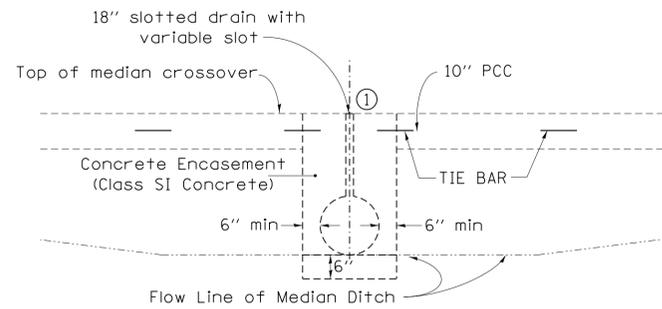
50' SINGLE LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 55 MPH, WORK ZONE SPEED LIMIT 45 MPH)

TYPICAL SECTION

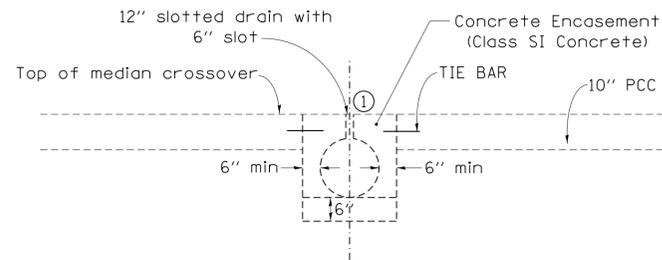


| TABLE OF OFFSETS AND DROPS | | | | | | | |
|---|------|-------|--------|--------|--------|---------|---------|
| Distance feet from location station | 0' | 53' | 75' | 100' | 125' | 150.00' | 161.73' |
| | | ⓑ | | | | | ⓒ |
| Offsets feet from inside edge of pavement | 25' | 23' | 18.57' | 14.14' | 10.37' | 7.25' | 6.00' |
| | ⓓ | | | | | | |
| Drop feet from inside edge of pavement | 0.5' | 0.46' | 0.37' | 0.28' | 0.021' | 0.15' | 0.12' |



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (1634.94 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (1533.52 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (52.9 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (472.79 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

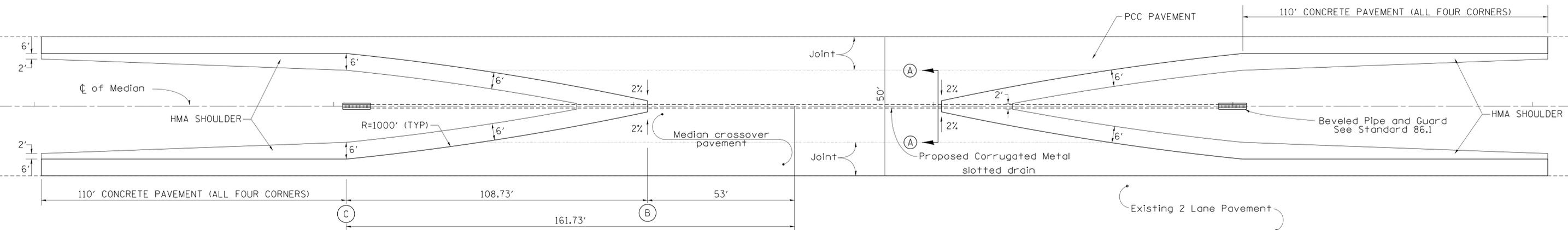
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 45mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---------------------------------------|-----------|---------------------------|------|-----------|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 12-07-10 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:34 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | | | | |

64' SINGLE LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 55 MPH, WORK ZONE SPEED LIMIT 45 MPH)

TYPICAL SECTION

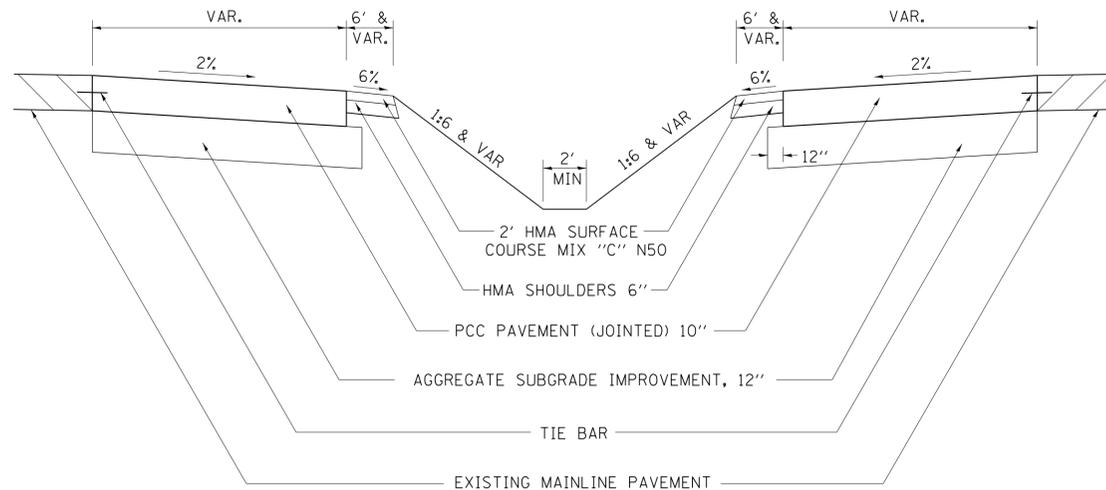
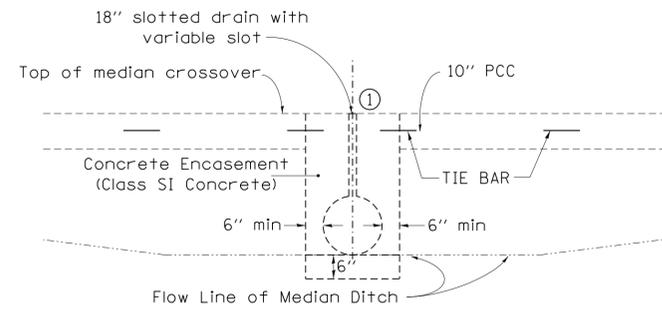


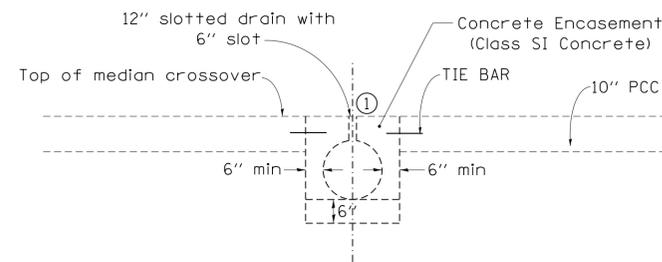
TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0 | 46' | 50' | 75' | 100' | 125' | 150' | 175' | 185.20' |
|---|-------|------|--------|--------|--------|--------|--------|-------|---------|
| Offsets feet from inside edge of pavement | 32' | 30' | 29.02' | 23.32' | 18.28' | 13.90' | 10.17' | 7.08' | 6' |
| Drop feet from inside edge of pavement | 0.64' | 0.6' | 0.58' | 0.47' | 0.37' | 0.28' | 0.20' | 0.14' | 0.12' |



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2071.96 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (1956.64 Sq. Yds.) | P. C. C. PAVEMENT, (JOINTED) 10" |
| (62.5 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (558.24 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

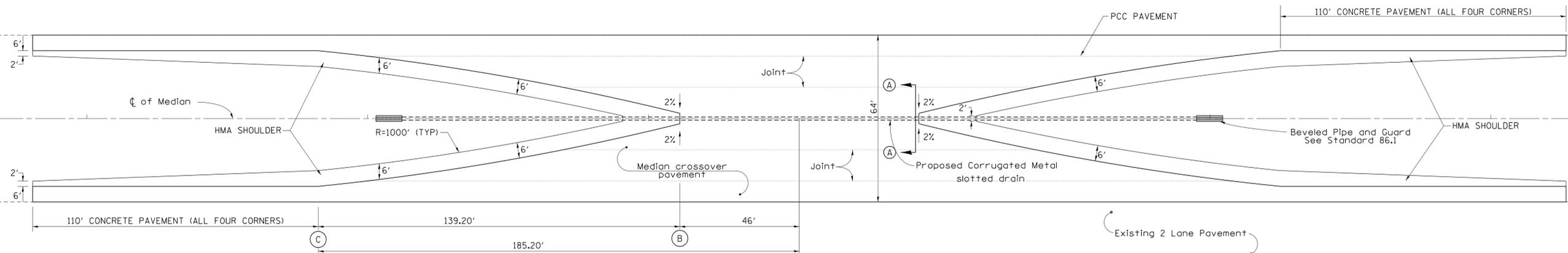
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 45mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

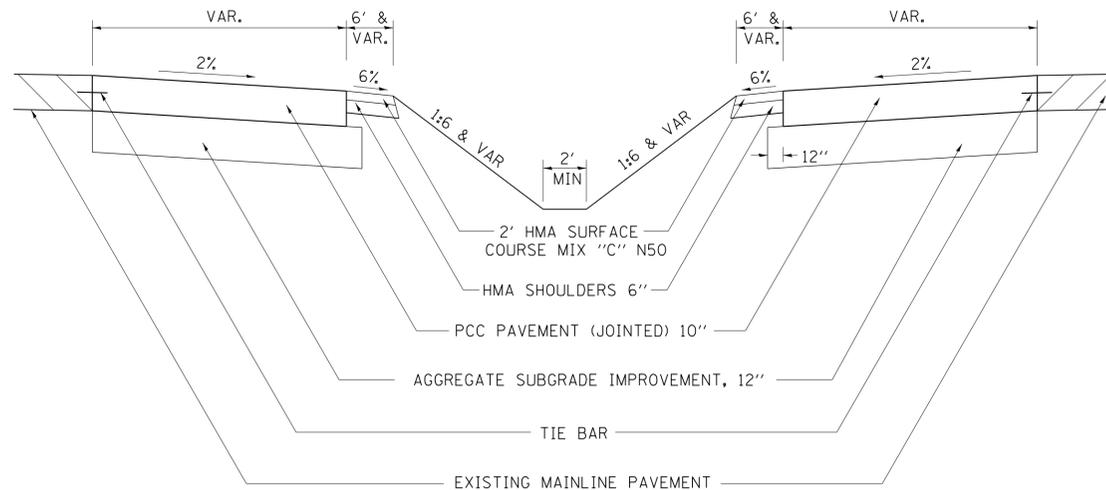
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---|-----------|-----------|------|-----------|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 12-07-10 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:35 2014 | DATE - | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |

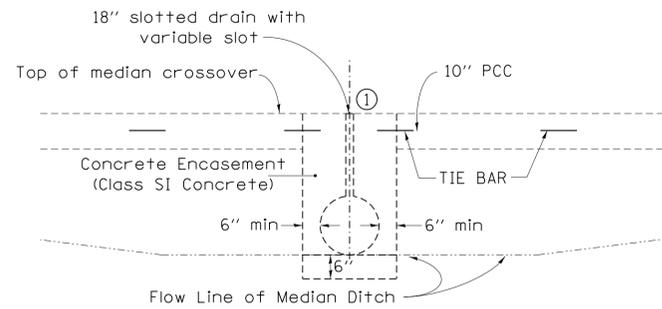
40' SINGLE LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

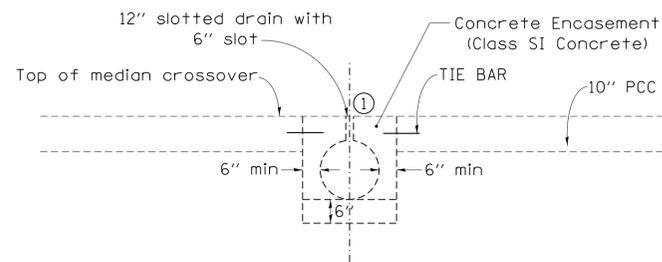
TYPICAL SECTION



| TABLE OF OFFSETS AND DROPS | | | | | | | |
|---|------|-------|--------|--------|-------|-------|--------|
| Distance feet from location station | 0 | 77' | 100' | 125' | 150' | 175' | 183.23 |
| | | ⓑ | | | | | ⓒ |
| Offsets feet from inside edge of pavement | 20' | 18' | 14.79' | 11.69' | 8.98' | 6.67' | 6' |
| | ⓓ | | | | | | |
| Drop feet from inside edge of pavement | 0.4' | 0.36' | 0.3' | 0.23' | 0.18' | 0.13' | 0.12' |



SECTION A-A
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A
(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (1709.35 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (1596.03 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (57.1 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (509.64 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

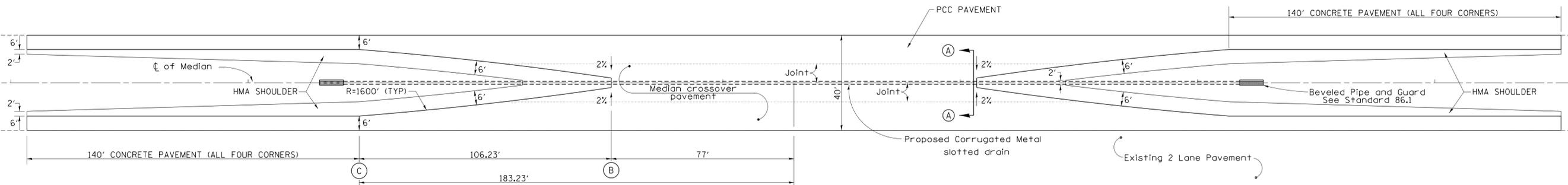
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

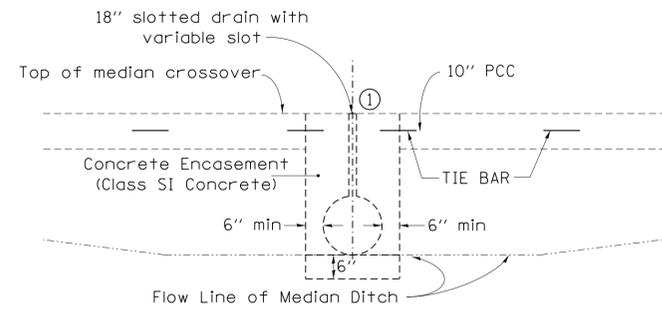
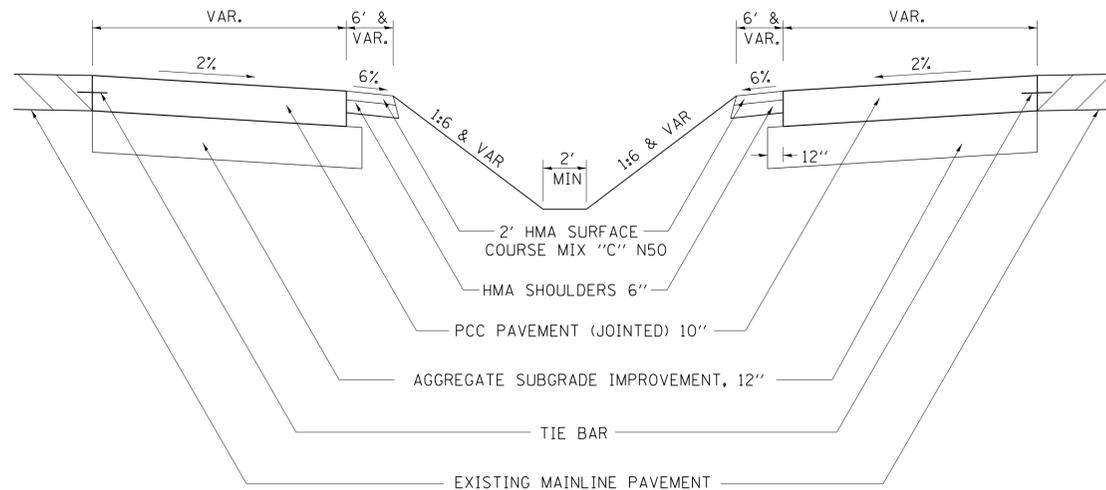
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|--------------------------------------|-----------------------------|------------|--------------------|---|--------------------------------|-----------|-----------|------|-----------|---------------------|---------------------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | DRAWN - | REVISED - 12-07-10 | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | CONTRACT NO. | |
| PLOT DATE = Mon Feb 10 13:44:36 2014 | DATE - | CHECKED - | REVISED - | | | | | | | | | | |

50' SINGLE LANE MEDIAN CROSSOVER

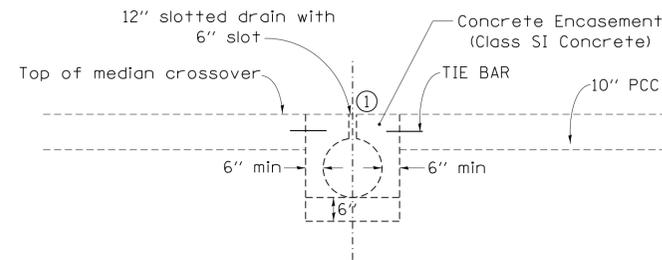
(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2084.0 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (1956.55 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (67.1 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (598.67 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

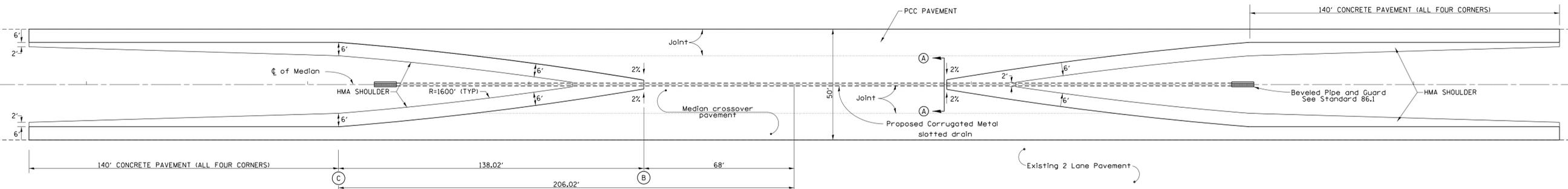
The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0' | 68' | 75' | 100' | 125' | 150' | 175' | 200' | 206.02' |
|---|------|-------|--------|--------|--------|--------|--------|-------|---------|
| | | ⓑ | | | | | | | ⓒ |
| Offsets feet from inside edge of pavement | 25' | 23' | 21.84' | 17.97' | 14.50' | 11.43' | 8.76' | 6.49' | 6' |
| Drop feet from inside edge of pavement | 0.5' | 0.46' | 0.44' | 0.36' | 0.29' | 0.23' | 0.175' | 0.13' | 0.12' |



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

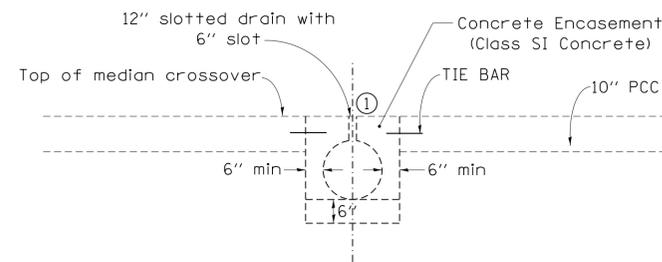
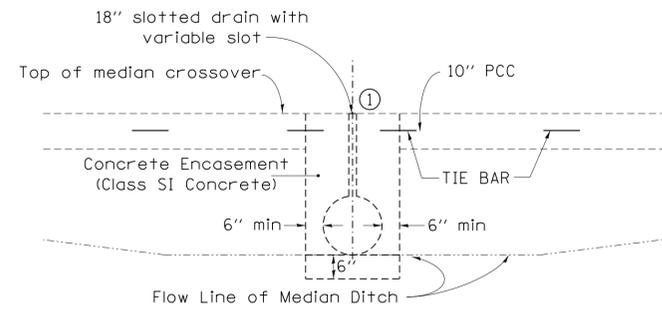
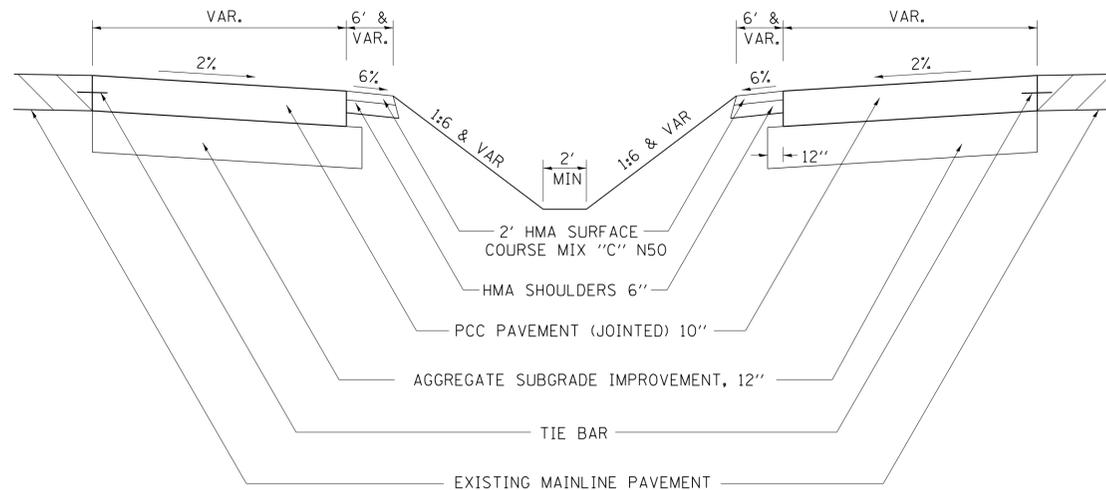
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|-----------------------------|-----------------------|---|---|---------------------------------------|-----------|-----------|------|--------------|---------------------|---------------------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 12-07-10 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | | | | | | CONTRACT NO. | | | | |
| | | | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

64' SINGLE LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2651.79 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (2509.74 Sq. Yds.) | P. C. C. PAVEMENT, (JOINTED) 10" |
| (79.2 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (707.03 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

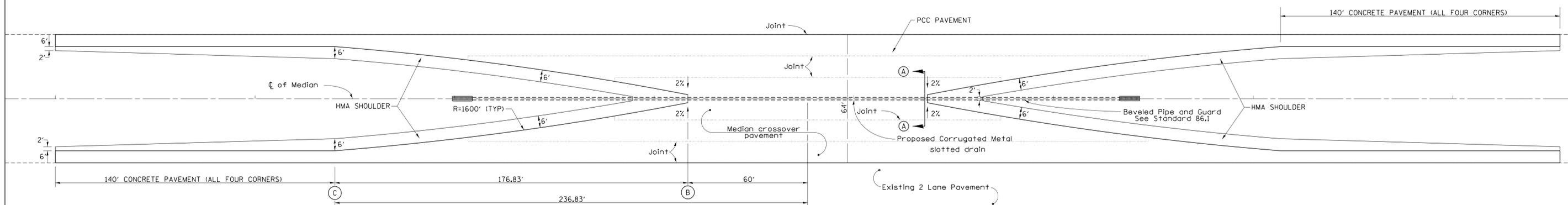
The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0 | 60' | 75' | 100' | 125' | 150' | 175' | 200' | 225' | 236.83' |
|---|-------|------|--------|--------|--------|-------|-------|--------|--------|---------|
| Offsets feet from inside edge of pavement | 32' | 30' | 27.18' | 22.80' | 18.84' | 15.27 | 12.11 | 9.35' | 6.98 | 6' |
| Drop feet from inside edge of pavement | 0.64' | 0.6' | 0.54' | 0.456' | 0.377' | 0.31' | 0.24' | 0.187' | 0.139' | 0.12' |



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

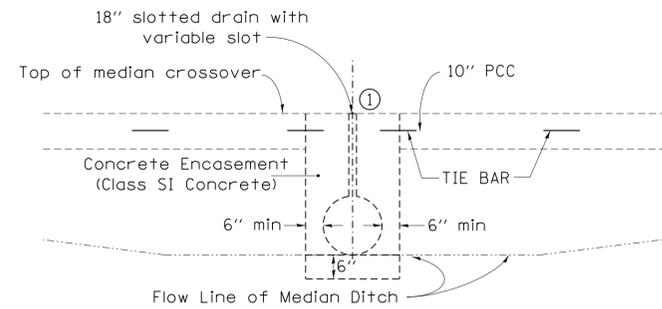
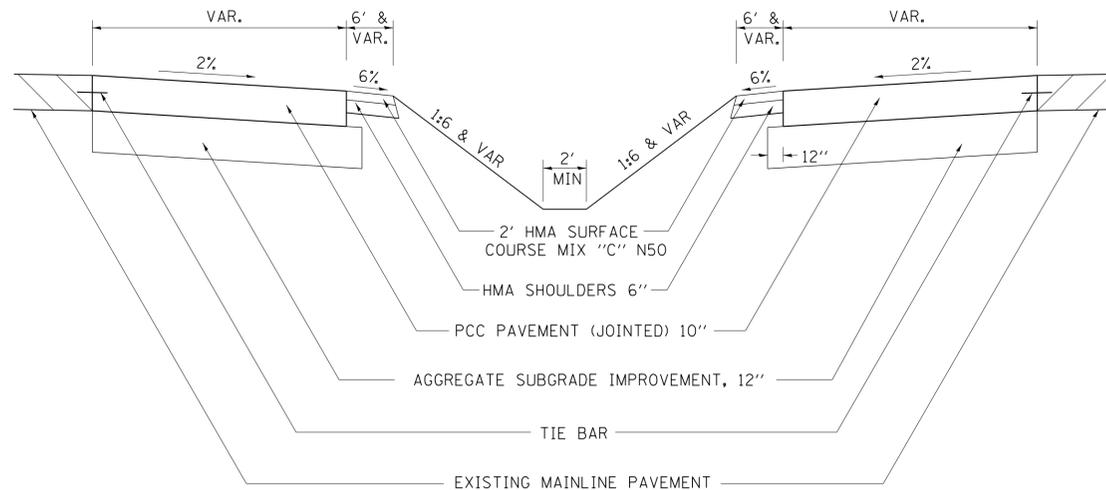
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|-----------------------|---|---|---|-----------|-----------|------|-----------|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 8-27-13 REVISED - 12-07-10 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - DATE - | REVISED - REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | PLOT DATE = Mon Feb 10 13:44:47 2014 | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |

88' SINGLE LANE MEDIAN CROSSOVER

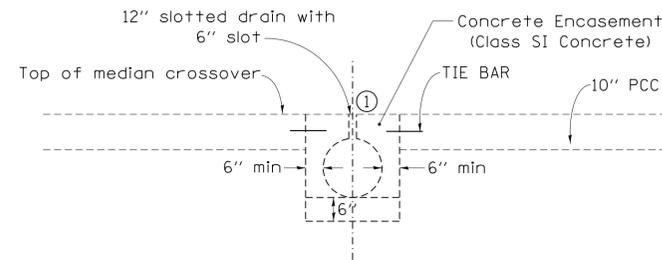
(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (3704.06 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (3535.98 Sq. Yds.) | P. C. C. PAVEMENT, (JOINTED) 10" |
| (96.79 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (864.23 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

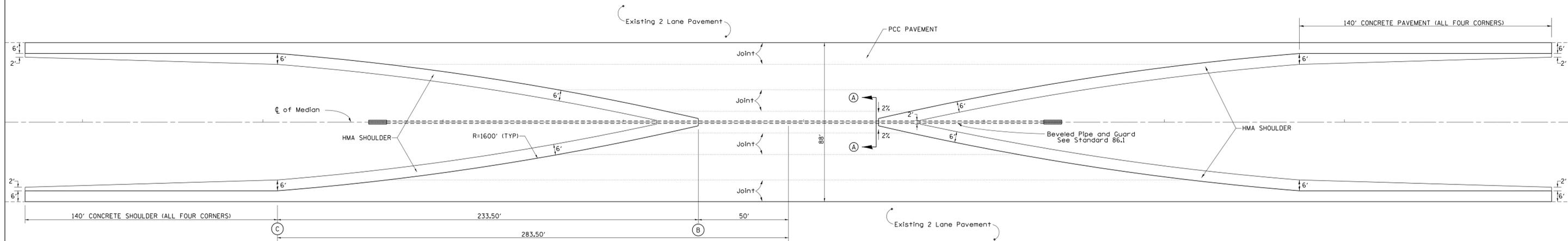
The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0 | 50' | 75' | 100' | 125' | 150' | 175' | 200' | 225' | 250' | 275' | 283.50' |
|---|-------|-------|--------|--------|--------|--------|--------|--------|--------|-------|-------|---------|
| Offsets feet from inside edge of pavement | 44' | 42' | 36.44' | 31.30' | 26.57' | 22.25' | 18.34' | 14.83' | 11.72' | 9.01' | 6.70' | 6' |
| Drop feet from inside edge of pavement | 0.88' | 0.84' | 0.73' | 0.63' | 0.53' | 0.43' | 0.37' | 0.30' | 0.23' | 0.18' | 0.13' | 0.12' |



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | |
|---------------------------------|--------------------------------------|------------|--------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 |
| | | DRAWN - | REVISED - 12-07-10 |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:48 2014 | DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

REGION 2 / DISTRICT 2 STANDARD

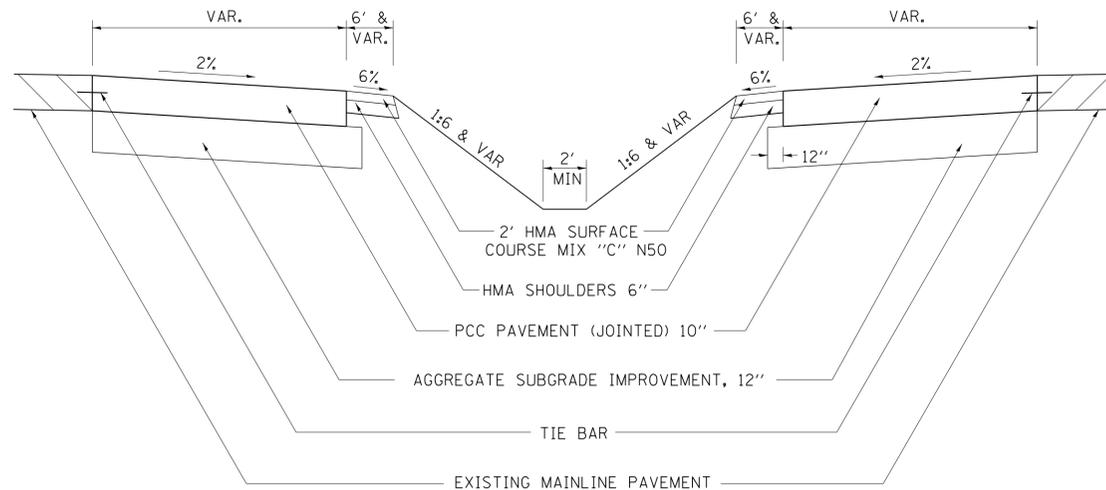
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|--------|--------------|-----------|
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

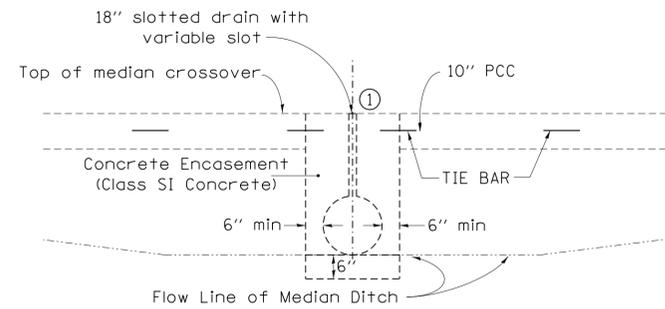
40' TWO LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 55 MPH, WORK ZONE SPEED LIMIT 45 MPH)

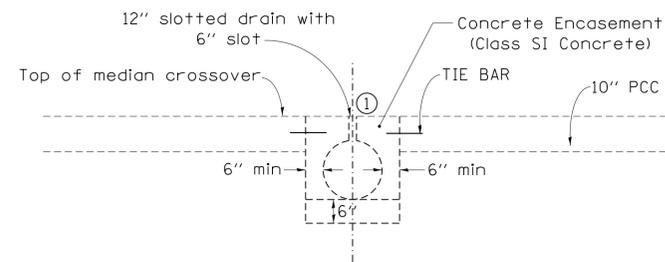
TYPICAL SECTION



| TABLE OF OFFSETS AND DROPS | | | | | | |
|---|------|--------|--------|--------|-------|---------|
| Distance feet from location station | 0 | 76.95' | 100' | 125' | 150' | 168.69' |
| | | ⓑ | | | | ⓒ |
| Offsets feet from inside edge of pavement | 20' | 18' | 14.22' | 10.70' | 7.79' | 6.00' |
| | ⓓ | | | | | |
| Drop feet from inside edge of pavement | 0.4' | 0.36' | 0.28' | 0.21' | 0.16' | 0.12' |



SECTION A-A
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A
(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|---|--|
| (1685.28 Sq. Yds.) (1572.43 Sq. Yds.) (57.28 Tons) (511.45 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" P.C.C. PAVEMENT, (JOINTED) 10" 2" HMA SURFACE COURSE, MIX "C", N50 HMA SHOULDERS 6" |
|---|--|

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

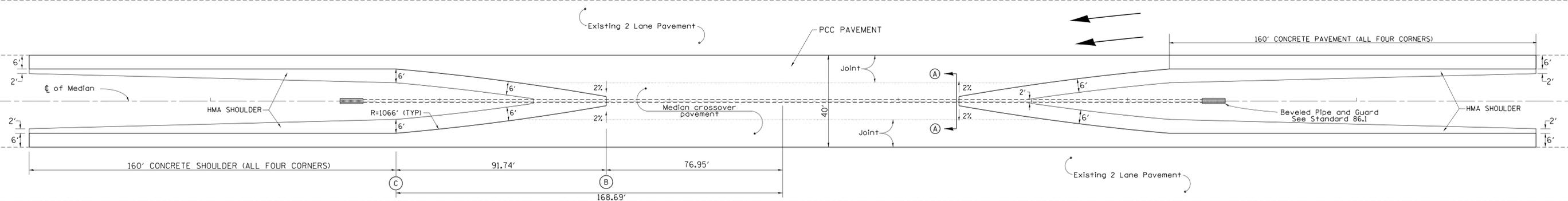
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 45mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|------------|-------------------|---|---------------------------------------|-----------|-----------|------|---|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - 4-04-11 | | | | | | | | | | |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | | | | | | | | | |
| | PLOT DATE = Mon Feb 10 13:44:48 2014 | DATE - | REVISED - | | | | | | | | | | |
| | | | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| | | | | | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

50' TWO LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 55 MPH, WORK ZONE SPEED LIMIT 45 MPH)

TYPICAL SECTION

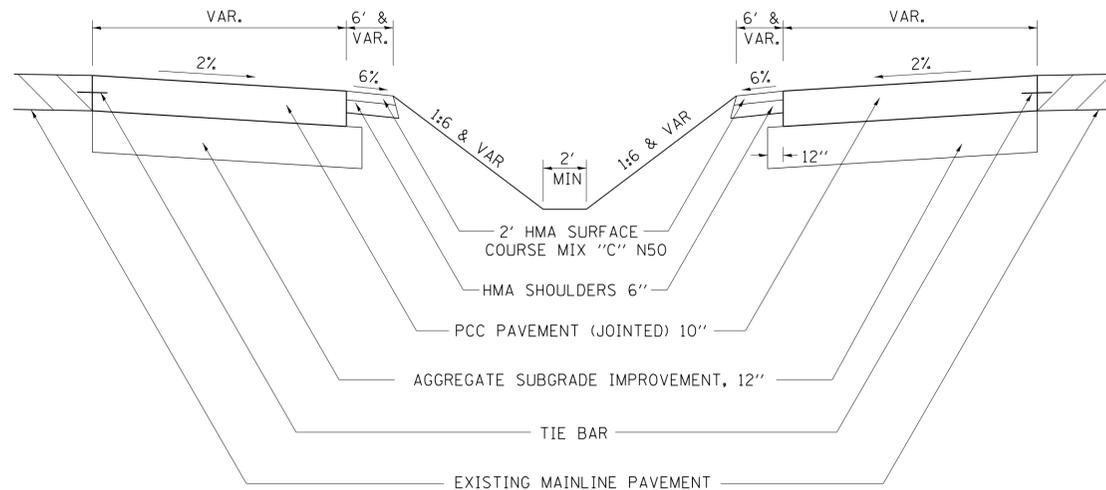


TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0 | 69.72' | 75' | 100' | 125' | 150' | 175' | 188.01' |
|---|------|--------|--------|--------|--------|-------|-------|---------|
| Offsets feet from inside edge of pavement | 25' | 23' | 21.95' | 17.35' | 13.37' | 9.99' | 7.21' | 6.00' |
| Drop feet from inside edge of pavement | 0.5' | 0.46' | 0.44' | 0.35' | 0.27' | 0.20' | 0.14' | 0.12' |

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2029.23 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (1904.29 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (65.64 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (586.07 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

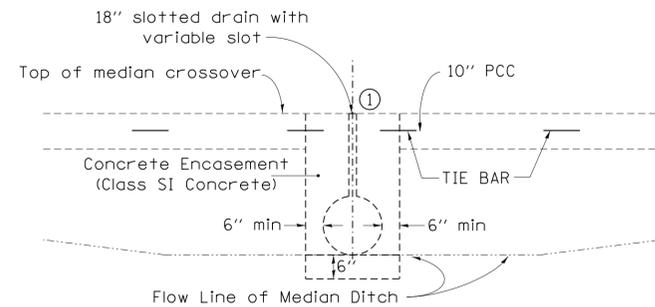
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 45mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

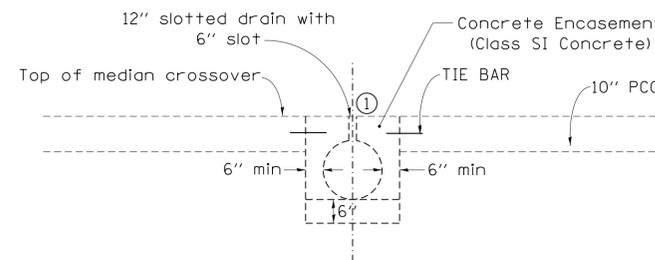
The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



SECTION A-A

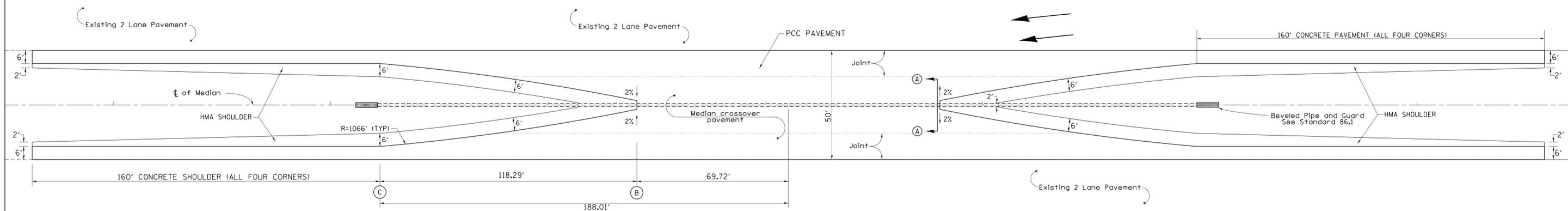
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|------------|-------------------|---|---------------------------------------|-----------|-----------|------|--------------|---------------------|---------------------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | DRAWN - | REVISED - 4-04-11 | | | | | | CONTRACT NO. | | | | |
| | PLOT DATE = Mon Feb 10 13:44:49 2014 | CHECKED - | REVISED - | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | |

64' TWO LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 55 MPH, WORK ZONE SPEED LIMIT 45 MPH)

TYPICAL SECTION

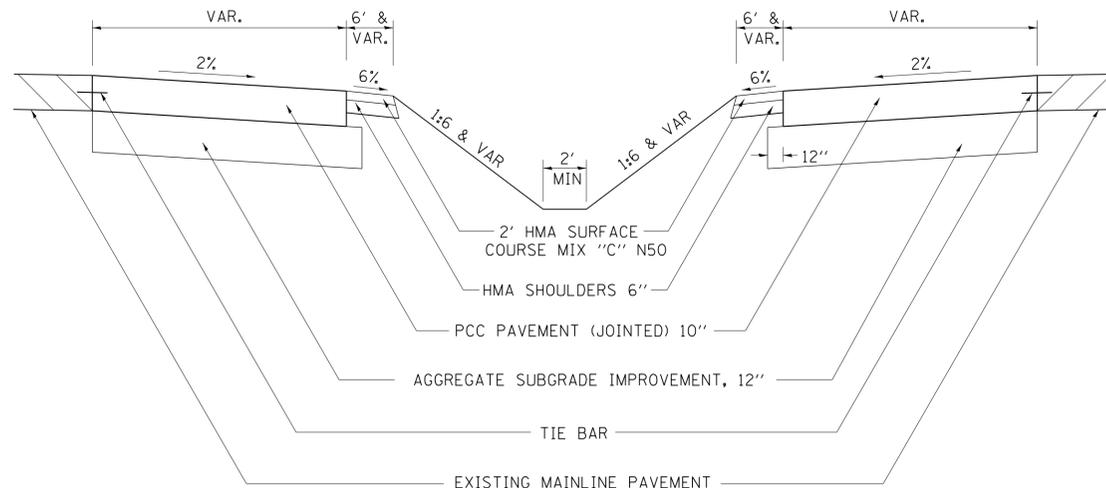


TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0 | 62.41' | 75' | 100' | 125' | 150' | 175' | 200' | 212.87' |
|---|-------|--------|--------|--------|--------|--------|-------|-------|---------|
| Offsets feet from inside edge of pavement | 32' | 30' | 27.14' | 21.92' | 17.33' | 13.35' | 9.97' | 7.20' | 6.00' |
| Drop feet from inside edge of pavement | 0.64' | 0.60' | 0.54' | 0.44' | 0.35' | 0.27' | 0.20' | 0.14' | 0.12' |

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2534.76 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (2394.89 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (75.73 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (676.15 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

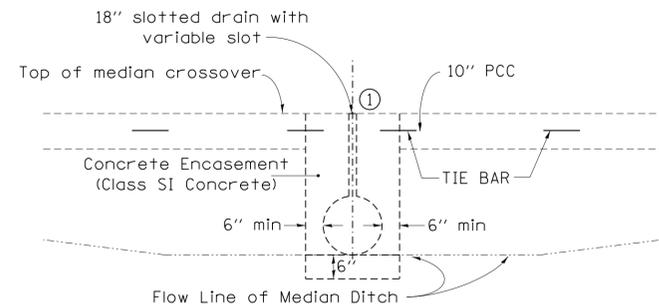
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 45mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

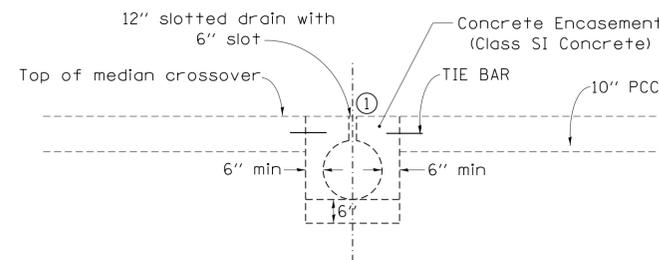
The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



SECTION A-A

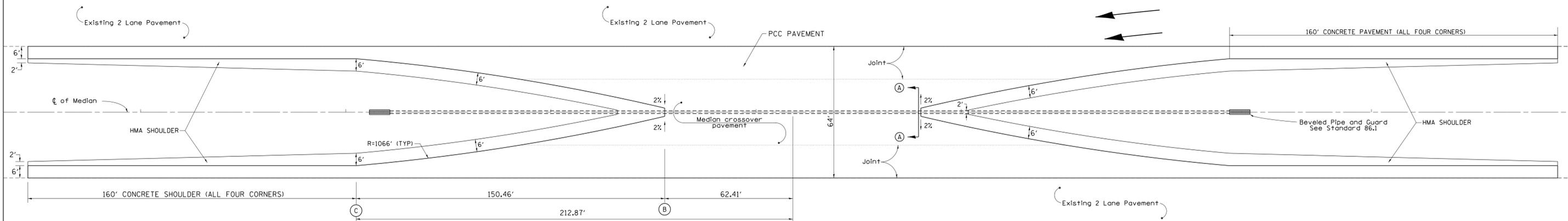
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

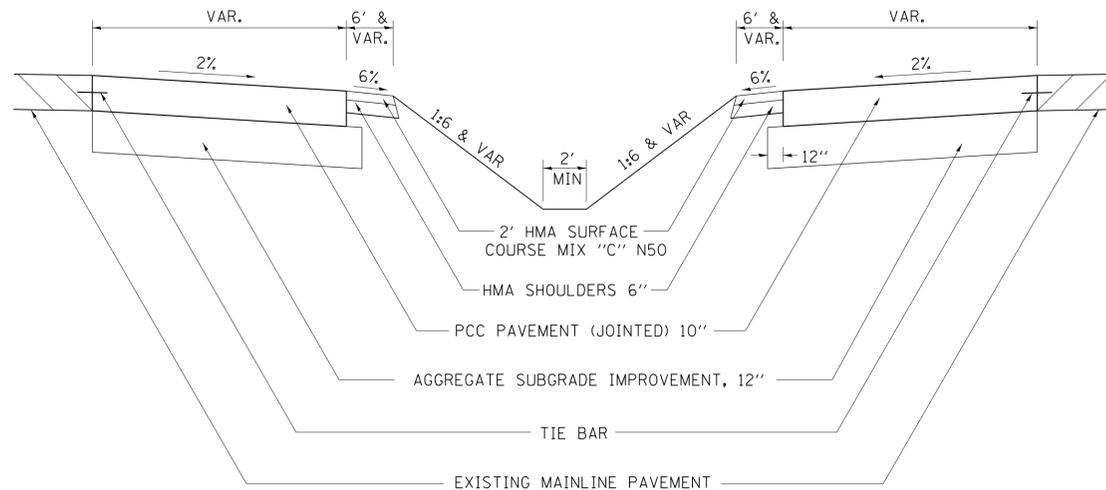
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|---------------------------------|--------------------------------------|------------|-------------------|---|---|--|--|--|---|---------|--------|--------------|--------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | DRAWN - | REVISED - 4-04-11 | | | | | | SCALE: SHEET NO. OF SHEETS STA. TO STA. | | | | CONTRACT NO. |
| | PLOT DATE = Mon Feb 10 13:44:50 2014 | CHECKED - | REVISED - | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | | | | | |
| | | DATE - | REVISED - | | | | | | | | | | |

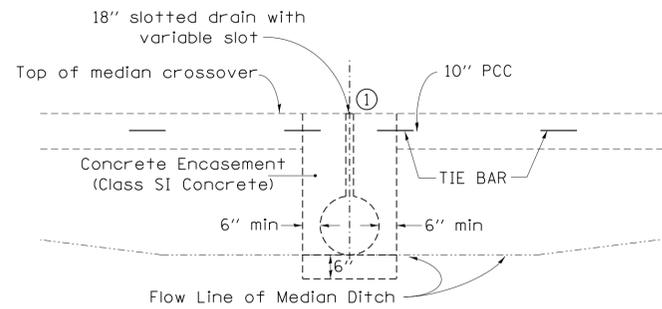
40' TWO LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION

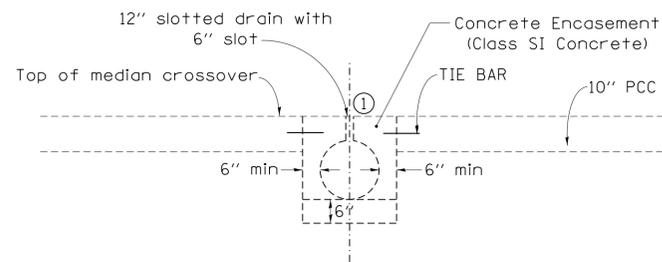


| TABLE OF OFFSETS AND DROPS | | | | | | | |
|---|-------|---------|--------|--------|-------|-------|---------|
| Distance feet from location station | 0 | 100.33' | 125' | 150' | 175' | 200' | 220.57' |
| | | ⓑ | | | | | ⓒ |
| Offsets feet from inside edge of pavement | 20' | 18' | 14.88' | 12.07' | 9.60' | 7.48' | 6.00' |
| | ⓓ | | | | | | |
| Drop feet from inside edge of pavement | 0.40' | 0.36' | 0.30' | 0.24' | 0.19' | 0.15' | 0.12' |



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2142.56 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (2003.87 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (71.01 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (634.04 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

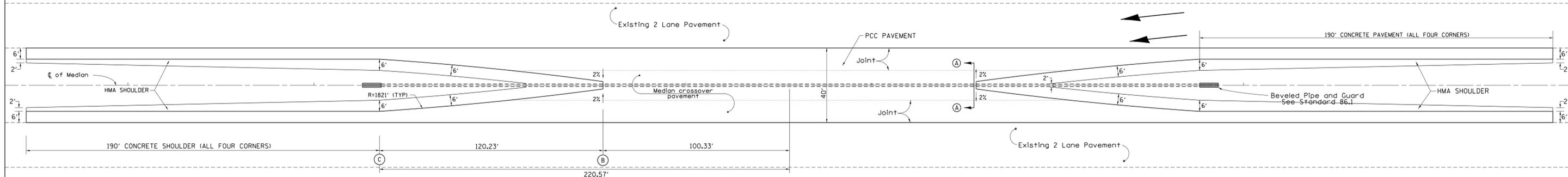
See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

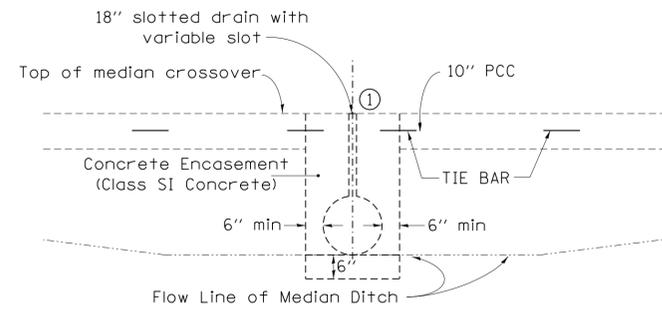
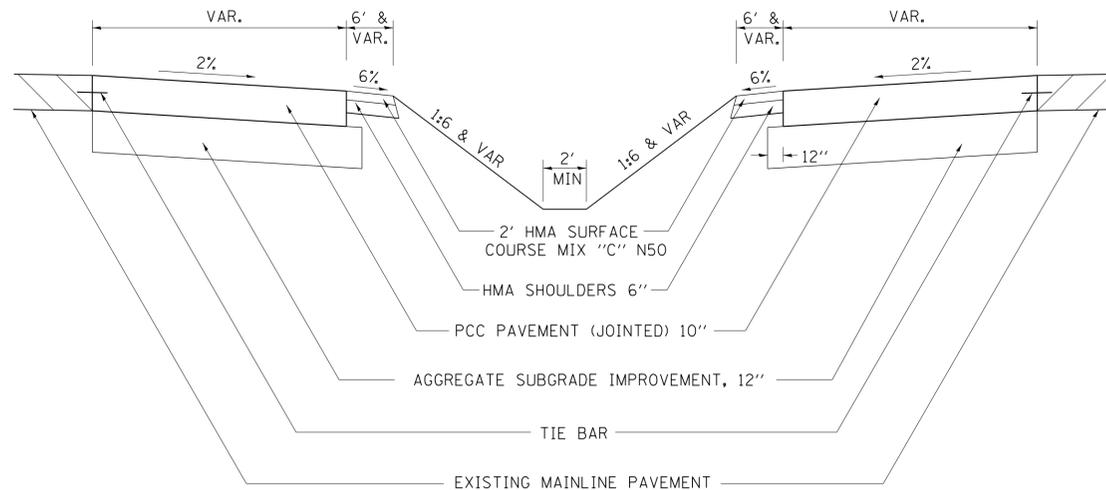
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | | |
|------------------------------------|--------------------------------------|------------|-------------------|---|---------------------------------------|--|--|--|--------------|-----------|-----------|--------------|-----------|---------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | DRAWN - | REVISED - 4-04-11 | | | | | | | | | | | |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - | | | | | | CONTRACT NO. | | | | | |
| | PLOT DATE = Mon Feb 10 13:44:50 2014 | DATE - | REVISED - | | | | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. |

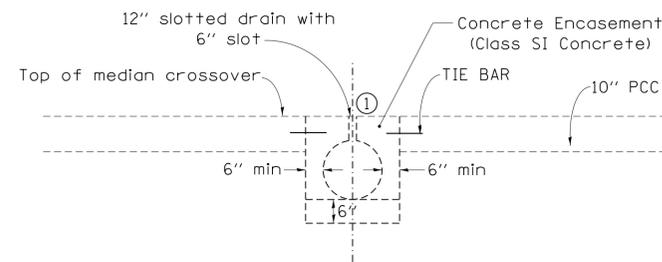
50' TWO LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



SECTION A-A
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A
(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (2593.23 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (2438.79 Sq. Yds.) | P.C.C. PAVEMENT, (JOINTED) 10" |
| (81.92 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (731.46 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

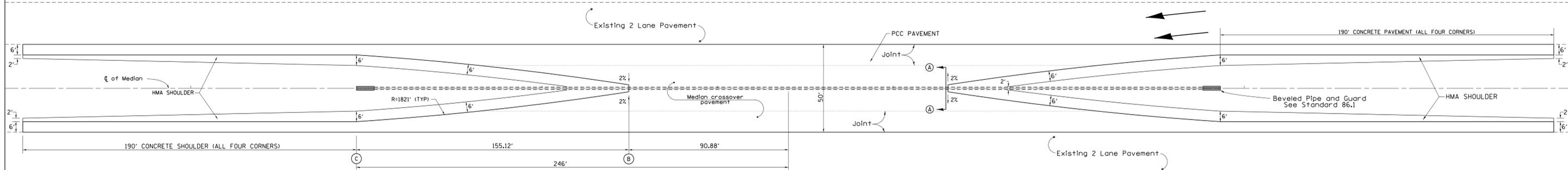
The crossover is designed using a 55mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

| TABLE OF OFFSETS AND DROPS | | | | | | | | | |
|---|-------|--------|--------|--------|--------|--------|-------|-------|-------|
| Distance feet from location station | 0 | 90.88' | 100' | 125' | 150' | 175' | 200' | 225' | 246' |
| | | ⓑ | | | | | | | ⓒ |
| Offsets feet from inside edge of pavement | 25' | 23' | 21.63' | 18.10' | 14.93' | 12.11' | 9.64' | 7.52' | 6.00' |
| | ⓓ | | | | | | | | |
| Drop feet from inside edge of pavement | 0.50' | 0.46' | 0.43' | 0.36' | 0.30' | 0.24' | 0.19' | 0.15' | 0.12' |



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

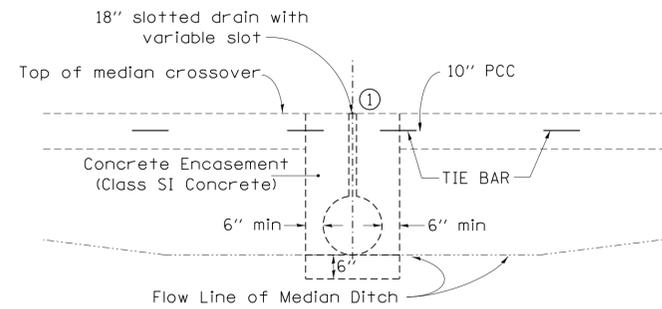
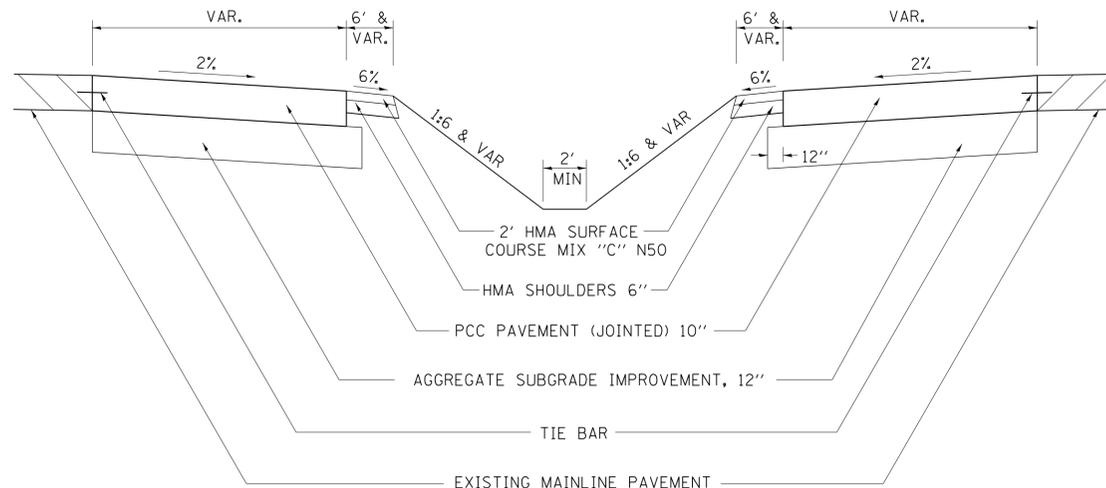
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|------------------------------------|-----------------------------|------------|-------------------|---|---------------------------------------|--|--|--|---|---------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - 4-04-11 | | | | | | | | | | |
| | | CHECKED - | REVISED - | | | | | | CONTRACT NO. | | | | |
| | | DATE - | REVISED - | | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

64' TWO LANE MEDIAN CROSSOVER

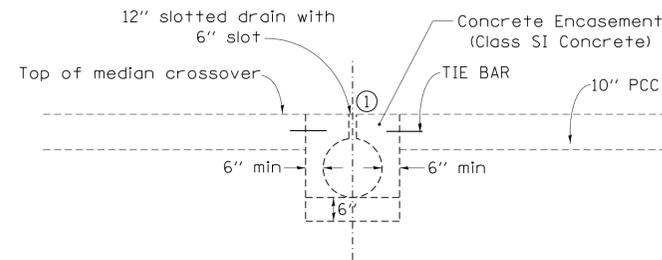
(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



SECTION A-A

(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



SECTION A-A

(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (3256.59 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (3082.80 Sq. Yds.) | P. C. C. PAVEMENT, (JOINTED) 10" |
| (95.09 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (848.99 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

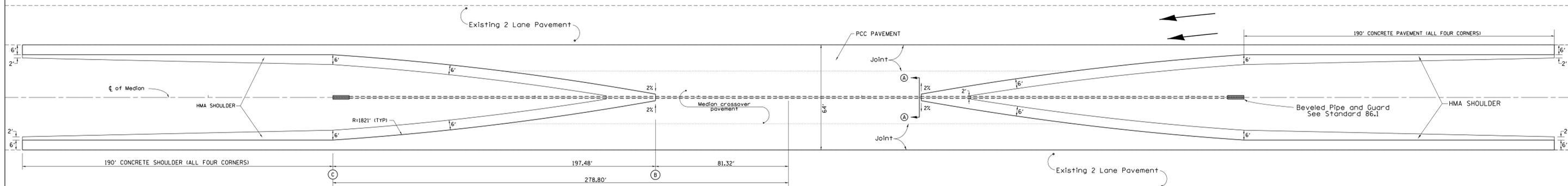
The crossover is designed using a 55mph design speed.

The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

| TABLE OF OFFSETS AND DROPS | | | | | | | | | | | |
|---|-------|--------|--------|--------|--------|--------|--------|--------|-------|-------|---------|
| Distance feet from location station | 0 | 81.32' | 100' | 125' | 150' | 175' | 200' | 225' | 250' | 275' | 278.80' |
| Offsets feet from inside edge of pavement | 32' | 30' | 26.79' | 22.80' | 19.16' | 15.88' | 12.95' | 10.37' | 8.14' | 6.26' | 6.00' |
| Drop feet from inside edge of pavement | 0.64' | 0.60' | 0.54' | 0.46' | 0.38' | 0.32' | 0.26' | 0.21' | 0.16' | 0.13' | 0.12' |



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

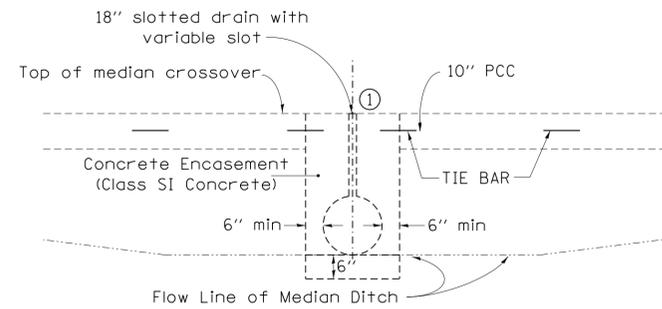
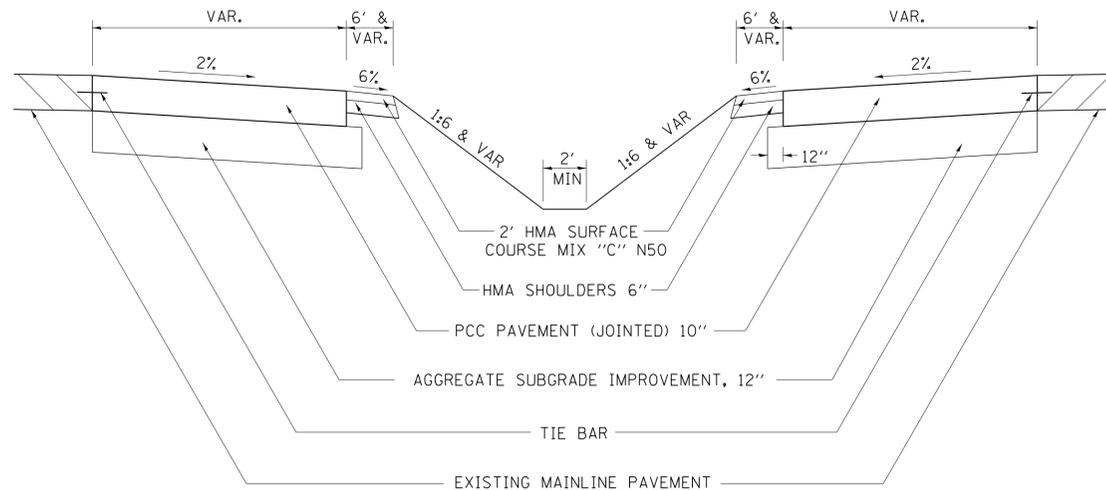
TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|--------------------------------------|-----------------------------|------------|-------------------|---|---------------------------------------|-----------|-----------|------|---|--------------|--------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - 4-04-11 | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | CONTRACT NO. | | | |
| PLOT DATE = Mon Feb 10 13:44:52 2014 | DATE - | REVISED - | | | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

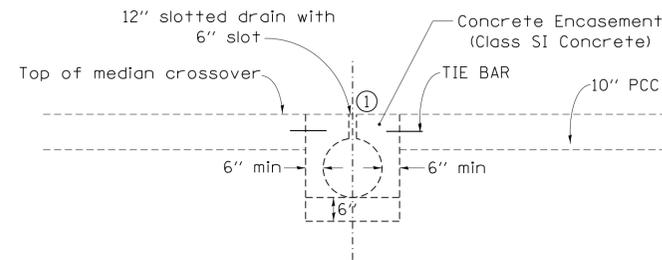
88' TWO LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



SECTION A-A
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A
(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

| | |
|--------------------|-------------------------------------|
| (4481.22 Sq. Yds.) | AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| (4279.37 Sq. Yds.) | P. C. C. PAVEMENT, (JOINTED) 10" |
| (114.14 Tons) | 2" HMA SURFACE COURSE, MIX "C", N50 |
| (1019.14 Sq. Yds.) | HMA SHOULDERS 6" |

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

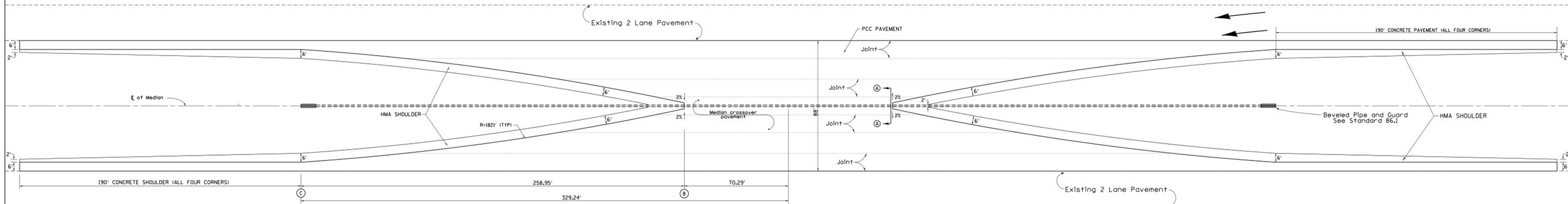
The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No.6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

TABLE OF OFFSETS AND DROPS

| Distance feet from location station | 0 | 70.29' ⓑ | 75' | 100' | 125' | 150' | 175' | 200' | 225' | 250' | 275' | 300' | 325' | 329.24' ⓒ |
|---|----------|-------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|-------|-------|--------------|
| Offsets feet from inside edge of pavement | 44' ⓓ | 42' | 41.00' | 35.93' | 31.23' | 26.86' | 22.86' | 19.22' | 15.94' | 13.00' | 10.42' | 8.18' | 6.29' | 6.00' |
| Drop feet from inside edge of pavement | 0.88' | 0.84' | 0.82' | 0.72' | 0.62' | 0.54' | 0.46' | 0.38' | 0.32' | 0.26' | 0.21' | 0.16' | 0.13' | 0.12' |



TYPICAL PLAN

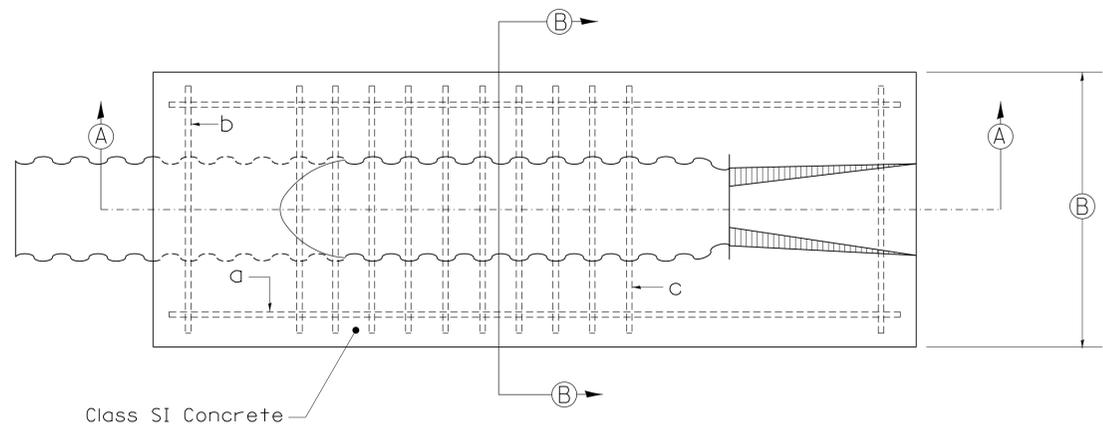
Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

| | | | | | | | | | | | | | |
|--------------------------------------|-----------------------------|------------|-------------------|---|---------------------------------------|-----------|-----------|------|-----------|---------------------|---------------------------|--------------|-----------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 8-27-13 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 1:8000' / in. | CHECKED - | REVISED - 4-04-11 | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | CONTRACT NO. | |
| PLOT DATE = Mon Feb 10 13:44:52 2014 | DATE - | REVISED - | | | | | | | | | | | |

BEVELED PIPE & GUARD DETAIL FOR MEDIAN CROSSOVER



PLAN VIEW

| 12 PIPE REINFORCING SCHEDULE | | | |
|------------------------------|----------|--------|-----|
| Mark Req'd | Bar Size | Length | No. |
| a | 5 | 110 | 2 |
| b | 5 | 32 | 2 |
| c | 8 | 34 | 10 |

| 18 PIPE REINFORCING SCHEDULE | | | |
|------------------------------|----------|--------|-----|
| Mark Req'd | Bar Size | Length | No. |
| a | 5 | 162 | 2 |
| b | 5 | 38 | 2 |
| c | 8 | 40 | 18 |

GENERAL NOTES:

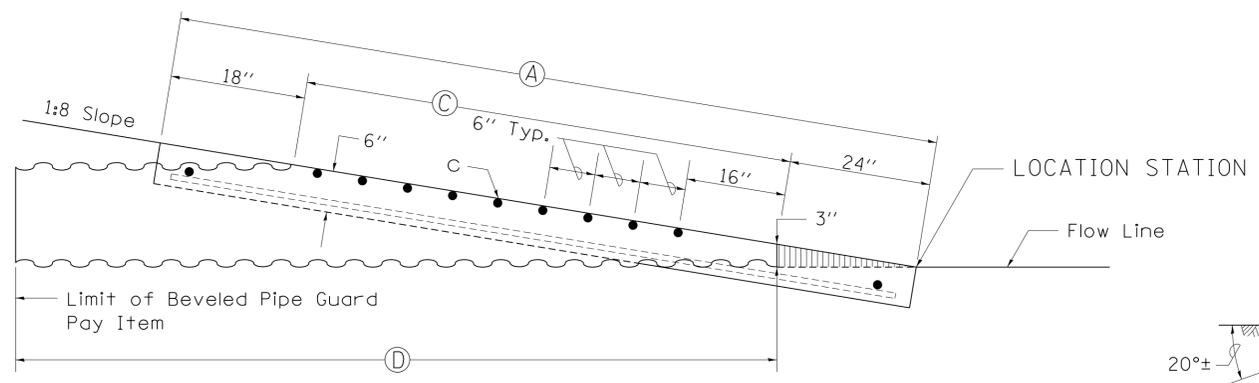
Details shown hereon are for the construction of beveled pipe and guard. Alternate designs, methods of construction or materials may be submitted to the Engineer for approval. All methods of construction and materials involved shall conform to current Standard Specifications.

Reinforcing steel used in construction of "Beveled Pipe and Guard" shall be deformed bars meeting the requirements of Article 1006.10 of the Standard Specifications. All steel bars shall be hot-dip galvanized in accordance with ASTM A 123 specifications.

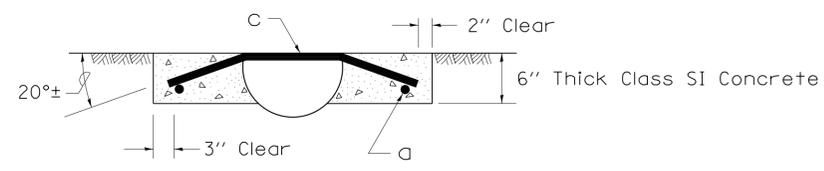
Concrete used in construction of the beveled pipe and guard shall be Class "SI" Concrete.

The corrugated metal pipe shall be cut to fit the 1:8 foreslope. Slots shall be cut into the C.M.P. for placement of the 'b' and 'c' bars. After the foreslope has been placed, the 'b' and 'c' bars shall be fitted into the slots cut in the C.M.P. so they will be in proper position when the concrete collar is poured.

This work shall be paid for at the contract unit price per Each for "Beveled Pipe and Guard", as shown hereon and as directed by the Engineer.



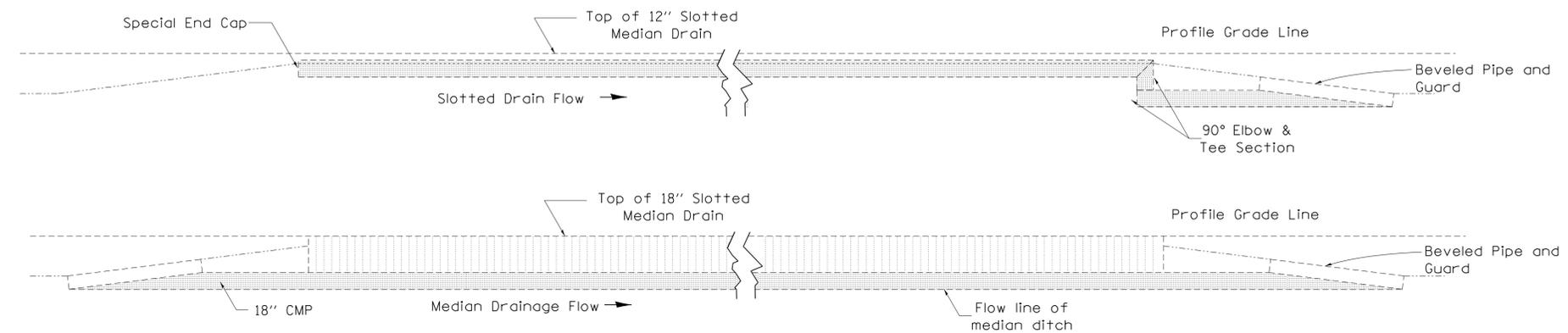
SECTION A-A



SECTION B-B

| TABLE OF DIMENSIONS | | | | |
|---------------------|-----------|----|----------|-----------|
| PIPE SIZE | A | B | C | D |
| 12 | 9' - 6" | 36 | 6' | 10' |
| 18 | 13' - 10" | 42 | 10' - 4" | 14' - 10" |

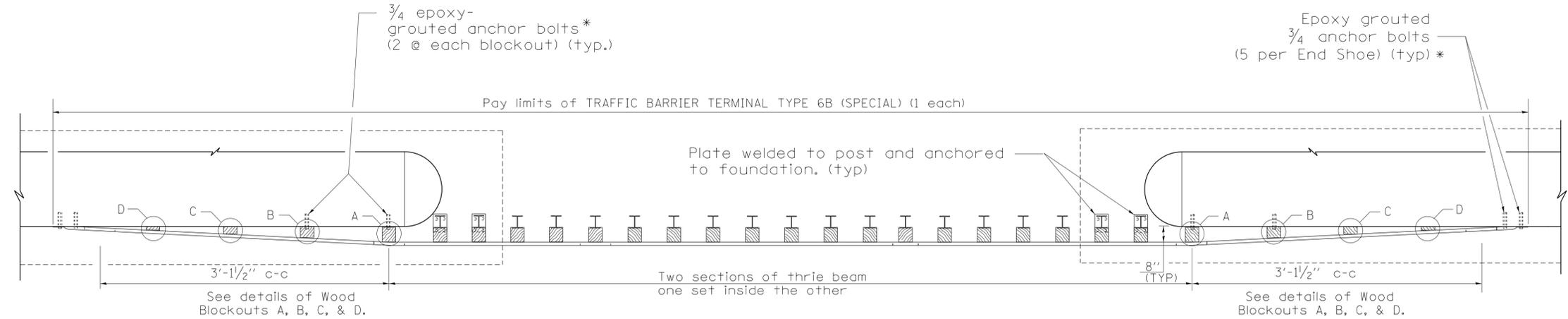
TYPICAL SECTION THRU CENTERLINE OF MEDIAN CROSSOVER



ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

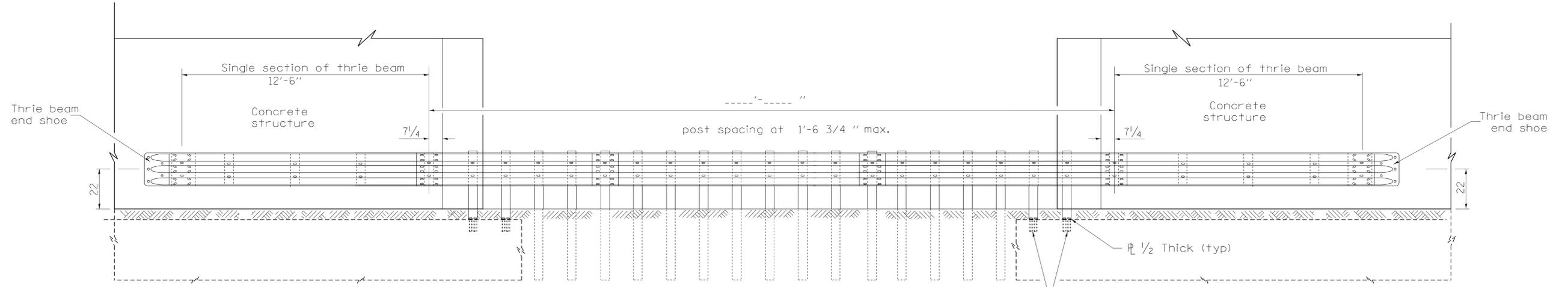
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|--------------------------------------|-----------------------------|-----------------------|--------------------------------|---|---------------------------------------|--|--|--------------|-----------|-----------|--------------|-----------|---------------------|---------------------------|--|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 5-27-09 REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REGION 2 / DISTRICT 2 STANDARD | | | F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | | |
| PLOT SCALE = 1:8000' / in. | | CHECKED - | REVISED - | | | | | CONTRACT NO. | | | | | | | |
| PLOT DATE = Mon Feb 10 13:44:53 2014 | | DATE - | REVISED - | | | | | SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | |

TRAFFIC BARRIER TERMINAL, TYPE 6B (SPECIAL)



PLAN

* With standard washers. After tightening, cut the anchor bolts flush with the nuts and damage the nuts to prevent them from loosening.



ELEVATION

GENERAL NOTES

This work shall be done according to Section 631 of the Standard Specifications and this detail.

See Standard 630001 for details of guardrail not shown.

Thrie beam rail shall be bolted to block-out at all posts.

Posts located above pier foundation shall have plate attached to post and anchored to foundation.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

All dimensions are in inches unless otherwise shown.

The Traffic barrier Terminal, Type 6B (Special) will be measured for payment, complete in place, in units of each.

This work shall be paid for at the contract unit price per each for TRAFFIC BARRIER TERMINAL, TYPE 6B (SPECIAL).

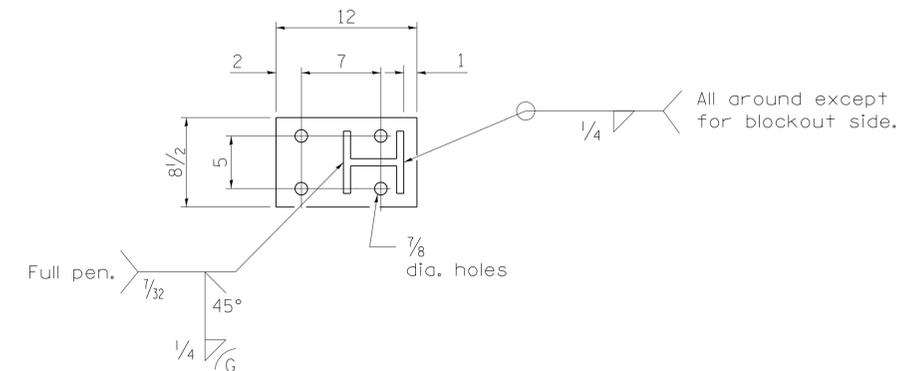


PLATE DETAIL

| | | | |
|------------------------------------|--------------------------------------|-----------------------|---------------------------------|
| FILE NAME = District 2 Standard | USER NAME = ID07/District 2 | DESIGNED - DRAWN - | REVISED - 10-18-11 REVISED - |
| | PLOT SCALE = 1:10000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:54 2014 | DATE - | REVISED - |

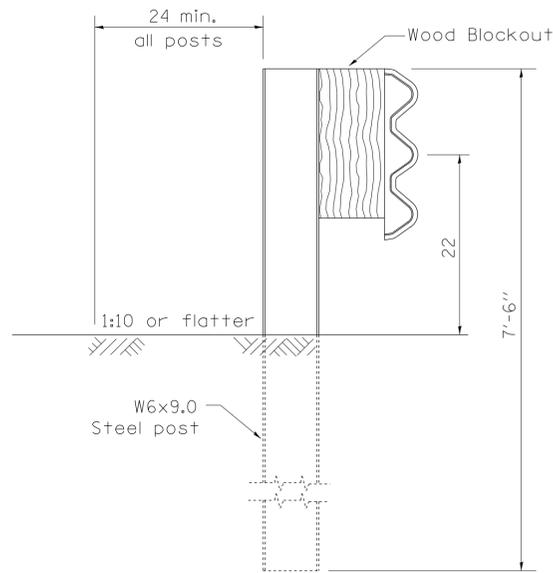
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

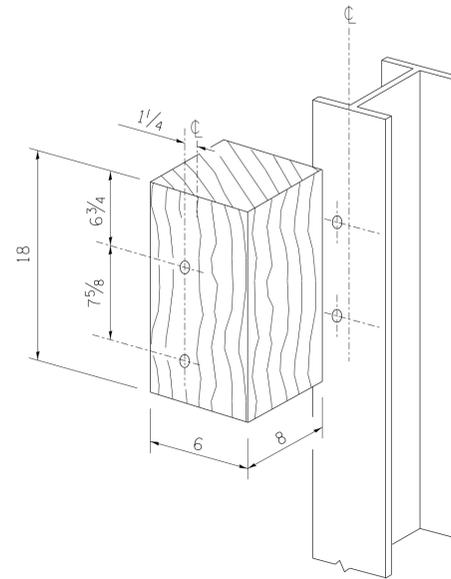
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

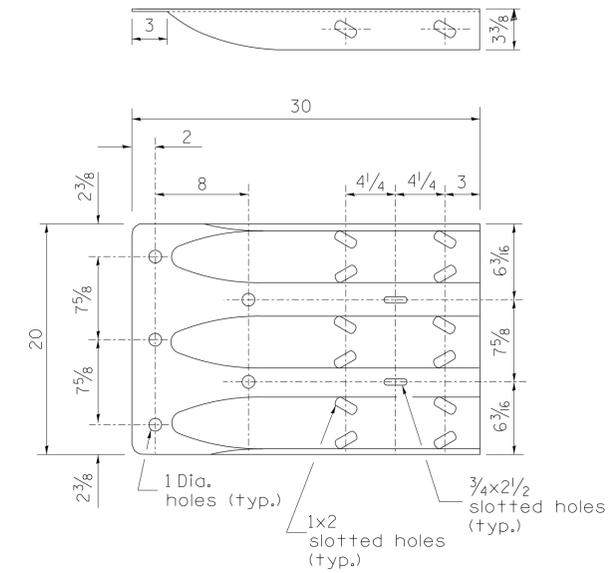
TRAFFIC BARRIER TERMINAL, TYPE 6B (SPECIAL)



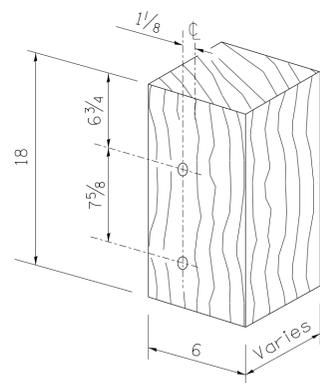
SECTION A-A



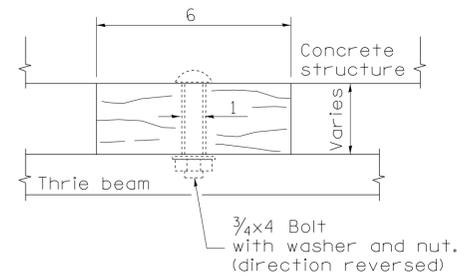
WOOD BLOCKOUT DETAIL



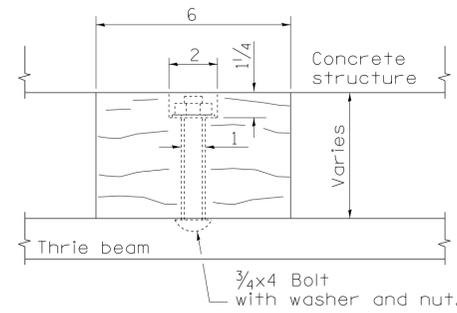
THRIE BEAM END SHOE DETAIL



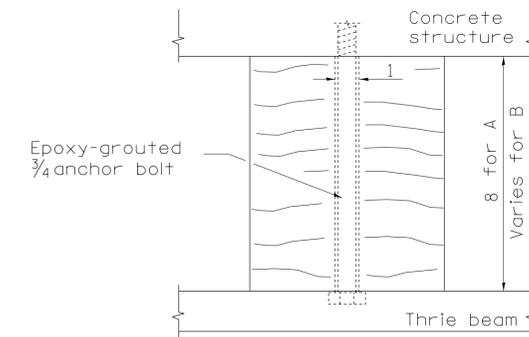
MODIFIED THICKNESS DETAIL
WOOD BLOCKOUTS A, B, C, & D



WOOD BLOCKOUT D



WOOD BLOCKOUT C



WOOD BLOCKOUT A & B

| | | | |
|------------------------------------|--------------------------------------|------------|--------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - | REVISED - 10-18-11 |
| | | DRAWN - | REVISED - |
| | PLOT SCALE = 1:0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:54 2014 | DATE - | REVISED - |

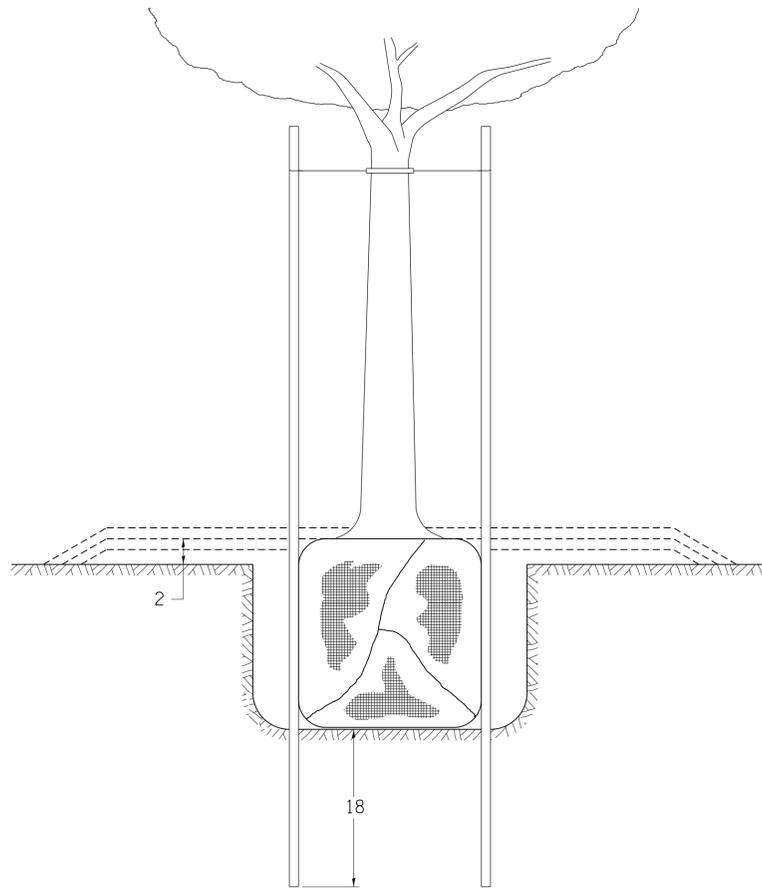
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

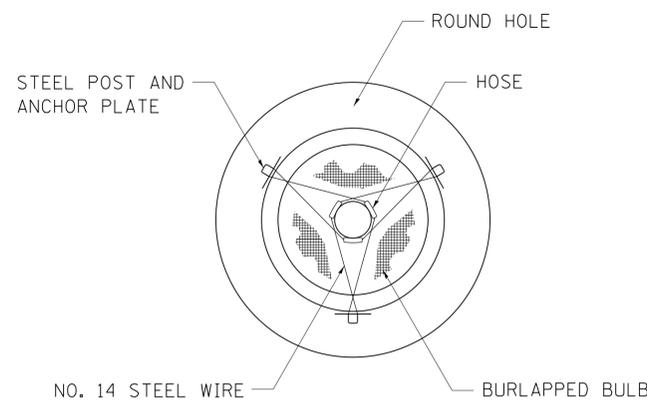
SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | | | |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |

DETAILS OF PLANTING AND BRACING TREES

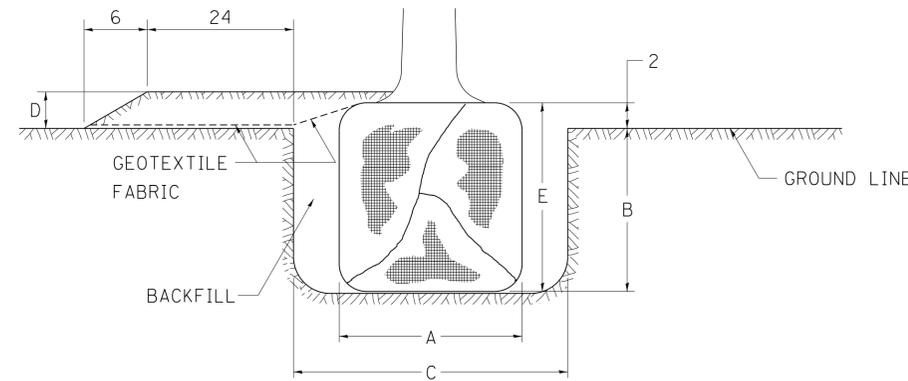


TREES SMALLER THAN 4 1/2 IN DIAMETER

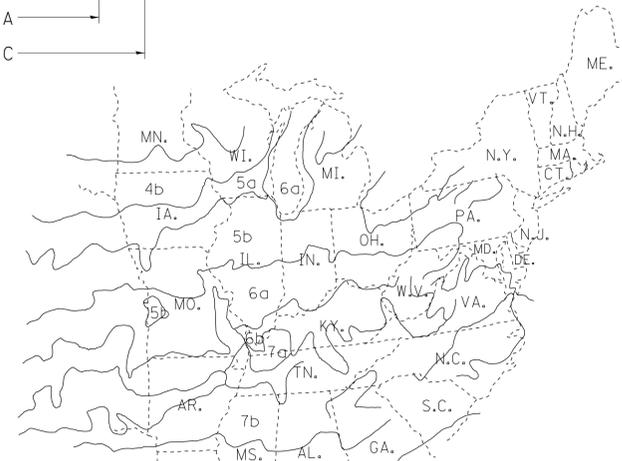
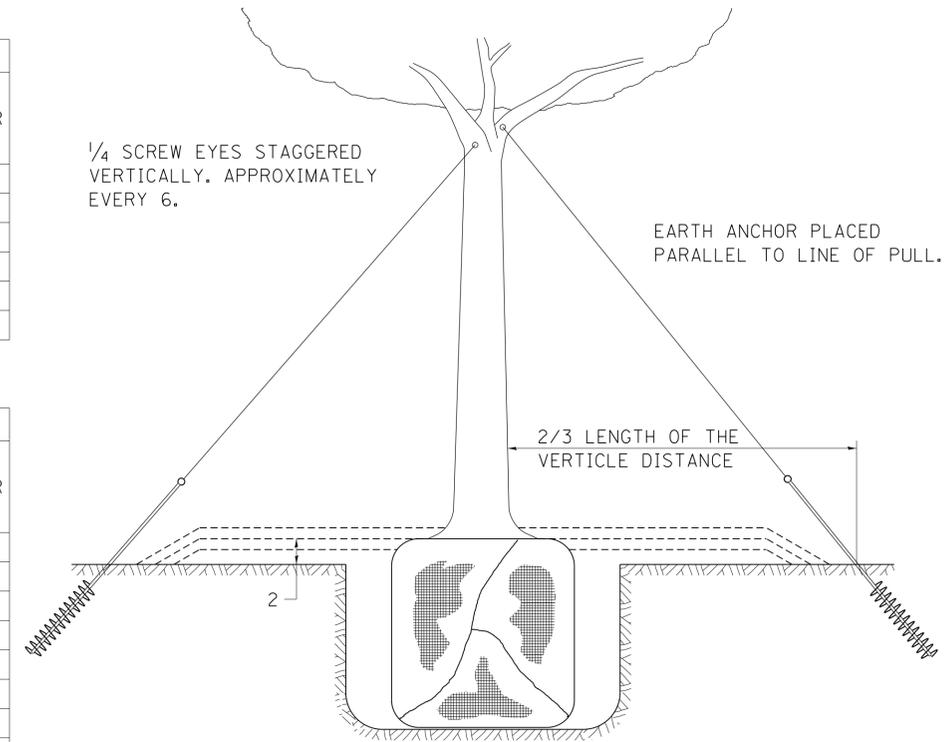


| SMALL | A | B | C | D | E | F |
|------------|-------------------------------|--------------------------|--------------------------|--------------------------|----------------------------|--------------------------------|
| TREE SIZE | DIAMETER OF BALL OR ROOT SYS. | DEPTH OF HOLE EXCAVATION | WIDTH OF HOLE EXCAVATION | THICKNESS OF MULCH COVER | DEPTH OF BALL OR ROOT SYS. | VOLUME OF MULCH COVER CU. YDS. |
| 5'-6' | 16 | 10 | 30 | 4 | 12 | 0.54 |
| 5'-6' BB | 16 | 10 | 30 | 4 | 12 | 0.54 |
| 6'-7' BB | 18 | 12 | 30 | 4 | 14 | 0.54 |
| 7'-8' BB | 20 | 11 | 30 | 4 | 13 | 0.54 |
| 8'-10' BB | 24 | 14 | 36 | 4 | 16 | 0.61 |
| 10'-12' BB | 26 | 15 | 36 | 4 | 17 | 0.61 |

| LARGE | A | B | C | D | E | F |
|------------|-------------------------------|--------------------------|--------------------------|--------------------------|----------------------------|--------------------------------|
| TREE SIZE | DIAMETER OF BALL OR ROOT SYS. | DEPTH OF HOLE EXCAVATION | WIDTH OF HOLE EXCAVATION | THICKNESS OF MULCH COVER | DEPTH OF BALL OR ROOT SYS. | VOLUME OF MULCH COVER CU. YDS. |
| 0-2 | 20 | 11 | 36 | 4 | 13 | 0.61 |
| 2-2 1/2 BB | 24 | 14 | 48 | 4 | 16 | 0.78 |
| 2 1/2-3 BB | 28 | 17 | 48 | 4 | 19 | 0.78 |
| 3-3 1/2 BB | 32 | 17 | 60 | 4 | 19 | 0.96 |
| 3 1/2-4 BB | 36 | 20 | 60 | 4 | 22 | 0.96 |
| 4-4 1/2 BB | 40 | 22 | 72 | 4 | 24 | 1.16 |
| 4 1/2-5 BB | 44 | 24 | 72 | 4 | 26 | 1.16 |
| 5-5 1/2 BB | 48 | 27 | 84 | 4 | 29 | 1.38 |

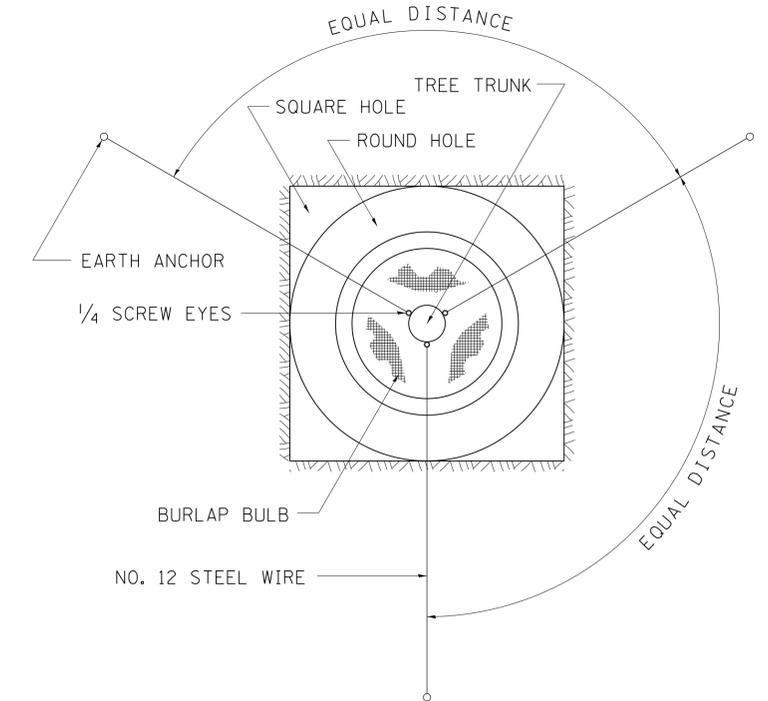


TREES OVER 4 1/2 IN DIAMETER



PLANT HARDINESS ZONE MAP

U.S. DEPARTMENT OF AGRICULTURE
AGRICULTURAL RESEARCH SERVICE
PUBLICATION NO. 814



ALL DIMENSIONS ARE IN INCHES
UNLESS OTHERWISE NOTED.

| | | | |
|------------------------------------|--------------------------------------|-----------------------|--------------------|
| FILE NAME = District 2 Standard | USER NAME = IDOT/District 2 | DESIGNED - DRAWN - | REVISED - 10-18-11 |
| | PLOT SCALE = 1,0000' / in. | CHECKED - | REVISED - |
| | PLOT DATE = Mon Feb 10 13:44:55 2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REGION 2 / DISTRICT 2 STANDARD

SCALE: SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|---------|--------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| CONTRACT NO. | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |